

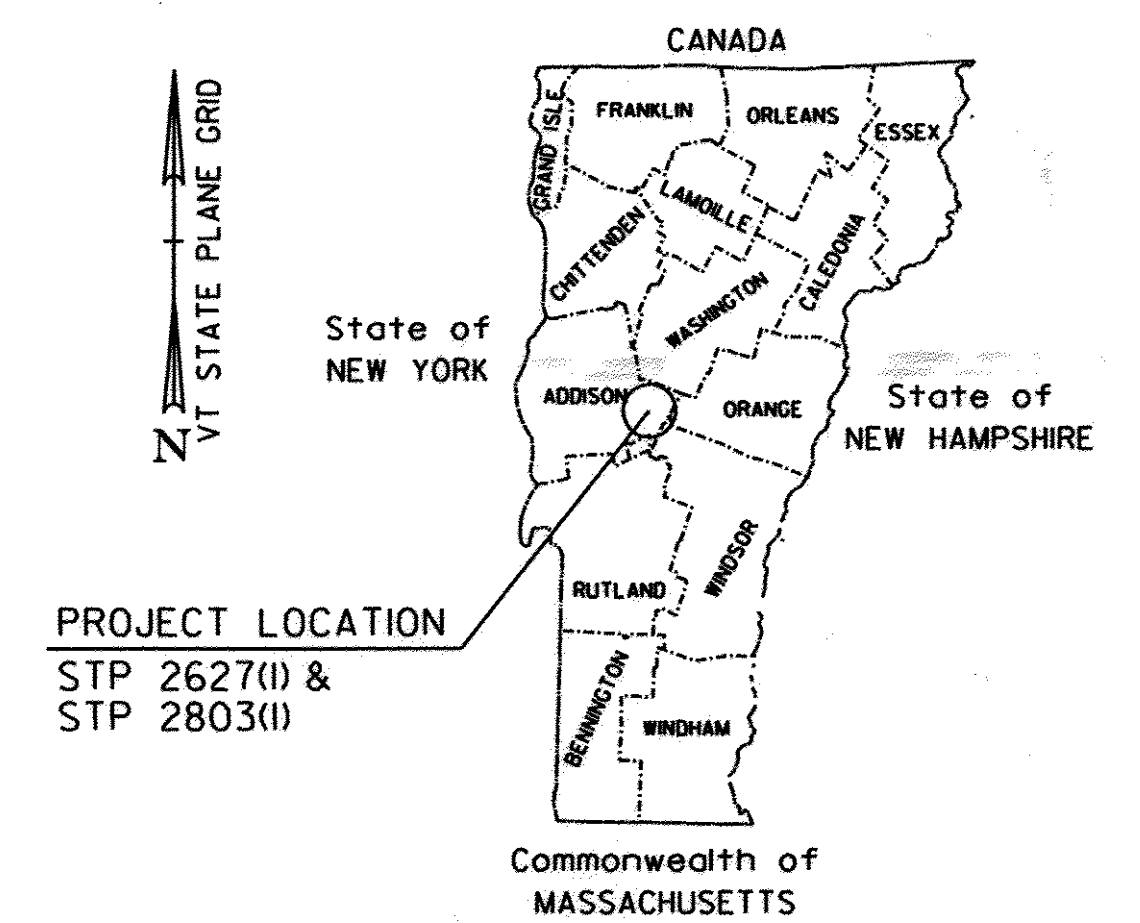
STATE OF VERMONT
AGENCY OF TRANSPORTATION



PROPOSED IMPROVEMENT
TOWNS OF MIDDLEBURY, RIPTON AND HANCOCK
COUNTY OF ADDISON
VT. ROUTE 125

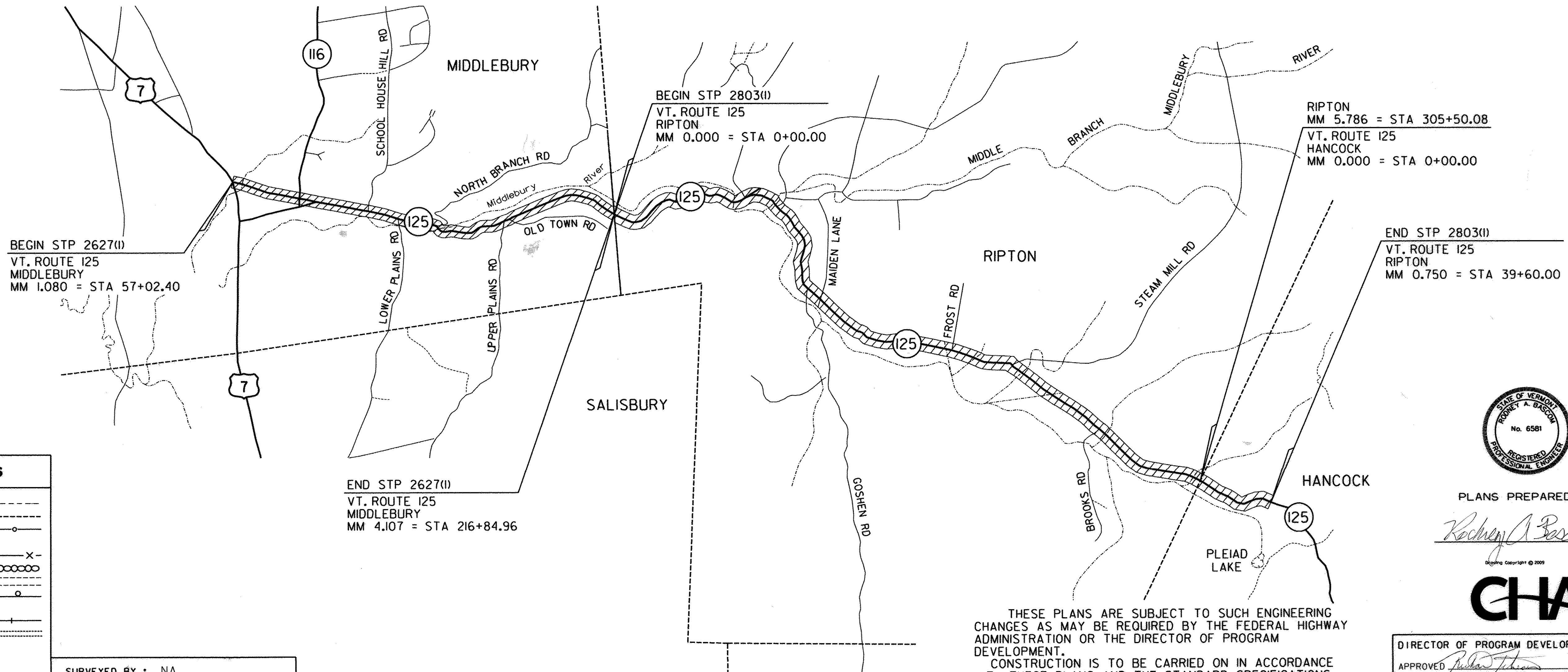
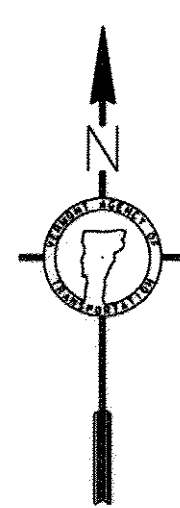
INDEX OF SHEETS
SEE SHEET 2

RECORD PLANS	
CONTRACTOR:	F.W. WHITCOMB CONSTRUCTION CORP. - WALPOLE, NH
RESIDENT ENGINEER:	JOSH HULETT
CONSTRUCTION BEGAN:	MAY 10, 2010
CONSTRUCTION COMPLETE:	JUNE 6, 2011
RECORD PLANS BY:	JOSH HULETT & DARREN CONNOLLY
I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.	
BY	<i>Josh Hulett</i> RESIDENT ENGINEER
DATE	9/10/2013
NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.	



MIDDLEBURY
STP 2627(I)
SEE SHEET 11
FOR ADDITIONAL PROJECT INFORMATION
AND THE SUPERPAVE DESIGN CRITERIA

RIPTON - HANCOCK
STP 2803(I)
SEE SHEET 50
FOR ADDITIONAL PROJECT INFORMATION
AND THE SUPERPAVE DESIGN CRITERIA

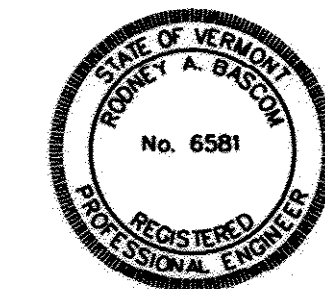


CONVENTIONAL SYMBOLS

COUNTY LINE	---
TOWN LINE	---
LIMITS OF ACCESS	---o---o---
POINT OF ACCESS	X
FENCE LINE	X---X---X---X---
STONE WALL	o---o---o---o---
TRAVELED WAY	---
GUARD RAIL	o---o---o---
RAILROAD	---
SURVEY LINE	---
CULVERT	---
POWER POLE	⊕
TELEPHONE POLE	⊕
TREES	⊙
CONTROL OF ACCESS	---/---/---
PROPERTY LINE	---
R.O.W. TAKING LINE	SR
SLOPE RIGHTS	SR
TOP OF CUT	△
TOE OF SLOPE	○

SURVEYED BY : NA
SURVEYED DATE : NA

DATUM
VERTICAL NA
HORIZONTAL NA



PLANS PREPARED BY

Kevin Marshia

Copyright © 2009



THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT.
CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

DIRECTOR OF PROGRAM DEVELOPMENT	APPROVED <i>Kevin Marshia</i> DATE 1-11-10
PROJECT MANAGER : KEVIN MARSHIA	
PROJECT NAME : MIDDLEBURY & RIPTON - HANCOCK	
PROJECT NUMBER : STP 2627(I) & STP 2803(I)	
SHEET 1 OF 114 SHEETS	

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6	COMPOSITE DRIVEWAY HANDWORK DETAILS SHEET
7	COMPOSITE DRIVEWAY HANDWORK TABLES SHEET
8	COMPOSITE ASPHALTIC PLUG JOINT DETAILS SHEET
9	COMPOSITE MISCELLANEOUS DETAILS SHEET
10	COMPOSITE TRAFFIC CONTROL PLAN SHEET

STP 2627(I) SHEETS

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12	PROJECT TYPICAL SHEET
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18	ITEM DETAIL SUMMARY SHEET
19	DITCH CLEANING DETAIL SHEET
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54-56	QUANTITY SHEETS
57	ITEM DETAIL SUMMARY SHEET
58	DRAINAGE SUMMARY SHEET
59	DITCH CLEANING DETAIL SHEET
60-86	PROJECT LAYOUT SHEETS
87	DRAINAGE PLAN
88-96	SUPERELEVATION BANKING DIAGRAMS SHEETS
97-112	TRAFFIC SIGN SUMMARY SHEETS
113	TRAFFIC SIGN DETAILS SHEET
114	MISCELLANEOUS DETAILS SHEET

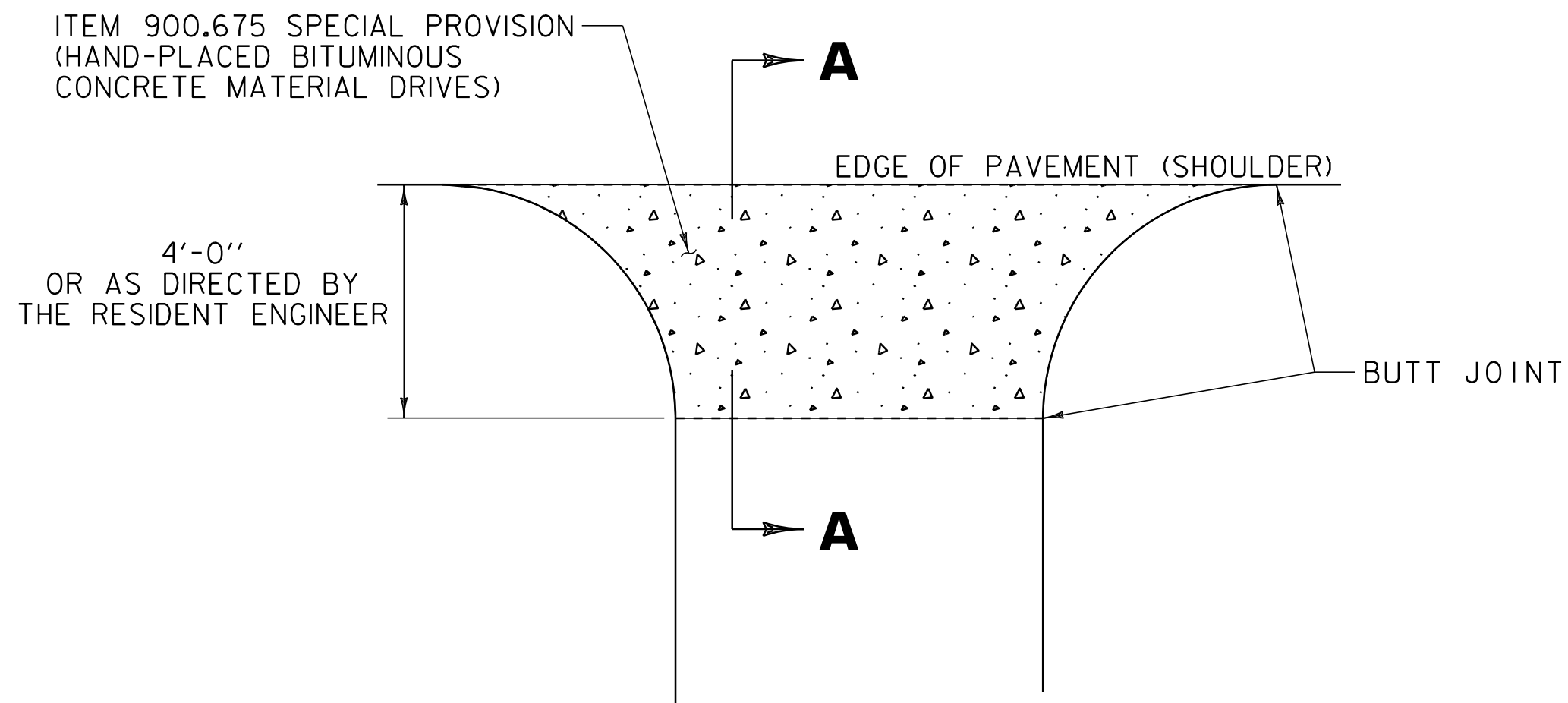
INDEX OF SHEETS

VAOT STANDARDS

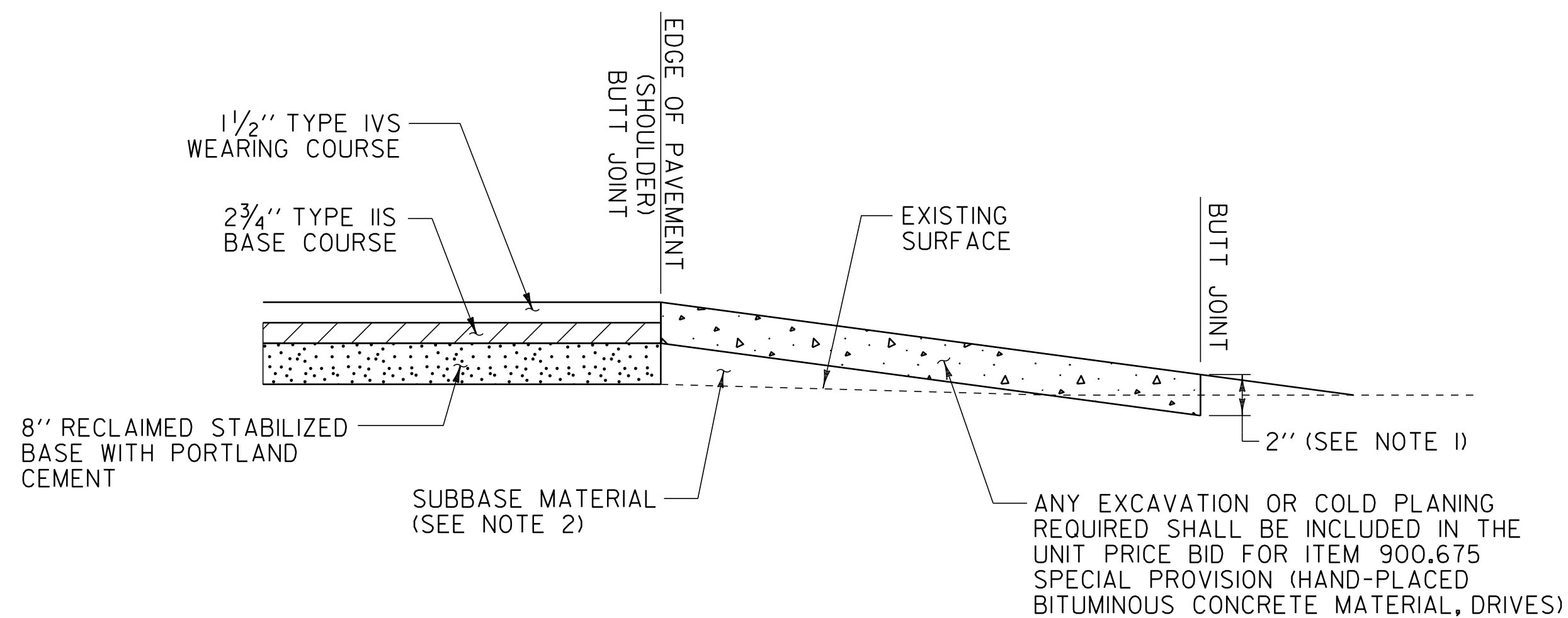
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C-3B	3-10-08
C-10	2-11-08
D-3	6-1-94
D-4	8-13-07
D-6	6-1-94
D-8	1-3-00
D-9	6-1-94
D-15	6-1-94
D-30	8-13-07
E-100	1-2-04
E-100A	1-2-04
E-101	5-30-03
E-102	6-30-03
E-102A	5-1-04
E-106	3-1-04
E-107	6-30-03
E-107A	6-8-09
E-108A	6-8-09
E-110	8-8-95
E-111	3-11-97
E-112	1-23-97
E-121	8-8-95
E-123	3-16-04
E-128A	8-8-95
E-128B	8-8-95
E-131	8-8-95
E-136B	8-8-95
E-138	5-30-03
E-141	9-20-95
E-142	9-20-95
E-153	5-1-04
E-154	5-1-04
E-155	5-1-04
E-163	5-20-99
E-164	6-8-09
E-191	2-1-99
E-192	10-12-00
E-193	8-18-95
G-1	1-3-00
G-1B	6-1-94
G-1D	1-3-00
G-19	11-15-02
SB-R6-82	1-6-95

