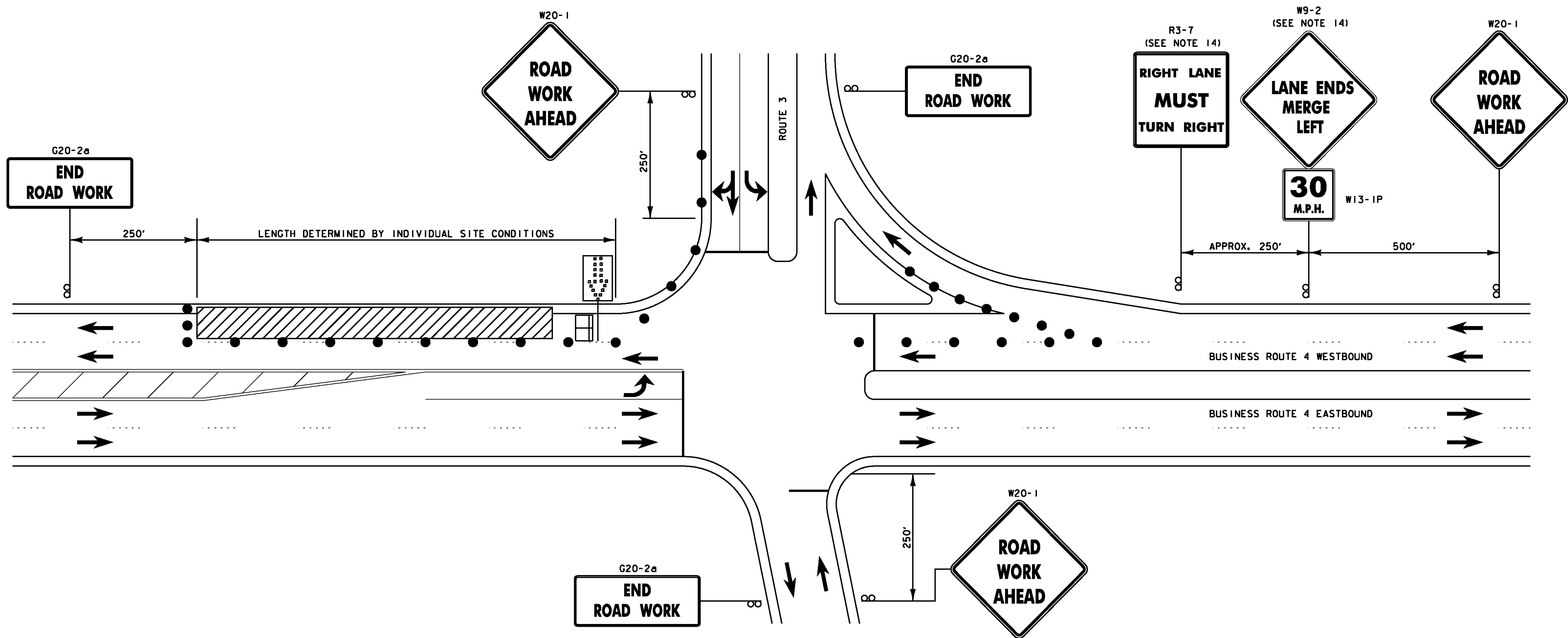


TRAFFIC CONTROL ON BUSINESS ROUTE 4, RIGHT LANE CLOSED

NOT TO SCALE



TRAFFIC CONTROL ON BUSINESS ROUTE 4, RIGHT LANE CLOSED
BRIDGE D4-4, WESTBOUND TRAVEL DIRECTION

NOT TO SCALE

- LEGEND**
- FLOW OF TRAFFIC
 - WORK AREA
 - REFLECTORIZED PLASTIC DRUM
 - TYPE III BARRICADE
 - FLASHING ARROW PANEL

POSTED SPEED LIMIT	TAPER LENGTHS (FT)		MAXIMUM CHANNELIZING DEVICE SPACING (FT)	
	SHOULDER W=10 FT (L/3)	MERGING W=12 FT (L)	TAPER	TANGENT
30	50	180	30	60
35	70	245	35	70
40	90	320	40	80

TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR POSTED SPEEDS OF 45 MPH OR GREATER
 $L = WS^2/60$ FOR POSTED SPEEDS OF 40 MPH OR LESS
 L = MINIMUM LENGTH OF TAPER
 W = WIDTH OF OFFSET IN FEET
 S = POSTED SPEED IN MPH

TRAFFIC CONTROL NOTES - BUSINESS ROUTE 4:

