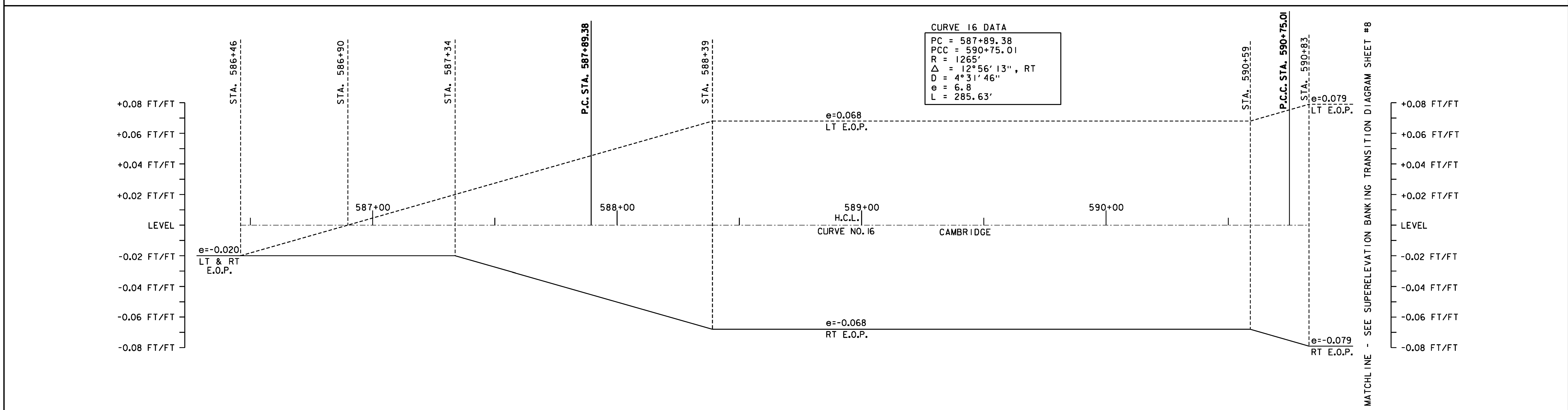


CURVE 14 & 15 BANKING TRANSITION DIAGRAM



CURVE 16 BANKING TRANSITION DIAGRAM

SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE ROADWAY.
2. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED. A MAXIMUM SUPERELEVATION RATE OF 0.08 IS USED IN AREAS WITH A POSTED SPEED ABOVE 30 MPH. IN AREAS WITH AN INTERSECTING SIDE ROAD A MAXIMUM SUPERELEVATION RATE OF 0.06 WAS USED. SEE VAOT STANDARD B-1 FOR MORE INFORMATION.



NOT TO SCALE

SUPERELEVATION BANKING TRANSITION DIAGRAM SHEET #7	PROJECT NAME: CAMBRIDGE-BAKERSFIELD
	PROJECT NUMBER: STP 2926(I)
	FILE NAME: p10b258.dgn
DESIGNED BY: STANTEC	PLOT DATE: 11/15/2012
IPARM FILE: p10b258sbd7.i	DRAWN BY: STANTEC
	CHECKED BY: STANTEC
	SHEET 108 OF 387

MATCHLINE - SEE SUPERELEVATION BANKING TRANSITION DIAGRAM SHEET #8