**COMPOSITE
INDEX
OF
SHEETS**

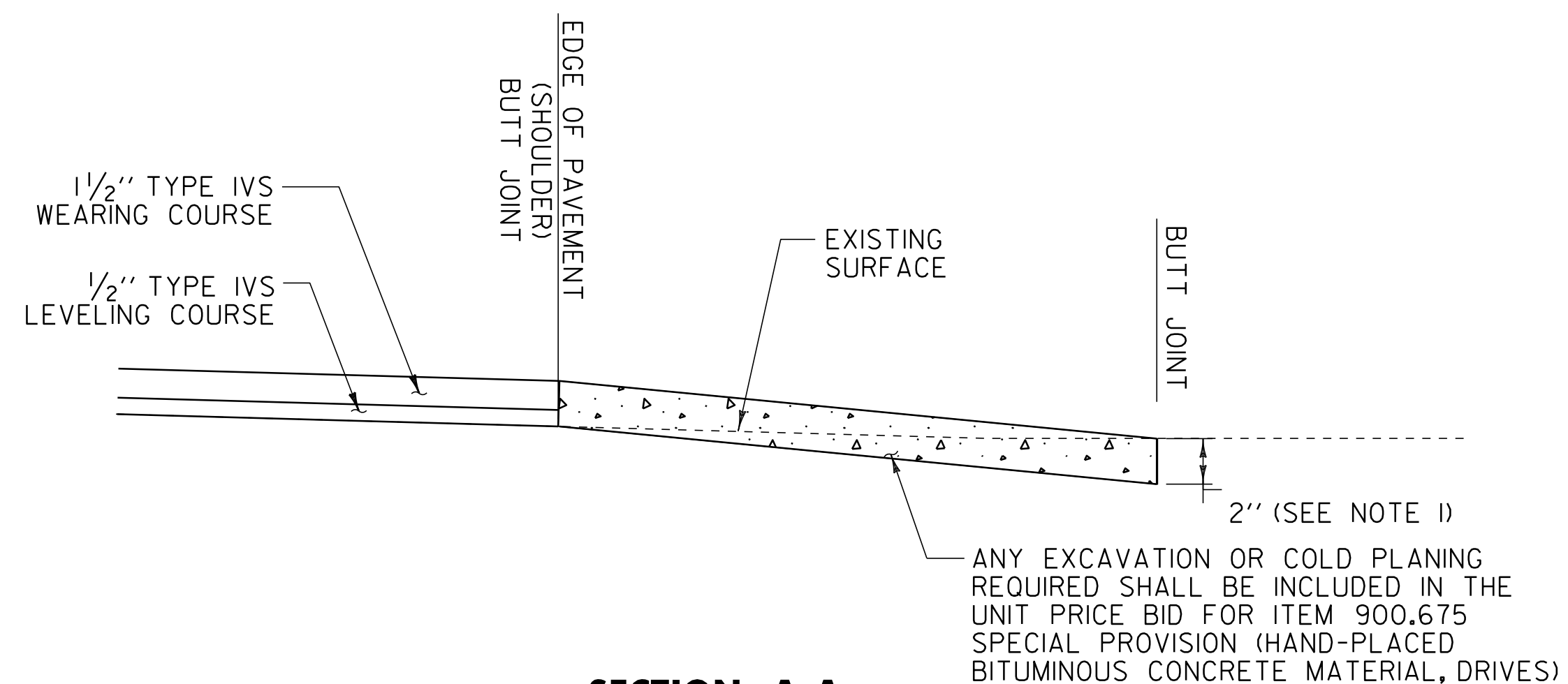
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PROJECT NUMBER: STP 2627(I) & STP 2803(I)	
FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:08
PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.
IPARM FILE: p06c058ind.i	SHEET 2 OF 114



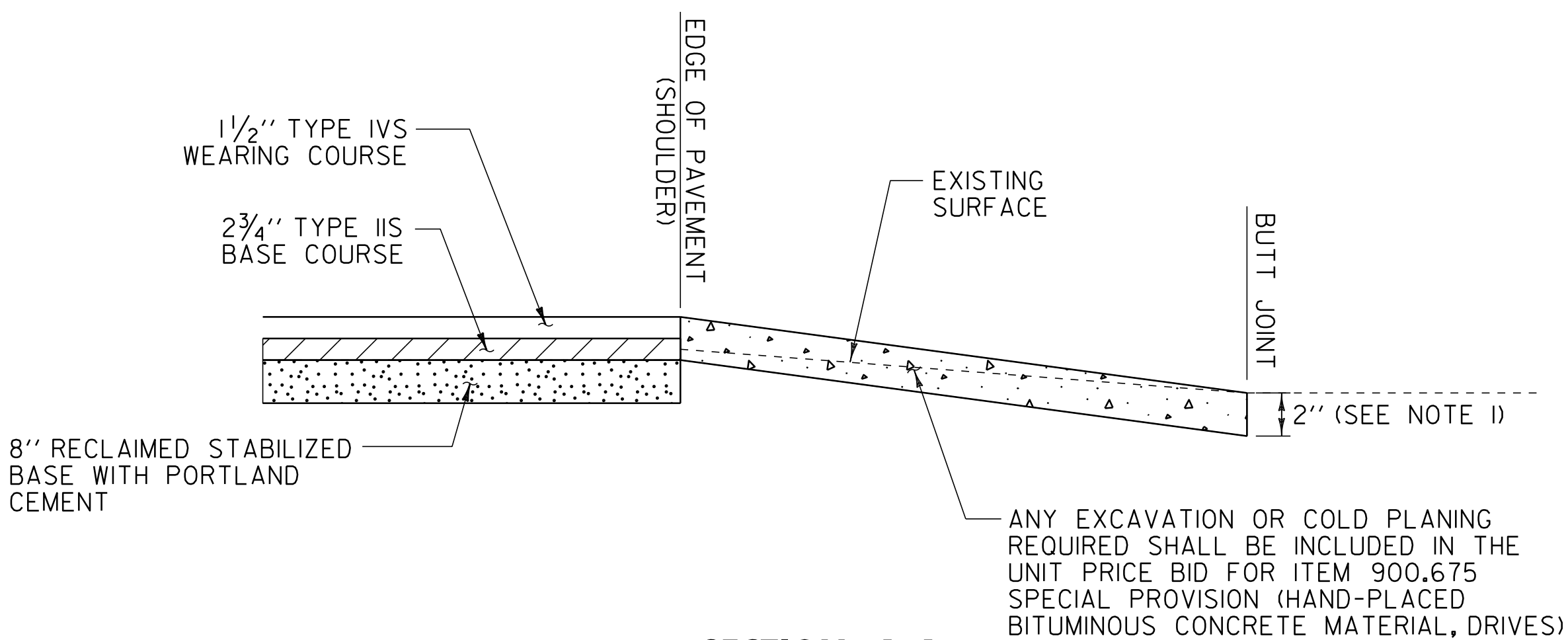
PLAN



SECTION A-A
FILL SECTION

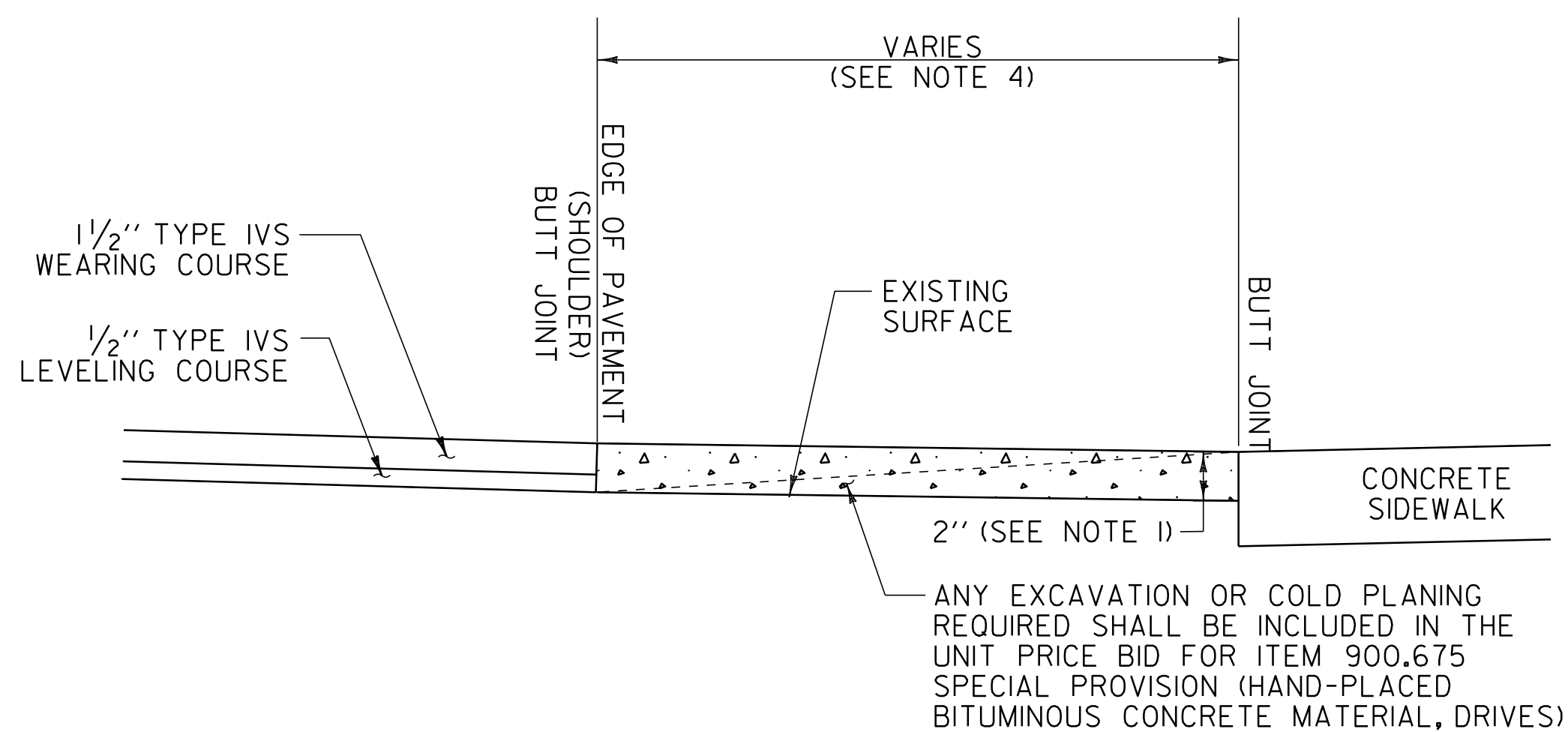


SECTION A-A
OVERLAY AREAS



SECTION A-A
CUT SECTION

RECLAIM AREAS

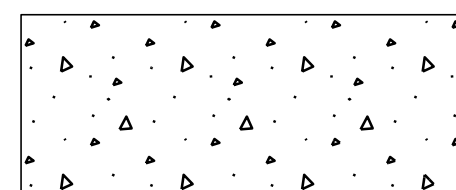


SECTION A-A
OVERLAY AREAS WITH SIDEWALK

NOTES:

- PAVING LIFT NOT TO EXCEED 2".
- THE COST OF PLACING SUBBASE MATERIAL, CLEANING EXISTING PAVED SURFACES, INCLUDING POWER EQUIPMENT, AND FOR FILLING JOINTS, CRACKS AND HOLES WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 900.675 SPECIAL PROVISION (HAND-PLACED BITUMINOUS CONCRETE MATERIAL, DRIVES).
- EXCAVATION OR COLD PLANING NEEDED TO ACHIEVE PROPER DRIVE SLOPES WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 900.675 SPECIAL PROVISION (HAND-PLACED BITUMINOUS CONCRETE MATERIAL, DRIVES).
- THE CONTRACTOR SHALL PAVE TO THE EXISTING CONCRETE SIDEWALK AS DIRECTED BY THE THE RESIDENT ENGINEER.

LEGEND



ITEM 900.675 SPECIAL PROVISION (HAND-PLACED BITUMINOUS CONCRETE MATERIAL, DRIVES)

NOT TO SCALE

COMPOSITE DRIVEWAY HANDWORK DETAILS SHEET

PROJECT NAME: MIDDLEBURY & RIPTON - HANCOCK
PROJECT NUMBER: STP 2627(I) & STP 2803(I)

FILE NAME: p08cl60.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.M.
IPARM FILE: p08cl60cdhd.i

PLOT DATE: 18-SEP-2013 17:08
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 6 OF 114

Bituminous Concrete Material, Drives

Station	Offset	Length	Width	SF	SY (SF / 9)	Date Completed
57+42	LT	68	4	272.00	30.22 ✓	10/22/2010
64+42	LT	28.5	6.5	185.25	20.58 ✓	10/22/2010
65+15	RT	27.5	4	110.00	12.22 ✓	10/22/2010
68+33	RT	27.5	4	110.00	12.22 ✓	10/22/2010
68+49	LT	23	4	92.00	10.22 ✓	10/22/2010
70+83	RT	17.5	4	70.00	7.78 ✓	10/22/2010
71+18	RT	18.5	4	74.00	8.22 ✓	10/22/2010
74+04	RT	14	13.5	189.00	21.00 ✓	10/22/2010
75+22	RT	19	11	209.00	23.22 ✓	10/22/2010
76+47	LT	35.5	5	177.50	19.72 ✓	10/22/2010
78+25	LT	55.5	6.5	360.75	40.08 ✓	10/22/2010
79+80	LT	22	7	154.00	17.11 ✓	10/22/2010
81+32	LT	18	4	72.00	8.00 ✓	10/22/2010
81+88	RT	37	4	148.00	16.44 ✓	10/22/2010
84+70	LT	45	7	315.00	35.00 ✓	11/16/2010
87+20	LT	21	8	168.00	18.67 ✓	11/16/2010
87+80	RT	25.5	5.5	140.25	15.58 ✓	11/16/2010
88+22	RT	14	8	112.00	12.44 ✓	11/16/2010
89+10	RT	17	7	119.00	13.22 ✓	11/16/2010
91+97	LT	15	6	90.00	10.00 ✓	11/15/2010
92+80	LT	36	8	288.00	32.00 ✓	11/15/2010
94+09	RT	23	10.5	241.50	26.83 ✓	11/9/2010
94+70	LT	30	14	420.00	46.67 ✓	11/10/2010
95+48	LT	16	17	272.00	30.22 ✓	11/15/2010
95+50	RT	18	14	252.00	28.00 ✓	11/9/2010
96+18	RT	19	14	266.00	29.56 ✓	11/9/2010
96+62	LT	18.5	17.5	323.75	35.97 ✓	11/15/2010
97+37	RT	22	13	286.00	31.78 ✓	11/9/2010
98+00	RT	82.5	4	330.00	36.67 ✓	11/9/2010
98+56	LT	66	7	462.00	51.33 ✓	11/10/2010
98+89	RT	25	12.5	312.50	34.72 ✓	11/9/2010
99+54	LT	11.5	14	161.00	17.89 ✓	11/10/2010
99+79	LT	15	14	210.00	23.33 ✓	11/10/2010
		9	5.5	49.50	5.50 ✓	11/10/2010
100+00	RT	92	13	1196.00	132.89 ✓	11/9/2010
100+71	RT	13	13.5	175.50	19.50 ✓	11/9/2010
101+76	LT	12	9	108.00	12.00 ✓	11/10/2010
101+96	LT	15.5	8.5	131.75	14.64 ✓	11/10/2010
		4.5	6.5	29.25	3.25 ✓	11/10/2010
102+62	LT	15	9	135.00	15.00 ✓	11/10/2010
103+42	RT	18	14	252.00	28.00 ✓	11/9/2010

Page 1 Total	=	1007.72	SY
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PREPARED BY: JOSH HULETT

CHECKED BY: DLDATE: 1.20.11

12/17/2010

1:00 PM

Bituminous Concrete Material, Drives

Station	Offset	Length	Width	SF	SY (SF / 9)	Date Completed
103+52	LT	28	10	280.00	31.11 ✓	11/15/2010
104+80	LT	13	10	130.00	14.44 ✓	11/10/2010
104+88	RT	13	6.5	84.50	9.39 ✓	11/9/2010
		24	14	336.00	37.33 ✓	11/9/2010
		18	6.5	117.00	13.00 ✓	11/9/2010
105+11	LT	12	10	120.00	13.33 ✓	11/10/2010
105+11	LT	3.5	16.5	57.75	6.42 ✓	11/10/2010
105+63	RT	17	14	238.00	26.44 ✓	11/12/2010
		6	6.5	39.00	4.33 ✓	11/12/2010
105+90	RT	20.5	13.5	276.75	30.75 ✓	11/9/2010
106+86	LT	12.5	10	125.00	13.89 ✓	11/10/2010
107+71	RT	14	13	182.00	20.22 ✓	11/12/2010
108+20	LT	41.5	10	415.00	46.11 ✓	11/15/2010
108+63	RT	33.5	11	368.50	40.94 ✓	11/12/2010
109+51	RT	30	11.5	345.00	38.33 ✓	11/12/2010
111+61	RT	21.5	10	215.00	23.89 ✓	11/12/2010
112+52	RT	16.5	8.5	140.25	15.58 ✓	11/12/2010
113+43	LT	37	12.5	462.50	51.39 ✓	11/12/2010
		95	11.5	1092.50	121.39 ✓	11/15/2010
113+84	RT	21.5	6.5	139.75	15.53 ✓	11/12/2010
114+43	LT	21.5	13	279.50	31.06 ✓	11/12/2010
114+65	RT	20.5	5	102.50	11.39 ✓	11/12/2010
114+79	LT	34	13	442.00	49.11 ✓	11/12/2010
		8.5	2.5	21.25	2.36 ✓	11/12/2010
115+64	RT	22.5	4.5	101.25	11.25 ✓	11/12/2010
116+31	LT	19.5	12.5	243.75	27.08 ✓	11/12/2010
117+31	LT	16.5	11.5	189.75	21.08 ✓	11/12/2010
122+26	LT	61	3.5	213.50	23.72 ✓	11/12/2010
122+76	LT	32	4	128.00	14.22 ✓	11/12/2010
126+64	RT	13	17	221.00	24.56 ✓	11/15/2010
		12.5	7	87.50	9.72 ✓	11/15/2010
129+60	RT	21	2	42.00	4.67 ✓	11/12/2010
130+55	RT	22	2	44.00	4.89 ✓	11/12/2010
131+96	LT	19	4.5	85.50	9.50 ✓	11/12/2010
132+22	RT	25.5	3	76.50	8.50 ✓	11/12/2010
132+62	LT	46	6.5	299.00	33.22 ✓	11/12/2010
133+05	RT	17.5	3.5	61.25	6.81 ✓	11/12/2010
146+30	RT	37	5	185.00	20.56 ✓	11/2/2010
158+06	LT	18.5	4	74.00	8.22 ✓	11/2/2010
173+45	RT	31	4.5	139.50	15.50 ✓	11/2/2010
178+42	RT	86	4	344.00	38.22 ✓	11/2/2010

Page 2 Total = 949.47 SY ✓

Page 1 + Page 2 = Final Total: 1007.72 + 949.47 = 1957.19 SY ✓

Ripton Stations

Station	Offset	Length	Width	SF	SY (SF / 9)	Date Completed
58+72	RT	69	4	276.00	30.67 ✓	11/2/2010
		22.5	4	90.00	10.00 ✓	11/16/2010
63+15	RT	43	9	387.00	43.00 ✓	11/16/2010
		51	5.5	280.50	31.17 ✓	11/16/2010
		54.5	6.5	354.25	39.36 ✓	11/17/2010
		39	13.5	526.50	58.50 ✓	11/17/2010
		45	1	45.00	5.00 ✓	11/17/2010
66+33	LT	70	4	280.00	31.11 ✓	11/17/2010
68+10	LT	43	9.5	408.50	45.39 ✓	11/17/2010
69+30	LT	42	9.5	399.00	44.33 ✓	11/17/2010
69+71	RT	56	4	224.00	24.89 ✓	11/17/2010
70+74	LT	20.5	4	82.00	9.11 ✓	11/17/2010
73+45	LT	19	4	76.00	8.44 ✓	11/17/2010
74+05	LT	12.5	4.5	56.25	6.25 ✓	11/17/2010
75+63	LT	51	4	204.00	22.67 ✓	11/17/2010
83+36	LT	16.5	8.5	140.25	15.58 ✓	11/18/2010
85+30	LT	27	3.5	94.50	10.50 ✓	11/18/2010
112+01	RT	25.5	4.5	114.75	12.75 ✓	11/18/2010
132+80	RT	45	4	180.00	20.00 ✓	11/18/2010
145+17	LT	45	4	180.00	20.00 ✓	11/18/2010
146+03	LT	26	3.5	91.00	10.11 ✓	11/18/2010
150+07	LT	19	18.5	351.50	39.06 ✓	11/18/2010
184+25	LT	28	4	112.00	12.44 ✓	11/19/2010
191+65	LT	28	4	112.00	12.44 ✓	11/18/2010
196+09	RT	14	4	56.00	6.22 ✓	11/19/2010
196+41	RT	13	4	52.00	5.78 ✓	11/19/2010
205+30	LT	20	3.5	70.00	7.78 ✓	11/19/2010
205+45	RT	13.5	4	54.00	6.00 ✓	11/19/2010
207+39	LT	20.5	4	82.00	9.11 ✓	11/19/2010
210+72	LT	16	11	176.00	19.56 ✓	11/19/2010
220+46	RT	28.5	4.5	128.25	14.25 ✓	11/11/2010
220+91	RT	10.5	4.5	47.25	5.25 ✓	11/19/2010
222+75	RT	16	6	96.00	10.67 ✓	11/11/2010
225+14	RT	4	3	12.00	1.33 ✓	11/11/2010
225+87	RT	6	5	30.00	3.33 ✓	11/11/2010
226+57	RT	50	4	200.00	22.22 ✓	11/11/2010
227+58	RT	21	4	84.00	9.33 ✓	11/11/2010
229+49	RT	21.5	3.5	75.25	8.36 ✓	11/11/2010
234+85	LT	31	4	124.00	13.78 ✓	11/11/2010
235+30	LT	18	4	72.00	8.00 ✓	11/11/2010

