

ASPHALTIC PLUG-TYPE
JOINT DETAIL
(NOT TO SCALE)

ASPHALTIC PLUG JOINT NOTES

1. INSTALLATION

- A. LOCATE THE JOINT CENTRALLY OVER THE DECK OVERLAY EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.
- B. REMOVE THE BITUMINOUS CONCRETE PAVEMENT FULL DEPTH AS SHOWN ON THE PLANS. THE PAVEMENT SHALL BE DRY AND SAW CUT TO THE LIMITS REQUIRED TO PLACE THE JOINT. A PNEUMATIC HAMMER AND CHISEL MAY BE USED ADJACENT TO THE CURB ONLY WHEN SAW CUTTING IS NOT POSSIBLE.
- C. BLAST CLEAN THE JOINT AREA OF DEBRIS AND ASPHALT. THOROUGHLY DRY THE JOINT AREA WITH COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.
- D. REPAIR SPALLED AND DEFECTIVE CONCRETE WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.
- E. PLACE PROPERLY SIZED HEAT RESISTANT BACKER ROD IN THE MOVEMENT GAP ALLOWING FOR 1" +/- OF BINDER ABOVE THE ROD.
- F. HEAT AND PLACE THE BINDER MATERIAL AS RECOMMENDED BY THE MANUFACTURER.
- G. PLACE 1/4" THICK BY 8" WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE THE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRESTAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER. THE STEEL PLATES MAY BE OMITTED WHERE THE ENGINEER DETERMINES THAT THE APPROACH SLAB OR BRIDGE DECK WILL PROVIDE INADEQUATE SUPPORT AND WHERE VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.
- H. HEAT AND MIX THE BINDER MATERIAL AND AGGREGATE AS RECOMMENDED BY THE MANUFACTURER.

I. INSTALLATION OF MATERIAL, COMPACTION, AND TOP COATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.

J. IMMEDIATELY AFTER TOP COATING, CAST AN ANTI-SKID MATERIAL OVER THE JOINT TO REDUCE THE RISK OF TRACKING.

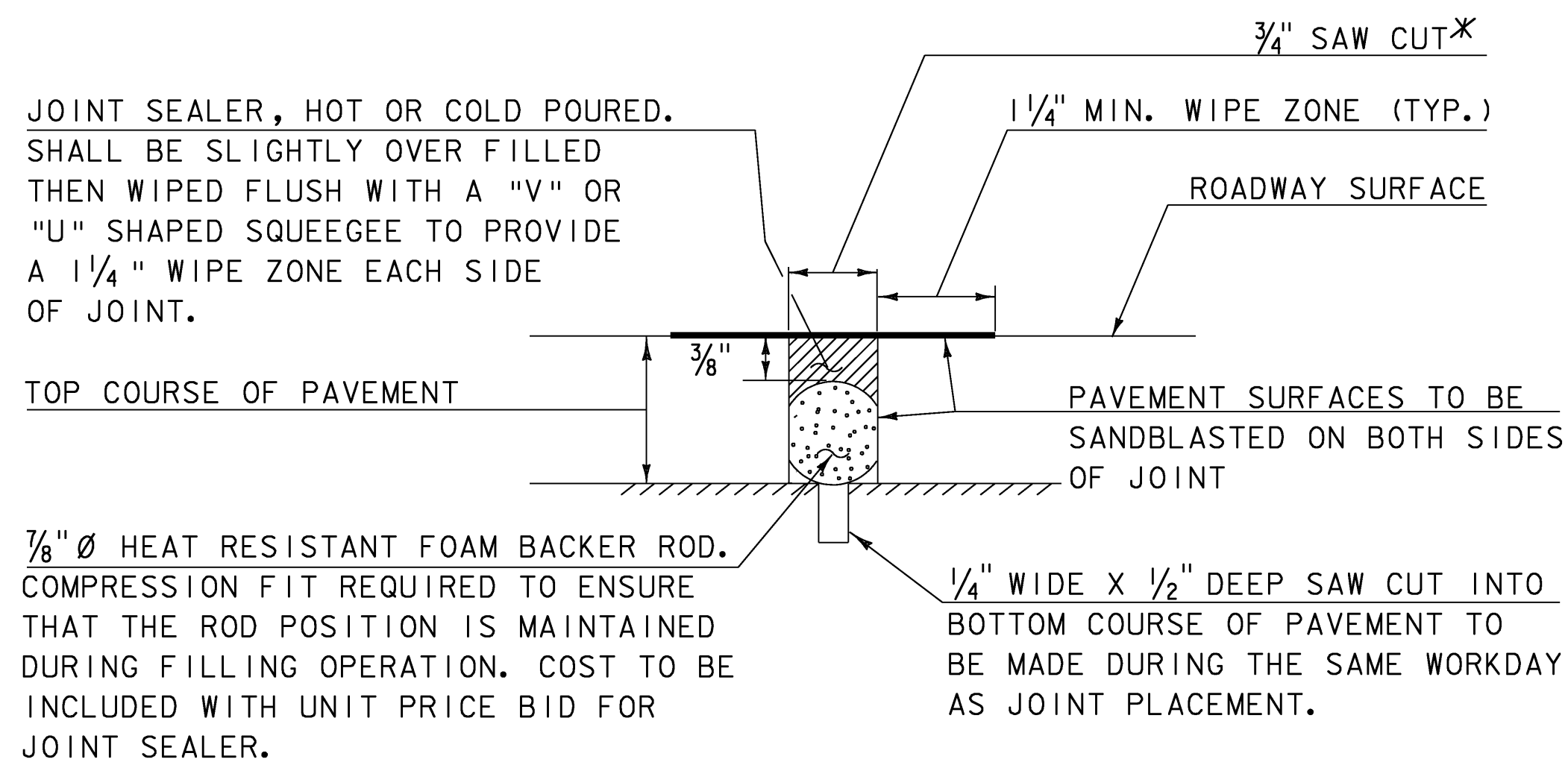
K. PROTECT JOINT FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 51 DEG C (125 DEG F) +/-.

