

STATE OF VERMONT
AGENCY OF TRANSPORTATION

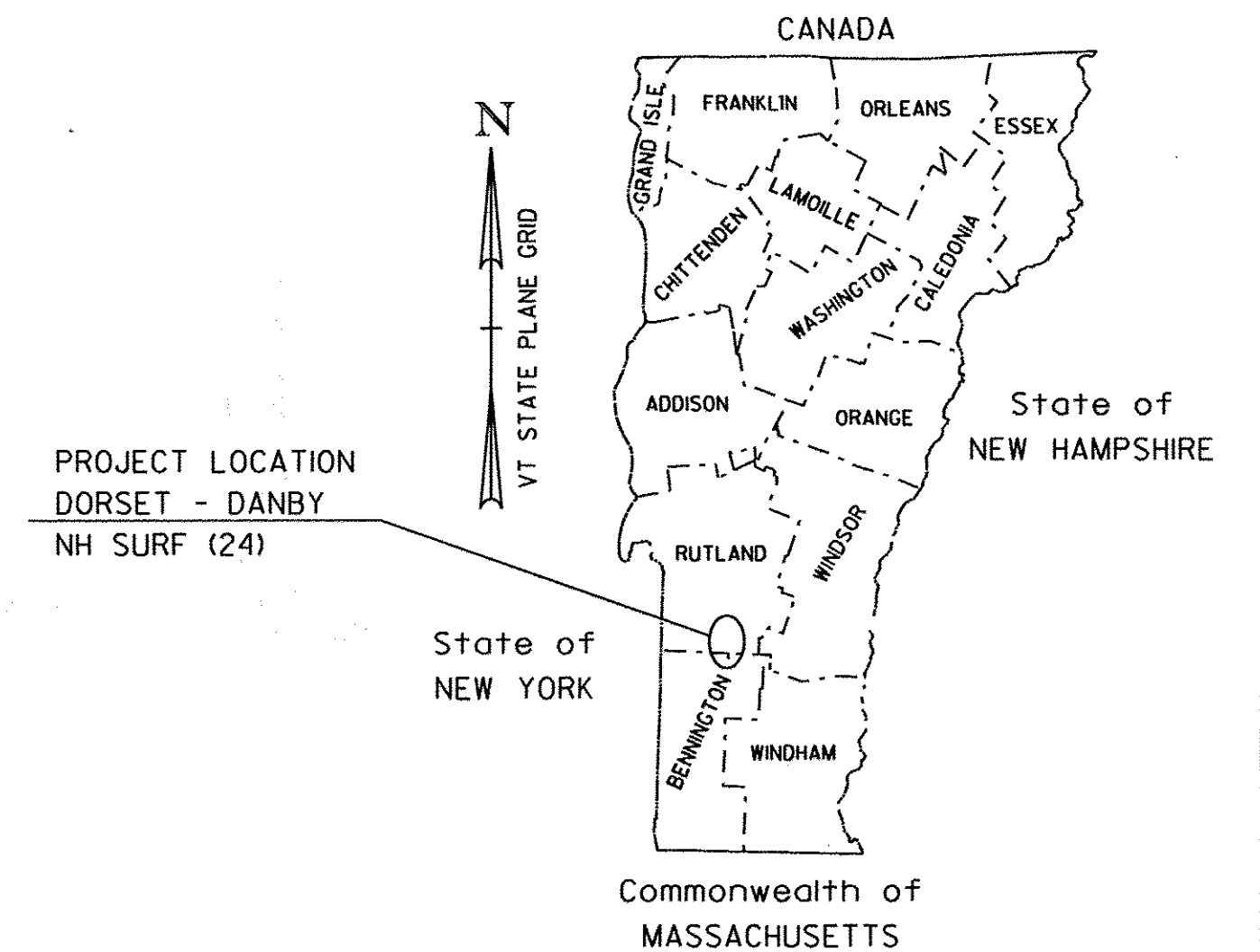


PROPOSED IMPROVEMENT
TOWNS OF DORSET, MOUNT TABOR & DANBY
COUNTIES OF BENNINGTON & RUTLAND
US ROUTE 7
PRINCIPAL ARTERIAL

BEGINNING IN THE TOWN OF DORSET AT MILE MARKER 1.745 AND EXTENDING NORTHERLY ALONG US ROUTE 7 FOR A DISTANCE OF 61,401.12 FT (11.629 MILES) THROUGH THE TOWN OF MOUNT TABOR, TO MILE MARKER 2.473 IN THE TOWN OF DANBY.

LENGTH OF ROADWAY = 61,401.12 FT = (11.629 MILES)
LENGTH OF PROJECT = 61,401.12 FT = (11.629 MILES)

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES SURFACE PREPARATION INVOLVING PATCHING, POT HOLE REPAIR, AND CRACK SEALING; SOME SPOT LEVELING; OVERLAYING WITH A THIN BITUMINOUS CONCRETE PAVEMENT WEARING COURSE AND TRAFFIC MARKINGS.



INDEX OF SHEETS

- 1 TITLE SHEET
- 2 SPECIAL PROVISION - (9.5 MM SBS POLYMER MODIFIED BITUMINOUS CONCRETE PAVEMENT)
- 3 ALTERNATE A TYPICAL SECTION
- 4 ALTERNATE B TYPICAL SECTION
- 5 PROJECT COLD PLANE DETAILS
- 6 ASPHALTIC PLUG JOINT DETAIL SHEET
- 7-8 QUANTITY SHEETS
- 9-12 PROJECT LAYOUT SHEETS 1 - 4
- 13 TH 15 INTERSECTION DETAIL SHEET
- 14 CONSTRUCTION APPROACH SIGNING SHEET
- 15 RUTTING DATA INFORMATION SHEET
- 16 ROUGHNESS DATA INFORMATION SHEET

STANDARDS

E-100	CONSTRUCTION APPROACH SIGNS	01/02/04
E-100A	SIDE ROAD CONSTRUCTION - APPROACH SIGNS	01/02/04
E-101	CONSTRUCTION SIGN DETAILS	05/30/03
E-102	CONSTRUCTION SIGN DETAILS	06/30/03
E-102A	CONSTRUCTION SIGN DETAILS	05/01/04
E-103	MAINLINE TRAFFIC CONTROL DIVIDED HIGHWAY ONE LANE CLOSED	03/01/04
E-105	TRAFFIC CONTROL FOR CONSTRUCTION VEHICLE U-TURNS ON DIVIDED HIGHWAY	05/01/04
E-106	TRAFFIC CONTROL - MISCELLANEOUS DETAILS	03/01/04
E-107A	BREAKAWAY BARRICADE DETAILS	06/08/09
E-108	CONSTRUCTION ZONE LONGITUDINAL DROP OFFS	06/08/09
E-108A	CONSTRUCTION ZONE LONGITUDINAL DROP OFFS FOR PAVING	06/08/09
E-191	PAVEMENT MARKING DETAILS	02/01/99
E-193	PAVEMENT MARKING DETAILS	08/18/95

RECORD PLANS

CONTRACTOR: PIKE INDUSTRIES, INC. - BERLIN, VT

RESIDENT ENGINEER: JOSH HULETT

CONSTRUCTION BEGAN: JULY 21, 2011

CONSTRUCTION COMPLETE: NOVEMBER 14, 2011

RECORD PLANS BY: JOSH HULETT & DARREN CONNOLLY

I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.

BY *Josh Hulett* RESIDENT ENGINEER

DATE 2/23/2012

NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.

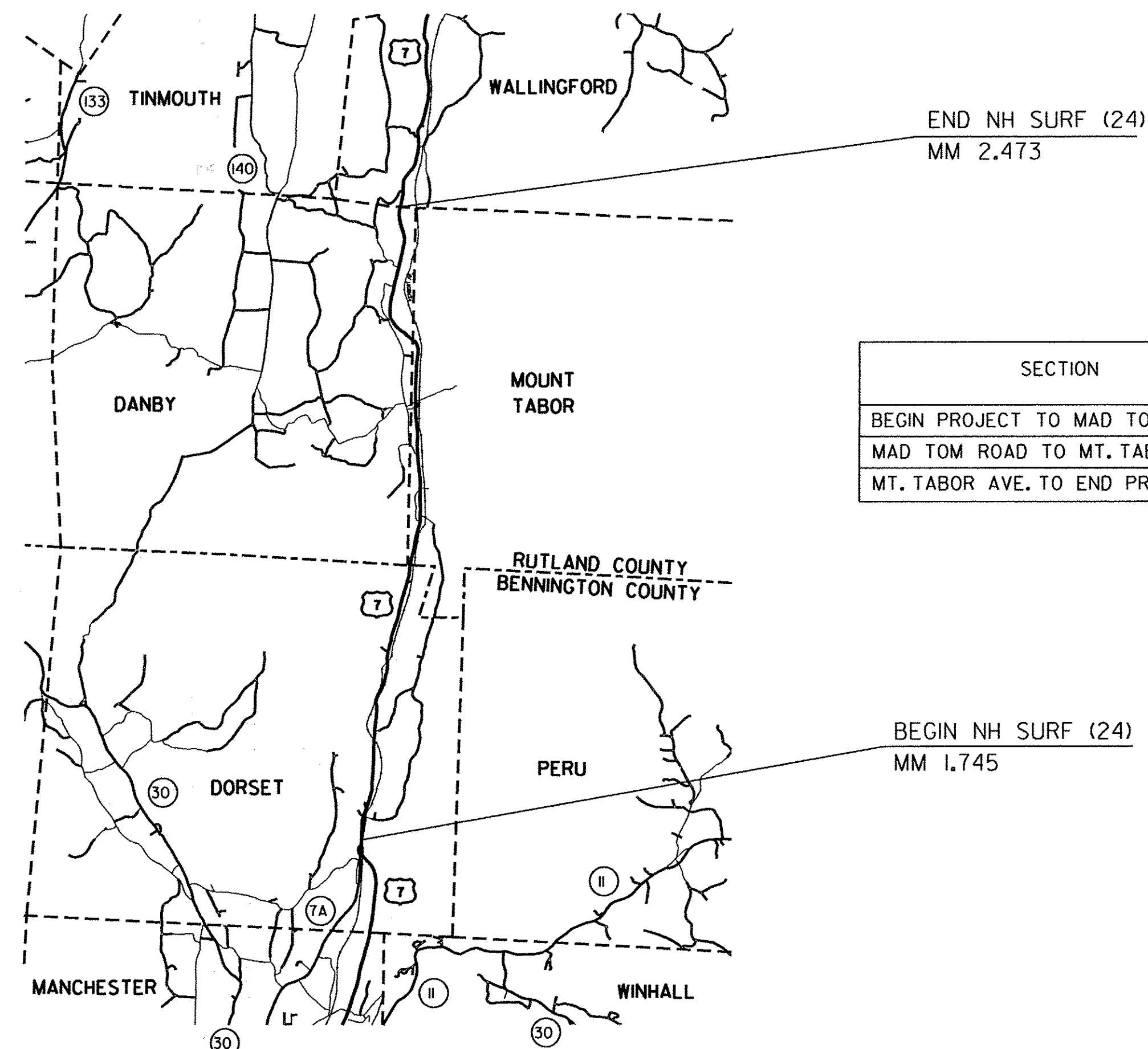
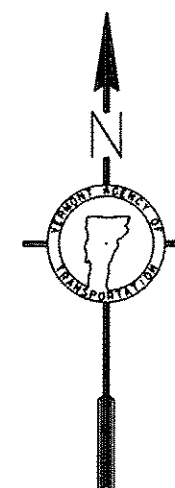
TRAFFIC DATA

SECTION	AADT		DHV		ESALS	
	2011	2021	2011	2021	2011~2021	2011~2031
BEGIN PROJECT TO MAD TOM ROAD	5400	5700	610	640	1,471,000	3,534,000
MAD TOM ROAD TO MT. TABOR AVE.	4100	4200	460	470	1,132,000	2,428,000
MT. TABOR AVE. TO END PROJECT	4400	4500	500	510	1,472,000	3,130,000

QUALITY ASSURANCE PROGRAM: LEVEL I

CONVENTIONAL SYMBOLS

COUNTY LINE	
TOWN LINE	
LIMITS OF ACCESS	
POINT OF ACCESS	
FENCE LINE	
STONE WALL	
TRAVELED WAY	
GUARD RAIL	
RAILROAD	
SURVEY LINE	
CULVERT	
POWER POLE	
TELEPHONE POLE	
TREES	
CONTROL OF ACCESS	
PROPERTY LINE	
R.O.W. TAKING LINE	
SLOPE RIGHTS	
TOP OF CUT	
TOE OF SLOPE	



THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT.

CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

DIRECTOR OF PROGRAM DEVELOPMENT

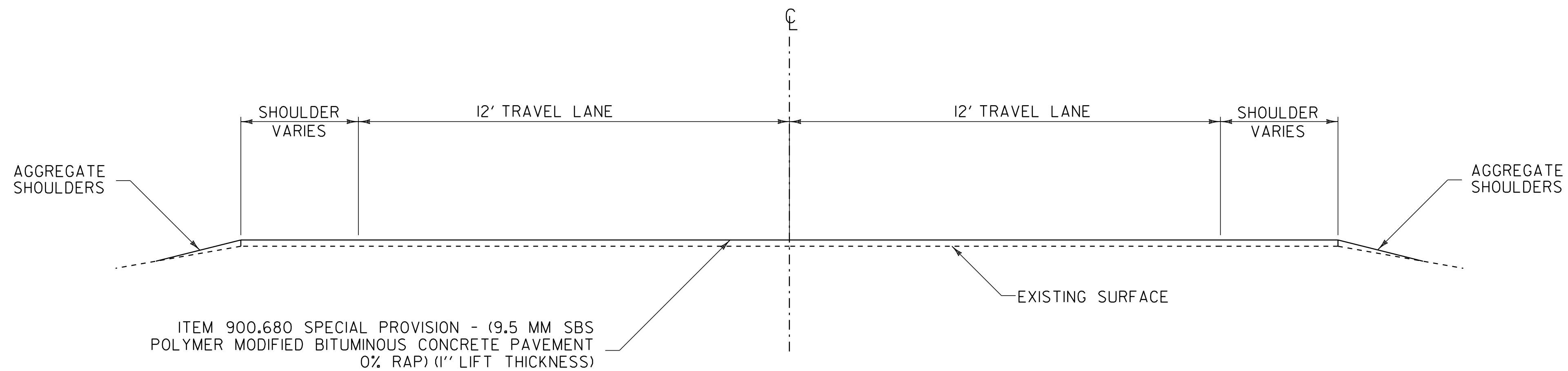
APPROVED *[Signature]* DATE 3/8/11

PROJECT MANAGER: MIKE FOWLER

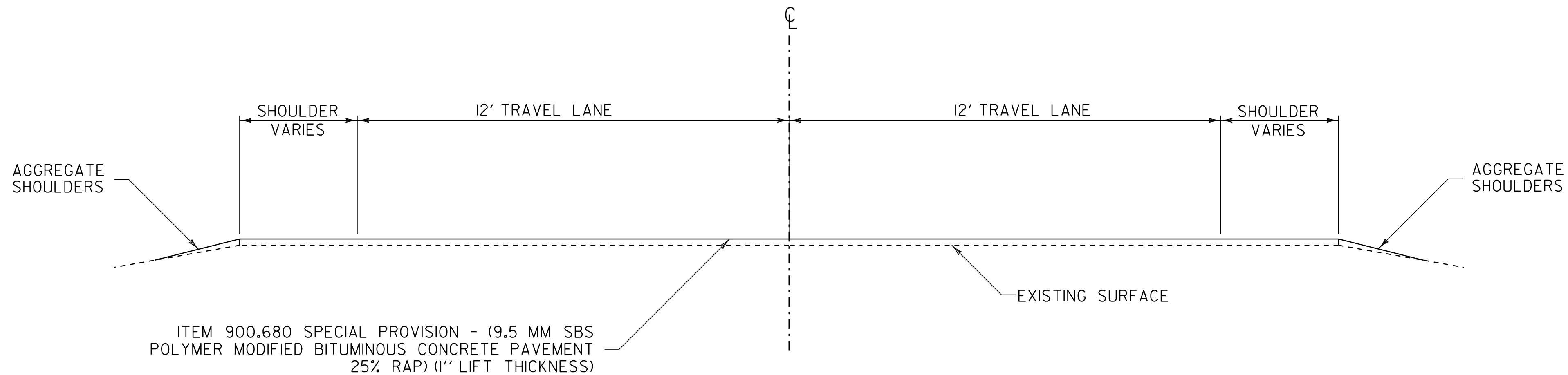
PROJECT NAME: DORSET - DANBY

PROJECT NUMBER: NH SURF (24)

SHEET 1 OF 16 SHEETS



TYPICAL SECTION
MM 1.745 - MM 2.745 (DORSET)