Page 1 Total = 713.75 SY

Ripton Stations

<u>Station</u>	<u>Offset</u>	<u>Length</u>	<u>Width</u>	<u>SF</u>	<u>SY</u> <i>(SF / 9)</i>	<u>Date Completed</u>
244+65	LT	15	4	60.00	6.67 ✓	11/5/2010
244+96	RT	14	4	56.00	6.22 ✓	11/5/2010
249+70	RT	18.5	4	74.00	8.22 ✓	11/5/2010
256+90	LT	20	4	80.00	8.89 ✓	11/5/2010
257+96	RT	13	3.5	45.50	5.06 ✓	11/11/2010
266+30	LT	26.5	4	106.00	11.78 ✓	11/5/2010
277+28	RT	28	4	112.00	12.44 ✓	11/5/2010
277+32	LT	23.5	4	94.00	10.44 ✓	11/5/2010
281+89	RT	44	4	176.00	19.56 ✓	11/5/2010
Page 2 Total				=	89.28 ✓	SY

Page 1 + Page 2 = Final Total: 713.75' + 89.28 = 803.03 SY ✓

STP 2627(1) MIDDLEBURY

STATION	POSITION	QUANTITY (SY)	DRIVE TYPE
MIDDLEBURY:			
64+40.0	LT	9	GRAVEL DRIVE
65+10.0	RT	9	GRAVEL DRIVE
68+45.0	RT	9	GRAVEL DRIVE
68+55.0	LT	9	ASPHALT DRIVE
70+95.0	RT	7	ASPHALT DRIVE
71+25.0	RT	9	GRAVEL DRIVE
74+15.0	RT	9	ASPHALT DRIVE
75+20.0	RT	9	ASPHALT DRIVE
76+20.0	RT	18	GRAVEL DRIVE
76+55.0	LT	11	GRAVEL DRIVE
78+30.0	LT	18	GRAVEL DRIVE
79+85.0	LT	7	GRAVEL DRIVE
81+40.0	LT	7	ASPHALT DRIVE
81+90.0	LT	18	ASPHALT DRIVE
84+70.0	LT	22	ASPHALT DRIVE
86+30.0	LT	9	GRAVEL DRIVE
87+30.0	LT	7	GRAVEL DRIVE
87+90.0	RT	13	ASPHALT DRIVE
88+40.0	LT	7	ASPHALT DRIVE
90+20.0	LT	7	ASPHALT DRIVE
91+55.0	LT	7	GRAVEL DRIVE
91+95.0	LT	9	GRAVEL DRIVE
92+80.0	RT	44	GRAVEL DRIVE
92+85.0	LT	13	GRAVEL DRIVE
94+10.0	RT	7	GRAVEL DRIVE
94+80.0	LT	20	GRAVEL DRIVE
95+40.0	LT	7	ASPHALT DRIVE
95+50.0	RT	7	GRAVEL DRIVE
96+25.0	RT	7	GRAVEL DRIVE
96+60.0	LT	7	GRAVEL DRIVE
97+40.0	RT	7	GRAVEL DRIVE
99+00.0	RT	7	GRAVEL DRIVE
99+70.0	LT	7	GRAVEL DRIVE
100+70.0	RT	7	GRAVEL DRIVE
101+70.0	LT	7	ASPHALT DRIVE
102+05.0	LT	9	ASPHALT DRIVE
102+60.0	LT	7	GRAVEL DRIVE
103+50.0	RT	7	GRAVEL DRIVE
103+55.0	LT	11	GRAVEL DRIVE
104+75.0	LT	7	GRAVEL DRIVE
104+90.0	RT	27	ASPHALT DRIVE
105+00.0	LT	7	GRAVEL DRIVE
105+60.0	RT	7	GRAVEL DRIVE
106+00.0	RT	7	GRAVEL DRIVE
106+90.0	LT	7	GRAVEL DRIVE
107+80.0	RT	7	GRAVEL DRIVE
108+25.0	LT	20	GRAVEL DRIVE
108+75.0	RT	18	GRAVEL DRIVE
109+60.0	RT	18	GRAVEL DRIVE
111+60.0	RT	7	GRAVEL DRIVE
112+60.0	RT	7	GRAVEL DRIVE
113+90.0	RT	9	ASPHALT DRIVE
114+50.0	LT	13	ASPHALT/GRAVEL
114+75.0	RT	7	ASPHALT DRIVE
114+90.0	LT	13	GRAVEL DRIVE
115+70.0	RT	7	ASPHALT DRIVE
116+40.0	LT	7	GRAVEL DRIVE
117+05.0	RT	7	GRAVEL DRIVE
117+40.0	LT	7	ASPHALT DRIVE

STP 2627(1) MIDDLEBURY (CONT'D)

STATION	POSITION	QUANTITY (SY)	DRIVE TYPE
MIDDLEBURY:			
117+45.0	RT	7	ASPHALT DRIVE
119+30.0	RT	13	GRAVEL DRIVE
120+10.0	RT	9	GRAVEL DRIVE
122+40.0	LT	24	GRAVEL DRIVE
122+85.0	LT	7	ASPHALT DRIVE
126+75.0	RT	9	GRAVEL DRIVE
129+75.0	RT	7	GRAVEL DRIVE
130+70.0	RT	7	ASPHALT DRIVE
131+60.0	LT	9	GRAVEL DRIVE
132+30.0	RT	13	GRAVEL DRIVE
132+45.0	LT	13	GRAVEL DRIVE
133+15.0	LT	9	GRAVEL DRIVE
133+80.0	RT	7	GRAVEL DRIVE
134+20.0	RT	9	GRAVEL DRIVE
136+10.0	RT	13	GRAVEL DRIVE
140+55.0	RT	7	GRAVEL DRIVE
146+15.0	RT	22	GRAVEL DRIVE
173+20.0	RT	7	GRAVEL DRIVE
SUBTOTAL:		812	
ROUNDING:		38	
TOTAL:		850	

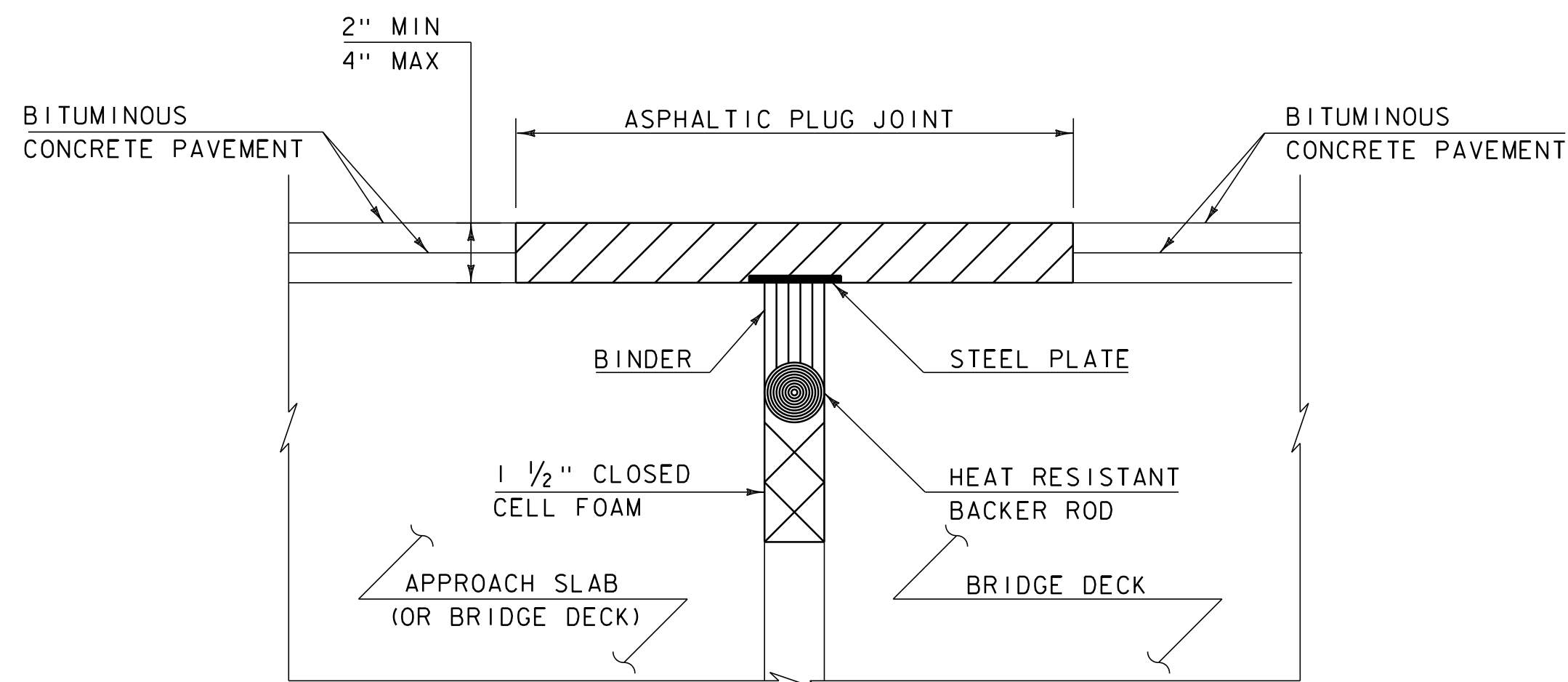
STP 2803(1) RIPTON - HANCOCK

STATION	POSITION	QUANTITY (SY)	DRIVE TYPE
RIPTON:			
57+72	LT	9	GRAVEL DRIVE
58+74	RT	12	GRAVEL DRIVE
61+44	LT	7	GRAVEL DRIVE
63+02	RT	57	GRAVEL DRIVE
65+67	RT	20	GRAVEL DRIVE
66+09	LT	36	GRAVEL DRIVE
66+39	RT	7	GRAVEL DRIVE
67+74	RT	4	FIELD ACCESS
67+95	LT	18	GRAVEL DRIVE
68+87	RT	12	GRAVEL DRIVE
69+11	LT	17	GRAVEL DRIVE
69+52	RT	26	GRAVEL DRIVE
70+10	LT	9	GRAVEL DRIVE
73+20	LT	7	GRAVEL DRIVE
73+69	LT	7	GRAVEL DRIVE
74+53	RT	7	GRAVEL DRIVE
75+46	LT	45	GRAVEL DRIVE
83+22	LT	9	GRAVEL DRIVE
85+16	LT	9	GRAVEL DRIVE
94+09	LT	7	FIELD ACCESS
95+84	LT	13	GRAVEL DRIVE
105+60	RT	9	ASPHALT/GRAVEL
112+03	RT	9	GRAVEL DRIVE
114+60	LT	9	GRAVEL DRIVE
132+65	RT	9	GRAVEL DRIVE
145+00	LT	9	GRAVEL DRIVE
145+90	LT	7	GRAVEL DRIVE
149+86	LT	13	GRAVEL DRIVE
169+26	RT	11	ASPHALT DRIVE
181+45	LT	7	GRAVEL DRIVE
184+03	LT	7	GRAVEL DRIVE
190+58	RT	13	ASPHALT DRIVE
205+01	LT	7	GRAVEL DRIVE
207+03	LT	7	GRAVEL DRIVE
208+00	LT	7	FIELD ACCESS
209+53	RT	13	GRAVEL DRIVE
210+39	LT	7	FIELD ACCESS
220+28	RT	7	GRAVEL DRIVE
225+63	LT	9	GRAVEL DRIVE
226+24	RT	7	GRAVEL DRIVE
227+35	RT	7	GRAVEL DRIVE
229+25	RT	7	GRAVEL DRIVE
234+68	LT	7	GRAVEL DRIVE
235+10	LT	7	GRAVEL DRIVE
244+46	LT	7	FIELD ACCESS
244+80	RT	7	FIELD ACCESS
248+39	RT	7	FIELD ACCESS
249+48	RT	7	FIELD ACCESS
256+60	LT	7	GRAVEL DRIVE
257+96	RT	7	GRAVEL DRIVE
265+94	LT	7	GRAVEL DRIVE
277+04	LT	7	GRAVEL DRIVE
277+09	RT	7	FIELD ACCESS
281+46	RT	9	GRAVEL DRIVE
HANCOCK:			
18+93	RT	81	GRAVEL DRIVE
SUBTOTAL:		686	
ROUNDING:		14	
TOTAL:		700	

SEE REVISED PAGES 6A ~ 6D

**COMPOSITE
DRIVEWAY
TABLES
SHEET**

PROJECT NAME: MIDDLEBURY & RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:08
PROJECT NUMBER: STP 2627(1) & STP 2803(1)	DRAWN BY: C.A.K.
FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
PROJECT LEADER: D.E.G.	SHEET 7 OF 114
DESIGNED BY: M.J.M.	
IPARM FILE: p08cl60hwt.i	



**ASPHALTIC PLUG-TYPE
JOINT DETAIL**

- ~~BR 13 VT. ROUTE 125 MIDDLEBURY STA 138+79.0 (20'-7")~~
- ~~BR 13 VT. ROUTE 125 MIDDLEBURY STA 139+85.0 (20'-7")~~
- ~~BR 15 VT. ROUTE 125 RIPTON STA 76+45.0 (19'-10")~~
- ~~BR 15 VT. ROUTE 125 RIPTON STA 76+71.0 (19'-10")~~

• NO JOINT FOUND

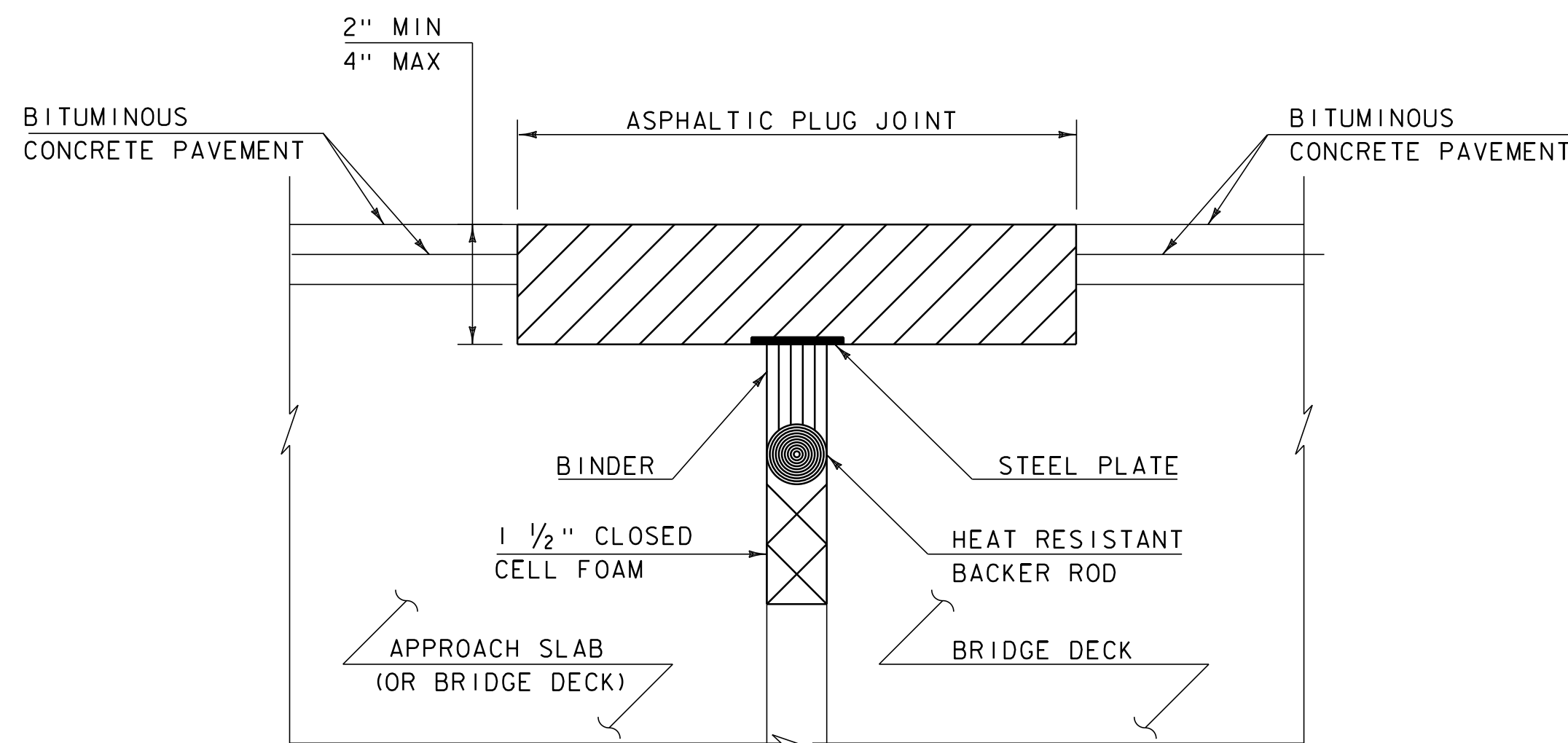
ASPHALTIC PLUG JOINT NOTES

1. INSTALLATION

- A. LOCATE THE JOINT CENTRALLY OVER THE DECK OVERLAY EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.
- B. REMOVE THE BITUMINOUS CONCRETE PAVEMENT FULL DEPTH AS SHOWN ON THE PLANS. THE PAVEMENT SHALL BE DRY AND SAW CUT TO THE LIMITS REQUIRED TO PLACE THE JOINT. A PNEUMATIC HAMMER AND CHISEL MAY BE USED ADJACENT TO THE CURB ONLY WHEN SAW CUTTING IS NOT POSSIBLE.
- C. BLAST CLEAN THE JOINT AREA OF DEBRIS, ASPHALT AND SHEET MEMBRANE. THOROUGHLY DRY THE JOINT AREA WITH COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.
- D. REPAIR SPALLED AND DEFECTIVE CONCRETE WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.
- E. PLACE PROPERLY SIZED HEAT RESISTANT BACKER ROD IN THE MOVEMENT GAP ALLOWING FOR 1" +/- OF BINDER ABOVE THE ROD.
- F. HEAT AND PLACE THE BINDER MATERIAL AS RECOMMENDED BY THE MANUFACTURER.
- G. PLACE 1/4" THICK BY 8" WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE THE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRESTAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER. THE STEEL PLATES MAY BE OMITTED WHERE THE ENGINEER DETERMINES THAT THE APPROACH SLAB OR BRIDGE DECK WILL PROVIDE INADEQUATE SUPPORT AND WHERE VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.
- H. HEAT AND MIX THE BINDER MATERIAL AND AGGREGATE AS RECOMMENDED BY THE MANUFACTURER.
- I. INSTALLATION OF MATERIAL, COMPACTION, AND TOP COATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- J. IMMEDIATELY AFTER TOP COATING, CAST AN ANTI-SKID MATERIAL OVER THE JOINT TO REDUCE THE RISK OF TRACKING.
- K. ONCE THE JOINT REACHES 82 DEG C (180 DEG F) +/-, WATER MAY BE USED TO EXPEDITE THE COOLING PROCESS.
- L. PROTECT JOINT FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 51 DEG C (125 DEG F) +/-.

