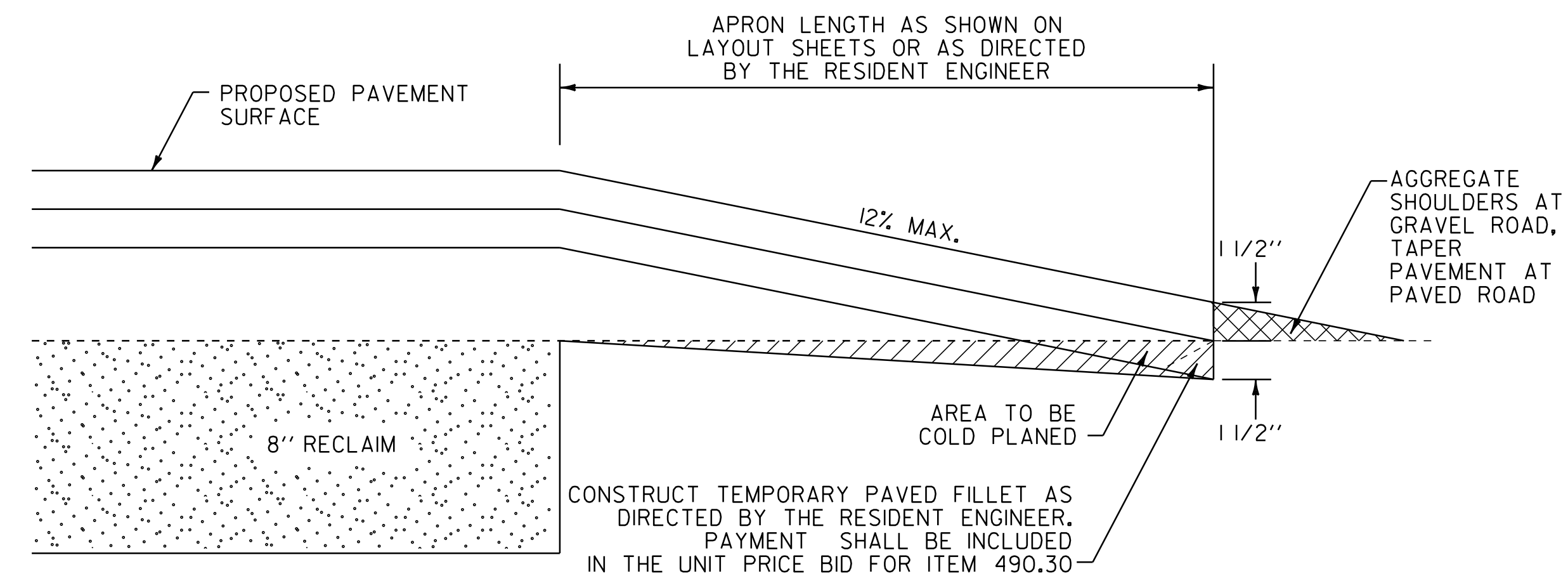


NOTES

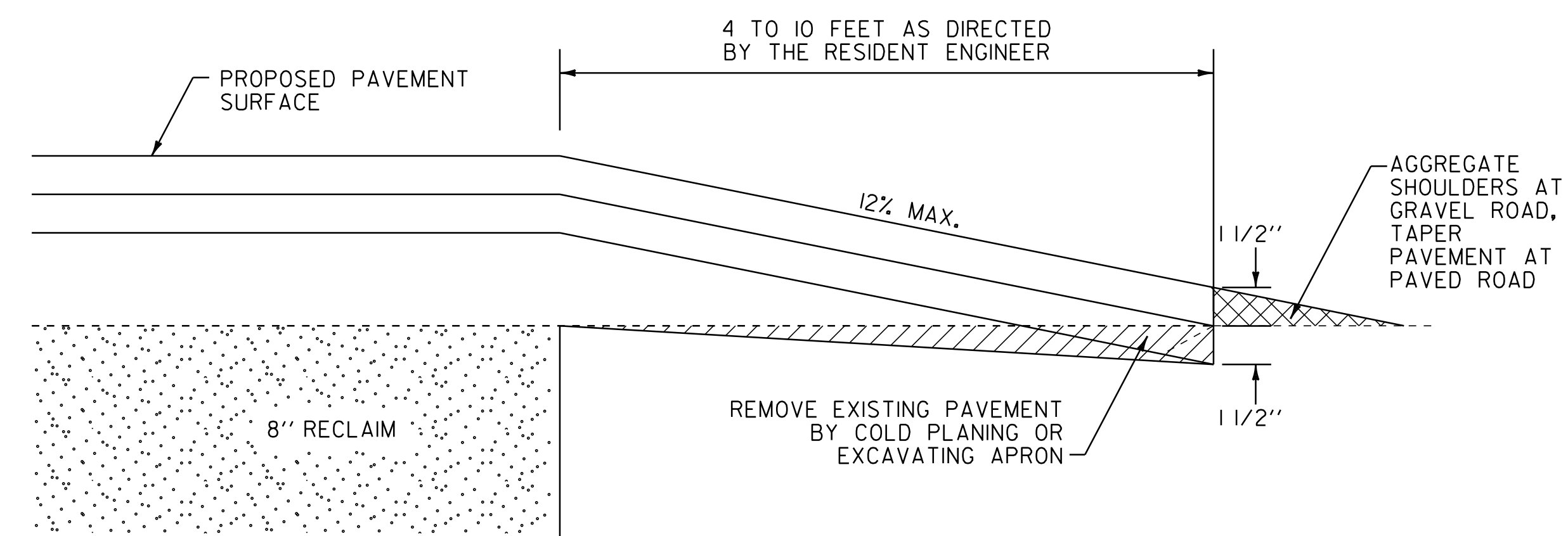
1. THE PAVEMENT WEARING COURSE SHALL BE SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TYPE IVS. THE ESTIMATED 1/2" LEVELING COURSE SHALL BE SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TYPE IVS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
2. GRASS GROWING ADJACENT TO PAVEMENT OR THROUGH CRACKS IN THE PAVEMENT WHICH MAY HAMPER THE PLACEMENT OF NEW BITUMINOUS CONCRETE SHALL BE REMOVED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. PAYMENT FOR THIS WORK WILL NOT BE MADE DIRECTLY, BUT WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 490.30 SUPERPAVE BITUMINOUS CONCRETE PAVEMENT.
3. SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = ±1/4". (TOTAL THICKNESS EXCLUDING LEVELING)
4. EMULSIFIED ASPHALT, TYPE RS-1, SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES, ON COLD PLANED SURFACES AND BETWEEN ALL COURSES OF PAVEMENT (NOT INCLUDING RECLAIMED SURFACES) AT THE RATE OF 0.025 GAL/SY OR AS DIRECTED BY THE RESIDENT ENGINEER. PAID AS ITEM 404.65, EMULSIFIED ASPHALT.
5. AN ESTIMATED THICKNESS OF 2" OF ITEM 900.680 SPECIAL PROVISION (SUBBASE, RAP FOR RECLAIMING), HAS BEEN INCLUDED FOR THE PROVISION OF IMPROVING GRADATION DEFICIENCIES AND/OR CORRECTING SUPERELEVATION, AS NECESSARY, OR AS DIRECTED BY THE RESIDENT ENGINEER. IF THERE IS NOT A SUFFICIENT AMOUNT OF COLD PLANE GRINDINGS AVAILABLE TO COMPLETE THESE REPAIRS, THE CONTRACTOR SHALL OBTAIN COLD PLANE GRINDINGS FROM ANOTHER SOURCE. THE COST ASSOCIATED WITH PROVIDING ADDITIONAL COLD PLANE GRINDINGS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 900.680 SPECIAL PROVISION (SUBBASE, RAP FOR RECLAIMING). THIS MATERIAL SHALL BE RECLAIMED INTO THE RECLAIMED STABILIZED BASE. PAYMENT FOR THIS SECOND RECLAIMING OPERATION WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 900.680 SPECIAL PROVISION (SUBBASE, RAP FOR RECLAIMING).