TYPICAL SECTION
MM 2.745 - MM 3.745 (DORSET)

NOTES:

1. ALL NECESSARY SURFACE PREPARATION INVOLVING PATCHING, POTHOLE REPAIR, AND CRACK SEALING SHALL BE PERFORMED PRIOR TO APPLICATION OF ANY SPOT LEVELING OR PLACEMENT OF THE WEARING COURSE. ALL CRACKS GREATER THAN 0.10 INCH AND UP TO 1.0 INCH IN WIDTH SHALL BE SEALED USING THE "BLOW AND GO" FILL METHOD. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE PAID FOR UNDER ITEM 417.20, BITUMINOUS CRACK SEALING, "BLOW AND GO" METHOD. THE PATCHING OF ALL CRACKS GREATER THAN 1.0 INCH AND ALL OTHER PATCHING AND POTHOLE REPAIR SHALL BE COMPLETED USING BITUMINOUS CONCRETE PAVEMENT IN ACCORDANCE WITH ITEM 900.680 SPECIAL PROVISION (BITUMINOUS CONCRETE PAVEMENT SURFACE PREPARATION, TYPE I). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED TO COVER ALL COSTS ASSOCIATED WITH THIS WORK.
2. FOLLOWING COMPLETION OF COLD PLANING, THE MILLED SURFACE FOR ALL BRIDGES SHALL ALSO RECEIVE CRACK SEALING AND RELATED PATCHING AND POTHOLE REPAIR TREATMENTS, AS DIRECTED BY THE RESIDENT ENGINEER.
3. ALL EXISTING PAVEMENT MARKINGS SHALL BE REMOVED PRIOR TO ANY CRACK SEALING BEING PERFORMED AND PRIOR TO APPLYING THE PAVER PLACED SURFACE TREATMENT. ALL LANE DELINEATION IS TO BE MAINTAINED DURING CONSTRUCTION BY THE USE OF LINE STRIPING TARGETS OR TEMPORARY PAINT.
4. A 50' COLD PLANED WEDGE SHALL BE CONSTRUCTED AT THE PROJECT BEGIN, PROJECT END, AND AT ALL BRIDGE APPROACHES OR AS DIRECTED BY THE RESIDENT ENGINEER. ANY SAWCUTTING AT BUTT JOINTS SHALL BE PAID INCIDENTAL TO ITEM 210.10, COLD PLANING, BITUMINOUS PAVEMENT. THE CONTRACTOR SHALL USE CAUTION WHEN COLD PLANING AND PAVING OPERATIONS OCCUR ADJACENT TO EXISTING DROP INLETS OR CATCH BASINS. ANY DAMAGE WHICH OCCURS TO THESE DRAINAGE STRUCTURES AS A RESULT OF THESE OPERATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE STATE OF VERMONT.
5. IF IT IS DETERMINED IN AREAS ALONG THE BASE OF THE GUARDRAIL THAT WINTER SAND AND OTHER DEBRIS HAS ACCUMULATED SUFFICIENTLY TO AFFECT PROPER CRACK SEALING AND RELATED PATCHING AND POTHOLE REPAIR TREATMENTS, THIS MATERIAL SHALL BE REMOVED PRIOR TO CRACK SEALING, PATCHING, AND POTHOLE REPAIR AS DIRECTED BY THE RESIDENT ENGINEER. AN ESTIMATED QUANTITY FOR ITEM 203.40 SHOULDER BERM REMOVAL HAS BEEN INCLUDED TO COVER THE COSTS ASSOCIATED WITH THIS WORK.
6. IT IS EXPECTED THAT SOME SPOT LEVELING WILL BE NECESSARY AS DIRECTED BY THE RESIDENT ENGINEER PRIOR TO THE PLACEMENT OF THE WEARING COURSE. AN ESTIMATED QUANTITY OF ITEM 900.680 SPECIAL PROVISION (BITUMINOUS CONCRETE PAVEMENT, SMALL QUANTITY) AND ITEM 900.683 SPECIAL PROVISION (EMULSIFIED ASPHALT)(RS-IH OR CRS-IH) HAS BEEN INCLUDED FOR THIS PURPOSE.
7. BRIDGES WITHIN THIS TYPICAL SECTION ARE: BRIDGE NUMBER 52, MM 3.501(DORSET) COMP. WF BEAM ST (COLD PLANE 3/4" THEN PAVE WITH SPECIAL PROVISION - (9.5 MM SBS POLYMER MODIFIED B.C.P. 25% RAP) (1" LIFT THICKNESS))

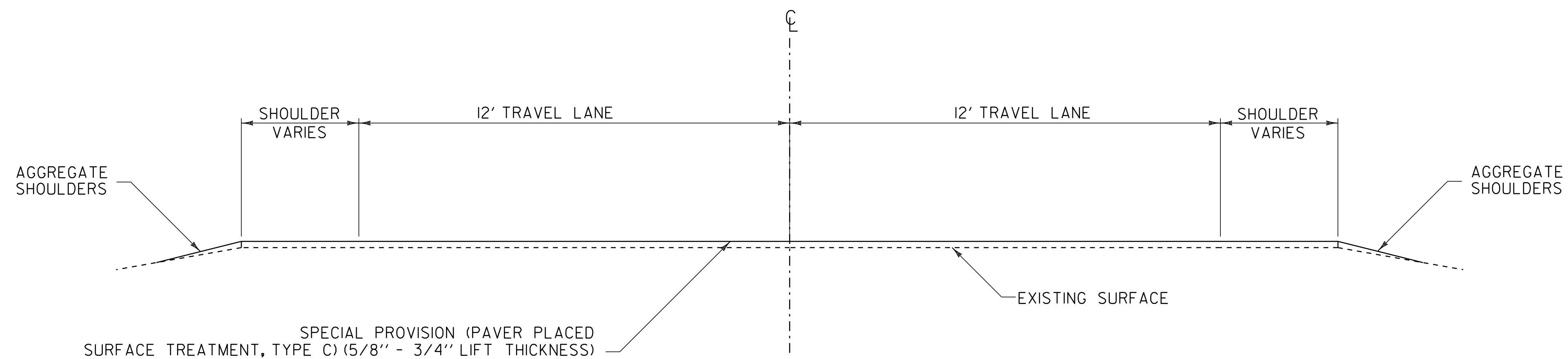
NOT TO SCALE

**SPECIAL PROVISION -
 (9.5 MM SBS POLYMER
 MODIFIED BITUMINOUS
 CONCRETE PAVEMENT)**

PROJECT NAME: DORSET - DANBY
 PROJECT NUMBER: NH SURF (24)

FILE NAME: I0b118\pi0b118.dgn
 PROJECT LEADER: FOWLER
 DESIGNED BY: HUNT
 IPARM FILE NAME: I0b118_02.i

PLOT DATE: 23-MAY-2012
 DRAWN BY: HUNT
 CHECKED BY: PAVT MGMT
 SHEET 2 OF 16



TYPICAL SECTION
MM 3.745 (DORSET) – MM 2.473 (DANBY)

NOTES:

1. ALL NECESSARY SURFACE PREPARATION INVOLVING PATCHING, POT HOLE REPAIR, AND CRACK SEALING SHALL BE PERFORMED PRIOR TO APPLICATION OF ANY SPOT LEVELING OR PLACEMENT OF THE WEARING COURSE. ALL CRACKS GREATER THAN 0.10 INCH AND UP TO 1.0 INCH IN WIDTH SHALL BE SEALED USING THE "BLOW AND GO" FILL METHOD. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE PAID FOR UNDER ITEM 417.20, BITUMINOUS CRACK SEALING, "BLOW AND GO" METHOD. THE PATCHING OF ALL CRACKS GREATER THAN 1.0 INCH AND ALL OTHER PATCHING AND POT HOLE REPAIR SHALL BE COMPLETED USING BITUMINOUS CONCRETE PAVEMENT IN ACCORDANCE WITH ITEM 900.680 SPECIAL PROVISION (BITUMINOUS CONCRETE PAVEMENT SURFACE PREPARATION, TYPE I). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED TO COVER ALL COSTS ASSOCIATED WITH THIS WORK.

2. FOLLOWING COMPLETION OF COLD PLANING, THE MILLED SURFACE FOR ALL BRIDGES SHALL ALSO RECEIVE CRACK SEALING AND RELATED PATCHING AND POT HOLE REPAIR TREATMENTS, AS DIRECTED BY THE RESIDENT ENGINEER.

3. ALL EXISTING PAVEMENT MARKINGS SHALL BE REMOVED PRIOR TO ANY CRACK SEALING BEING PERFORMED AND PRIOR TO APPLYING THE PAVER PLACED SURFACE TREATMENT. ALL LANE DELINEATION IS TO BE MAINTAINED DURING CONSTRUCTION BY THE USE OF LINE STRIPING TARGETS OR TEMPORARY PAINT.

4. A 50' COLD PLANED WEDGE SHALL BE CONSTRUCTED AT THE PROJECT BEGIN, PROJECT END, AND AT ALL BRIDGE APPROACHES OR AS DIRECTED BY THE RESIDENT ENGINEER. ANY SAWCUTTING AT BUTT JOINTS SHALL BE PAID INCIDENTAL TO ITEM 210.10, COLD PLANING, BITUMINOUS PAVEMENT. THE CONTRACTOR SHALL USE CAUTION WHEN COLD PLANING AND PAVING OPERATIONS OCCUR ADJACENT TO EXISTING DROP INLETS OR CATCH BASINS. ANY DAMAGE WHICH OCCURS TO THESE DRAINAGE STRUCTURES AS A RESULT OF THESE OPERATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE STATE OF VERMONT.

5. IF IT IS DETERMINED IN AREAS ALONG THE BASE OF THE GUARDRAIL THAT WINTER SAND AND OTHER DEBRIS HAS ACCUMULATED SUFFICIENTLY TO AFFECT PROPER CRACK SEALING AND RELATED PATCHING AND POT HOLE REPAIR TREATMENTS, THIS MATERIAL SHALL BE REMOVED PRIOR TO CRACK SEALING, PATCHING, AND POT HOLE REPAIR AS DIRECTED BY THE RESIDENT ENGINEER. AN ESTIMATED QUANTITY FOR ITEM 203.40 SHOULDER BERM REMOVAL HAS BEEN INCLUDED TO COVER THE COSTS ASSOCIATED WITH THIS WORK.

6. IT IS EXPECTED THAT SOME SPOT LEVELING WILL BE NECESSARY AS DIRECTED BY THE RESIDENT ENGINEER PRIOR TO THE PLACEMENT OF THE WEARING COURSE. AN ESTIMATED QUANTITY OF ITEM 900.680 SPECIAL PROVISION (BITUMINOUS CONCRETE PAVEMENT, SMALL QUANTITY) AND ITEM 900.683 SPECIAL PROVISION (EMULSIFIED ASPHALT)(RS-IH OR CRS-IH) HAS BEEN INCLUDED FOR THIS PURPOSE.

7. BRIDGES WITHIN THIS TYPICAL SECTION ARE:

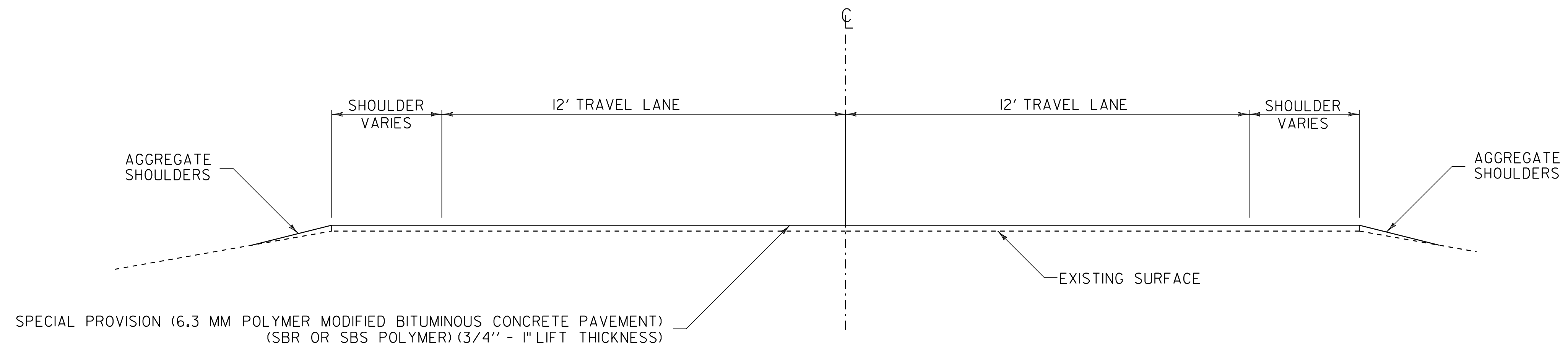
- ~~BRIDGE NUMBER 54, MM 4.992 (DORSET) COMP. WF BEAM S (COLD PLANE 3/4" THEN PAVE WITH SPECIAL PROVISION (PAVER PLACED SURFACE TREATMENT, TYPE C))~~
- BRIDGE NUMBER 56C, MM 3.001 (MT. TABOR) COMP. WF BEAM ST (COLD PLANE 3/4" THEN PAVE WITH SPECIAL PROVISION (PAVER PLACED SURFACE TREATMENT, TYPE C))
- BRIDGE NUMBER 66, MM 1.564 (DANBY) CONC. SLAB CC (COLD PLANE 3/4" THEN PAVE WITH SPECIAL PROVISION (PAVER PLACED SURFACE TREATMENT, TYPE C))

NOT TO SCALE

**ALTERNATE A
TYPICAL
SECTION**

PROJECT NAME: DORSET - DANBY
PROJECT NUMBER: NH SURF (24)

FILE NAME: I0b118\p10b118.dgn PLOT DATE: 23-MAY-2012 16:06
PROJECT LEADER: FOWLER DRAWN BY: HUNT
DESIGNED BY: HUNT CHECKED BY: PAVT MGMT
IPARM FILE NAME: I0b118_03.i SHEET 3 OF 16



TYPICAL SECTION
MM 3.745 (DORSET) – MM 2.473 (DANBY)

NOTES:

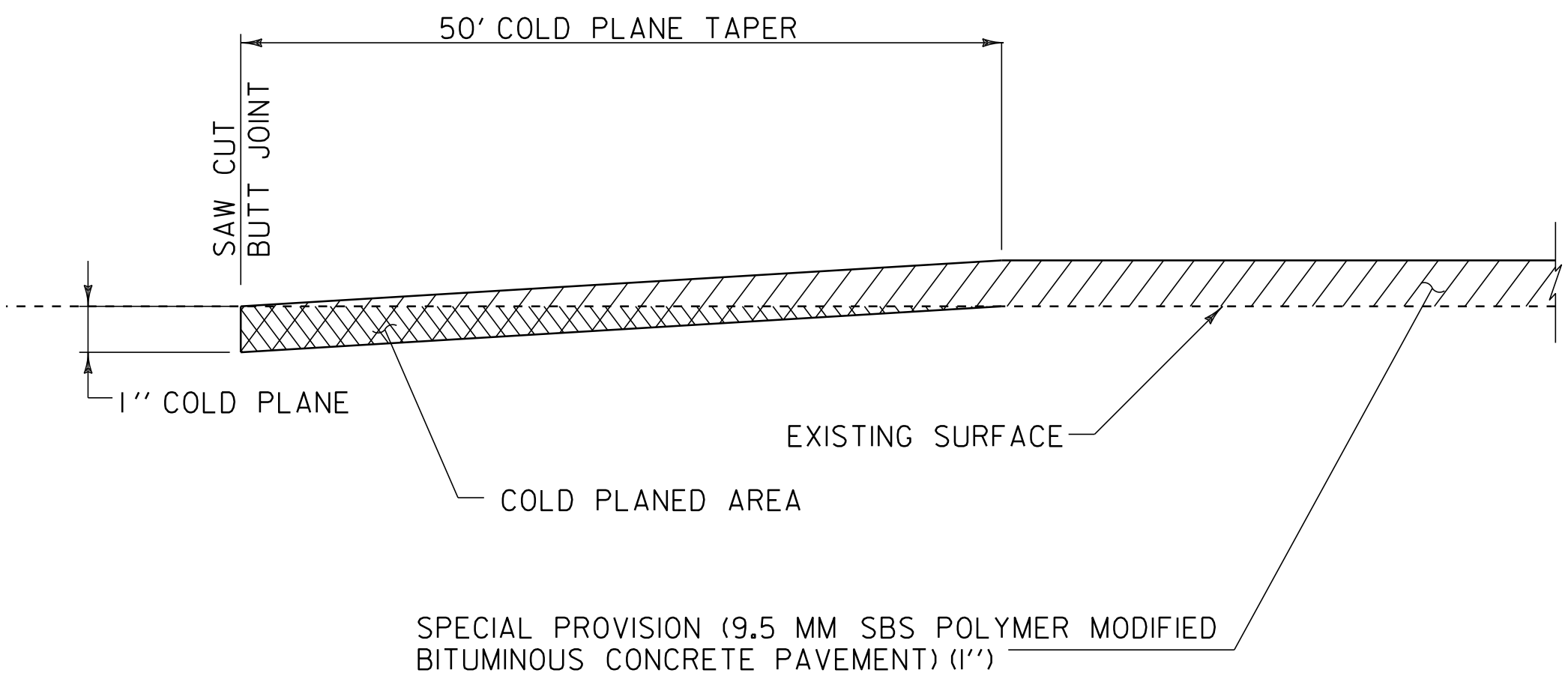
1. ALL NECESSARY SURFACE PREPARATION INVOLVING PATCHING, POTHOLE REPAIR, AND CRACK-SEALING SHALL BE PERFORMED PRIOR TO APPLICATION OF ANY SPOT LEVELING OR PLACEMENT OF THE POLYMER MODIFIED SURFACE TREATMENT. ALL CRACKS GREATER THAN 0.10" AND UP TO 1.0" IN WIDTH SHALL BE SEALED USING THE "BLOW AND GO" FILL METHOD. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE PAID FOR UNDER ITEM 417.20, BITUMINOUS CRACK SEALING, "BLOW AND GO" METHOD. THE PATCHING OF ALL CRACKS GREATER THAN 1.0" AND ALL OTHER PATCHING AND POT-HOLE REPAIR SHALL BE COMPLETED USING BITUMINOUS CONCRETE PAVEMENT IN ACCORDANCE WITH ITEM 900.680 SPECIAL PROVISION (BITUMINOUS CONCRETE PAVEMENT SURFACE PREPARATION, TYPE I). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED TO COVER ALL COSTS ASSOCIATED WITH THIS WORK.
2. FOLLOWING COMPLETION OF COLD PLANING, THE MILLED SURFACE FOR ALL BRIDGES SHALL ALSO RECEIVE CRACK-SEALING AND RELATED PATCHING AND POTHOLE REPAIR TREATMENTS, AS DIRECTED BY THE RESIDENT ENGINEER.
3. ALL EXISTING PAVEMENT MARKINGS SHALL BE REMOVED PRIOR TO ANY CRACK SEALING BEING PERFORMED AND PRIOR TO APPLYING THE POLYMER MODIFIED SURFACE TREATMENT. ALL LANE DELINEATION IS TO BE MAINTAINED DURING CONSTRUCTION BY THE USE OF LINE STRIPING TARGETS OR TEMPORARY PAINT.
4. A 50' COLD PLANED WEDGE SHALL BE CONSTRUCTED AT THE PROJECT BEGIN, PROJECT END, AND AT ALL BRIDGE APPROACHES OR AS DIRECTED BY THE RESIDENT ENGINEER. ANY SAWCUTTING AT BUTT JOINTS SHALL BE PAID INCIDENTAL TO ITEM 210.10, COLD PLANING, BITUMINOUS PAVEMENT. THE CONTRACTOR SHALL USE CAUTION WHEN COLD PLANING AND PAVING OPERATIONS OCCUR ADJACENT TO EXISTING DROP INLETS OR CATCH BASINS. ANY DAMAGE WHICH OCCURS TO THESE DRAINAGE STRUCTURES AS A RESULT OF THESE OPERATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE STATE OF VERMONT.
5. IF IT IS DETERMINED IN AREAS ALONG THE BASE OF THE GUARDRAIL THAT WINTER SAND AND OTHER DEBRIS HAS ACCUMULATED SUFFICIENTLY TO AFFECT PROPER CRACK-SEALING AND RELATED PATCHING AND POTHOLE REPAIR TREATMENTS, THIS MATERIAL SHALL BE REMOVED PRIOR TO CRACK-SEALING, PATCHING, AND POTHOLE REPAIR AS DIRECTED BY THE RESIDENT ENGINEER. AN ESTIMATED QUANTITY FOR ITEM 203.40 SHOULDER BERM REMOVAL HAS BEEN INCLUDED TO COVER THE COSTS ASSOCIATED WITH THIS WORK.
6. PRIOR TO THE PLACEMENT OF THE POLYMER MODIFIED SURFACE TREATMENT, EMULSIFIED ASPHALT SHALL BE APPLIED TO ALL EXISTING PAVEMENT SURFACES AND ON ALL COLD PLANED SURFACES AT A RATE OF 0.080 GAL/SY (+/- 0.01GAL/SY) OR AS DIRECTED BY THE RESIDENT ENGINEER. EMULSIFIED ASPHALT SHALL BE RS-IH OR CRS-IH PER THE MANUFACTURER'S RECOMMENDATION AND PAID UNDER ITEM 900.683 SPECIAL PROVISION (EMULSIFIED ASPHALT)(RS-IH OR CRS-IH).
7. IT IS EXPECTED THAT SOME SPOT LEVELING WILL BE NECESSARY AS DIRECTED BY THE RESIDENT ENGINEER PRIOR TO THE PLACEMENT OF THE WEARING COURSE. AN ESTIMATED QUANTITY OF ITEM 900.680 SPECIAL PROVISION (BITUMINOUS CONCRETE PAVEMENT, SMALL QUANTITY) AND ITEM 900.683 SPECIAL PROVISION (EMULSIFIED ASPHALT)(RS-IH OR CRS-IH) HAS BEEN INCLUDED FOR THIS PURPOSE.
8. THE GYRATION SPECIFICATION FOR THE 6.3 MM POLYMER - MODIFIED BITUMINOUS CONCRETE PAVEMENT SHALL BE 75.
9. BRIDGES WITHIN THIS TYPICAL SECTION ARE:
 BRIDGE NUMBER 54, MM 4.992 (DORSET) COMP. WF BEAM S (COLD PLANE 3/4" THEN PAVE WITH SPECIAL PROVISION (6.3 MM POLYMER MODIFIED BITUMINOUS CONCRETE PAVEMENT))
 BRIDGE NUMBER 56C, MM 3.001 (MT. TABOR) COMP. WF BEAM ST (COLD PLANE 3/4" THEN PAVE WITH SPECIAL PROVISION (6.3 MM POLYMER MODIFIED BITUMINOUS CONCRETE PAVEMENT))
 BRIDGE NUMBER 66, MM 1.564 (DANBY) CONC. SLAB CC (COLD PLANE 3/4" THEN PAVE WITH SPECIAL PROVISION (6.3 MM POLYMER MODIFIED BITUMINOUS CONCRETE PAVEMENT))

NOT TO SCALE

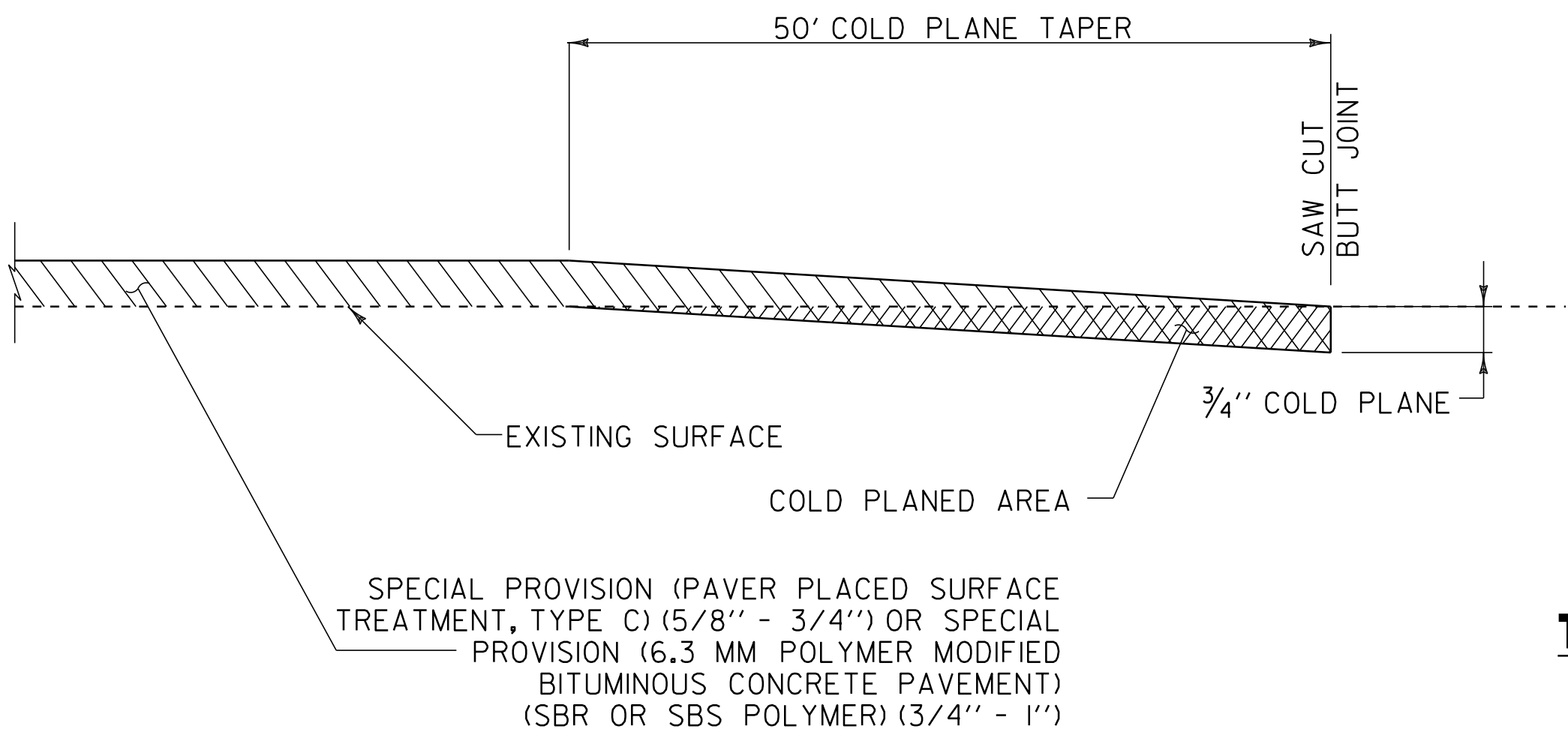
**ALTERNATE B
TYPICAL
SECTION**

PROJECT NAME: DORSET - DANBY
 PROJECT NUMBER: NH SURF (24)

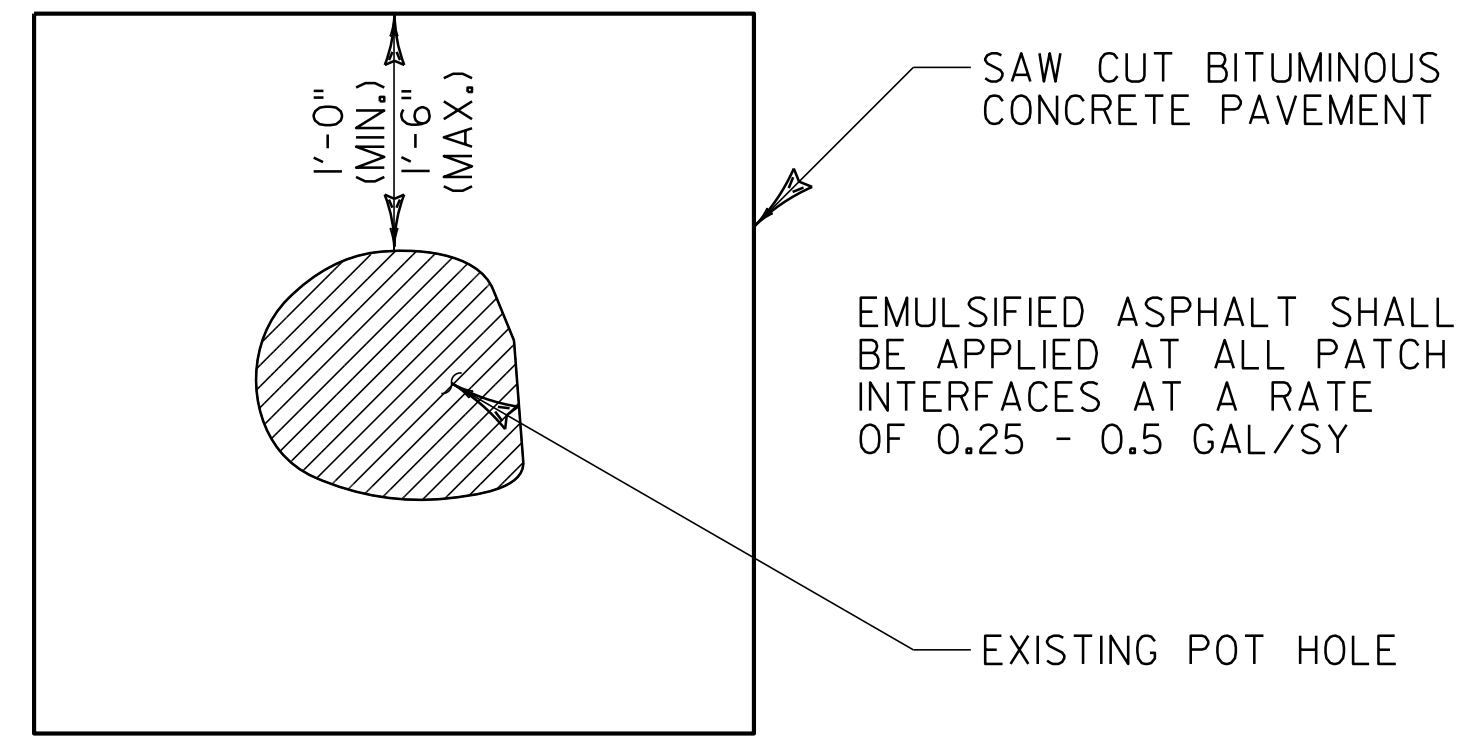
FILE NAME: I0b118\p10b118.dgn PLOT DATE: 23-MAY-2012 16:06
 PROJECT LEADER: FOWLER DRAWN BY: HUNT
 DESIGNED BY: HUNT CHECKED BY: PAVT MGMT
 IPARM FILE NAME: I0b118_04.i SHEET 4 OF 16