2. WEATHER LIMITATIONS. (APPLY BINDER MATERIAL ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL OR AS RECOMMENDED BY THE MANUFACTURER):

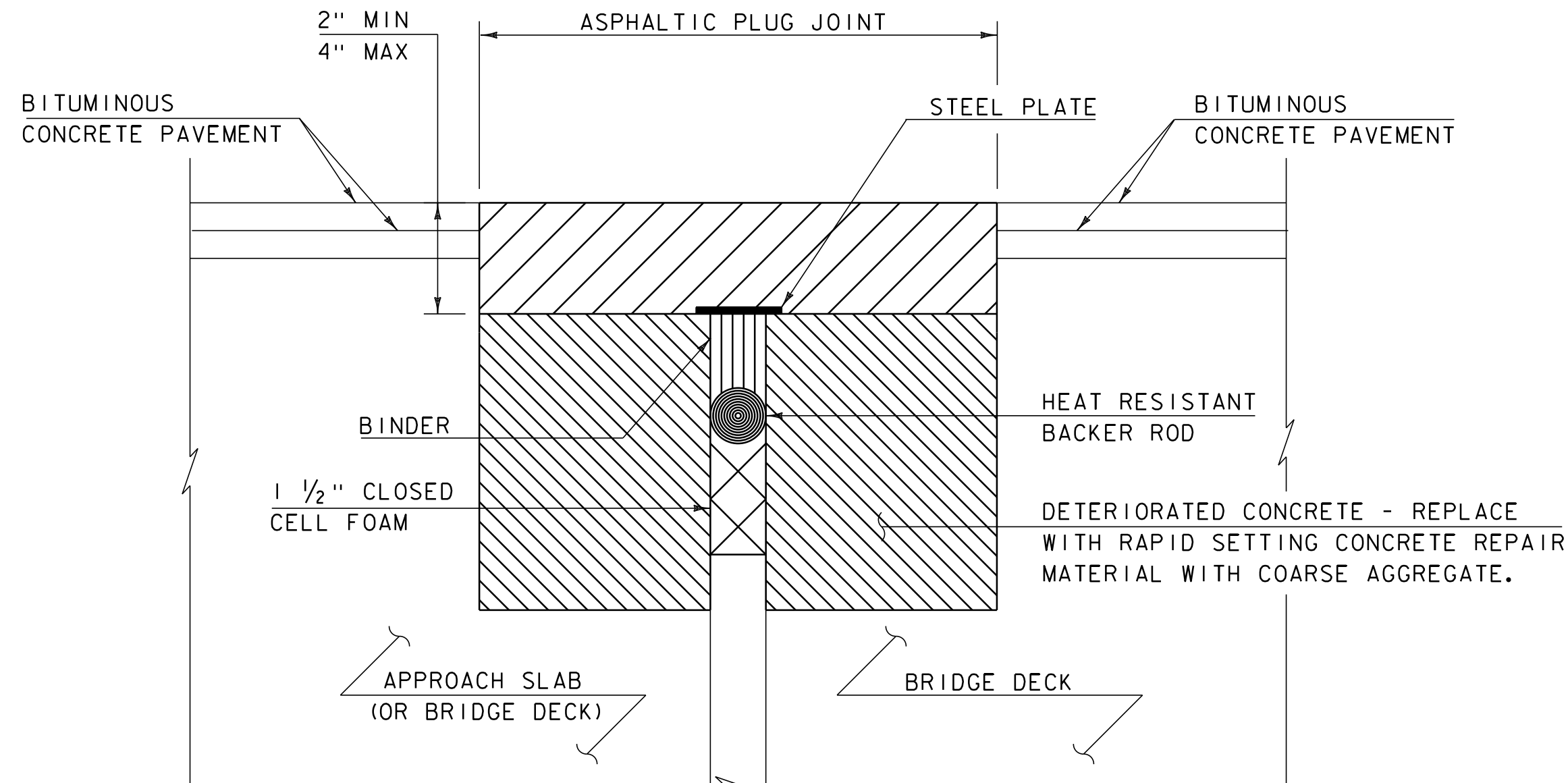
- A. THE AMBIENT AIR TEMPERATURE IS AT LEAST 10 DEG C (50 DEG F) AND RISING.
- B. THE ROAD SURFACE IS DRY.
- C. WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF SATISFACTORY WORK.



**ASPHALTIC PLUG-TYPE JOINT DETAIL
REMOVAL OF < 2" DETERIORATED CONCRETE**

NOTES:

1. UPON ENCOUNTERING UP TO 2" AVERAGE OF DETERIORATED CONCRETE, THE CONTRACTOR SHALL REMOVE THE DETERIORATED MATERIAL AND REPLACE IT WITH THE ASPHALTIC PLUG JOINT MATERIAL AS DIRECTED BY THE RESIDENT ENGINEER.
2. REMOVAL OF THE DETERIORATED CONCRETE WILL NOT BE PAID SEPARATELY BUT WILL BE CONSIDERED INCIDENTAL TO THE UNIT BID PRICE FOR THE ITEM 516.10. THE ADDITIONAL PLUG JOINT MATERIAL BELOW THE DESIGN DEPTH TO REPLACE THE DETERIORATED CONCRETE WILL BE CONSIDERED INCIDENTAL TO THE UNIT BID PRICE FOR THE ITEM 516.10.



**ASPHALTIC PLUG-TYPE JOINT DETAIL
REMOVAL OF > 2" DETERIORATED CONCRETE**

NOTES:

1. UPON ENCOUNTERING GREATER THAN 2" AVERAGE OF DETERIORATED CONCRETE, THE CONTRACTOR SHALL REMOVE THE DETERIORATED MATERIAL AND REPLACE IT WITH RAPID SETTING CONCRETE REPAIR MATERIAL WITH COARSE AGGREGATE FORMED TO EXISTING ELEVATION.
2. REMOVAL OF THE DETERIORATED CONCRETE WILL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 580.20 "RAPID SETTING CONCRETE REPAIR MATERIAL WITH COARSE AGGREGATE".
3. REINFORCING STEEL NOT SHOWN FOR CLARITY.

NOT TO SCALE

**COMPOSITE
ASPHALTIC
PLUG JOINT
DETAILS
SHEET**

PROJECT NAME: MIDDLEBURY & RIPTON - HANCOCK

PROJECT NUMBER: STP 2627(II) & STP 2803(II)

FILE NAME: p08cl60.dgn

PROJECT LEADER: D.E.G.

DESIGNED BY: M.J.M.

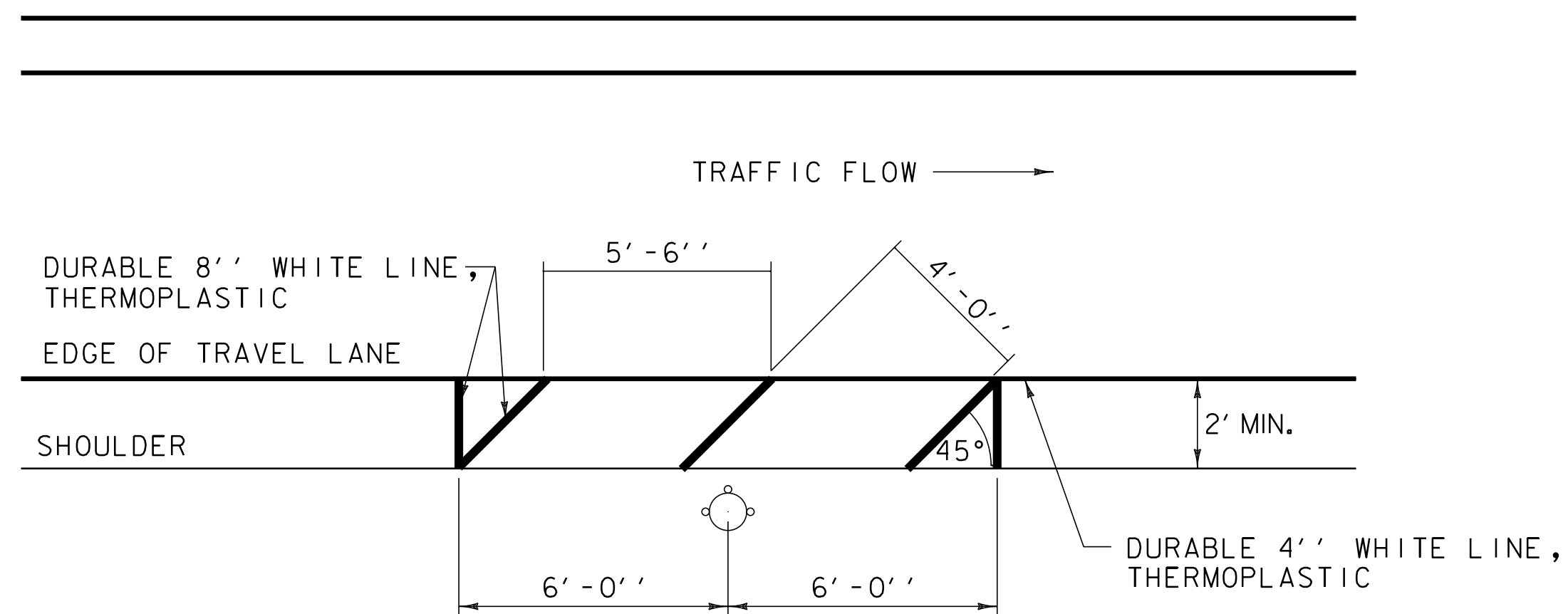
IPARM FILE: p08cl60asp.i

PLOT DATE: 18-SEP-2013 17:08

DRAWN BY: C.A.K.

CHECKED BY: D.W.E.

SHEET 8 OF 114



FIRE HYDRANT PAVEMENT MARKING DETAIL

VT. ROUTE 125 MIDDLEBURY: STA 67+86.0 RT
 STA 75+96.0 RT
 STA 84+05.0 RT
 STA 92+24.0 RT
 STA 100+35.0 RT
 STA 110+12.0 RT
 STA 117+28.0 RT
 STA 122+60.0 RT
 STA 128+14.0 RT

VT. ROUTE 125 RIPTON: STA 228+60.0 RT

NOTE:

1. THE CONTRACTOR SHALL ADJUST THE PLACEMENT OF THE FIRE HYDRANT PAVEMENT MARKINGS TO MEET THE EXISTING SITE CONSTRAINTS AS DIRECTED BY THE RESIDENT ENGINEER.

WEATHERING BOX BEAM GUARDRAIL INVENTORY

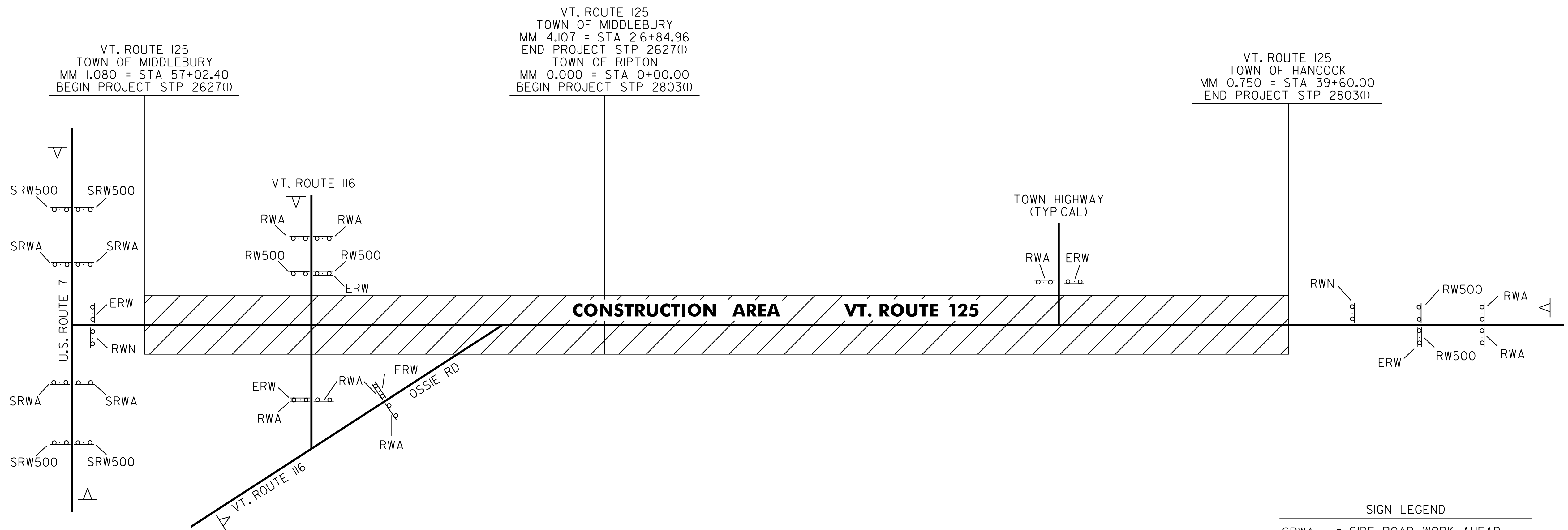
QUANTITY	DESCRIPTION	LENGTH (FT)	RADIUS (FT)
500	POST	9	N/A
17	CAPPED END RAIL	8	N/A
2	ANGLED END RAIL	N/A	N/A
104	SPLICE PLATES	N/A	N/A
200	SHELF BRACKETS	N/A	N/A
11	STRAIGHT BOX BEAM RAIL	18	N/A
3	STRAIGHT BOX BEAM RAIL	18	N/A
1	STRAIGHT BOX BEAM RAIL	18	N/A
1	SHOP CURVE BOX BEAM RAIL	18	150
2	SHOP CURVE BOX BEAM RAIL	18	270
2	SHOP CURVE BOX BEAM RAIL	18	320
1	SHOP CURVE BOX BEAM RAIL	18	510
1	SHOP CURVE BOX BEAM RAIL	18	590
1	SHOP CURVE BOX BEAM RAIL	18	602
1	SHOP CURVE BOX BEAM RAIL	24	700
1	SHOP CURVE BOX BEAM RAIL	24	70
1	SHOP CURVE BOX BEAM RAIL	24	80
1	SHOP CURVE BOX BEAM RAIL	24	100
1	SHOP CURVE BOX BEAM RAIL	24	200
1	SHOP CURVE BOX BEAM RAIL	24	205
1	SHOP CURVE BOX BEAM RAIL	24	230
2	SHOP CURVE BOX BEAM RAIL	24	260
1	SHOP CURVE BOX BEAM RAIL	24	265
1	SHOP CURVE BOX BEAM RAIL	24	310
1	SHOP CURVE BOX BEAM RAIL	24	345
5	SHOP CURVE BOX BEAM RAIL	24	480
1	SHOP CURVE BOX BEAM RAIL	24	500
1	SHOP CURVE BOX BEAM RAIL	24	518
1	SHOP CURVE BOX BEAM RAIL	24	580
1	SHOP CURVE BOX BEAM RAIL	24	N/A

NOTES:

1. THE ABOVE INVENTORY OF WEATHERING BOX BEAM GUARDRAIL IS LOCATED AT VTRANS' DISTRICT 5 MAINTENANCE YARD IN COLCHESTER. THE CONTRACTOR SHALL VERIFY THE ABOVE INVENTORY OF AVAILABLE WEATHERING BOX BEAM GUARDRAIL PRIOR TO PERFORMING ANY WORK ON THE EXISTING WEATHERING BOX BEAM GUARDRAIL AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL CONTACT THE DISTRICT 5 MANAGER AT (802) 655-1580 TO COORDINATE ACCESS TO THEIR FACILITIES.

NOT TO SCALE

COMPOSITE MISCELLANEOUS DETAILS SHEET	PROJECT NAME: MIDDLEBURY & RIPTON - HANCOCK
	PROJECT NUMBER: STP 2627(I) & STP 2803(I)
	FILE NAME: p08cl60.dgn
	PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.M.	PLOT DATE: 18-SEP-2013 17:08
IPARM FILE: p08cl60cmdt.i	CHECKED BY: D.W.E.
	SHEET 9 OF 114



STP 2627(1)

HIGHWAY NAME	RWA	SRWA	ERW	SRW500	RW500	RWN	OTHER
U.S. ROUTE 7		4		4			2-PORTABLE CHANGEABLE MESSAGE SIGNS
VT. ROUTE 125 - BEGIN PROJECT			1			1	
WILMAR ROAD (TH-42)	1		1				
VT. ROUTE 116	4		2		2		2-PORTABLE CHANGEABLE MESSAGE SIGNS
KING'S ROW (TH-38)	1		1				
OSSIE ROAD	2		1				
MAPLE COURT (TH-29)	1		1				
GRIST MILL ROAD (TH-33)	1		1				
SCHOOL HOUSE HILL ROAD (TH-27)	1		1				
LOWER PLAINS ROAD (TH-23)	1		1				
NORTH BRANCH ROAD (TH-35)	1		1				
UPPER PLAINS ROAD (TH-11)	1		1				
TOTAL	14	4	12	4	2	1	4-PORTABLE CHANGEABLE MESSAGE SIGNS

STP 2803(1)

HIGHWAY NAME	RWA	SRWA	ERW	SRW500	RW500	RWN	OTHER
LINCOLN ROAD (TH-1)	1		1				
NATURAL TPKE (TH-11)	1		1				
GOSHEN ROAD (TH-18)	1		1				
MAIDEN LANE (TH-2)	1		1				
ROBERT FROST ROAD (TH-19)	1		1				
STEAM MILL ROAD (TH-20)	1		1				
BROOK ROAD (TH-21)	1		1				
VT. ROUTE 125 - END PROJECT	2		1		2	1	1-PORTABLE CHANGEABLE MESSAGE SIGN
TOTAL	9		8		2	1	1-PORTABLE CHANGEABLE MESSAGE SIGN

- SIGN LEGEND**
- SRWA = SIDE ROAD WORK AHEAD
 - SRW500 = SIDE ROAD WORK 500 FEET
 - RWA = ROAD WORK AHEAD
 - RW500 = ROAD WORK 500 FEET
 - ERW = END ROAD WORK
 - RWN = ROAD WORK NEXT 8 MILES
 - ◁ = PORTABLE CHANGEABLE MESSAGE SIGN

CONSTRUCTION APPROACH SIGNING

- NOTES:
- SEE VAOT STD. E-100 & E-100A FOR ADDITIONAL SIGN PLACEMENT DETAILS.
 - CONSTRUCTION ZONE SIGN LAYOUT SHALL BE IN ACCORDANCE WITH SECTION 6 OF THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
 - CONSTRUCTION SIGNS SHALL BE IN NEW OR LIKE NEW CONDITION PER VAOT STANDARDS.
 - DIAMOND SHAPED SIGNS SHALL BE 4' X 4' WITH BLACK LEGEND ON FLUORESCENT ORANGE BACKGROUND.
 - RETROREFLECTIVE SIGN SHEETING SHALL BE MINIMUM TYPE VII FOR ALL SOLID SUBSTRATE SIGNS AND MINIMUM TYPE VI FOR ALL ROLL-UP SIGNS.
 - ALL SIGN STANDS AND POST INSTALLATIONS SHALL BE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM REPORT (NCHRP) 350 COMPLIANT.
 - PORTABLE CHANGEABLE MESSAGE SIGNS ARE TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER.
 - PAYMENT FOR CONSTRUCTION SIGNING WILL BE MADE UNDER ITEM 641.10 TRAFFIC CONTROL (STP 2627(I)) AND ITEM 641.10 TRAFFIC CONTROL (STP 2803(I)).