6. PRIOR TO RECLAIMING, ANY EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER WILL BE EXCAVATED TO THE DEPTH OF RECLAIMING OR AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATED MATERIAL WILL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT AS DIRECTED BY THE RESIDENT ENGINEER. THIS WORK WILL BE PAID FOR USING THE APPROPRIATE RENTAL ITEMS. THE METHOD OF REMOVAL AND THE USE OF RENTAL ITEMS SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO ANY WORK BEING DONE.
7. PRIOR TO PAVING IN COLD PLANE AREAS, ANY EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER SHALL BE EXCAVATED TO A DEPTH OF 3" OR AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATION WILL BE PAID FOR AS ALL PURPOSE EXCAVATOR RENTAL, TYPE I. MATERIAL REMOVED SHALL BE REPLACED WITH ITEM 301.26 SUBBASE OF CRUSHED GRAVEL, FINE GRADED. EXCAVATED MATERIAL SHALL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT AREA AS DIRECTED BY THE RESIDENT ENGINEER.
8. COLD PLANING TO BE COMPLETED ACCORDING TO TYPICAL OR AS NOTED OTHERWISE ON THE PLANS. THE COLD PLANING AND PAVING SHALL MATCH THE EXISTING CONDITIONS AT THE BEGINNING AND END OF CONSTRUCTION AREAS BY THE USE OF A VERTICAL COLD PLANE JOINT. SEE DETAIL ON PROJECT TYPICAL SHEET #3.
9. ALL EDGES OF PAVEMENT SHALL BE BACKED UP TO FULL HEIGHT WITH AGGREGATE SHOULDERS AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.12 AGGREGATE SHOULDERS.