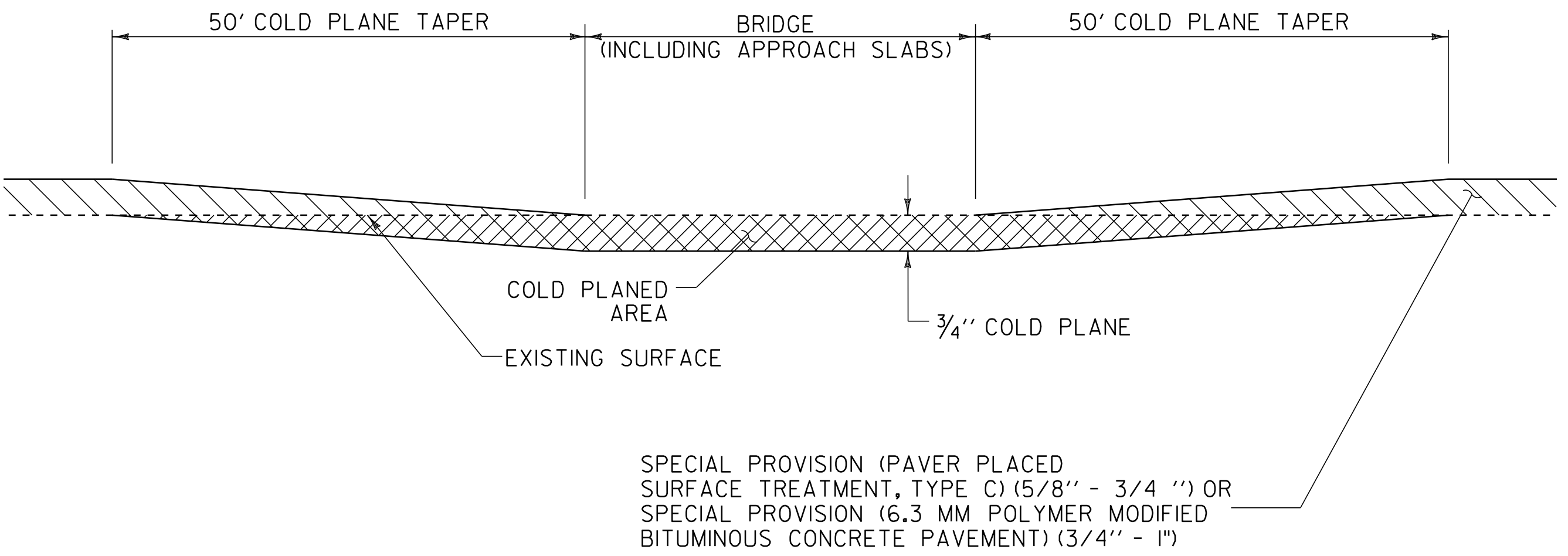
COLD PLANE DETAIL AT BEGIN/END PROJECT



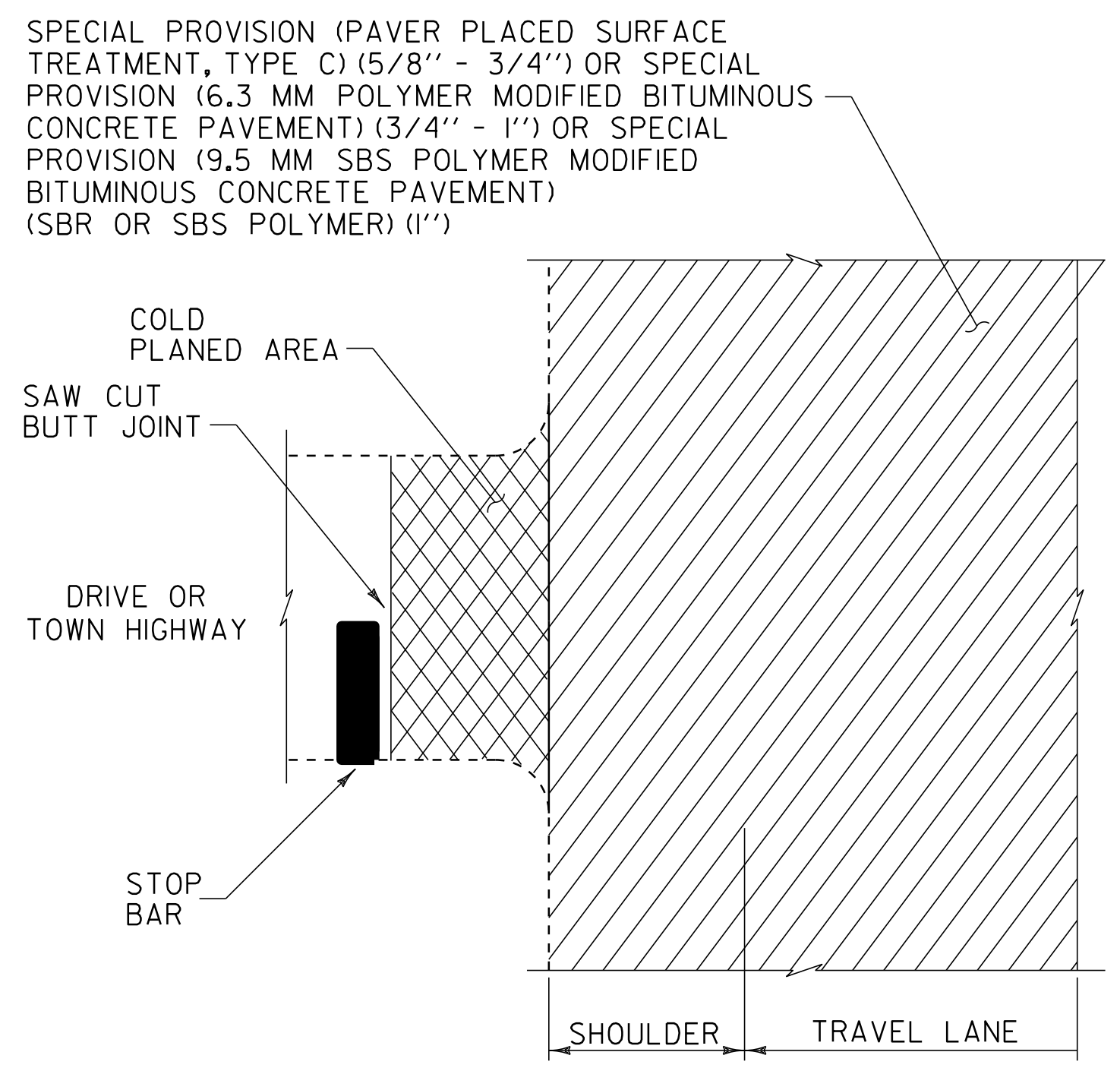
COLD PLANE DETAIL AT END PROJECT



TYPICAL - POT HOLE REPAIR
NOT TO SCALE



BRIDGE COLD PLANE DETAIL



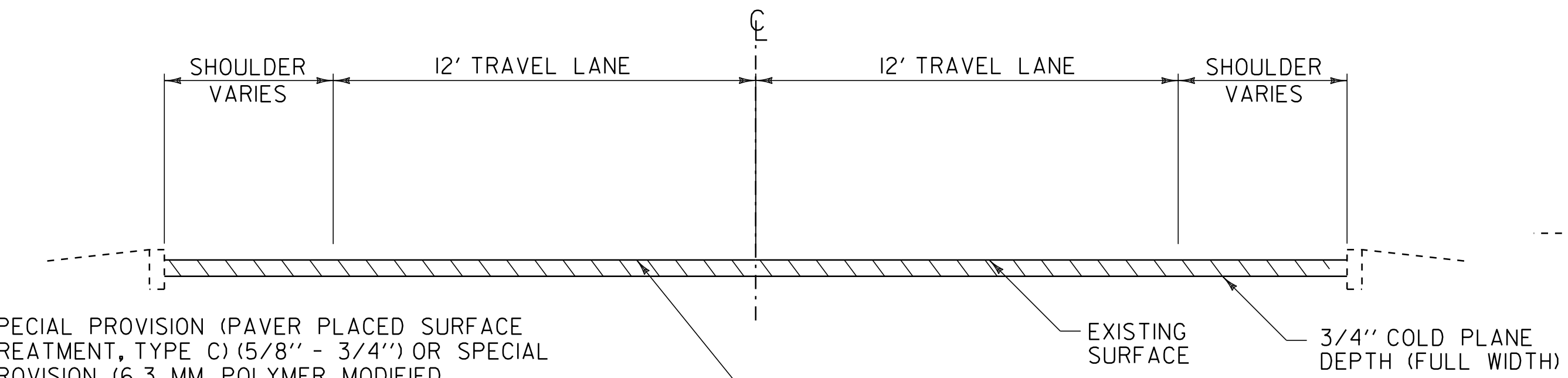
COLD PLANE DETAILS AT DRIVES & TOWN HIGHWAYS

NOTES:
1. REFER TO ASPHALTIC PLUG JOINT DETAIL SHEET. ALL NEW JOINTS TO BE PAID FOR UNDER ITEM 516.10, "BRIDGE EXPANSION JOINT, ASPHALTIC PLUG".

2. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID DAMAGING DRAINAGE STRUCTURES AND EXPANSION JOINTS. ANY DAMAGE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE EXPENSE OF THE CONTRACTOR.

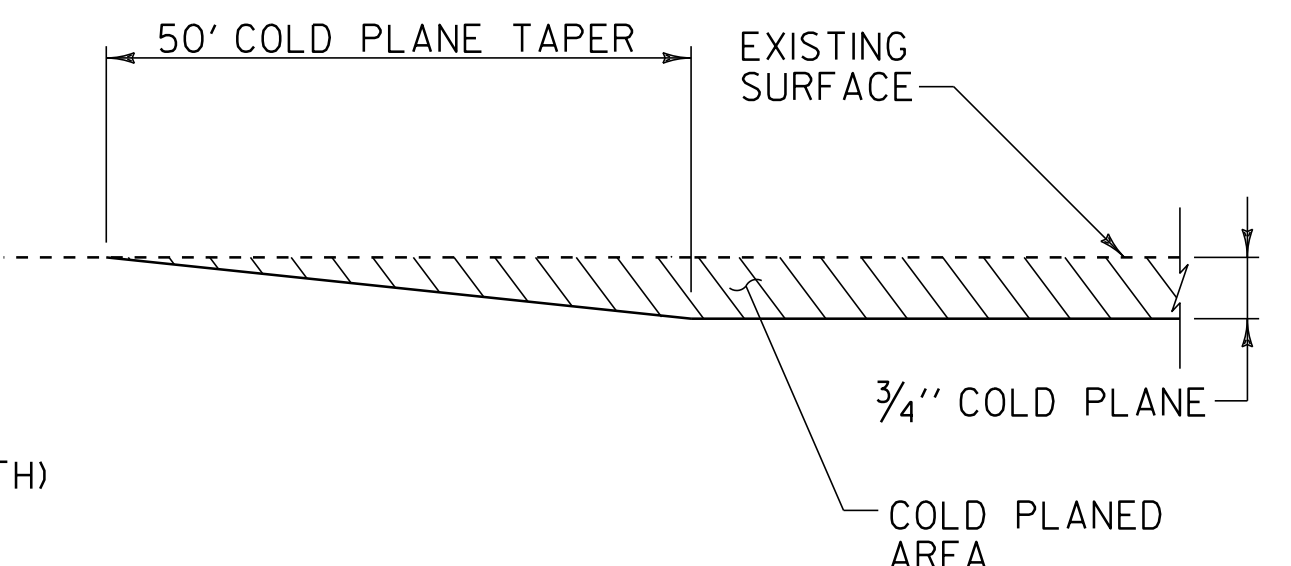
3. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID THE ACCUMULATION OF DEBRIS IN THE DRAINAGE STRUCTURES LOCATED AT CURB LINE AND IN THE EXPANSION JOINTS. THE CONTRACTOR SHALL EXAMINE THESE BRIDGE FEATURES ON A DAILY BASIS TO ENSURE THAT DEBRIS HAS NOT ACCUMULATED. ANY DEBRIS WHICH IS PRESENT SHALL BE REMOVED BY THE CONTRACTOR AT NO COST TO THE STATE.

4. THE CONTRACTOR SHALL USE CAUTION WHEN COLD PLANING AND PAVING OPERATIONS OCCUR ON BRIDGE DECKS. SHOULD ANY DAMAGE OCCUR TO THE DECK OR MEMBRANE AS A RESULT OF THESE OPERATIONS THE RESIDENT ENGINEER SHALL CONTACT THE VTRANS CONSTRUCTION STRUCTURES ENGINEER TO PROVIDE AN ASSESSMENT OF THE DAMAGE AND RECOMMEND ANY NECESSARY REPAIRS. THE CONSTRUCTION STRUCTURES ENGINEER WILL ALSO DETERMINE IF THE DAMAGE WAS AVOIDABLE AND IS THE FAULT OF THE CONTRACTOR. IF THE CONTRACTOR IS DETERMINED TO BE AT FAULT, THE RECOMMENDED REPAIRS SHALL BE COMPLETED AT NO COST TO THE STATE.



TYPICAL SECTION IN CURB AREAS

DORSET: MM 5.140 TO MM 5.365



COLD PLANE DETAIL IN CURB AREAS

NOT TO SCALE

PROJECT COLD PLANE DETAILS	PROJECT NAME: DORSET - DANBY	PLOT DATE: 23-MAY-2012
	PROJECT NUMBER: NH SURF (24)	DRAWN BY: HUNT
	FILE NAME: I0b118\pi0b118.dgn	CHECKED BY: PAVT MGMT
	PROJECT LEADER: FOWLER	SHEET 5 OF 16
	DESIGNED BY: HUNT	
	IPARM FILE NAME: I0b118_05.i	

ASPHALTIC PLUG JOINT NOTES

INSTALLATION:

1. LOCATE THE JOINT CENTRALLY OVER THE DECK OVERLAY EXPANSION GAP OR FIXED JOINT, MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.
2. REMOVE THE BITUMINOUS CONCRETE PAVEMENT FULL DEPTH AS SHOWN ON THE PLANS. THE PAVEMENT SHALL BE DRY AND SAW CUT TO THE LIMITS REQUIRED TO PLACE THE JOINT. A PNEUMATIC HAMMER AND CHISEL MAY BE USED ADJACENT TO THE CURB ONLY WHEN SAW CUTTING IS NOT POSSIBLE.
3. BLAST CLEAN THE JOINT AREA OF DEBRIS, ASPHALT AND SHEET MEMBRANE. THOROUGHLY DRY THE JOINT AREA WITH COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.
4. REPAIR MATERIAL GREATER THAN 4 INCHES FROM FINISHED GRADE WITH RAPID SETTING CONCRETE REPAIR MATERIAL WITH COURSE AGGREGATE MEETING THE REQUIREMENTS OF SUBSECTION 780.04.
5. PLACE PROPERLY SIZED HEAT RESISTANT BACKER ROD IN THE MOVEMENT GAP ALLOWING FOR 1" +/- OF BINDER ABOVE THE ROD.
6. HEAT AND PLACE THE BINDER MATERIAL AS RECOMMENDED BY THE MANUFACTURER.
7. PLACE 1/4" THICK BY 8" WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE THE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRE-STAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER. THE STEEL PLATES MAY BE OMITTED WHERE THE ENGINEER DETERMINES THAT THE APPROACH SLAB OR BRIDGE DECK WILL PROVIDE INADEQUATE SUPPORT AND WHERE VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.
8. HEAT AND MIX THE BINDER MATERIAL AND AGGREGATE AS RECOMMENDED BY THE MANUFACTURER.
9. INSTALLATION OF MATERIAL, COMPACTION, AND TOP COATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
10. IMMEDIATELY AFTER TOP COATING, CAST AN ANTI-SKID MATERIAL OVER THE JOINT TO REDUCE THE RISK OF TRACKING.
11. ONCE THE JOINT REACHES 82 DEG C (180 DEG F) +/-, WATER MAY BE USED TO EXPEDITE THE COOLING PROCESS.
12. PROTECT JOINT FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 51 DEG C (125 DEG F) +/-.

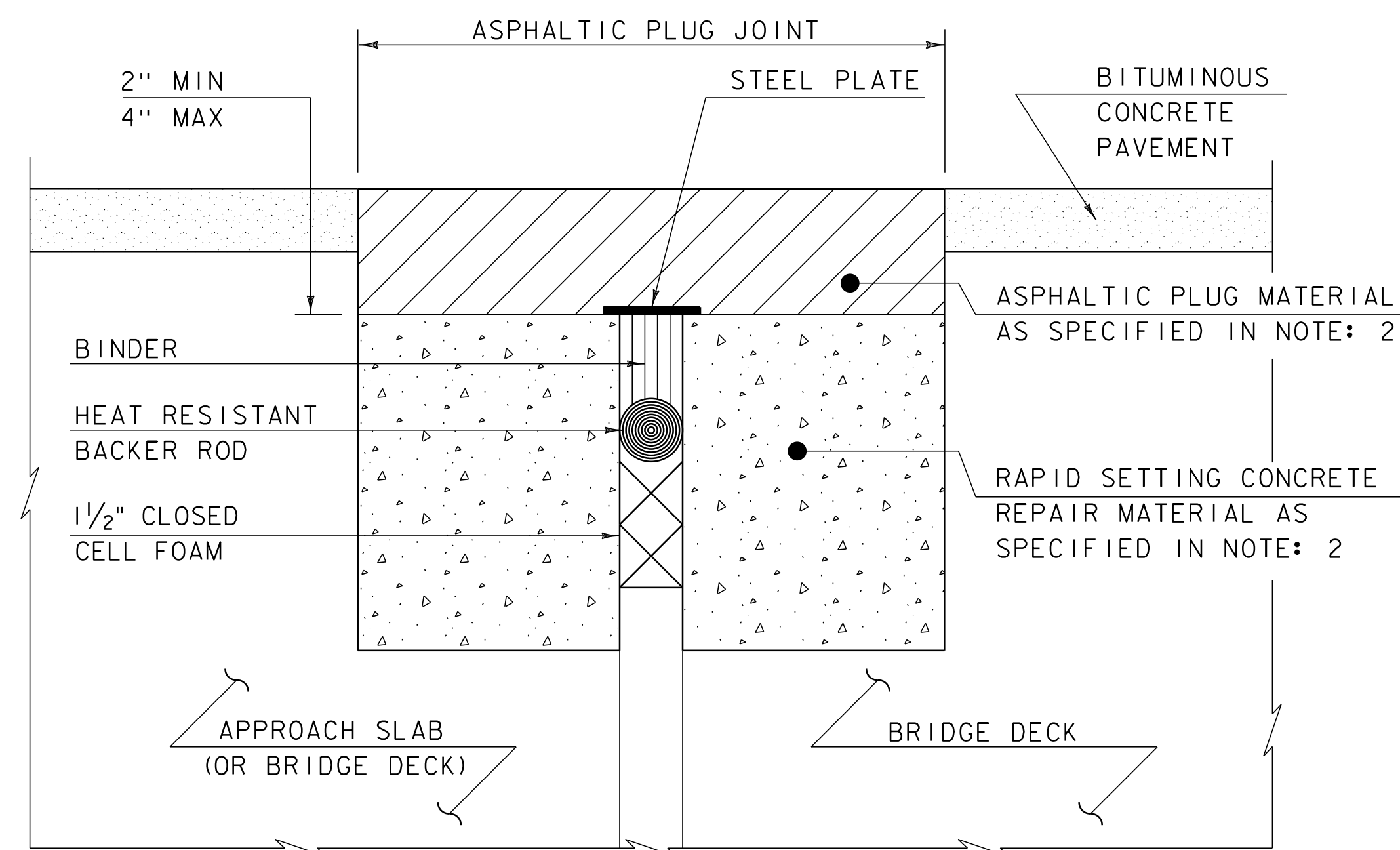
WEATHER LIMITATIONS

APPLY BINDER MATERIAL ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL OR AS RECOMMENDED BY THE MANUFACTURER:

1. THE AMBIENT AIR TEMPERATURE IS AT LEAST 10 DEG C (50 DEG F) AND RISING.
2. THE ROAD SURFACE IS DRY.
3. WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF SATISFACTORY WORK.

