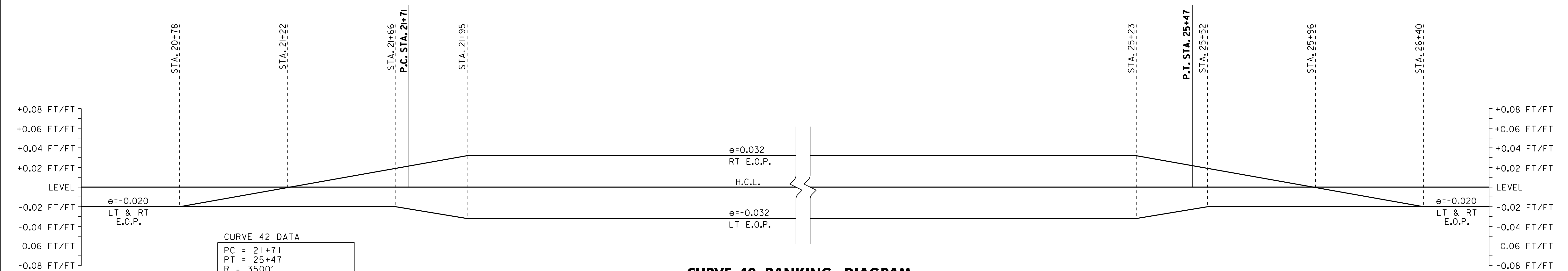


CURVE 41 DATA
 PC = 16+24
 PT = 19+00
 R = 2000'
 $\Delta = 7^\circ 54' 01''$, LT
 D = 2° 51' 53"
 e = 4.76
 L = 276'

CURVE 41 BANKING DIAGRAM
 CURVE 41 RADIUS = 2,000' LEFT



CURVE 42 DATA
 PC = 21+71
 PT = 25+47
 R = 3500'
 $\Delta = 6^\circ 08' 44''$, LT
 D = 1° 38' 13"
 e = 5.04
 L = 375'

CURVE 42 BANKING DIAGRAM
 CURVE 42 RADIUS = 3,500' LEFT

SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE MAXIMUM ROLL-OVER BETWEEN LANE AND SHOULDER CROSS SLOPES ON THE OUTSIDE (HIGH SIDE) OF A SUPERELEVATED CURVE SHALL BE SEVEN PERCENT. SHOULDER CROSS SLOPE ON THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL BE A MINIMUM OF SIX PERCENT AND MATCH THE ADJACENT LANE CROSS SLOPE WHEN THE LANE CROSS SLOPE EXCEEDS SIX PERCENT.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED. A MAXIMUM SUPERELEVATION RATE OF 0.08 IS USED IN AREAS WITH A POSTED SPEED ABOVE 30 MPH. IN THE 30 MPH ZONE A MAXIMUM SUPERELEVATION RATE OF 0.04 WAS USED. IN AREAS WITH AN INTERSECTING SIDE ROAD A MAXIMUM SUPERELEVATION RATE OF 0.06 WAS USED. SEE VAOT STANDARD B-1 FOR MORE INFORMATION.

NOT TO SCALE

SUPERELEVATION BANKING DIAGRAMS SHEET #10



PROJECT NAME: WESTFORD - FAIRFAX	PLOT DATE: 31-OCT-2012 13:59
PROJECT NUMBER: STP 2804(1)	DRAWN BY: STANTEC
FILE NAME: p08c212.dgn	CHECKED BY: JLL
DESIGNED BY: STANTEC	SHEET 61 OF 79
IPARM FILE: p08c212sbd10.i	