NOT TO SCALE

COMPOSITE TRAFFIC CONTROL PLAN SHEET	PROJECT NAME: MIDDLEBURY & RIPTON - HANCOCK
	PROJECT NUMBER: STP 2627(I) & STP 2803(I)
	FILE NAME: p08cl60.dgn
	PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.M.	PLOT DATE: 18-SEP-2013 17:08
IPARM FILE: p08cl60+cp.i	DRAWN BY: C.A.K.
	CHECKED BY: D.W.E.
	SHEET 10 OF 114

STATE OF VERMONT AGENCY OF TRANSPORTATION

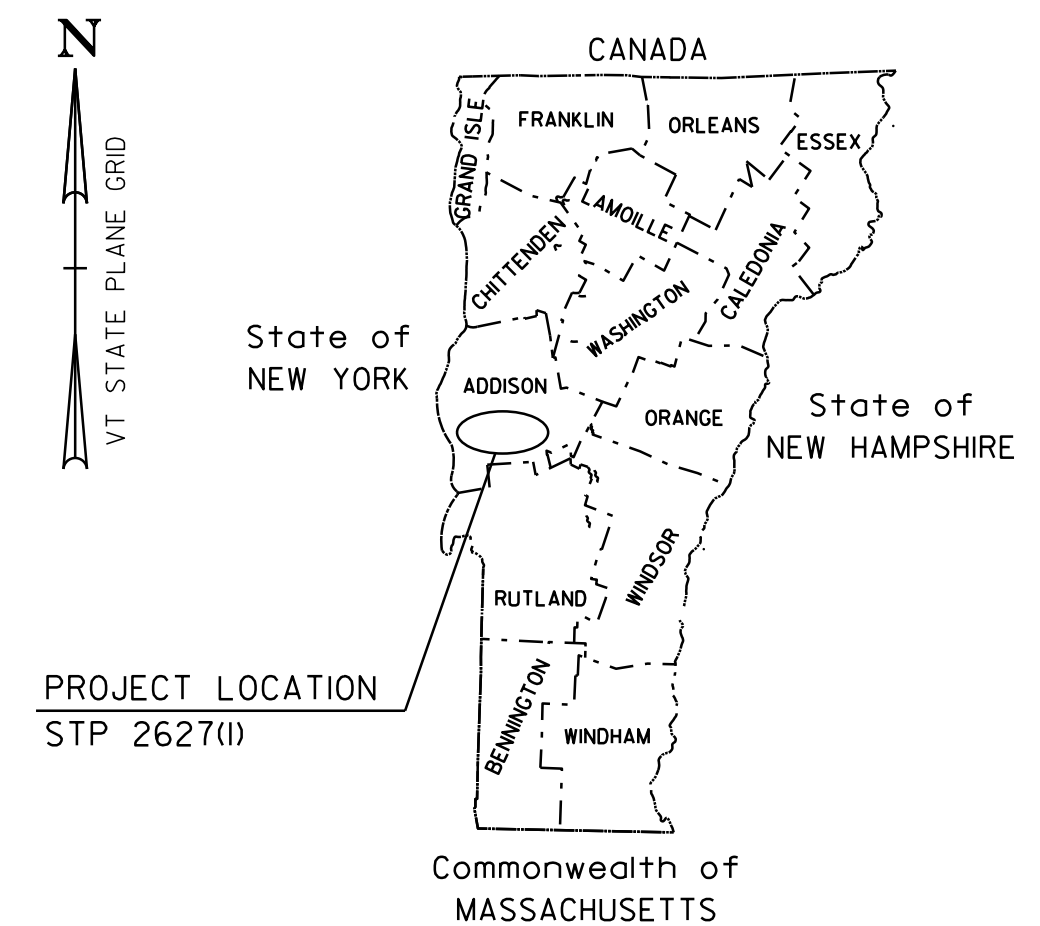


PROPOSED IMPROVEMENT TOWN OF MIDDLEBURY COUNTY OF ADDISON VERMONT ROUTE 125

BEGINNING IN MIDDLEBURY ON VT. ROUTE 125 AT MM 1.080 = STA 57+02.40 AND EXTENDING
EASTERLY A DISTANCE OF 15,982.56 FEET (3.027 MILES) TO MM 4.107 = STA 216+84.96.

LENGTH OF ROADWAY = 15,982.56 FT = (3.027 MILES)
LENGTH OF PROJECT = 15,982.56 FT = (3.027 MILES)

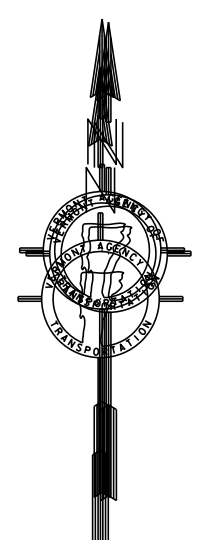
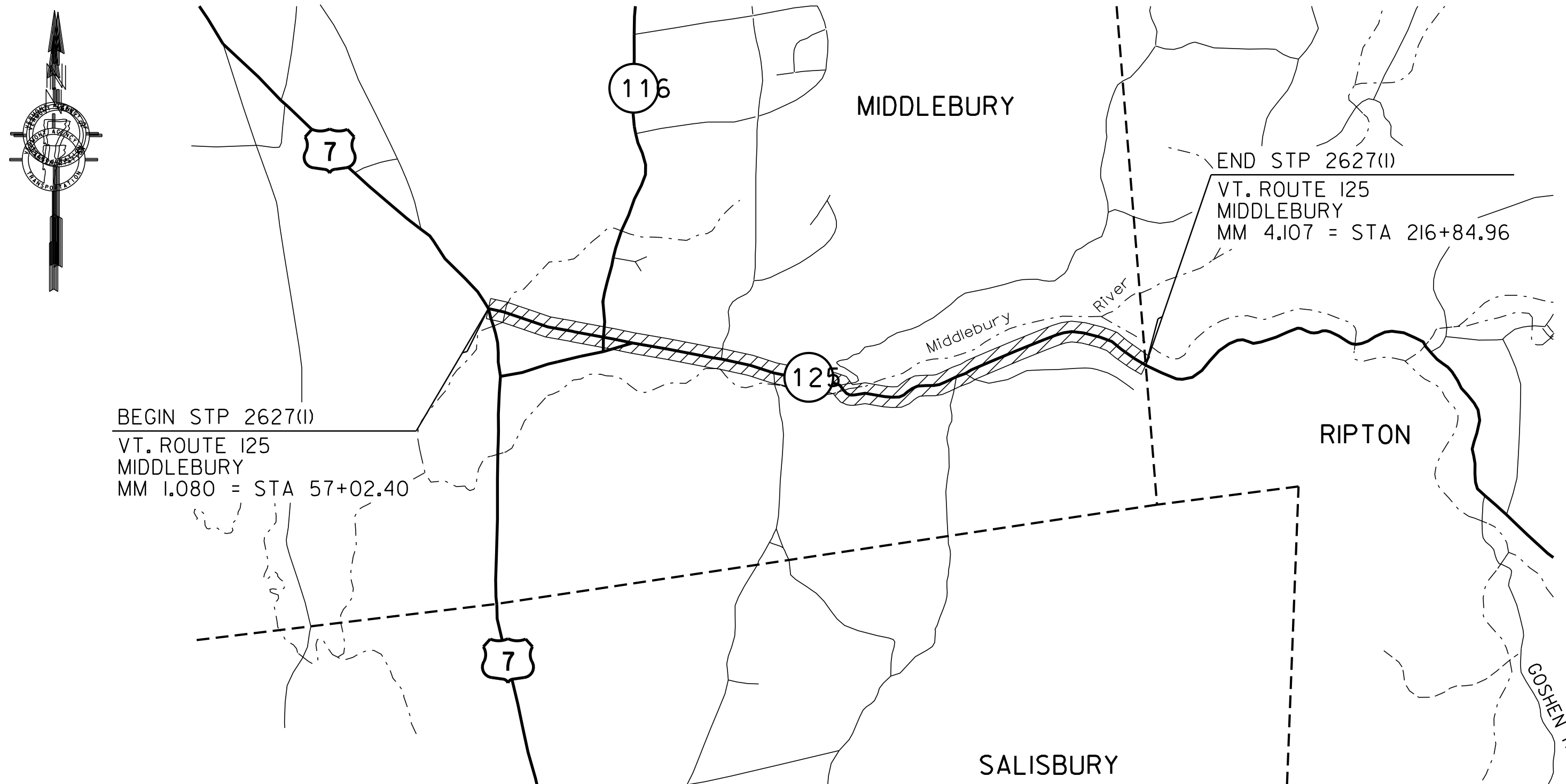
WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES RECLAIMING AND/OR OVERLAYING SEGMENTS OF
THE EXISTING HIGHWAY AND RESURFACING WITH A SHIM/LEVELING COURSE AND A WEARING COURSE.
PAVEMENT MARKINGS, GUARDRAIL IMPROVEMENTS, DRAINAGE IMPROVEMENTS, NEW SIGNS AND INCIDENTAL ITEMS.



BITUMINOUS CONCRETE SUPERPAVE MIXTURE DESIGN CRITERIA	
DESIGN LANE/DESIGN LIFE ESAL	1,287,000
DESIGN NUMBER OF GYRATIONS	65
PERFORMANCE GRADE ASPHALT BINDER	SEE SECTION 490 GENERAL SPECIAL PROVISIONS

TRAFFIC DATA

SECTION	ADT		DHV		ESALS	ESALS
	2009	2019	2009	2019	2009-2019	2009-2029
BEGIN PROJECT TO VT. ROUTE 116	3400	3700	380	420	398,000	933,000
VT. ROUTE 116 TO OSSIE STREET	4100	5300	540	600	822,000	2,142,000
OSSIE STREET TO LOWER PLAINS ROAD	4800	5300	540	600	987,000	2,574,000
LOWER PLAINS ROAD TO END PROJECT	1900	2100	210	240	316,000	736,000



CONVENTIONAL SYMBOLS

COUNTY LINE	----
TOWN LINE	----
LIMITS OF ACCESS	—○—○—○—○—○—○—
POINT OF ACCESS	X
FENCE LINE	X—X—X—X—X—X—
STONE WALL	○○○○○○○○○○○○○○○○○○○○
TRAVELED WAY	----
GUARD RAIL	○—○—○—○—○—○—
RAILROAD	
SURVEY LINE	----
CULVERT	—+—+—+—+—+—+—
POWER POLE	⊕
TELEPHONE POLE	⊕
TREES	⊕
CONTROL OF ACCESS	—//—//—//—//—//—//—
PROPERTY LINE	----
R.O.W. TAKING LINE	—SR—△—SR—○—SR—
SLOPE RIGHTS	—○—△—△—△—○—
TOP OF CUT	—△—△—△—
TOE OF SLOPE	—○—○—○—

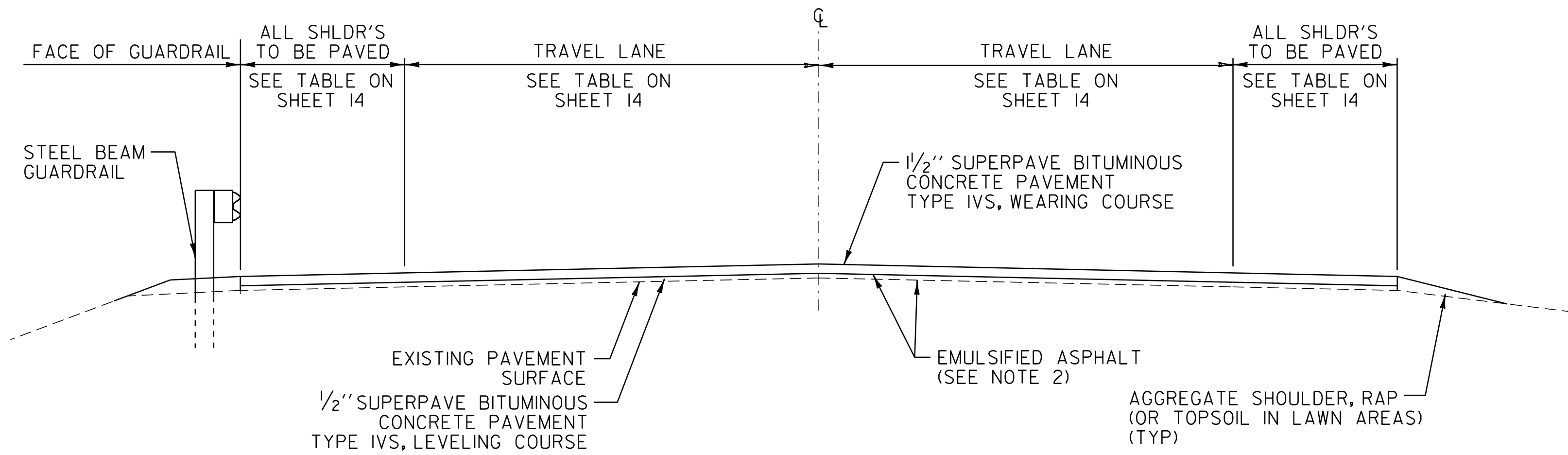
SURVEYED BY :	NA
SURVEYED DATE :	NA
DATUM	
VERTICAL	NA
HORIZONTAL	NA

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT.
CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

PROJECT MANAGER :	KEVIN MARSHIA
PROJECT NAME :	MIDDLEBURY
PROJECT NUMBER :	STP 2627 (I)
SHEET	11 OF 114 SHEETS

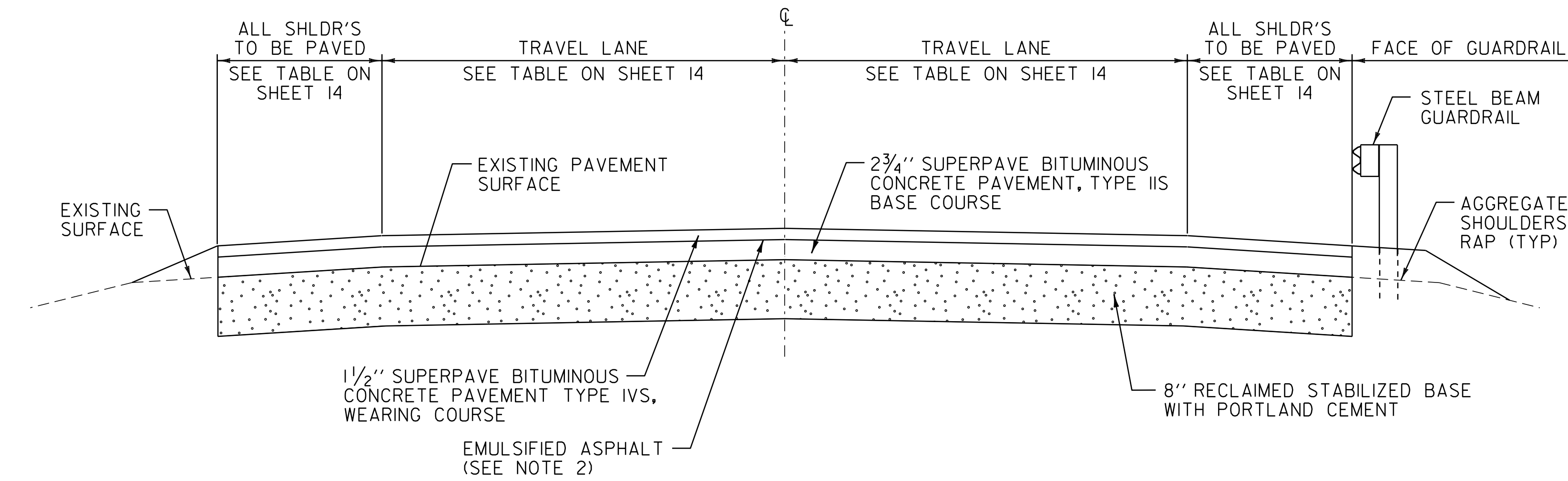
NOTES

1. THE WEARING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. THE BASE COURSE SHALL BE TYPE IIS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. THE LEVELING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT.
2. EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES, BETWEEN ALL COURSES OF PAVEMENT AND ON COLD PLANED SURFACES AT THE RATE OF 0.025 GAL/SY OR AS DIRECTED BY THE RESIDENT ENGINEER.
3. SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = +/- 1/4" (TOTAL PAVEMENT THICKNESS EXCLUDING LEVELING).
4. PRIOR TO RECLAIMING, ANY EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER SHALL BE EXCAVATED TO THE DEPTH OF RECLAIMING OR AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATED MATERIAL WILL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT AS DIRECTED BY THE RESIDENT ENGINEER. THIS WORK WILL BE PAID FOR USING THE APPROPRIATE RENTAL ITEM(S). THE METHOD OF REMOVAL AND THE USE OF RENTAL ITEM(S) SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO ANY WORK BEING DONE. MATERIAL REMOVED SHALL BE REPLACED WITH ITEM 301.28 SUBBASE OF CRUSHED GRAVEL, FINE GRADED. THIS WORK SHALL BE DONE BEFORE THE BASE COURSE IS PAVED TO ALLOW ALL OF THE LIFTS OF PAVEMENT THE PROJECT AREA. SEE LAYOUT PLANS FOR EXISTING AND PROPOSED PAVEMENT WIDTHS. AN ADDITIONAL QUANTITY OF ITEM 301.28 SUBBASE OF CRUSHED GRAVEL, FINE GRADED HAS BEEN INCLUDED TO CORRECT SUPERELEVATION AND GRADATION WITHIN THE RECLAIMED SECTION. AN ESTIMATED THICKNESS OF 2" HAS BEEN INCLUDED FOR THE ENTIRE RECLAIMED SURFACE AREA.
5. ALL EDGES OF PAVEMENT SHALL BE BACKED UP TO FULL HEIGHT WITH AGGREGATE SHOULDER MATERIAL AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.12 AGGREGATE SHOULDERS, RAP. ADDITIONAL MATERIAL REQUIRED AFTER THE COLD PLANE GRINDINGS HAVE BEEN USED WILL BE PAID FOR UNDER ITEM 4012.12 AGGREGATE SHOULDERS.
6. DITCHING MATERIAL OR EARTH BORROW SHALL BE USED FOR THE CONSTRUCTION OF MANUFACTURED TERMINAL FLARES WHICH SHALL BE CAPPED WITH AN ESTIMATED 3" DEPTH OF AGGREGATE SHOULDER MATERIAL AS DIRECTED BY THE RESIDENT ENGINEER. THE QUANTITIES INCLUDED REFLECT 5 TONS OF AGGREGATE SHOULDER MATERIAL AND 25 CUBIC YARDS OF DITCHING MATERIAL OR EARTH BORROW FOR EACH GUARDRAIL TERMINAL.
7. THE PROPOSED GUARDRAIL SHALL BE INSTALLED IN A LOCATION THAT MAXIMIZES THE DISTANCE FROM THE CENTER OF THE ROAD TO THE FACE OF GUARDRAIL. 3' OF BACKING IS REQUIRED BEHIND THE FACE OF GUARDRAIL WITH 6' POSTS. IF THIS CANNOT BE OBTAINED, THEN 8' POSTS SHALL BE USED. AN ESTIMATED QUANTITY OF 621.205 STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS HAS BEEN INCLUDED, AND SHALL BE USED AS DIRECTED BY THE RESIDENT ENGINEER.
8. ITEM 609.15 DUST AND ICE CONTROL WITH CALCIUM CHLORIDE HAS AN ESTIMATED QUANTITY TO BE USED ON SIDE ROAD APPROACHES WHICH WILL BE PAVED.
9. AN ESTIMATED QUANTITY OF ITEM 619.17 YIELDING MARKER POSTS HAS BEEN INCLUDED TO DELINEATE PIPE INLETS, PIPE OUTLETS AND DROP INLETS LOCATED OUTSIDE OF THE PAVEMENT SURFACE OR AS DIRECTED BY THE RESIDENT ENGINEER.
10. ESTIMATED QUANTITIES OF ITEM 613.10 STONE FILL, TYPE I; ITEM 649.31 GEOTEXTILE UNDER STONE FILL; AND ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I HAVE BEEN INCLUDED TO PROVIDE FOR REPAIR OF EXISTING STONE LINED DRAINAGE OUTLETS OR WASHOUTS LOCATED ALONG THE ROADWAY SIDE SLOPES AS DIRECTED BY THE RESIDENT ENGINEER.
11. WORK FOR REPAIR OF EXISTING WEATHERING STEEL BOX BEAM GUARDRAIL SHALL BE PERFORMED IN ACCORDANCE WITH GUARDRAIL, WEATHERING OF SECTION 900. PAYMENT WILL BE MADE UNDER ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING).