NOT TO SCALE

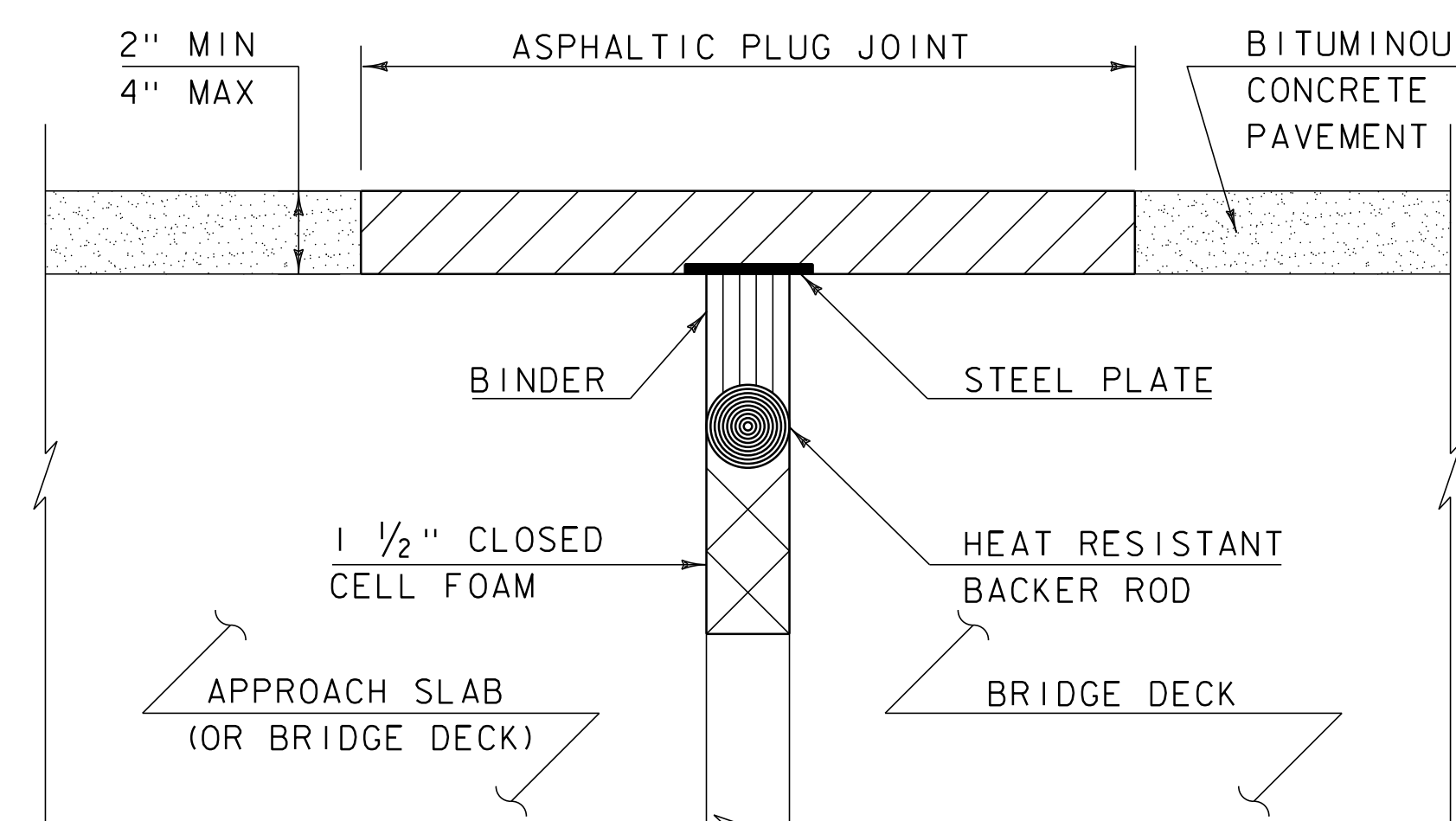
ASPHALTIC PLUG JOINT DETAIL SHEET	PROJECT NAME: DORSET - DANBY
	PROJECT NUMBER: NH SURF (24)
	FILE NAME: I0b118\p10b118.dgn PROJECT LEADER: FOWLER DESIGNED BY: HUNT IPARM FILE NAME: I0b118_06.i
	PLOT DATE: 23-MAY-2012 16:06 DRAWN BY: HUNT CHECKED BY: PAVT MGMT SHEET 6 OF 16



ASPHALTIC PLUG-TYPE JOINT DETAIL - REHAB

NOTES: (NOT TO SCALE)

1. THE CONTRACTOR SHALL REMOVE ALL ASPHALTIC PLUG JOINT MATERIAL AND DETERIORATED CONCRETE AS DIRECTED BY THE ENGINEER. REMOVAL OF THE FIRST 4 INCHES OF MATERIAL SHALL BE INCLUDED IN THE BID PRICE FOR ITEM 516.10 BRIDGE EXPANSION JOINT, ASPHALTIC PLUG. ANY REMOVAL OF MATERIAL GREATER THAN 4 INCHES SHALL BE INCLUDED IN THE BID PRICE OF ITEM 580.20 RAPID SETTING CONCRETE REPAIR MATERIAL WITH COURSE AGGREGATE.
2. THE CONTRACTOR SHALL REPLACE REMOVED MATERIAL THAT IS LESS THAN 4" FROM FINISHED GRADE WITH ASPHALTIC PLUG JOINT MATERIAL MEETING THE REQUIREMENTS OF SUBSECTION 707.15. ALL REMOVED MATERIAL THAT IS GREATER THAN 4 INCHES FROM FINISHED GRADE SHALL BE REPLACED WITH RAPID SETTING CONCRETE REPAIR MATERIAL WITH COARSE AGGREGATE MEETING THE REQUIREMENTS OF SUBSECTION 780.04.
3. REINFORCING STEEL NOT SHOWN FOR CLARITY.



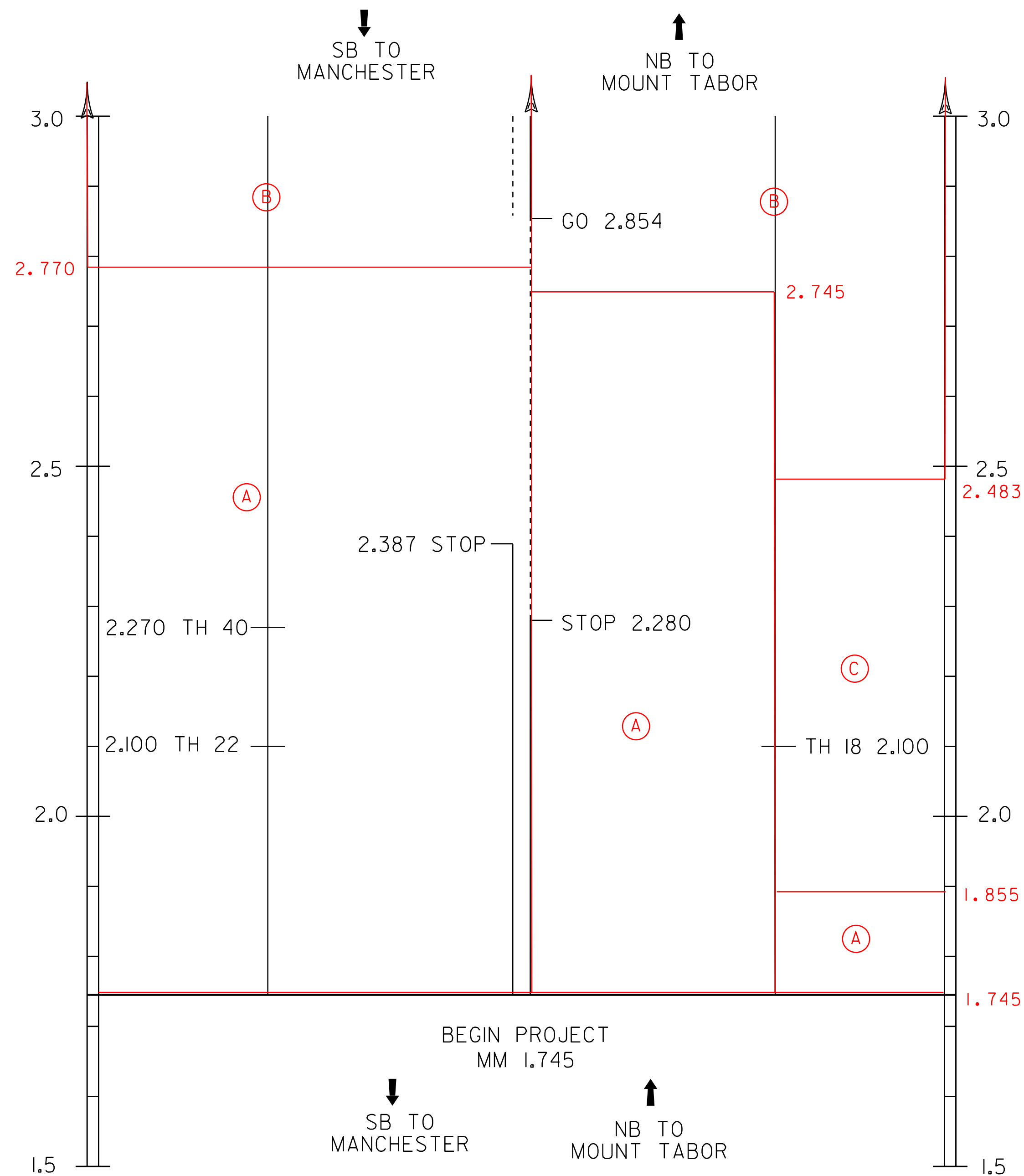
ASPHALTIC PLUG-TYPE JOINT DETAIL - NEW (NOT TO SCALE)

LOCATIONS

BRIDGE 52 MM 3.501 (DORSET) JOINT 1 38 LF
JOINT 2 38 LF

~~NOT TOUCHED BRIDGE 54 MM 4.992 (DORSET) JOINT 1 38 LF~~
~~JOINT 2 38 LF~~

BRIDGE 56C MM 3.001 (MT. TABOR) JOINT 1 38 LF
JOINT 2 38 LF

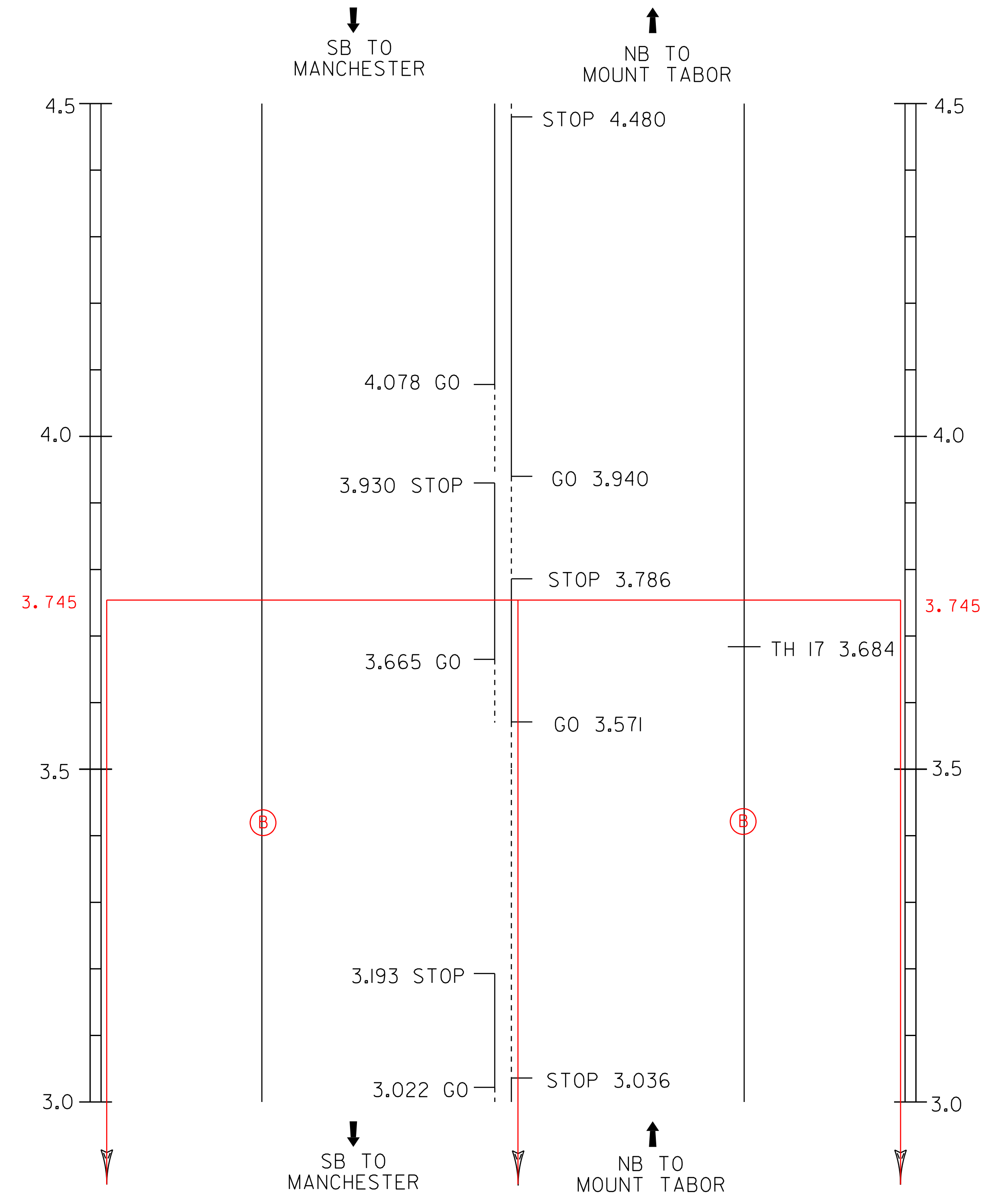


646.20 4 INCH WHITE LINE
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 MM 1.745 - MM 4.500 LT & RT
 (WITH BREAKS OR RADII AT TOWN HIGHWAYS)

646.21 4 INCH YELLOW LINE
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 MM 1.745 - MM 2.280 SOLID LT & RT
 MM 2.280 - MM 2.387 SOLID LT, DASHED RT
 MM 2.387 - MM 2.854 DASHED RT
 MM 2.854 - MM 3.022 DASHED LT, SOLID RT
 MM 3.022 - MM 3.036 SOLID LT & RT
 MM 3.036 - MM 3.193 SOLID LT, DASHED RT
 MM 3.193 - MM 3.571 DASHED RT
 MM 3.571 - MM 3.665 DASHED LT, SOLID RT
 MM 3.665 - MM 3.786 SOLID LT & RT
 MM 3.786 - MM 3.930 SOLID LT, DASHED RT
 MM 3.930 - MM 3.940 DASHED RT
 MM 3.940 - MM 4.078 DASHED LT, SOLID RT
 MM 4.078 - MM 4.480 SOLID LT & RT
 MM 4.480 - MM 4.500 SOLID LT, DASHED RT
 (WITH CENTERLINE BREAKS AT TOWN HIGHWAYS)

TEST SECTION

- (A) 9.5 MM SBS POLYMER-MODIFIED BIT CONC. PAVEMENT
- (B) 9.5 MM SBS POLYMER-MODIFIED BIT CONC. PAVEMENT 25% RAP
- (C) BIT CONC. PAVEMENT, SMALL QTY