10. ESTIMATED QUANTITIES OF ITEMS 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I, ITEM 608.37 TRUCK RENTAL AND ITEM 608.40 LOADER RENTAL, TYPE I HAVE BEEN INCLUDED FOR THE PROVISION OF CONSTRUCTING GUARDRAIL END SECTION FLARES WITH EXCAVATED DITCHING MATERIAL. AN ESTIMATED QUANTITY OF ITEM 203.30 EARTH BORROW HAS BEEN INCLUDED IN THE CASE THAT THE DITCHING MATERIAL IS NOT SUITABLE TO USE IN THE GUARDRAIL END SECTION FLARE AREA. 25 CUBIC YARDS OF EARTH BORROW HAVE BEEN ESTIMATED FOR EACH NEW GUARDRAIL END SECTION FLARE. ITEM 653.20 TEMPORARY EROSION MATTING SHALL BE PLACED ON ALL SLOPES CREATED BY THE GUARDRAIL END SECTION FLARE. THE QUANTITIES INCLUDED REFLECT 25 SY OF ITEM 653.20 TEMPORARY EROSION MATTING FOR EACH NEW GUARDRAIL END SECTION FLARE TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER.
11. THE PROPOSED GUARDRAIL SHALL BE INSTALLED IN A LOCATION THAT MAXIMIZES THE DISTANCE FROM THE CENTER OF THE ROAD TO THE FACE OF GUARDRAIL AS DIRECTED BY THE RESIDENT ENGINEER.
12. ALL DRIVES SHALL RECEIVE A PAVED APRON AS DIRECTED BY THE RESIDENT ENGINEER. SEE THE DRIVE DETAIL SHEETS FOR LOCATIONS, DETAILS AND PAYMENT PROVISIONS.
13. AN ESTIMATED QUANTITY OF ITEM 619.17 YIELDING MARKER POSTS HAS BEEN INCLUDED TO DELINEATE PIPE INLETS, PIPE OUTLETS AND DROP INLETS LOCATED OUTSIDE OF THE PAVEMENT SURFACE OR AS DIRECTED BY THE RESIDENT ENGINEER.
14. STEEL BEAM GUARDRAIL WITH STEEL POSTS SHALL BE USED ON THIS PROJECT.
15. A QUANTITY FOR ITEM 604.412 REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I HAS BEEN INCLUDED TO BE USED AS INDICATED ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. ALL D'S SHALL BE RAISED OR REHABILITATED SUCH THAT THE NEW GRATE ELEVATION IS EVEN WITH THE SURROUNDING TERRAIN.
16. AN ESTIMATED QUANTITY OF ITEM 601.0910 15" CPEP HAS BEEN INCLUDED FOR USE AS DIRECTED BY THE RESIDENT ENGINEER WHEN BURYING GUARDRAIL ENDS INTO BACKSLOPES. FOR DETAILS, SEE GUARDRAIL DETAIL SHEET #1.