OVERLAY TYPICAL SECTION

VT. ROUTE 125 MIDDLEBURY STA 57+02.40 TO MIDDLEBURY STA 142+56.0



RECLAIMED STABILIZED TYPICAL SECTION

VT. ROUTE 125 MIDDLEBURY STA 142+56.0 TO MIDDLEBURY STA 216+84.96

TYPICAL ORDER OF OPERATIONS FOR RECLAIMED STABILIZED BASE WITH PORTLAND CEMENT PAVEMENT REHABILITATION

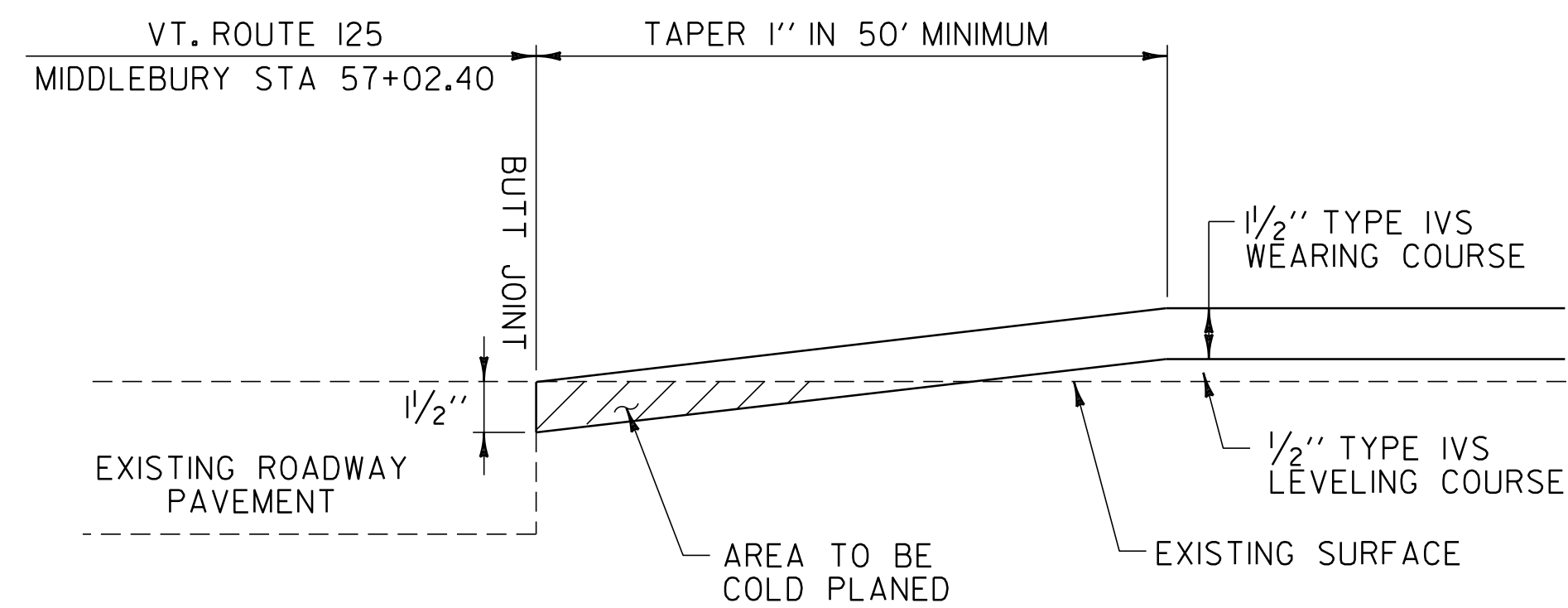
STEP	DESCRIPTION	PAYMENT ITEM(S)
1	REPAIR EXISTING SHOULDERS AS DIRECTED BY THE RESIDENT ENGINEER	ITEM 301.28 SUBBASE OF CRUSHED GRAVEL, FINE GRADED
2	RECLAIM EXISTING ROADBED 8" USING WATER FOR STABILIZATION AND COMPACTION	ITEM 310.20 RECLAIMED STABILIZED BASE, WATER USED FOR STABILIZATION SHALL BE CONSIDERED INCIDENTAL TO ITEM 310.20 PER SPECIFICATION AS DIRECTED BY THE RESIDENT ENGINEER
3	CORRECT SUPERELEVATION DEFICIENCIES	ITEM 301.28 SUBBASE OF CRUSHED GRAVEL, FINE GRADED
4	PLACE PORTLAND CEMENT ON ROADBED SURFACE AND RECLAIM ROADBED 8", CURE, MICROCRACK, AND APPLY FOG SEAL SURFACE TREATMENT	ITEM 900.675 SPECIAL PROVISION (RECLAIMED STABILIZED BASE, PORTLAND CEMENT), ITEM 900.680 SPECIAL PROVISION (PORTLAND CEMENT FOR BASE STABILIZATION) AND ITEM 900.683 SPECIAL PROVISION (FOG SEAL SURFACE TREATMENT)
5	PLACE 2 3/4" TYPE IIS BASE COURSE	ITEM 490.30 SUPERPAVE BITUMINOUS CONCRETE PAVEMENT
6	PLACE 1 1/2" TYPE IVS WEARING COURSE	ITEM 490.30 SUPERPAVE BITUMINOUS CONCRETE PAVEMENT

NOTE:

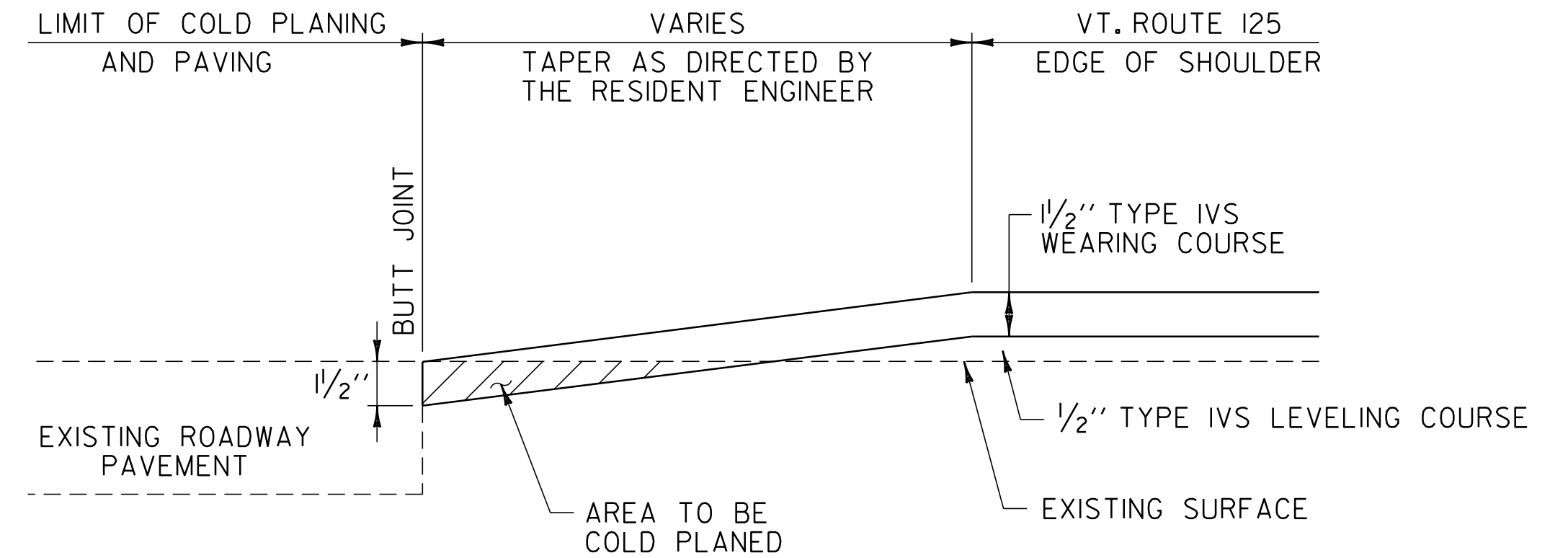
1. THIS LIST OF PROCEDURES FOR REHABILITATING THE EXISTING ROADBED IS PRESENTED FOR INFORMATIONAL PURPOSES ONLY AND SHALL NOT BE CONSIDERED ALL INCLUSIVE. THE CONTRACTOR SHALL PREPARE THEIR OWN SCHEDULE OF OPERATIONS TO COMPLETE THE PROPOSED WORK UTILIZING THE APPROPRIATE PAY ITEMS AS SHOWN IN THE PLANS AS DIRECTED BY THE RESIDENT ENGINEER.

NOT TO SCALE

PROJECT TYPICAL SHEET	PROJECT NAME: MIDDLEBURY	
	PROJECT NUMBER: STP 2627(1)	
	FILE NAME: p06c222.dgn	PLOT DATE: 18-SEP-2013 17:08
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
	DESIGNED BY: M.J.L.	CHECKED BY: D.W.E.
	IPARM FILE: p06c222+yp.i	SHEET 12 OF 114

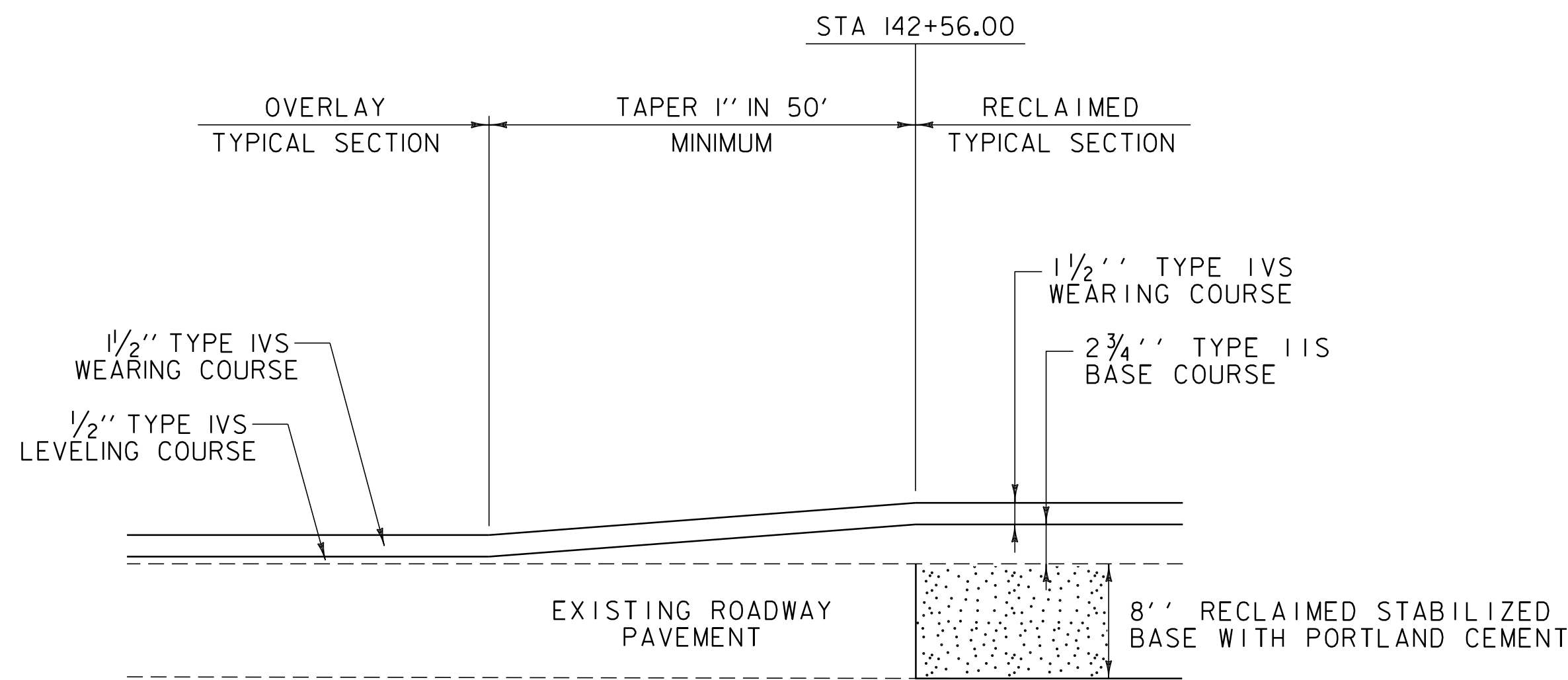


OVERLAY APPROACH AREA DETAIL
VT. ROUTE 125 MIDDLEBURY STA 57+02.40 - BEGIN PROJECT

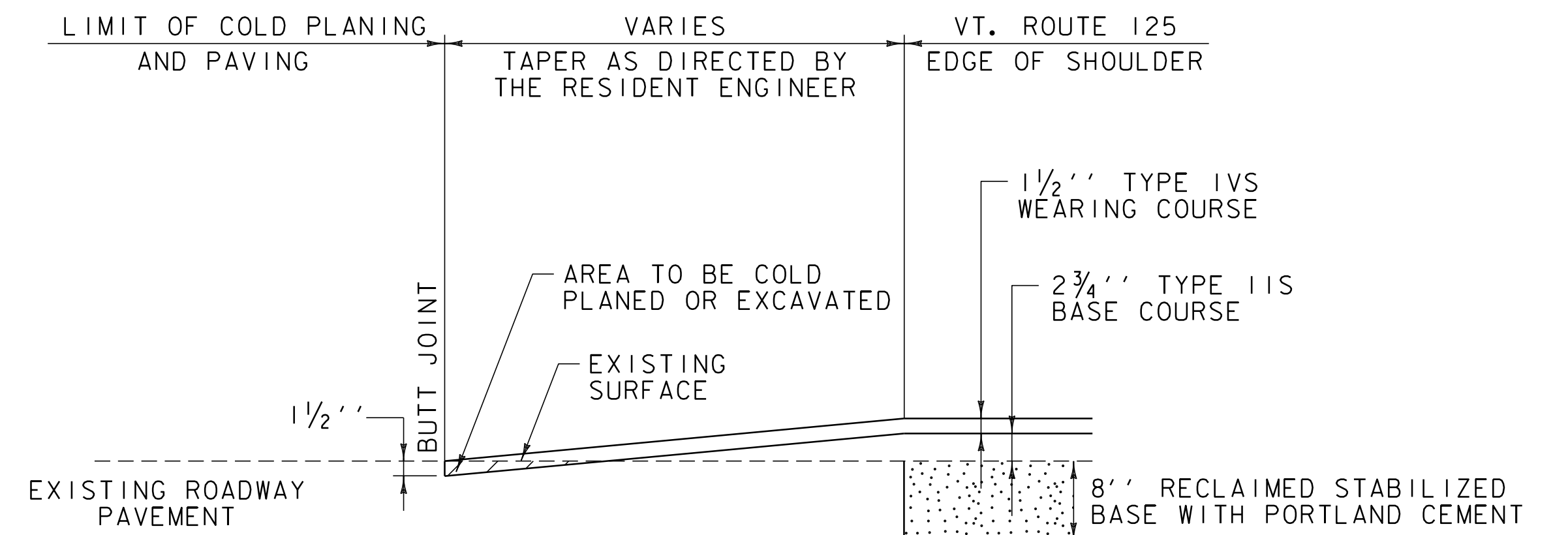


OVERLAYSIDE ROAD DETAIL

MIDDLEBURY: TH-42	STA 77+29.92	RT
VT. ROUTE 116	STA 83+63.52	LT
VT. ROUTE 116	STA 83+63.52	RT
TH-38	STA 85+90.56	LT
OSSIE ROAD	STA 91+50.24	RT
TH-29	STA 97+89.12	LT
TH-33	STA 110+56.32	RT
TH-27	STA 110+61.60	LT
TH-23	STA 123+02.40	RT
TH-35	STA 138+44.16	LT



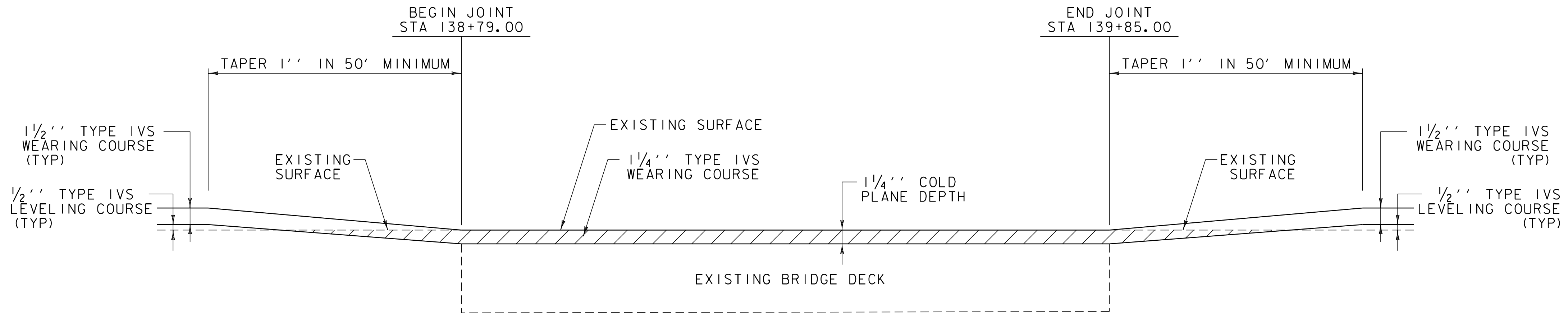
RECLAIMED TO OVERLAY TRANSITION DETAIL
VT. ROUTE 125 MIDDLEBURY STA 142+56.00



RECLAIMED SIDE ROAD DETAIL
MIDDLEBURY: TH-11 STA 169+59.36 RT

NOT TO SCALE

PAVING DETAILS SHEET #1	PROJECT NAME: MIDDLEBURY	
	PROJECT NUMBER: STP 2627(1)	
	FILE NAME: p06c222.dgn	PLOT DATE: 18-SEP-2013 17:08
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.L.	CHECKED BY: D.W.E.	
IPARM FILE: p06c222pdl.i	SHEET 13 OF 114	



BRIDGE TRANSITION AREA DETAIL
BR 13 VT. ROUTE 125 MIDDLEBURY STA 138+79.00 TO STA 139+85.00

SEEDING FORMULA
RATE: DOUBLE IF HYDROSEEDING

% WT.	LBS./A.	NAME	PUR %	GERM %
38	32	CREEPING RED FESCUE	98	90
29	24	SPARTAN HARD FESCUE	95	85
15	12	AZAY SHEEP'S FESCUE	95	87
15	12	ANNUAL RYE GRASS	95	90
3	--	INERTS	--	--
100.0	80			

MIDDLEBURY

EXISTING WIDTHS	
STATION	WIDTH
57+02.40	35'-4"
59+10.00	37'-0"
60+40.00	27'-2"
74+75.00	26'-5"
81+25.00	26'-5"
93+00.00	34'-7"
103+00.00	25'-5"
118+00.00	26'-0"
128+00.00	24'-1"
135+00.00	24'-4"
157+50.00	26'-8"
174+00.00	25'-5"
188+50.00	27'-11"
202+50.00	29'-6"

GENERAL NOTES

- SEED MIXTURE:**
SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS WEED SEED.
- SEED:**
TO BE APPLIED PER SEEDING FORMULA OR AS DIRECTED BY THE RESIDENT ENGINEER.
- FERTILIZER:**
FORMULA 10-20-10 TO BE USED WITH SEED APPLIED AT THE RATE OF 500 LBS/ACRE (HYDRO SEEDERS MAY USE 19-19-19 FORMULA).
- AGRICULTURAL LIMESTONE:**
TO BE APPLIED AT THE RATE OF 2 TONS/ACRE OR AS DIRECTED BY THE RESIDENT ENGINEER.
- TOPSOIL:**
TO BE USED WITH SEED AS INDICATED ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.

