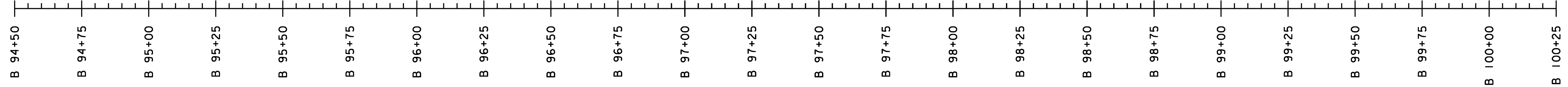
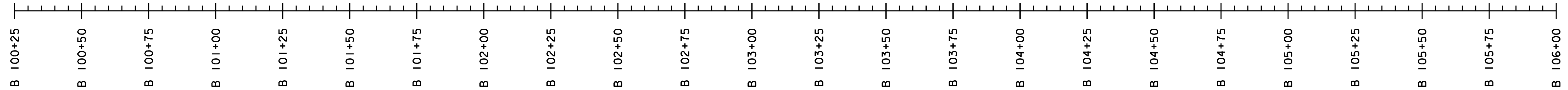


+0.080 FT/FT
+0.060 FT/FT
+0.040 FT/FT
+0.020 FT/FT
LEVEL
-0.020 FT/FT
-0.040 FT/FT
-0.060 FT/FT
-0.080 FT/FT



SEE BELOW

+0.080 FT/FT
+0.060 FT/FT
+0.040 FT/FT
+0.020 FT/FT
LEVEL
-0.020 FT/FT
-0.040 FT/FT
-0.060 FT/FT
-0.080 FT/FT



SEE NEXT SHEET

SUPERELEVATION BANKING NOTES:

1. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08. A MAXIMUM SUPERELEVATION RATE OF 0.06 WAS USED WHERE SIDE ROADS INTERSECTED ALONG THE HIGH SIDE OF A CURVE.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.

NOT TO SCALE

**SUPERELEVATION
DIAGRAMS
SHEET #40**

PROJECT NAME:	STOCKBRIDGE-BETHEL
PROJECT NUMBER:	STP 2910(1)
FILE NAME:	z10b214.sbd.40.dgn
PROJECT LEADER:	D.E.G.
DESIGNED BY:	L.M.B.
IPARM FILE:	z10b214sbd40.1
PLOT DATE:	3/17/2014
DRAWN BY:	W.G.P.
CHECKED BY:	J.P.S.
SHEET	186 OF 397