SIDE ROAD AREA DETAIL - TOWN HIGHWAYS
FULL WIDTH OF TOWN HIGHWAY

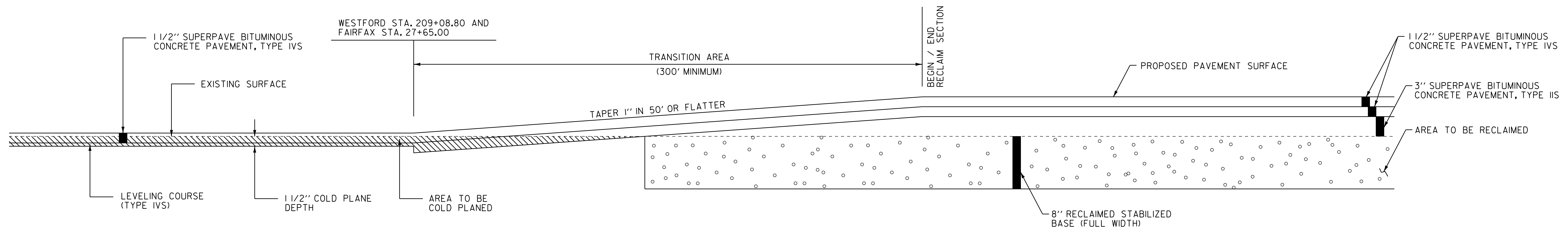
NOTE: THIS DETAIL SHALL BE USED FOR THE SIDE ROADS LISTED BELOW AS DIRECTED BY THE RESIDENT ENGINEER.

WOODS HOLLOW ROAD (T.H. 7) WESTFORD STA. 264+88, LT
WESTFORD-MILTON ROAD (T.H. 2) WESTFORD STA. 303+24, LT



DRIVEWAY TRANSITION DETAIL AT RECLAIM AREAS

NOTE: THIS DETAIL SHALL BE USED FOR ALL DRIVES WITHIN THE RECLAIM AREA AS DIRECTED BY THE RESIDENT ENGINEER.



COLD PLANE / RECLAIM TRANSITION AREA DETAIL

FULL ROADWAY WIDTH
WESTFORD STA. 209+08.80 TO 212+08.8
FAIRFAX STA. 24+65.00 TO 27+65.00



NOT TO SCALE

PROJECT TYPICAL SHEET #2

PROJECT NAME: WESTFORD - FAIRFAX
PROJECT NUMBER: STP 2804(1)

FILE NAME: p08c212.dgn
PROJECT LEADER: JLL
DESIGNED BY: STANTEC
IPARM FILE: p08c212pts02.i

PLOT DATE: 31-OCT-2012 13:56
DRAWN BY: STANTEC
CHECKED BY: JLL
SHEET 3 OF 79