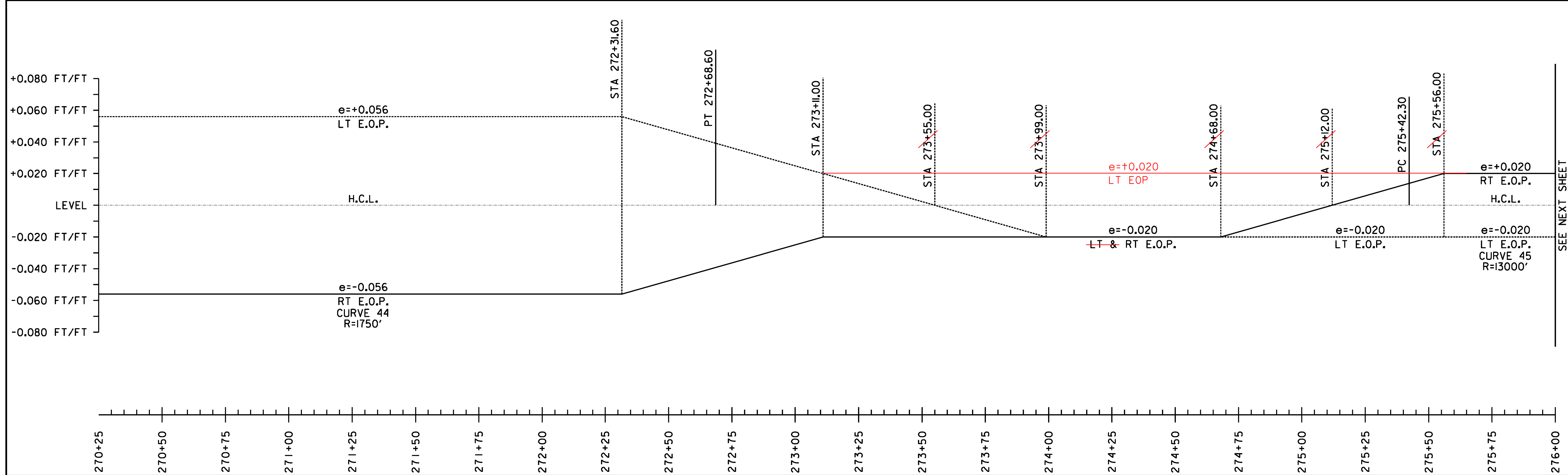


SEE BELOW



SEE NEXT SHEET

**SUPERELEVATION BANKING NOTES:**

1. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08. A MAXIMUM SUPERELEVATION RATE OF 0.06 WAS USED WHERE SIDE ROADS INTERSECTED ALONG THE HIGH SIDE OF A CURVE.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.

**NOT TO SCALE**

<b>SUPERELEVATION DIAGRAMS SHEET #24</b>	PROJECT NAME: STOCKBRIDGE-BETHEL
	PROJECT NUMBER: STP 2910(1)
	FILE NAME: z10b214_sbd_24.dgn
	DESIGNED BY: L.M.B.
	PLOT DATE: 3/17/2014
	DRAWN BY: W.G.P.
	CHECKED BY: J.P.S.
	SHEET 170 OF 397

FILE NAME: \\1720\projects\1720\22426\10400\1\STP\1720\z10b214\_sbd\_24.dgn  
 DATE: 3/17/2014  
 USER: J.P.S.