2. WEATHER LIMITATIONS. (APPLY BINDER MATERIAL ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL OR AS RECOMMENDED BY THE MANUFACTURER):

A. THE AMBIENT AIR TEMPERATURE IS AT LEAST 10 DEG C (50 DEG F) AND RISING.

B. THE ROAD SURFACE IS DRY.

C. WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF SATISFACTORY WORK.

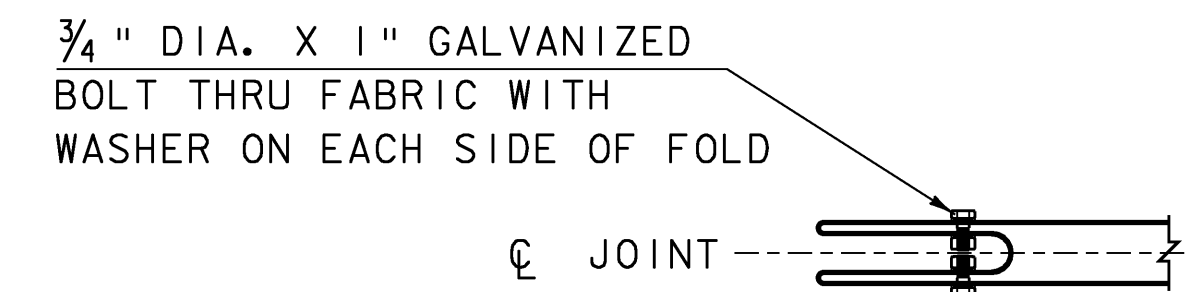


SAWED PAVEMENT JOINT DETAIL
(NOT TO SCALE)

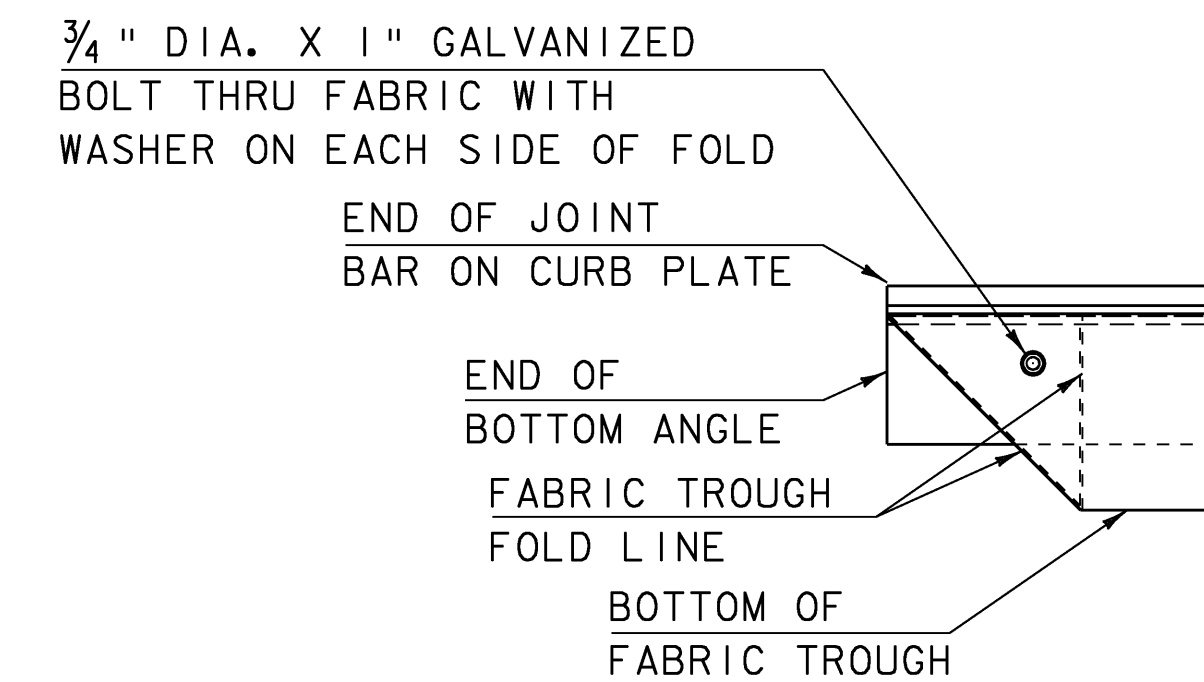
* JOINT IS TO BE LOCATED ACCURATELY BY STRING LINING, OR OTHER MEANS, PRIOR TO PAVING, SO THAT THE SAW CUTS WILL BE MADE DIRECTLY OVER THE END OF CONCRETE DECK. JOINT SHALL BE CUT DRY IN A SINGLE PASS AND BE SEALED WITHIN 24 HOURS OR PRIOR TO EXPOSURE TO TRAFFIC. JOINT SHALL BE CLEANED PRIOR TO APPLYING THE JOINT SEALER.