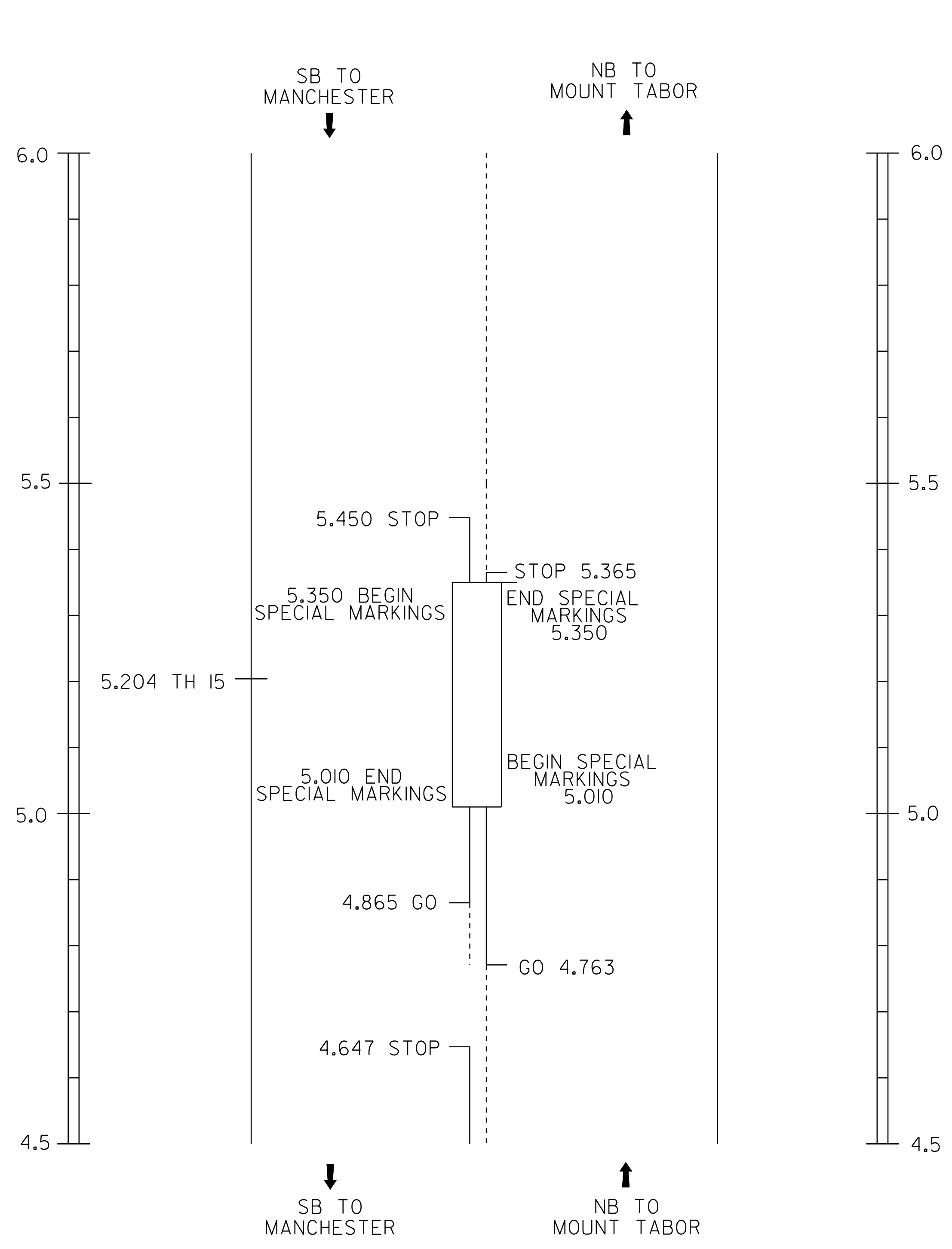


NOT TO SCALE

NOTES:
 1. THIS SHEET TO BE USED FOR THE LAYOUT OF ALL CENTERLINE PAVEMENT MARKINGS.
 THE RESIDENT ENGINEER MAY CONTACT KEITH SWEET, PAVEMENT MARKING SUPERVISOR
 AT (802) 828-5573 FOR ASSISTANCE LAYING OUT THE CENTERLINE DURING CONSTRUCTION.

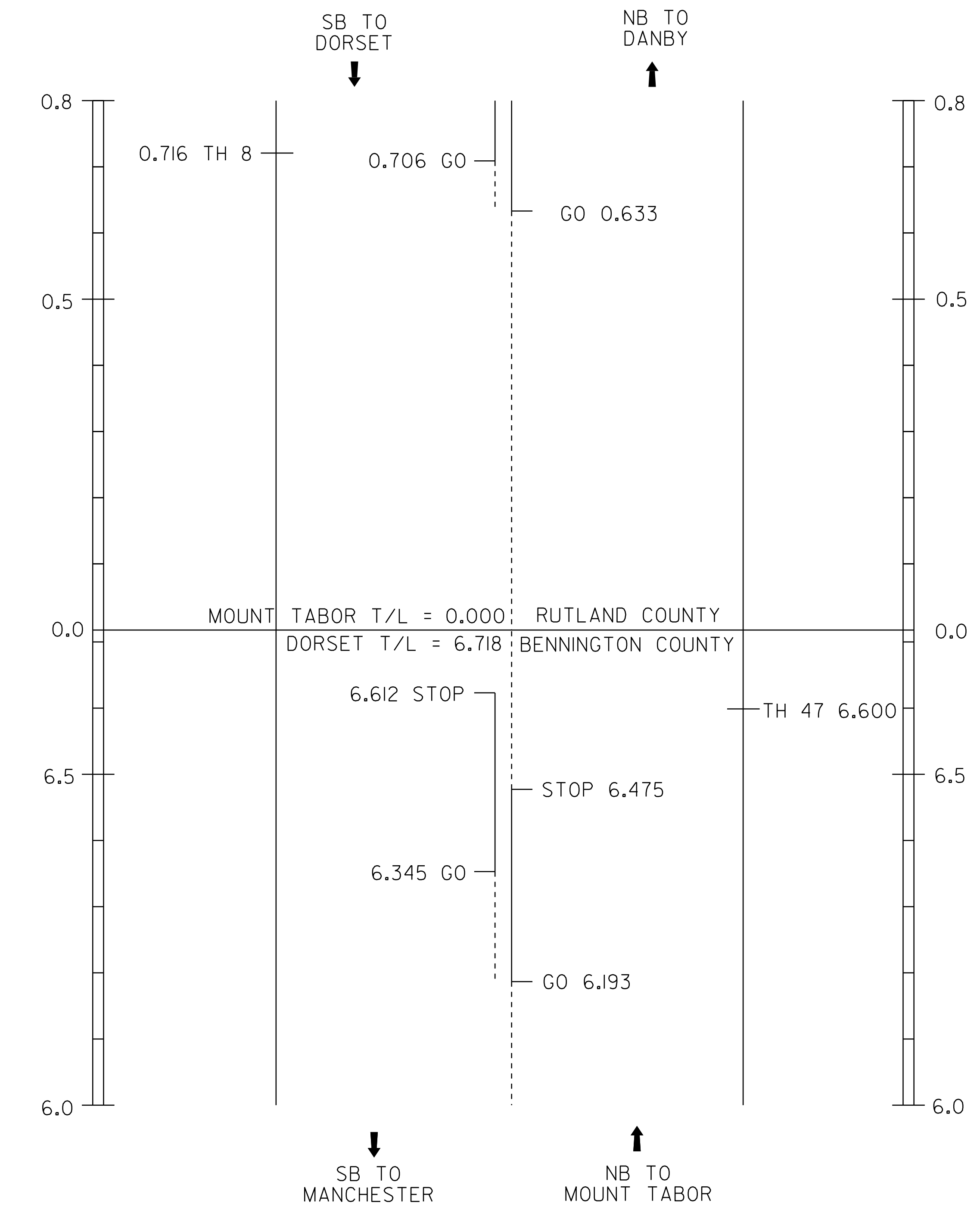
2. TWO (2) APPLICATIONS OF FINAL PAVEMENT MARKINGS WILL BE REQUIRED. SEE SPECIAL PROVISIONS
 FOR COMPLETION DATE REQUIREMENTS.

PROJECT LAYOUT SHEET #1	PROJECT NAME: DORSET - DANBY
	PROJECT NUMBER: NH SURF (24)
	FILE NAME: I0b118\p10b118.dgn
	PLOT DATE: 23-MAY-2012 16:06
PROJECT LEADER: FOWLER	DRAWN BY: HUNT
DESIGNED BY: HUNT	CHECKED BY: PAVT MGMT
IPARM FILE NAME: I0b118_09.i	SHEET 9 OF 16



646.20 4 INCH WHITE LINE
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 MM 4.500 - MM 5.086 LT & RT
 MM 5.288 - MM 6.718 LT & RT
 MM 0.000 - MM 0.800 LT & RT
 (WITH BREAKS OR RADII AT TOWN HIGHWAYS)

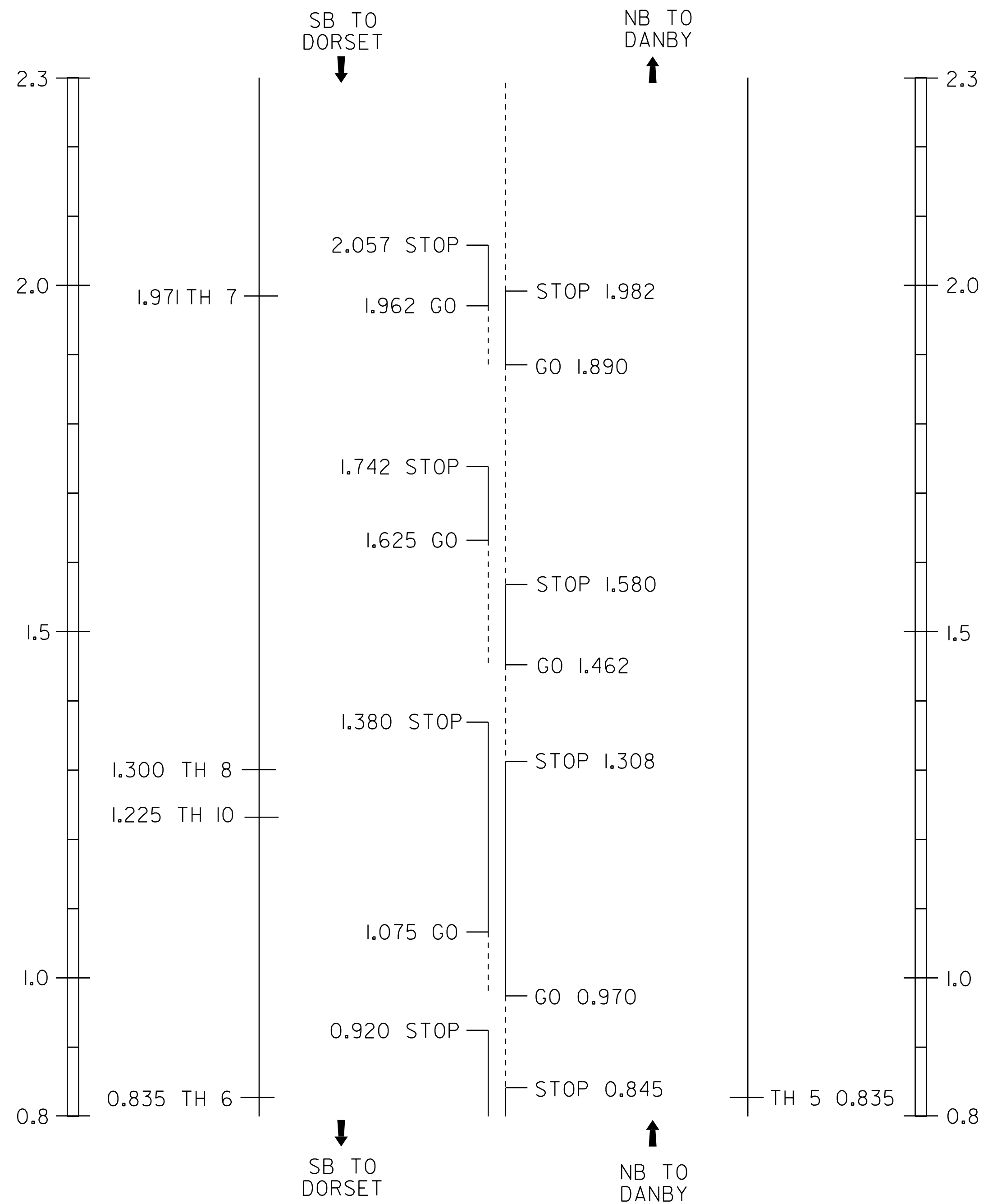
646.21 4 INCH YELLOW LINE
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 MM 4.500 - MM 4.647 SOLID LT, DASHED RT
 MM 4.647 - MM 4.763 DASHED RT
 MM 4.763 - MM 4.865 DASHED LT, SOLID RT
 MM 4.865 - MM 5.086 SOLID LT & RT
 MM 5.288 - MM 5.365 SOLID LT & RT
 MM 5.365 - MM 5.450 SOLID LT, DASHED RT
 MM 5.450 - MM 6.193 DASHED RT
 MM 6.193 - MM 6.345 DASHED LT, SOLID RT
 MM 6.345 - MM 6.475 SOLID LT & RT
 MM 6.475 - MM 6.612 SOLID LT, DASHED RT
 MM 6.612 - MM 6.718 DASHED RT
 MM 0.000 - MM 0.633 DASHED RT
 MM 0.633 - MM 0.706 DASHED LT, SOLID RT
 MM 0.706 - MM 0.800 SOLID LT & RT
 (WITH CENTERLINE BREAKS AT TOWN HIGHWAYS)



NOTES:
 1. THIS SHEET TO BE USED FOR THE LAYOUT OF ALL CENTERLINE PAVEMENT MARKINGS. THE RESIDENT ENGINEER MAY CONTACT KEITH SWEET, PAVEMENT MARKING SUPERVISOR AT (802) 828-5573 FOR ASSISTANCE LAYING OUT THE CENTERLINE DURING CONSTRUCTION.
 2. TWO (2) APPLICATIONS OF FINAL PAVEMENT MARKINGS WILL BE REQUIRED. SEE SPECIAL PROVISIONS FOR COMPLETION DATE REQUIREMENTS.

NOT TO SCALE

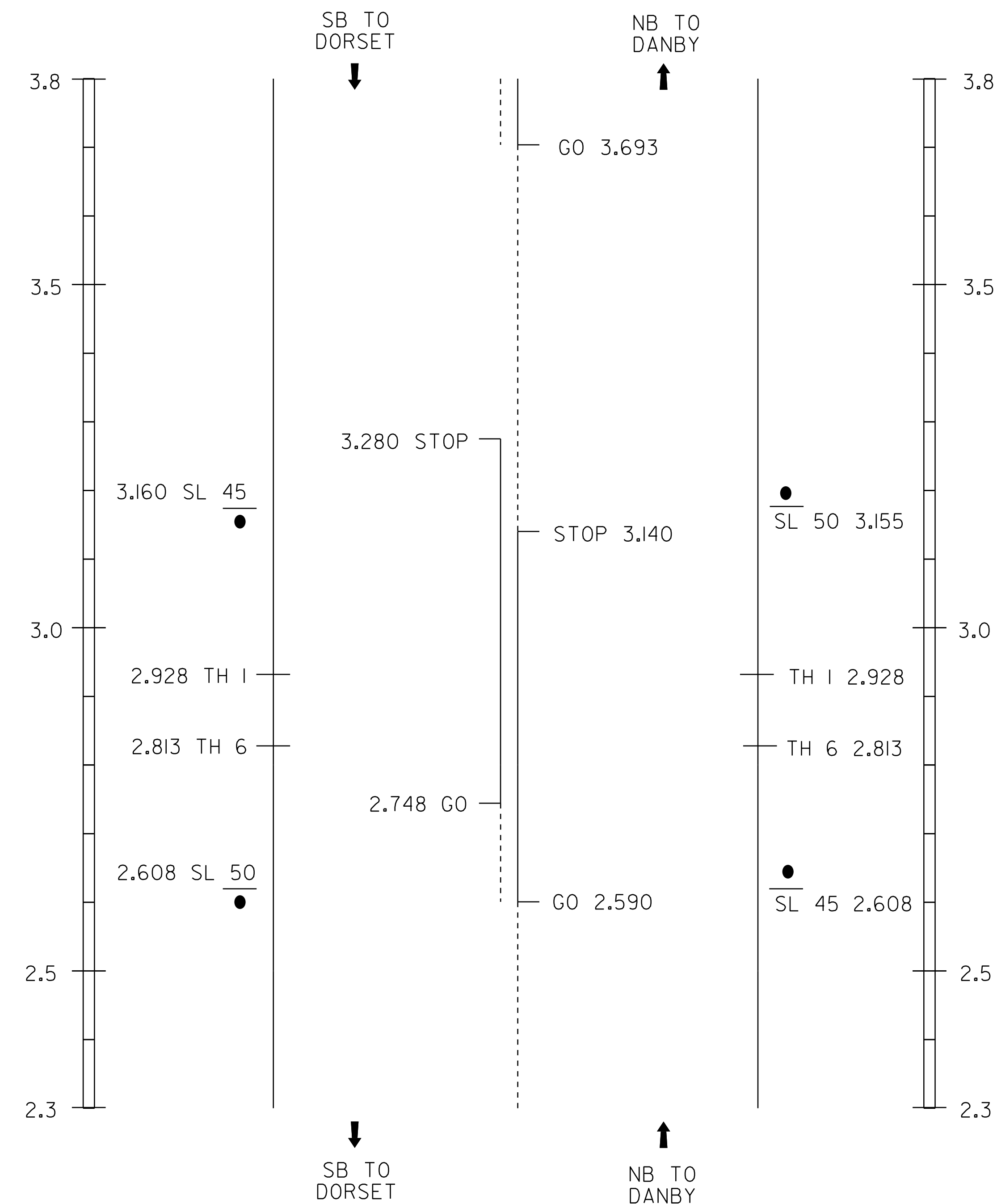
PROJECT LAYOUT SHEET #2	PROJECT NAME:	DORSET - DANBY	
	PROJECT NUMBER:	NH SURF (24)	
	FILE NAME:	10b118\p10b118.dgn	
	PLOT DATE:	23-MAY-2012	
DESIGNED BY:	HUNT	DRAWN BY:	HUNT
CHECKED BY:	PAVT MGMT	SHEET	10 OF 16
IPARM FILE NAME:	10b118_10.i		