PROJECT PAVING LIMITS

TOWN & ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING TON	NOTES
MIDDLEBURY:						
VT. ROUTE 125	57+02.40	59+10.00	5'-0" - 12'-0" - 12'-0" - 8'-0"	1/2"	25	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	59+10.00	60+40.00	5'-0" - 12'-0" - 12'-0" - 8'-0"	1/2"	13	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	60+40.00	138+79.00	2'-0" - 11'-0" - 11'-0" - 2'-0"	1/2"	661	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	138+79.00	139+85.00	2'-0" - 11'-0" - 11'-0" - 2'-0"	1/4"	-	BR 13 COLD PLANE 1/4" & PAVE WITH 1/4" TYPE IVS
VT. ROUTE 125	139+85.00	142+56.00	2'-0" - 11'-0" - 11'-0" - 2'-0"	1/2"	23	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	142+56.00	216+84.96	3'-0" - 11'-0" - 11'-0" - 3'-0"	1/2"	-	RECLAIM 8", PAVE WITH 2 3/4" TYPE IIS & PAVE WITH 1/2" TYPE IVS

NOT TO SCALE

PAVING DETAILS SHEET #2	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:08
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.L.	SHEET 14 OF 14

QUANTITY SHEET 1

SUMMARY OF ESTIMATED QUANTITIES

BRIDGE	FULL C.E.	ROADWAY	QUANTITIES GRAND TOTAL	UNIT	ITEMS	ITEM NO.	ROUNDING
		350	350	CY	UNCLASSIFIED EXCAVATION	203.17	5.8
		275	275	CY	EARTH BORROW	203.30	5
		500	500	CY	TRENCH EXCAVATION OF EARTH	204.20	EST.
		1	1	CY	TRENCH EXCAVATION OF EARTH, EXPLORATORY (N.A.B.I.)	204.22	EST.
		2000	2000	SY	COLD PLANING, BITUMINOUS PAVEMENT	210.10	96
		4300	4300	TON	SUBBASE OF CRUSHED GRAVEL, FINE GRADED	301.28	56
		30	30	CY	SUBBASE OF DENSE GRADED CRUSHED STONE	301.35	2
		65	65	TON	SUBBASE, RAP	301.40	0.4
		23,500	23,500	SY	RECLAIMED STABILIZED BASE	310.20	388
		500	500	TON	AGGREGATE SHOULDERS	402.12	9
		150	150	TON	AGGREGATE SHOULDERS, RAP	402.13	-
		125	125	CWT	EMULSIFIED ASPHALT	404.65	19
		1	1	LU	PRICE ADJUSTMENT ASPHALT CEMENT (N.A.B.I.)	406.50	EST.
					BEGIN OPTION AA		
		9200	9200	TON	SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (0.0% RAP CONTENT)	490.30	145
		9200	9200	TON	SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (0.0% < RAP CONTENT < 15.0%)	490.30	145
		9200	9200	TON	SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (15.0% <= RAP CONTENT < 25.0%)	490.30	145
		9200	9200	TON	SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (25.0% <= RAP CONTENT <= 50.0%)	490.30	145
					END OPTION AA		
		1	1	LU	AIR VOIDS PAY ADJUSTMENT (N.A.B.I.)	490.31	EST.
		1	1	LU	MAT DENSITY PAY ADJUSTMENT (N.A.B.I.)	490.32	EST.
		1	1	LU	SURFACE TOLERANCE PAY ADJUSTMENT (N.A.B.I.)	490.33	EST.
		1	1	LU	LONGITUDINAL JOINT COMPACTION PAY ADJUSTMENT (N.A.B.I.)	490.34	EST.
50			50	LF	BRIDGE EXPANSION JOINT, ASPHALTIC PLUG	516.10	8.8
238			238	LF	REMOVAL OF EXISTING RAILING	525.10	0.4
238			238	LF	BRIDGE RAILING REPAIR, TYPE II	525.60	0.4
50			50	CF	RAPID SETTING CONCRETE REPAIR MATERIAL WITH COARSE AGGREGATE	580.20	EST.
		30	30	LF	15" CPEP	601.0910	-
		6	6	EA	REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES CLASS I	604.412	-
		5	5	EA	REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES CLASS II	604.415	-
		5	5	EA	REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES CLASS III	604.418	-
		1000	1000	LF	6 INCH UNDERDRAIN PIPE	605.10	EST.
		10	10	EA	UNDERDRAIN FLUSHING BASIN	605.95	EST.
		250	250	HR	POWER GRADER RENTAL	608.15	EST.
		75	75	HR	ALL PURPOSE EXCAVATOR RENTAL, TYPE I	608.25	EST.
		100	100	HR	POWER BROOM RENTAL, TYPE I	608.30	EST.
		100	100	HR	POWER BROOM RENTAL, TYPE II	608.31	EST.

DETAILED SUMMARY OF QUANTITIES

QUANTITIES	UNIT	ITEMS
		RECLAIMED STABILIZED BASE
23,112	SY	MAINLINE
388	SY	ROUNDING
23,500	SY	TOTAL
		SPECIAL PROVISION (RECLAIMED STABILIZED BASE, PORTLAND CEMENT)
23,112	SY	MAINLINE
388	SY	ROUNDING
23,500	SY	TOTAL
		COLD PLANING, BITUMINOUS PAVEMENT
306	SY	MAINLINE
1598	SY	SIDEROADS
96	SY	ROUNDING
2000	SY	TOTAL
		SUPERPAVE BITUMINOUS CONCRETE PAVEMENT
4212	TON	WEARING COURSE (TYPE IVS)
723	TON	LEVELING COURSE (TYPE IVS)
3708	TON	BASE COURSE (TYPE IIS)
199	TON	SIDE ROADS
198	TON	PULLOUTS
15	TON	TH-II INTERSECTION
145	TON	ROUNDING
9200	TON	TOTAL
		AGGREGATE SHOULDERS
35	TON	CAPPING FLARED GUARDRAIL END SECTIONS
456	TON	BACK UP SHOULDERS
9	TON	ROUNDING
500	TON	TOTAL
		AGGREGATE SHOULDERS, RAP
150	TON	BACK UP SHOULDERS
150	TON	TOTAL
		RAP MATERIAL GENERATED FROM PROJECT
215	TON	TOTAL RAP MATERIAL GENERATED
65	TON	SUBBASE, RAP
150	TON	AGGREGATE SHOULDERS, RAP

DETAILED SUMMARY OF QUANTITIES

QUANTITIES	UNIT	ITEMS

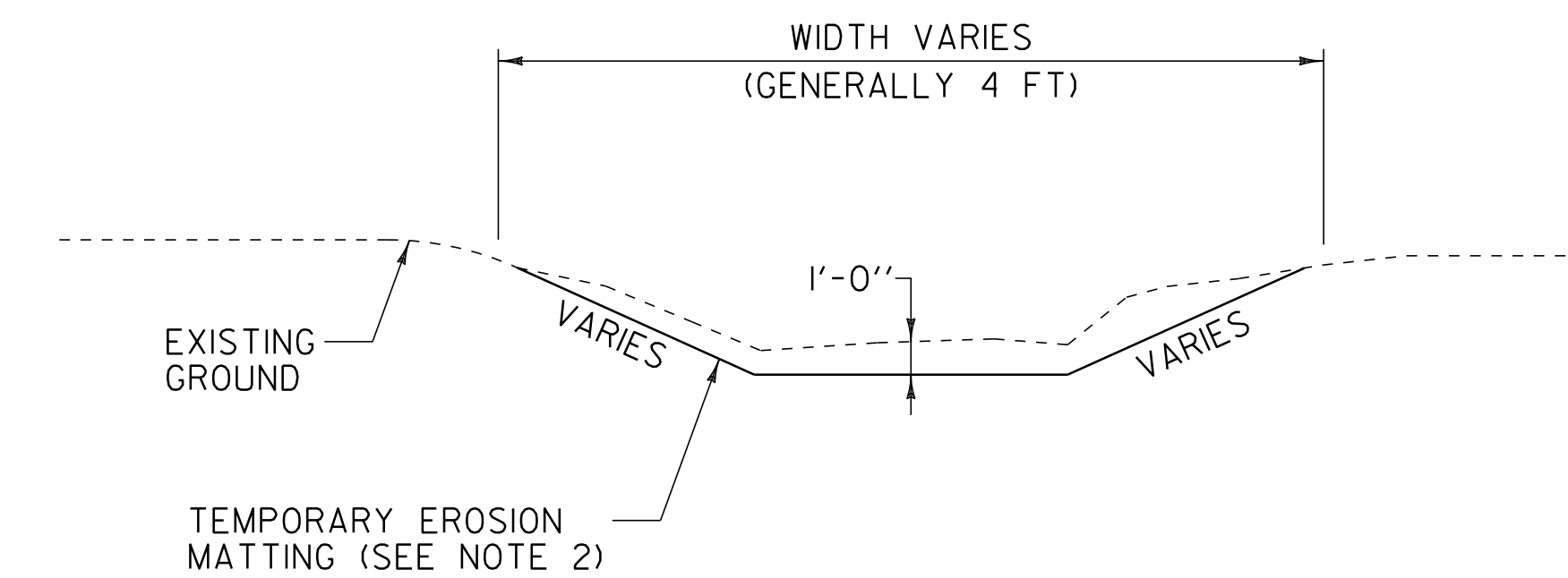
QUANTITY SHEET #1

PROJECT NAME:	MIDDLEBURY
PROJECT NUMBER:	STP 2627(1)
FILE NAME:	p06c222.dgn
PROJECT LEADER:	D.E.G.
DESIGNED BY:	M.J.L.
IPARM FILE:	p06c222quan1.i
PLOT DATE:	18-SEP-2013 17:08
DRAWN BY:	C.A.K.
CHECKED BY:	D.W.E.
SHEET	15 OF 114

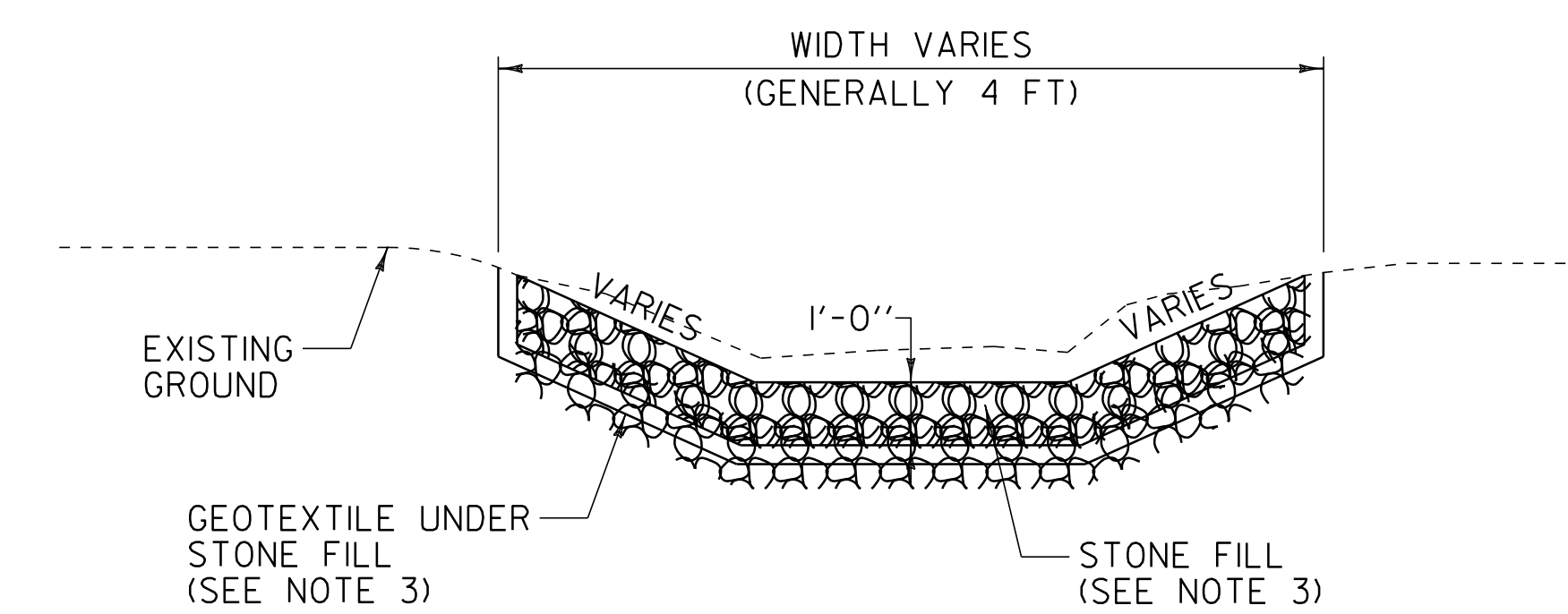
ITEM DETAIL SUMMARY SHEET

LOCATION			GUARDRAIL														SIDEWALK					MISCELLANEOUS							REMARKS					
BEGIN STATION	END STATION	POS.	203.30	402.12	402.13	621.20	621.21	621.50	621.60	621.79	621.80	653.20	900.620	900.640	900.640	900.640	900.640	203.17	301.40	618.10	618.11	618.30	204.20	601.0910	604.412	604.415	604.418	605.10		605.95	676.10			
			EARTH BORROW	AGG. SHLDRS	AGG. SHLDRS, RAP	S.B. G.R., GALV.	HD S.B. G.R., GALV.	MANUF. TERM. SECT. FLARED	ANCHOR FOR S.B. G.R.	ADJUST HEIGHT OF G.R.	REMOVAL & DISPOS. G.R.	TEMP. EROSION MATTING	MANUF. TERM. SECT., FLARED/WEATHERING	STEEL BEAM GUARDRAIL, WEATHERING	S.B. G.R., WEATH. W/8' POST	BOX BEAM REPAIRS, WEATH. (NEW MTRLS)	BOX BEAM REPAIRS, WEATH. (SLVGD MTRLS)	UNCLFD. EXCAV.	SUB-BASE, RAP	PCC SW, 5 INCH	PCC SW, 8 INCH	DETECT. WARNING SURFACE	TREN. EXCAV. EARTH	15 INCH CPEP	REHABING CLASS I	D'S CLASS II	CB'S CLASS III	OR MH'S		6 INCH UDRAIN PIPE	UDRAIN FLUSH BASIN	DELIN. W/STEEL POST		
			CY	TON	TON	LF	LF	EA	EA	LF	LF	SY	EA	LF	LF	LF	LF	CY	TON	SY	SY	SF	CY	LF	EA	EA	EA	LF		EA	EA			
MIDDLEBURY:																																		
57+02.40	216+84.96	LT/RT		456	150					1000																6	5	5				SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER		
83+36.0		LT																1.3	1.5	4.2												CONSTRUCT SIDEWALK RAMP, TYPE 6 WITH DETECTABLE WARNING SURFACE		
83+36.0		RT																1.3	1.5	4.2												CONSTRUCT SIDEWALK RAMP, TYPE 6 WITH DETECTABLE WARNING SURFACE		
83+82.0		LT																0.9	1.0	2.8												CONSTRUCT SIDEWALK RAMP, TYPE 6 WITH DETECTABLE WARNING SURFACE		
83+82.0		RT																0.9	1.0	2.8												CONSTRUCT SIDEWALK RAMP, TYPE 6 WITH DETECTABLE WARNING SURFACE		
85+63.0		LT																0.9	1.0	2.8		10.0										CONSTRUCT SIDEWALK RAMP, TYPE I WITH DETECTABLE WARNING SURFACE		
86+29 86+15.0		LT																0.9	1.0	2.8		10.0										CONSTRUCT SIDEWALK RAMP, TYPE I WITH DETECTABLE WARNING SURFACE		
91+10.0		RT																0.9	1.0	2.8		10.0										CONSTRUCT SIDEWALK RAMP, TYPE I WITH DETECTABLE WARNING SURFACE		
91+51.0 92+00	92+00	RT																0.9	1.0	2.8		10.0										CONSTRUCT SIDEWALK RAMP, TYPE I WITH DETECTABLE WARNING SURFACE		
91+58.0	93+33.0	RT																24.3	48.6		97.2											CONSTRUCT P.C.C. SIDEWALK		
97+71.0		LT																0.9	1.0	2.8		10.0										CONSTRUCT SIDEWALK RAMP, TYPE I WITH DETECTABLE WARNING SURFACE		
98+02.0		LT																0.9	1.0	2.8		10.0 20										CONSTRUCT SIDEWALK RAMP, TYPE I WITH DETECTABLE WARNING SURFACE		
110+38.0		RT																0.9	1.0	2.8		10.0 20										CONSTRUCT SIDEWALK RAMP, TYPE 6 WITH DETECTABLE WARNING SURFACE		
110+44.0		LT																0.9	1.0	2.8		10.0 20										CONSTRUCT SIDEWALK RAMP, TYPE 6 WITH DETECTABLE WARNING SURFACE		
110+69.0		RT																0.9	1.0	2.8		10.0										CONSTRUCT SIDEWALK RAMP, TYPE I WITH DETECTABLE WARNING SURFACE		
110+75 122+77.0		LT RT																0.9	1.0	2.8		10.0										CONSTRUCT SIDEWALK RAMP, TYPE I WITH DETECTABLE WARNING SURFACE		
123+31.0		RT																0.9	1.0	2.8		10.0										CONSTRUCT SIDEWALK RAMP, TYPE I WITH DETECTABLE WARNING SURFACE		
137+49.0	138+51.0	RT				64.5 77.0	32.5		1		96.0																					PROVIDE ANCHOR AT 137+55.0. SEE VAOT STANDARD G-ID. CONNECT NEW HD STEEL BEAM GR TO NEW BRIDGE RAIL AT 138+51.0. SEE SHEETS 47&48 FOR BR 13 DETAILS.		
138+40.0 138+40.00	138+82.0	LT				64.5 14.5	52.5 57.5		2		60.0																					CONNECT NEW HD STEEL BEAM GR TO NEW BRIDGE RAIL AT 138+82.0. SEE SHEETS 47&48 FOR BR 13 DETAILS.		
139+82.3	140+51.0	RT	25	5			41.3	1			60.0	25																			1	CONNECT NEW HD STEEL BEAM GR TO NEW BRIDGE RAIL AT 139+82.3. SEE SHEETS 47&48 FOR BR 13 DETAILS.		
139+85.0	216+84.96	LT/RT																					500 68.4										INSTALL NEW MTS, FLARED FROM 140+13.5 TO 140+51.0 SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER	
139+88.3	140+57.0	LT	25	5			32.5 41.3	1			60.0	25																			1	CONNECT NEW HD STEEL BEAM GR TO NEW BRIDGE RAIL AT 139+88.3. SEE SHEETS 47&48 FOR BR 13 DETAILS. INSTALL NEW MTS, FLARED FROM 140+19.5 TO 140+57.0.		
145+88.0 56	155+38.0 5.50	LT	25	5		800 937.5		1	1 -2		948.0	25																			1	PROVIDE ANCHOR AT 145+88.0 & 146+48.0. SEE SHEET 49 FOR BURIED GR ENDS SECTION INTO BACKSLOPES. INSTALL NEW MTS, FLARED FROM 155+00.5 TO 155+38.0		
173+75.0 71.5	177+70.0 84.0	RT	50	10		337.50 350.0		2			415.0	50																			2	INSTALL NEW MTS, FLARED FROM 173+45.0 TO 173+82.5 & 177+32.5 TO 177+70.0.		
178+13.5	216+84.96 208+26.0	LT LT	50	10							3957.5 3007.0		1 -2	1	3462.5 2150.0	462.5 787.5																2	INSTALL NEW MTS, FLARED/WEATHERING FROM 178+13.5 TO 178+51.0 & 207+88.5 TO 208+26.0.	
208+30	216+84.96	LT																															SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER.	
SUBTOTALS			175	491	150	1379.5	172.6	5	5	1000	4646	175	2	1	2150.0	787.5	100	100	38.6	64.6	44.8	97.2	170.0	500	30	6	5	5	1000	10	7	<p>•• REVISED 02-16-10 BY SEH ADDED ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING) (NEW MATERIAL) AND ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING) (SALVAGED MATERIAL). REMOVED ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). ADDED ITEM 621.79 ADJUST HEIGHT OF GUARDRAIL.</p>		
ROUNDING			-	9	-	20.5	2.4	-	-	-	54	-	-	-	50.0	12.5	-	-	-	0.4	0.2	2.8	-	-	-	-	-	-	-	-	-			
TOTALS			175	500	150	1400	175	5	5	1000	4700	175	2	1	2200	800	100	100	38.6	65	45	100	170	500	30	6	5	5	1000	10	7			
94+50		RT																															<p style="text-align: center;">ITEM DETAIL SUMMARY SHEET</p>	PROJECT NAME: MIDDLEBURY
94+50		LT																																PROJECT NUMBER: STP 2627(II)
																											PROJECT LEADER: D.E.G.	DESIGNED BY: M.J.L.	IPARM FILE: p06c222ids.i	PLOT DATE: 18-SEP-2013 17:08	DRAWN BY: C.A.K.	CHECKED BY: D.W.E.	SHEET 18 OF 114	

LOCATION				FEET OF DITCHING				MISC. ITEMS					REMARKS	LOCATION				FEET OF DITCHING				MISC. ITEMS					REMARKS							
SITE	STATION	STATION	POS.	PERCENT GRADE				STONE FILL, TYPE I	STONE FILL, TYPE II	GEOT. UNDER STONE FILL	SEED	TEMP. EROS. MATT.		SITE	STATION	STATION	POS.	PERCENT GRADE				STONE FILL, TYPE I	STONE FILL, TYPE II	GEOT. UNDER STONE FILL	SEED	TEMP. EROS. MATT.								
				0-1	1-2.5	2.5-10	>10											0-1	1-2.5	2.5-10	>10													
				613.10	613.11	649.31	651.15											653.20	613.10	613.11	649.31							651.15	653.20					
CY				CY				SY	LB	SY	CY				CY				SY	LB	SY													
VT. ROUTE 125																																		
MIDDLEBURY:																																		
1	60+90	64+70	RT		380																													
2	75+00	83+00	LT		800																													
3	141+00	154+10	RT				1310	194		873																								
4	191+40	216+85	RT				2545		377	1697																								
ROUNDINGS					-	-	-	6	23	30	-	-																						
TOTALS					1180	1310	2545	200	400	2600	9	525																						



DITCH DETAIL



STONE DITCH DETAIL

NOTES:

- PIPE INLET AND OUTLET AREAS AND DITCH CLEANING THROUGHOUT THE PROJECT SHALL BE PERFORMED AT LOCATIONS IDENTIFIED ON THIS SHEET AND AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT WILL BE UNDER THE APPLICABLE EQUIPMENT RENTAL ITEM(S).
- ESTIMATED QUANTITIES OF TEMPORARY EROSION MATTING AND SEED HAVE BEEN INCLUDED AS NEEDED OR AS DIRECTED BY THE RESIDENT ENGINEER.
- ESTIMATED QUANTITIES OF STONE FILL TYPE I, STONE FILL TYPE II, AND GEOTEXTILE UNDER STONE FILL HAVE BEEN INCLUDED AS NEEDED. STONE FILL TYPE I SHALL BE USED IN ALL DITCHES WITH A GRADE BETWEEN 2.5 AND 10 PERCENT AND STONE FILL TYPE II SHALL BE USED IN ALL DITCHES WITH A GRADE OVER 10 PERCENT OR AS DIRECTED BY THE RESIDENT ENGINEER.