BRIDGE NUMBER	CROSSING OVER	LENGTH OF BRIDGE (FT)	CURB TO CURB PAVEMENT WIDTH (FT)	TOWN	MILE MARKER
VT 100B BR 7	MAD RIVER	307.25	30.00	MORETOWN	5.85

BRIDGE LOCATION TABLE



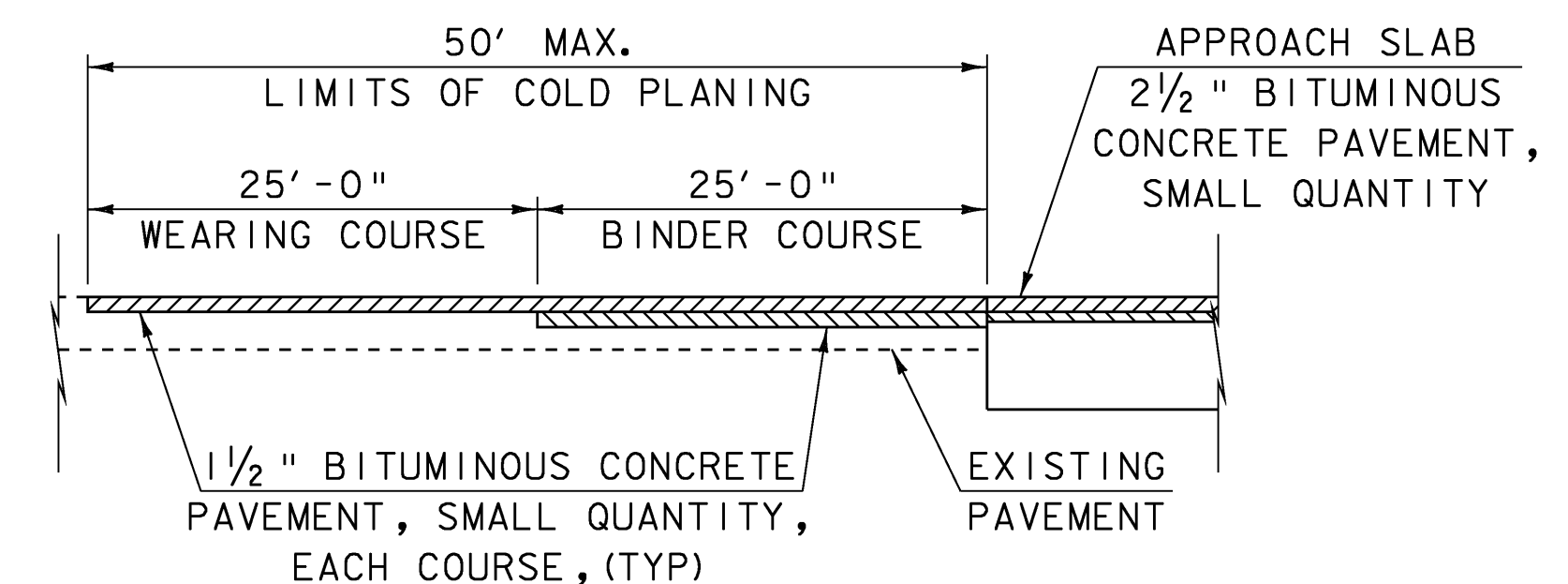
PLAN VIEW - TROUGH FOLD
(NOT TO SCALE)



FOLDED TROUGH END DETAIL
(NOT TO SCALE)

NOTES:

1. TROUGH SHALL BE FOLDED ON HIGH SIDE OF BRIDGE ONLY.
2. BOLTS, NUTS AND WASHERS FOR FOLD SHALL MEET REQUIREMENTS OF 714.05 AND BE GALVANIZED PER AASHTO M298, CLASS 50, TYPE 1.



COLD PLANE APPROACH DETAIL

NTS
ALONG MAIN LINE ONLY

PROJECT NAME: MORETOWN	PLOT DATE: 15-APR-2009
PROJECT NUMBER: STP DECK (37)	DRAWN BY: D.D.BEARD
FILE NAME: 08c2l8de+alls.dgn	CHECKED BY: H.J.SALLS
PROJECT LEADER: C.P.WILLIAMS	SHEET 10 OF 25
DESIGNED BY: H.J.SALLS	
MISC DETAIL SHEET	