646.20 4 INCH WHITE LINE
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 MM 0.800 - MM 3.800 LT & RT
 (WITH BREAKS OR RADII AT TOWN HIGHWAYS)

646.21 4 INCH YELLOW LINE
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT

MM 0.800 - MM 0.845 SOLID LT & RT
 MM 0.845 - MM 0.920 SOLID LT, DASHED RT
 MM 0.920 - MM 0.970 DASHED RT
 MM 0.970 - MM 1.075 DASHED LT, SOLID RT
 MM 1.075 - MM 1.308 SOLID LT & RT
 MM 1.308 - MM 1.380 SOLID LT, DASHED RT
 MM 1.380 - MM 1.462 DASHED RT
 MM 1.462 - MM 1.580 DASHED LT, SOLID RT
 MM 1.580 - MM 1.625 DASHED CL
 MM 1.625 - MM 1.742 SOLID LT, DASHED RT
 MM 1.742 - MM 1.890 DASHED RT
 MM 1.890 - MM 1.962 DASHED LT, SOLID RT
 MM 1.962 - MM 1.982 SOLID LT & RT
 MM 1.982 - MM 2.057 SOLID LT, DASHED RT
 MM 2.057 - MM 2.590 DASHED RT
 MM 2.590 - MM 2.748 DASHED LT, SOLID RT
 MM 2.748 - MM 3.140 SOLID LT & RT
 MM 3.140 - MM 3.280 SOLID LT, DASHED RT
 MM 3.280 - MM 3.693 DASHED RT
 MM 3.693 - MM 3.800 DASHED LT, SOLID RT
 (WITH CENTERLINE BREAKS AT TOWN HIGHWAYS)



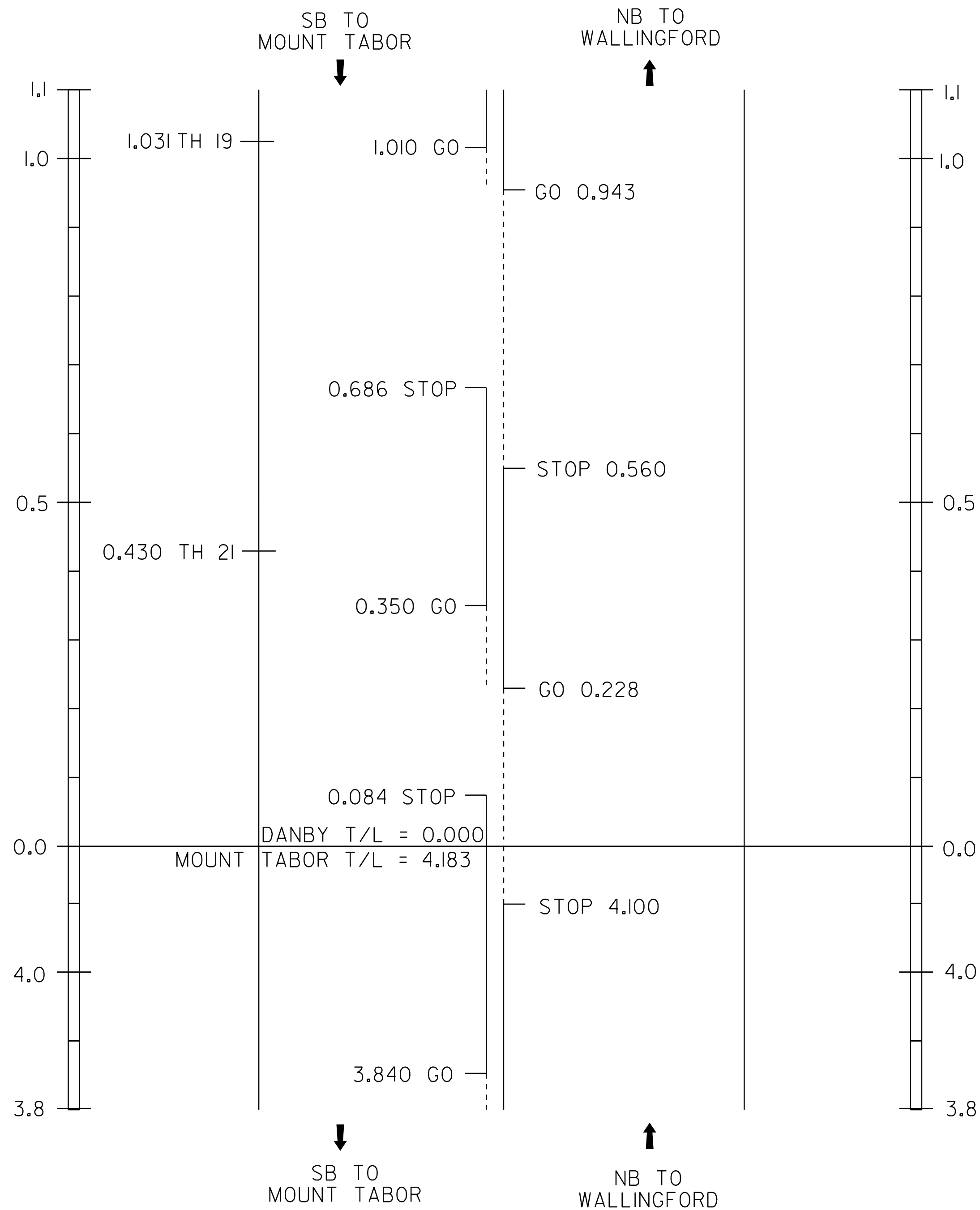
NOTES:
 1. THIS SHEET TO BE USED FOR THE LAYOUT OF ALL CENTERLINE PAVEMENT MARKINGS. THE RESIDENT ENGINEER MAY CONTACT KEITH SWEET, PAVEMENT MARKING SUPERVISOR AT (802) 828-5573 FOR ASSISTANCE LAYING OUT THE CENTERLINE DURING CONSTRUCTION.
 2. TWO (2) APPLICATIONS OF FINAL PAVEMENT MARKINGS WILL BE REQUIRED. SEE SPECIAL PROVISIONS FOR COMPLETION DATE REQUIREMENTS.

NOT TO SCALE

PROJECT LAYOUT SHEET #3

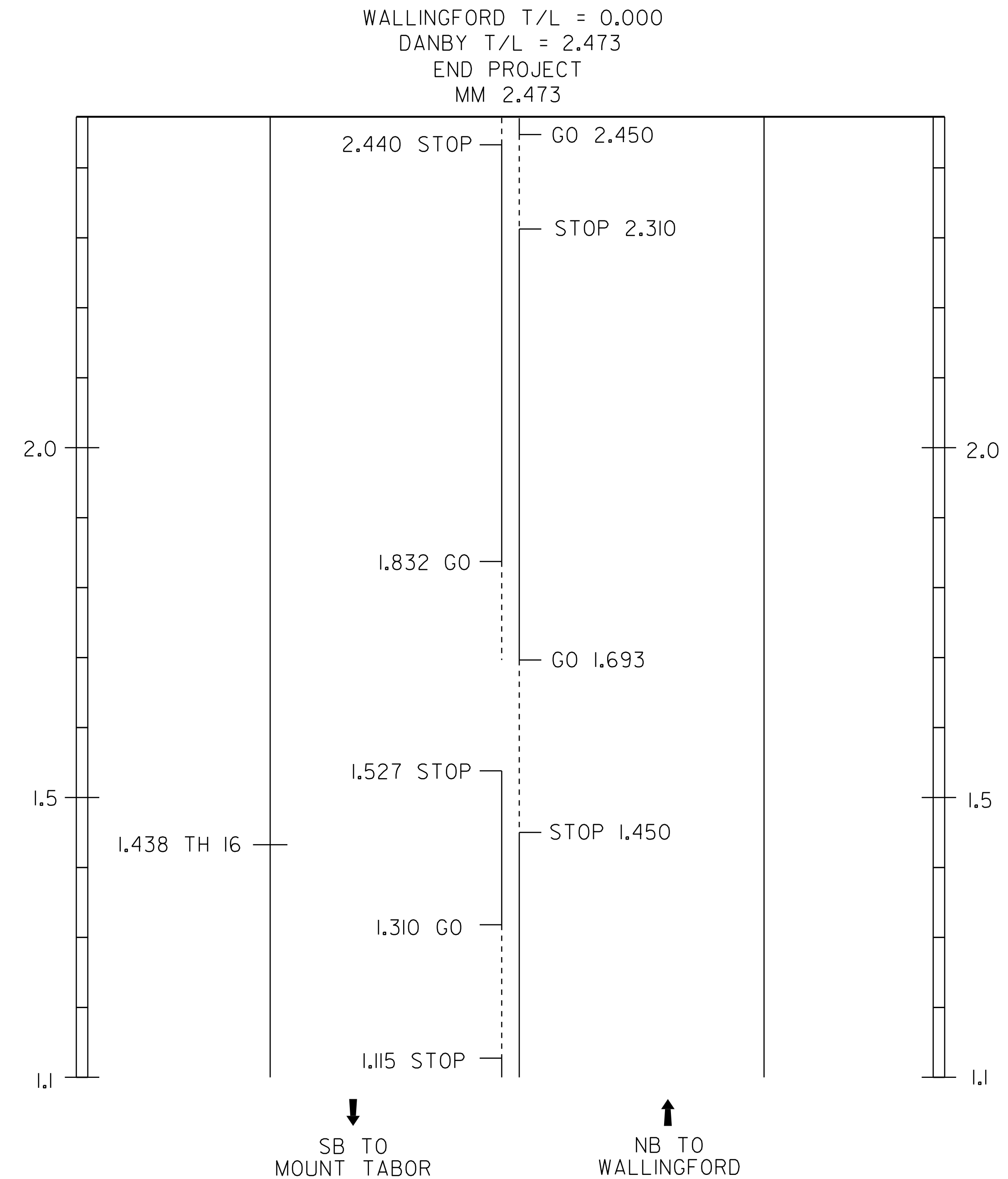
PROJECT NAME: DORSET - DANBY
 PROJECT NUMBER: NH SURF (24)

FILE NAME: I0b118\p10b118.dgn
 PROJECT LEADER: HUNT
 DESIGNED BY: HUNT
 IPARM FILE NAME: I0b118_11.i
 PLOT DATE: 23-MAY-2012
 DRAWN BY: HUNT
 CHECKED BY: PAVT MGMT
 SHEET 11 OF 16



646.20 4 INCH WHITE LINE
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 MM 3.800 - MM 4.183 LT & RT
 MM 0.000 - MM 2.473 LT & RT
 (WITH BREAKS OR RADII AT TOWN HIGHWAYS)

646.21 4 INCH YELLOW LINE
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 MM 3.800 - MM 3.840 DASHED LT, SOLID RT
 MM 3.840 - MM 4.100 SOLID LT & RT
 MM 4.100 - MM 4.183 SOLID LT, DASHED RT
 MM 0.084 - MM 0.228 DASHED RT
 MM 0.228 - MM 0.350 DASHED LT, SOLID RT
 MM 0.350 - MM 0.560 SOLID LT & RT
 MM 0.560 - MM 0.686 SOLID LT, DASHED RT
 MM 0.686 - MM 0.943 DASHED RT
 MM 0.943 - MM 1.010 DASHED LT, SOLID RT
 MM 1.010 - MM 1.115 SOLID LT & RT
 MM 1.115 - MM 1.310 DASHED LT, SOLID RT
 MM 1.310 - MM 1.450 SOLID LT & RT
 MM 1.450 - MM 1.527 SOLID LT, DASHED RT
 MM 1.527 - MM 1.693 DASHED RT
 MM 1.693 - MM 1.832 DASHED LT, SOLID RT
 MM 1.832 - MM 2.310 SOLID LT & RT
 MM 2.310 - MM 2.440 SOLID LT, DASHED RT
 MM 2.440 - MM 2.450 DASHED CL
 MM 2.450 - MM 2.473 DASHED LT, SOLID RT
 (WITH CENTERLINE BREAKS AT TOWN HIGHWAYS)



WALLINGFORD T/L = 0.000
 DANBY T/L = 2.473
 END PROJECT
 MM 2.473

NOTES:
 1. THIS SHEET TO BE USED FOR THE LAYOUT OF ALL CENTERLINE PAVEMENT MARKINGS. THE RESIDENT ENGINEER MAY CONTACT KEITH SWEET, PAVEMENT MARKING SUPERVISOR AT (802) 828-5573 FOR ASSISTANCE LAYING OUT THE CENTERLINE DURING CONSTRUCTION.
 2. TWO (2) APPLICATIONS OF FINAL PAVEMENT MARKINGS WILL BE REQUIRED. SEE SPECIAL PROVISIONS FOR COMPLETION DATE REQUIREMENTS.

NOT TO SCALE

PROJECT LAYOUT SHEET #4

PROJECT NAME: DORSET - DANBY
 PROJECT NUMBER: NH SURF (24)

FILE NAME: I0b118\pl0b118.dgn
 PROJECT LEADER: FOWLER
 DESIGNED BY: HUNT
 IPARM FILE NAME: I0b118_I2.1

PLOT DATE: 23-MAY-2012
 DRAWN BY: HUNT
 CHECKED BY: PAVT MGMT
 SHEET 12 OF 16

646.20 4 INCH WHITE LINE
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT

MM 5.086 - MM 5.288 LT & RT (SOLID)
 MM 5.124 - MM 5.162 RT (DOTTED)
 MM 5.162 - MM 5.200 RT (SOLID)
 MM 5.212 - MM 5.250 RT (SOLID)
 MM 5.250 - MM 5.288 RT (DOTTED)
 (WITH BREAKS OR RADII AT TOWN HIGHWAYS)

646.23 TEMPORARY 8 INCH YELLOW LINE

MM 5.086 - MM 5.162 RT (DIAGONALS)
 MM 5.212 - MM 5.288 LT (DIAGONALS)

646.21 4 INCH YELLOW LINE
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT

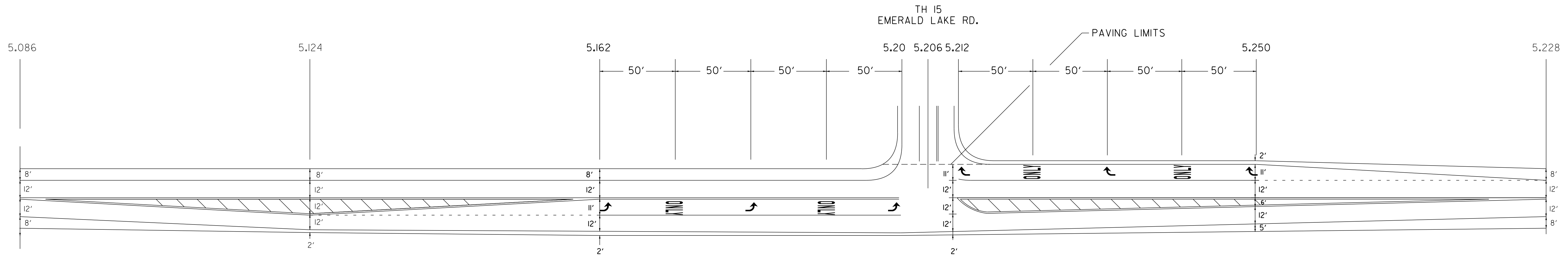
MM 5.086 - MM 5.288 SOLID LT & RT
 MM 5.086 - MM 5.162 (DBL SOLID) RT
 MM 5.212 - MM 5.288 RT (DBL SOLID) RT
 (WITH CENTERLINE BREAKS AT TOWN HIGHWAYS)