1. THE TRAFFIC CONTROL PLAN SHOWN IS A SCHEMATIC ONLY AND SHOULD BE USED AS A REFERENCE. THE CONTRACTOR SHALL SUBMIT A SITE SPECIFIC TRAFFIC CONTROL PLAN FOR BRIDGES D4-3 AND D4-4 TO VTRANS FOR APPROVAL. PAYMENT FOR PREPARING AND SUBMITTING THE TRAFFIC CONTROL PLAN, AND MAKING NECESSARY REVISIONS TO THE PLAN, WILL BE INCLUDED IN THE UNIT PRICE BID FOR CONTRACT ITEM 641.10 - TRAFFIC CONTROL. THE CONTRACTOR SHALL ALLOW TWO WEEKS FOR APPROVAL OF THE TRAFFIC CONTROL PLAN. NO WORK SHALL COMMENCE UNTIL THE CONTRACTOR HAS AN APPROVED TRAFFIC CONTROL PLAN.
2. THE EXISTING SPEED LIMIT FOR BUSINESS ROUTE 4 IN THIS AREA VARIES BETWEEN 35 MPH AND 40 MPH. AN ADVISORY SPEED LIMIT OF 30 MPH WILL BE POSTED IN THE WORK ZONE FOR THIS PROJECT.
3. CONSTRUCTION SIGNS SHALL BE INSTALLED SO AS NOT TO OBSTRUCT EXISTING SIGNS.
4. ALL SIGNS SHALL BE IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND THE "STANDARD HIGHWAY SIGNS" BOOK (SHS) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION (FHWA).
5. ORANGE CONSTRUCTION SIGNS SHALL HAVE RETROREFLECTIVE SHEETING EQUAL TO OR EXCEEDING THE REQUIREMENTS OF SECTION 750 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION FOR TYPE VII, VIII OR IX, UNLESS OTHERWISE NOTED.
6. ROLL UP SIGNS SHALL HAVE A RETROREFLECTIVE SHEETING EQUAL TO OR EXCEEDING THE REQUIREMENTS OF SECTION 750 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION FOR TYPE VI.
7. SIGNS SHALL BE ERECTED BEFORE THE START OF ANY WORK AND SHALL BE COVERED UNTIL WORK COMMENCES AND DURING PERIODS OF INACTIVITY. EACH SIGN SHALL BE ERECTED IN A NEAT AND WORKMAN LIKE MANNER. SIGNS SHALL BE REMOVED UPON COMPLETION OF THE WORK AT THE DISCRETION OF THE ENGINEER.
8. FIXED SIGNS SHALL BE SET SECURELY IN THE GROUND. THE BOTTOM OF A SIGN SHALL BE AT LEAST SEVEN FEET ABOVE THE EDGE OF PAVEMENT. THE NEAREST EDGE OF A SIGN SHALL BE AT LEAST SIX FEET OUTSIDE THE SHOULDER POINT OR FOUR FEET OUTSIDE THE GUARDRAIL.
9. PORTABLE SIGNS SHALL BE PLACED ON THE EDGE OF ROADWAY AND A ONE FOOT MINIMUM HEIGHT ABOVE TRAVELED WAY. WHEN PLACED BEHIND THE GUARDRAIL, THE BOTTOM OF THE SIGN FACE SHALL BE ABOVE THE TOP OF THE GUARDRAIL. ALL VEGETATION THAT INTERFERES WITH VISIBILITY OF THE SIGNS SHALL BE REMOVED. PAYMENT FOR REMOVAL IS INCIDENTAL TO THE APPROPRIATE CONTRACT ITEMS.
10. WHERE SIGN INSTALLATIONS ARE NOT PROTECTED BY GUARDRAIL OR OTHER APPROVED TRAFFIC BARRIERS, ALL SIGN STANDS AND POST INSTALLATIONS SHALL BE "NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM" (NCHRP) REPORT 350 COMPLIANT. NO SIGN POSTS SHALL EXTEND OVER THE TOP OF THE SIGN INSTALLED ON SAID POST(S). WHEN ANCHORS ARE INSTALLED STUB SHALL NOT BE GREATER THAN FOUR INCHES ABOVE EXISTING GROUND.
11. THE NUMBER OF CHANNELIZING DEVICES, TYPE III BARRICADES AND OTHER TRAFFIC CONTROL DEVICES SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. THE ACTUAL NUMBER REQUIRED ARE TO BE DETERMINED BASED ON INDIVIDUAL DETOUR CONDITIONS (TAPERS, SPEED LIMITS, LENGTH OF DETOUR, CURVE ETC.). WARNING LIGHTS SHALL NOT BE USED ON CHANNELIZING DEVICES.
12. AT NO TIME WILL THE CONTRACTOR BE ALLOWED TO HAVE WORKERS' VEHICLES, CONSTRUCTION EQUIPMENT OR STOCKPILED MATERIALS WITHIN THE CLEAR ZONE OF BUSINESS ROUTE 4 WITHOUT POSITIVE PROTECTION. POSITIVE PROTECTION SHALL BE AS DIRECTED BY THE ENGINEER. THE CLEAR ZONE IS DEFINED AS FOLLOWS:
BUSINESS ROUTE 4 - 16 FEET FROM THE TRAVELED WAY
13. BRIDGE D4-4, EASTBOUND RIGHT LANE CLOSED: THE INTERSECTION WITH SIMONS AVE CLOSEST TO BRIDGE D4-4 FALLS WITHIN THE MERGING TAPER OF THE RIGHT LANE. PROVIDE A GAP IN THE REFLECTORIZED PLASTIC DRUMS AT THIS INTERSECTION, SO VEHICLES CAN ENTER AND EXIT SIMONS AVE.
14. BRIDGE D4-4, WESTBOUND RIGHT THRU LANE CLOSED: PLACE SIGN W9-2 AT FAR SIDE OF THE BUSINESS ROUTE 4 AND EAST PROCTOR ROAD INTERSECTION. PLACE SIGN R3-7 AT BEGINNING OF SLIP LANE TAPER TO ROUTE 3.
15. SIGN R9-9 SHOULD BE INSTALLED AT THE BEGINNING OF THE CLOSED SIDEWALK AND AT INTERSECTIONS PRECEDING THE CLOSED SIDEWALK.

TRAFFIC CONTROL SHEET (3)

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tc_3.dgn	