646.30 LETTER OR SYMBOL

MM 5.162 RT ↵
 MM 5.171 RT 0,N,L,Y
 MM 5.180 RT ↵
 MM 5.189 RT 0,N,L,Y
 MM 5.200 RT ↵
 MM 5.212 LT ↶
 MM 5.221 LT 0,N,L,Y
 MM 5.231 LT ↶
 MM 5.240 LT 0,N,L,Y
 MM 5.250 LT ↶

646.692 TEMPORARY LETTER OR SYMBOL, PAINT

MM 5.162 RT ↵
 MM 5.180 RT ↵
 MM 5.200 RT ↵
 MM 5.212 LT ↶
 MM 5.231 LT ↶
 MM 5.250 LT ↶



NOT TO SCALE

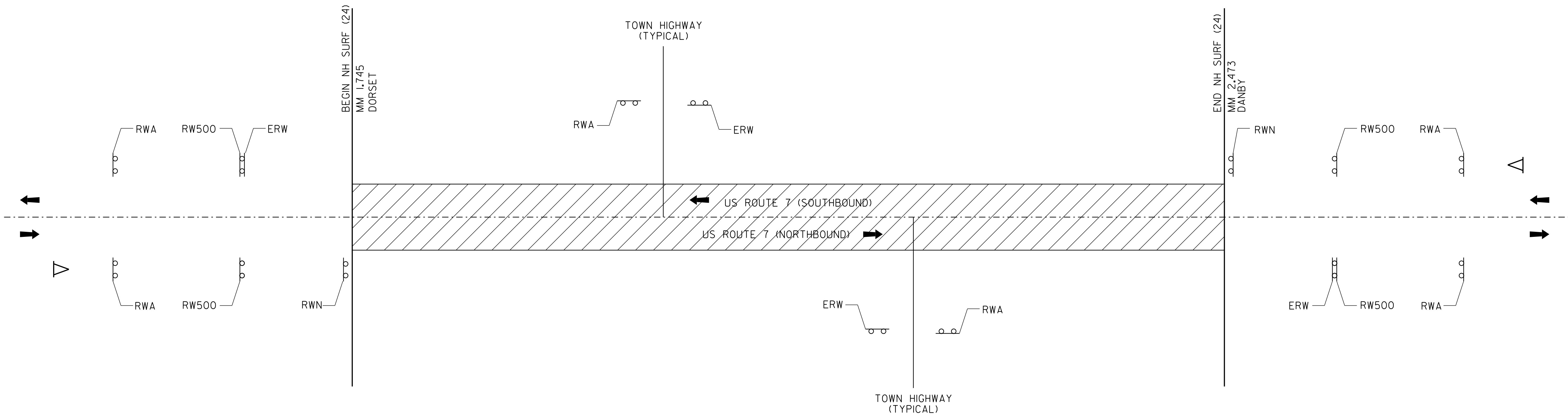
**TH - 15
 INTERSECTION
 DETAIL SHEET**

PROJECT NAME: DORSET - DANBY
 PROJECT NUMBER: NH SURF (24)

FILE NAME: I0b118\p10b118.dgn
 PROJECT LEADER: FOWLER
 DESIGNED BY: WILDER
 IPARM FILE NAME: I0b118_13.i

PLOT DATE: 23-MAY-2012
 DRAWN BY: WILDER
 CHECKED BY: PAVT MGMT
 SHEET 13 OF 16

CONSTRUCTION APPROACH SIGNING



NOTES:
 1. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE RESIDENT ENGINEER FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION. THE COST OF PREPARING THIS PLAN (AND MAKING CHANGES IF NECESSARY) SHALL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 641.10, TRAFFIC CONTROL.

2. THE CONTRACTOR SHALL INCLUDE A CONSTRUCTION SIGN APPROACH PACKAGE FOR EXPECTED LANE CLOSURES AND WORK ZONE SPEED REDUCTIONS IN COMPLIANCE WITH VTRANS STANDARDS E-103, E-106 AND THE LATEST EDITION OF THE MUTCD. PAYMENT FOR PROVIDING THIS PACKAGE SHALL BE INCIDENTAL TO ITEM 641.10, TRAFFIC CONTROL.

3. THE BID PRICE FOR TRAFFIC CONTROL, ITEM 641.10, SHALL INCLUDE ALL APPROACH AND ON-PROJECT CONSTRUCTION SIGNING, PORTABLE ARROW BOARDS, BARRIERS, BARRELS, CONES, BARRICADES, TEMPORARY REGULATORY AND WARNING SIGNS, AND POSTS AS DETAILED IN VTRANS STANDARDS. ALL ADJUSTING, RELOCATING, AND REMOVING OF THESE DEVICES AS DIRECTED BY THE RESIDENT ENGINEER SHALL ALSO BE INCLUDED. THE FOLLOWING ITEMS WILL BE PAID FOR SEPARATELY: 630.10 - UNIFORMED TRAFFIC OFFICERS, AND 630.15 - FLAGGERS, 646.602 - TEMPORARY 4 INCH WHITE LINE, PAINT, 646.612 - TEMPORARY 4 INCH YELLOW LINE, PAINT, 646.652 - TEMPORARY 8 INCH YELLOW LINE, PAINT, 646.692 - TEMPORARY LETTER OR SYMBOL, AND 646.76 - LINE STRIPING TARGETS.

4. PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) SHALL BE PROVIDED FOR USE ALONG THIS PROJECT. THE PLACEMENT OF THESE UNITS AS WELL AS THE MESSAGE WILL BE APPROVED BY THE RESIDENT ENGINEER. THESE SIGNS WILL BE PAID FOR UNDER ITEM 641.15, PORTABLE CHANGEABLE MESSAGE SIGN.

FOR THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL POSITION A PCMS PRIOR TO THE BEGIN AND END PROJECT LIMITS WARNING MOTORISTS OF EXPECTED ROADWAY CONDITIONS AND REDUCED ROADWAY WIDTHS.

PCMS SHOULD NOT REPLACE ANY OF THE SIGNING DETAILED IN THE MUTCD AND SHOULD NOT BE USED IF STANDARD TRAFFIC CONTROL DEVICES ADEQUATELY PROVIDE THE INFORMATION THE MOTORISTS NEED TO TRAVEL SAFELY.

THE PCMS SHALL CONSIST OF EITHER ONE OR TWO PHASES. TYPICALLY, A PHASE SHALL CONSIST OF UP TO THREE LINES OF EIGHT CHARACTERS PER LINE. THE PCMS SHOULD BE USED AS A SUPPLEMENT AND NOT AS A SUBSTITUTE FOR CONVENTIONAL SIGNS AND PAVEMENT MARKINGS.

THE PCMS SHOULD COMMUNICATE WHAT INFORMATION MOTORISTS NEED TO KNOW. UNNECESSARY INFORMATION SHOULD BE AVOIDED. MESSAGES SHOULD BE UPDATED PERIODICALLY TO DESCRIBE THE WORK ACTIVITY OCCURRING SO THAT THE PCMS CONTINUES TO COMMAND THE ATTENTION OF MOTORISTS.




5. NO CONSTRUCTION SIGNS SHALL BE INSTALLED AS TO INTERFERE OR OBSTRUCT THE VIEW OF EXISTING TRAFFIC CONTROL DEVICES, STOPPING SIGHT DISTANCE, AND CORNER SIGHT DISTANCE FROM DRIVES AND TOWN HIGHWAYS.

6. REFER TO VT. STATE STANDARDS AND THE LATEST EDITION OF THE MUTCD FOR TEMPORARY TRAFFIC CONTROL SIGN COLORS.

7. EXISTING SPEED LIMIT SIGNS SHALL BE COVERED WHEN REDUCED SPEED SIGNS ARE POSTED. KEEP RECORDS WHEN POSTING THE WORK ZONE SPEED LIMIT FOR LEGAL PURPOSES; DOCUMENTING DATES, TIMES, AND LOCATIONS OF SIGNS. WHEN WORK ZONE SPEED LIMIT IS NOT IN USE ALL ASSOCIATED SIGNS SHALL BE COVERED, TURNED AND/OR LAID FLAT SO AS THE MOTORING PUBLIC CANNOT READ THESE SIGNS.

8. PORTABLE OR STATIONARY WORK ZONE SPEED LIMIT SIGNS SHOULD BE SPACED EVERY 1.5 TO 2 MILES WHERE APPLICABLE AS A REMINDER TO THE MOTORIST TRAVELING THROUGH THE WORK ZONE WHAT SPEED THEY SHOULD BE TRAVELLING.

9. WHEN REDUCED REGULATORY SPEED LIMIT SIGNS ARE USED, THE RESUMPTION OF THE USUAL SPEED LIMIT SHALL BE INDICATED BY AN APPROPRIATE SPEED LIMIT SIGN AT THE END OF THE WORK ZONE.

- LEGEND**
- RWA = ROAD WORK AHEAD
 - RW500 = ROAD WORK IN 500 FEET
 - RWN = ROAD WORK NEXT (1/2 MILES)
 - ERW = END ROAD WORK
 -  = PORTABLE CHANGEABLE MESSAGE SIGN
 -  = WORK AREA
 -  = DIRECTION OF TRAFFIC FLOW

NOT TO SCALE

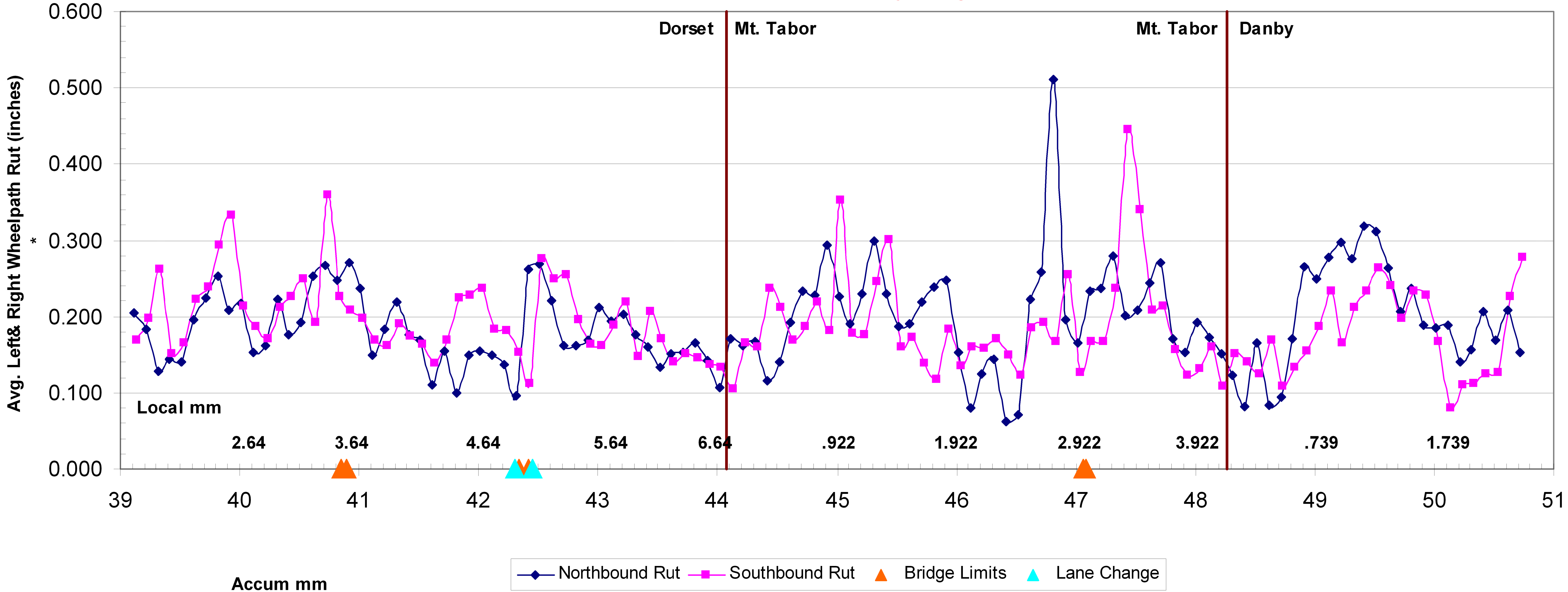
CONSTRUCTION APPROACH SIGNING SHEET	PROJECT NAME: DORSET - DANBY
	PROJECT NUMBER: NH SURF (24)
	FILE NAME: I0b118\p10b118.dgn PROJECT LEADER: FOWLER DESIGNED BY: HUNT IPARM FILE NAME: I0b118_14.1
	PLOT DATE: 23-MAY-2012 DRAWN BY: HUNT CHECKED BY: PAVT MGMT SHEET 14 OF 16

RUTTING DATA INFORMATION

US 7 Dorset-Mt. Tabor-Danby NH Surf(24) Preconstruction

Profiled 6/17/10

NB Lane Rut = .193" SB Lane Rut = .190" **Project Avg. Rut = .192"**



FOR INFORMATIONAL PURPOSES ONLY

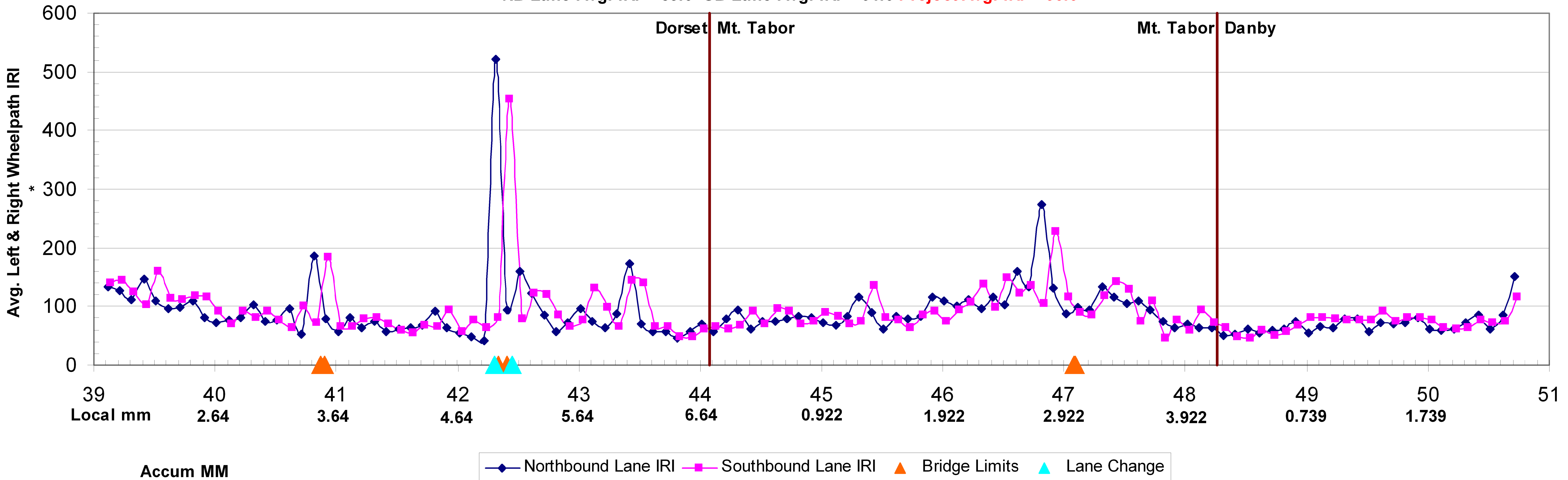
RUTTING DATA INFORMATION SHEET

PROJECT NAME:	DORSET - DANBY
PROJECT NUMBER:	NH SURF (24)
FILE NAME:	I0b118\p10b118.dgn
PROJECT LEADER:	FOWLER
DESIGNED BY:	HUNT
IPARM FILE NAME:	I0b118_15.i
PLOT DATE:	23-MAY-2012
DRAWN BY:	HUNT
CHECKED BY:	PAVT MGMT
SHEET	15 OF 16

US 7 Dorset-Mt. Tabor-Danby NH Surf(22) Preconstruction

Profiled 6/17/10

NB Lane Avg. IRI = 89.6 SB Lane Avg. IRI = 91.6 **Project Avg. IRI = 90.6**



FOR INFORMATIONAL PURPOSES ONLY

**ROUGHNESS
DATA
INFORMATION
SHEET**

PROJECT NAME:	DORSET - DANBY		
PROJECT NUMBER:	NH SURF (24)		
FILE NAME:	I0b118\p10b118.dgn	PLOT DATE:	23-MAY-2012
PROJECT LEADER:	FOWLER	DRAWN BY:	HUNT
DESIGNED BY:	HUNT	CHECKED BY:	PAVT MGMT
IPARM FILE NAME:	I0b118_16.1	SHEET	16 OF 16