NOT TO SCALE

DITCH CLEANING DETAIL SHEET	PROJECT NAME: MIDDLEBURY
	PROJECT NUMBER: STP 2627(1)
	FILE NAME: p06c222.dgn
	PROJECT LEADER: D.E.G.
	DESIGNED BY: M.J.L.
IPARM FILE: p06c222dcd.i	PLOT DATE: 18-SEP-2013 17:08
	DRAWN BY: C.A.K.
	CHECKED BY: D.W.E.
	SHEET 19 OF 114

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND
 RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 57+02.40 - STA 67+00.00 EDGE LINE LT & RT

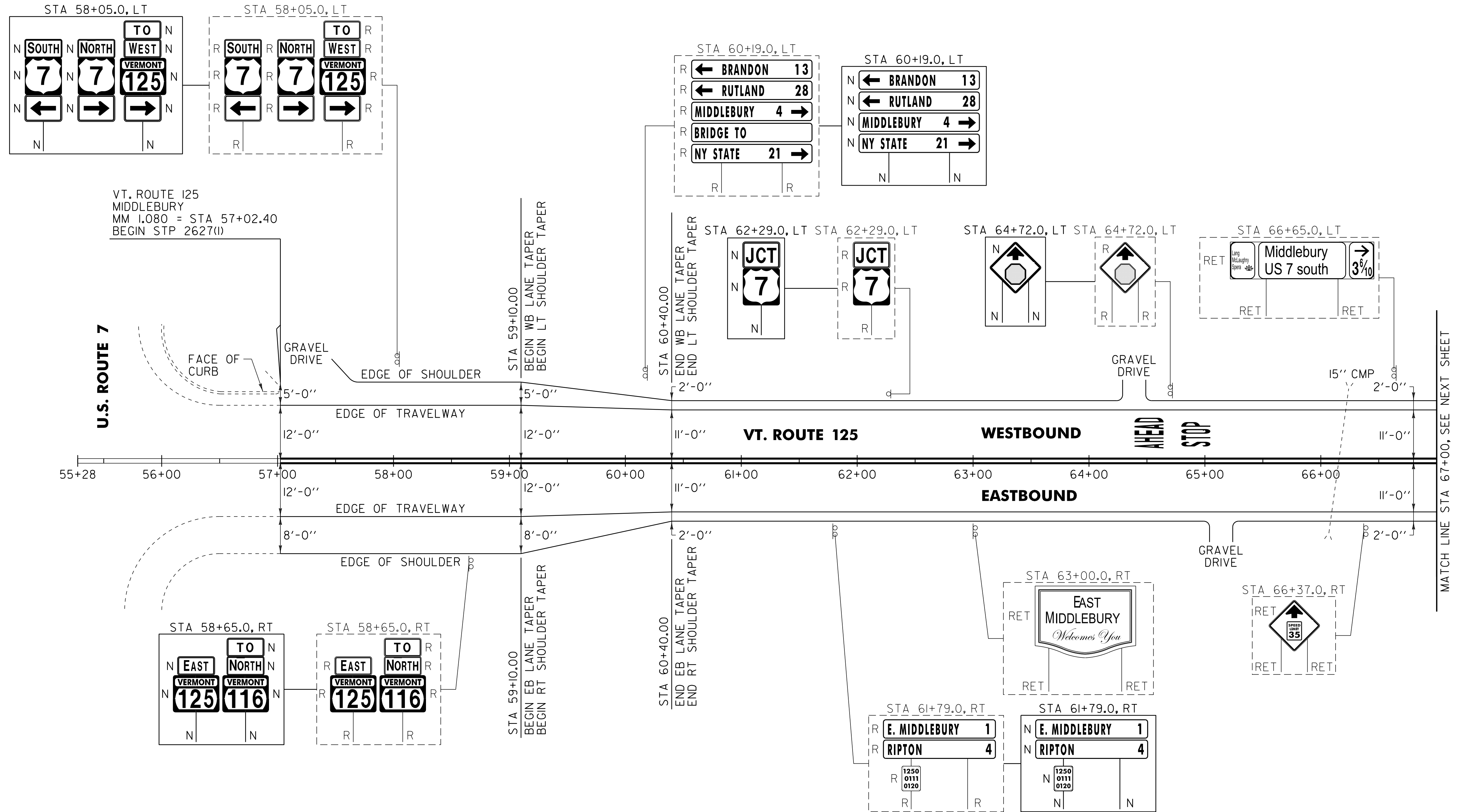
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR
 TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 57+02.40 - STA 67+00.00 LT C RT
 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND
 RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 57+02.40 - STA 67+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR
 TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 57+02.40 - STA 67+00.00 LT C RT
 S - S

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 MIDDLEBURY:
 STA 64+51.80 LT, "A,H,E,A,D" (5 EA)
 STA 64+92.20 LT, "S,T,O,P" (4 EA)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 MIDDLEBURY:
 STA 64+51.80 LT, "A,H,E,A,D" (5 EA)
 STA 64+92.20 LT, "S,T,O,P" (4 EA)



SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #1	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:08
	PROJECT NUMBER: STP 2627(I)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 20 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222101.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 67+00.00 - STA 81+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 67+00.00 - STA 81+00.00 LT C RT
 STA 77+29.92 DOUBLE SOLID RT, TH-42 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 67+00.00 - STA 81+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 67+00.00 - STA 81+00.00 LT C RT
 STA 77+29.92 DOUBLE SOLID RT, TH-42 S - S

646.442 DURABLE 8 INCH WHITE LINE, THERMOPLASTIC
 MIDDLEBURY:
 STA 67+80.0 - STA 67+92.0 FIRE HYDRANT, NO PARKING, RT
 STA 75+90.0 - STA 76+02.0 FIRE HYDRANT, NO PARKING, RT

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 MIDDLEBURY:
 STA 77+29.92 RT, TH-42

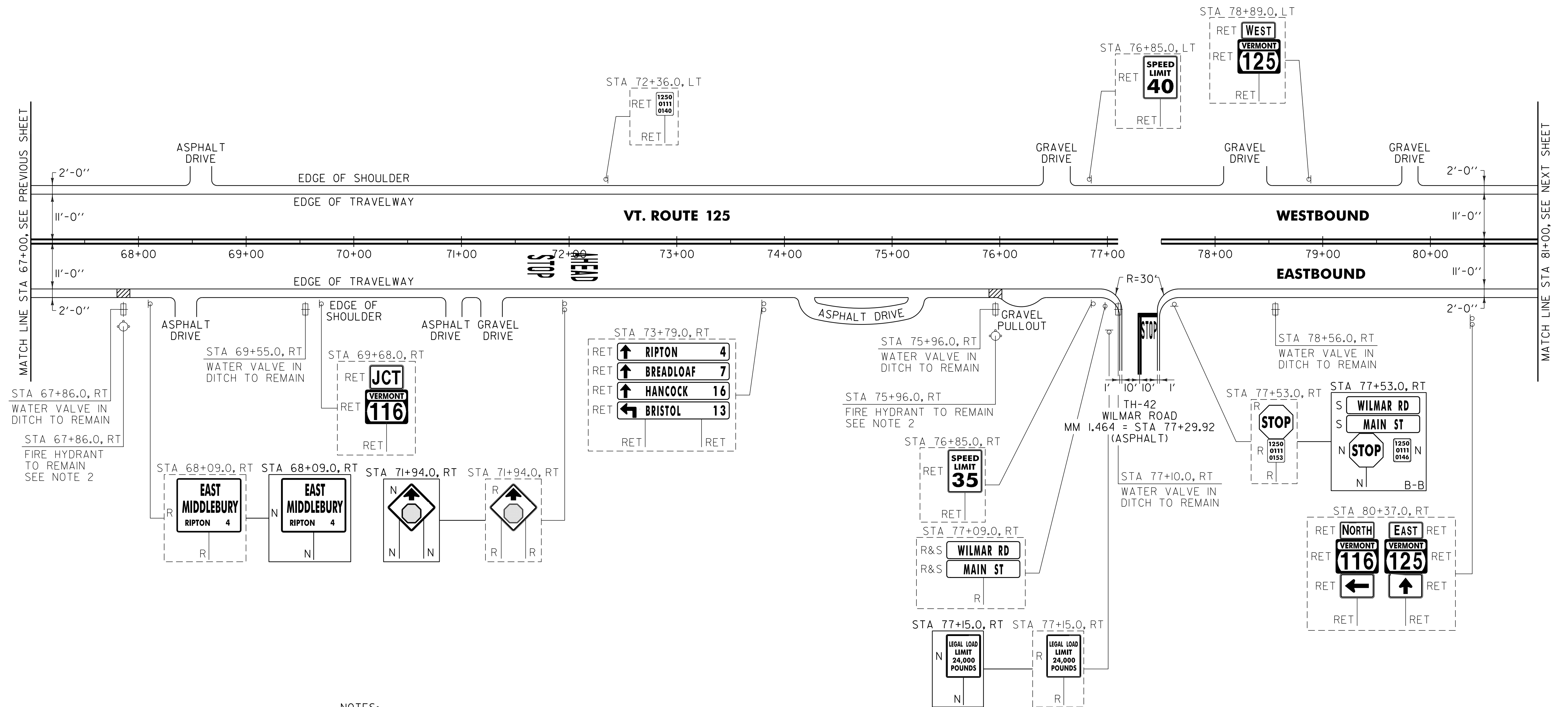
646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 MIDDLEBURY:
 STA 77+29.92 RT, TH-42

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 MIDDLEBURY:
 STA 71+73.80 RT, "S,T,O,P" (4 EA)
 STA 72+14.20 RT, "A,H,E,A,D" (5 EA)
 STA 77+29.92 RT, TH-42 "S,T,O,P" (4 EA)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 MIDDLEBURY:
 STA 71+73.80 RT, "S,T,O,P" (4 EA)
 STA 72+14.20 RT, "A,H,E,A,D" (5 EA)
 STA 77+29.92 RT, TH-42 "S,T,O,P" (4 EA)

675.50 REMOVING SIGNS
 AS SHOWN - 7

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 2



- NOTES:
1. PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
 2. SEE SHEET 9 FOR FIRE HYDRANT PAVEMENT MARKING DETAIL.
 3. PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE 1.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #2	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:08
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 21 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222102.1	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 81+00.00 - STA 86+50.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 81+00.00 - STA 86+50.00 S - S
 STA 83+63.52 DOUBLE SOLID LT, VT. ROUTE 116
 STA 83+63.52 DOUBLE SOLID RT, VT. ROUTE 116
 STA 85+90.56 DOUBLE SOLID LT, TH-38

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 81+00.00 - STA 86+50.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 81+00.00 - STA 86+50.00 S - S
 STA 83+63.52 DOUBLE SOLID LT, VT. ROUTE 116
 STA 83+63.52 DOUBLE SOLID RT, VT. ROUTE 116
 STA 85+90.56 DOUBLE SOLID LT, TH-38

646.502 DURABLE CROSSWALK MARKING, THERMOPLASTIC
 MIDDLEBURY:
 STA 83+39.00 LT & RT
 STA 83+63.52 LT, VT ROUTE 116
 STA 83+63.52 RT, VT ROUTE 116
 STA 83+87.00 LT & RT
 STA 85+90.56 LT, TH-38

646.702 TEMPORARY CROSSWALK MARKING, PAINT
 MIDDLEBURY:
 STA 83+39.00 LT & RT
 STA 83+63.52 LT, VT ROUTE 116
 STA 83+63.52 RT, VT ROUTE 116
 STA 83+87.00 LT & RT
 STA 85+90.56 LT, TH-38

675.50 REMOVING SIGNS AS SHOWN - 19
 675.60 ERECTING SALVAGED SIGNS AS SHOWN - 6

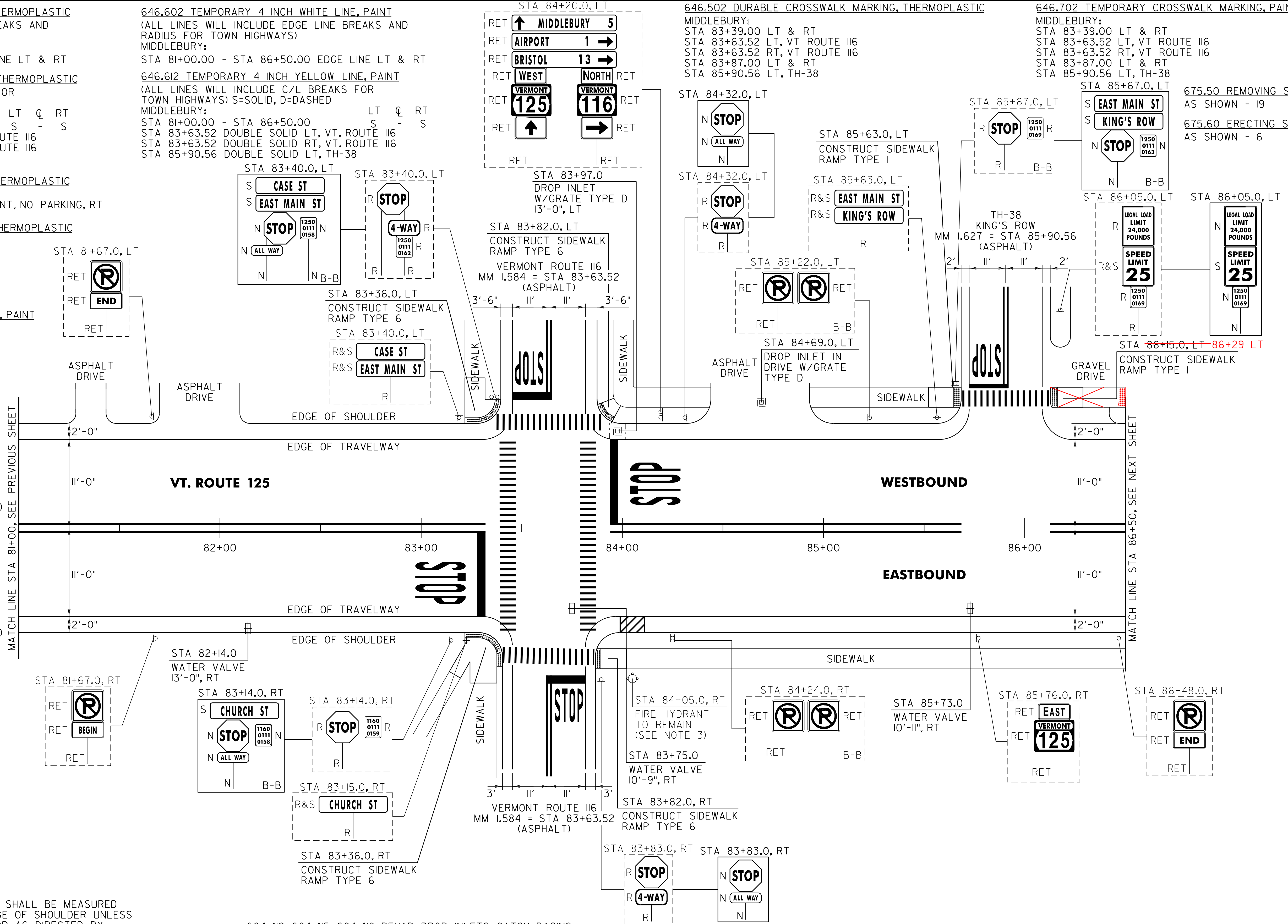
646.442 DURABLE 8 INCH WHITE LINE, THERMOPLASTIC
 MIDDLEBURY:
 STA 83+99.0 - STA 84+11.0 FIRE HYDRANT, NO PARKING, RT

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 MIDDLEBURY:
 STA 83+30.00 RT
 STA 83+63.52 LT, VT. ROUTE 116
 STA 83+63.52 RT, VT. ROUTE 116
 STA 83+96.00 LT
 STA 85+90.56 LT, TH-38

646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 MIDDLEBURY:
 STA 83+30.00 RT
 STA 83+63.52 LT, VT. ROUTE 116
 STA 83+63.52 RT, VT. ROUTE 116
 STA 83+96.00 LT
 STA 85+90.56 LT, TH-38

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 MIDDLEBURY:
 STA 83+22.00 RT, "S,T,O,P" (4 EA)
 STA 83+63.52 LT, VT. ROUTE 116 "S,T,O,P" (4 EA)
 STA 83+63.52 RT, VT. ROUTE 116 "S,T,O,P" (4 EA)
 STA 84+04.00 LT, "S,T,O,P" (4 EA)
 STA 85+90.56 LT, TH-38 "S,T,O,P" (4 EA)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 MIDDLEBURY:
 STA 83+22.00 RT, "S,T,O,P" (4 EA)
 STA 83+63.52 LT, VT. ROUTE 116 "S,T,O,P" (4 EA)
 STA 83+63.52 RT, VT. ROUTE 116 "S,T,O,P" (4 EA)
 STA 84+04.00 LT, "S,T,O,P" (4 EA)
 STA 85+90.56 LT, TH-38 "S,T,O,P" (4 EA)



NOTES:

- PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.
- SEE SHEET 9 FOR FIRE HYDRANT PAVEMENT MARKING DETAIL.

~~604.412, 604.415, 604.418~~ REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, II, III (SEE NOTE 2)
 MIDDLEBURY:
 STA 83+97.0 LT
~~STA 84+69.0 LT - DID NOT TOUCH~~

629.20 ADJUST ELEVATION OF VALVE BOX
 MIDDLEBURY:
 STA 82+14.0 RT
 STA 83+75.0 RT
 STA 85+73.0 RT

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

PROJECT LAYOUT SHEET #3	NOT TO SCALE	
	PROJECT NAME: MIDDLEBURY	PROJECT NUMBER: STP 2627(1)
	FILE NAME: p06c222.dgn	PLOT DATE: 18-SEP-2013 17:09
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.L.	CHECKED BY: D.W.E.	
IPARM FILE: p06c222103.1	SHEET 22 OF 114	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 86+50.00 - STA 100+50.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 86+50.00 - STA 100+50.00 S - S
 STA 91+50.24 DOUBLE SOLID RT, OSSIE ROAD
 STA 97+89.12 DOUBLE SOLID LT, TH-29

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 86+50.00 - STA 100+50.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 86+50.00 - STA 100+50.00 S - S
 STA 91+50.24 DOUBLE SOLID RT, OSSIE ROAD
 STA 97+89.12 DOUBLE SOLID LT, TH-29

646.442 DURABLE 8 INCH WHITE LINE, THERMOPLASTIC
 MIDDLEBURY:
 STA 92+18.0 - STA 92+30.0 FIRE HYDRANT, NO PARKING, RT
 STA 100+29.0 - STA 100+41.0 FIRE HYDRANT, NO PARKING, RT

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 MIDDLEBURY:
 STA 91+50.24 RT, OSSIE ROAD
 STA 97+89.12 LT, TH-29

646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 MIDDLEBURY:
 STA 91+50.24 RT, OSSIE ROAD
 STA 97+89.12 LT, TH-29

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 MIDDLEBURY:
 STA 90+41.80 LT, "A,H,E,A,D" (5 EA)
 STA 90+82.20 LT, "S,T,O,P" (4 EA)
 STA 91+50.24 RT, OSSIE ROAD "S,T,O,P" (4 EA)
 STA 97+89.12 LT, TH-29 "S,T,O,P" (4 EA)

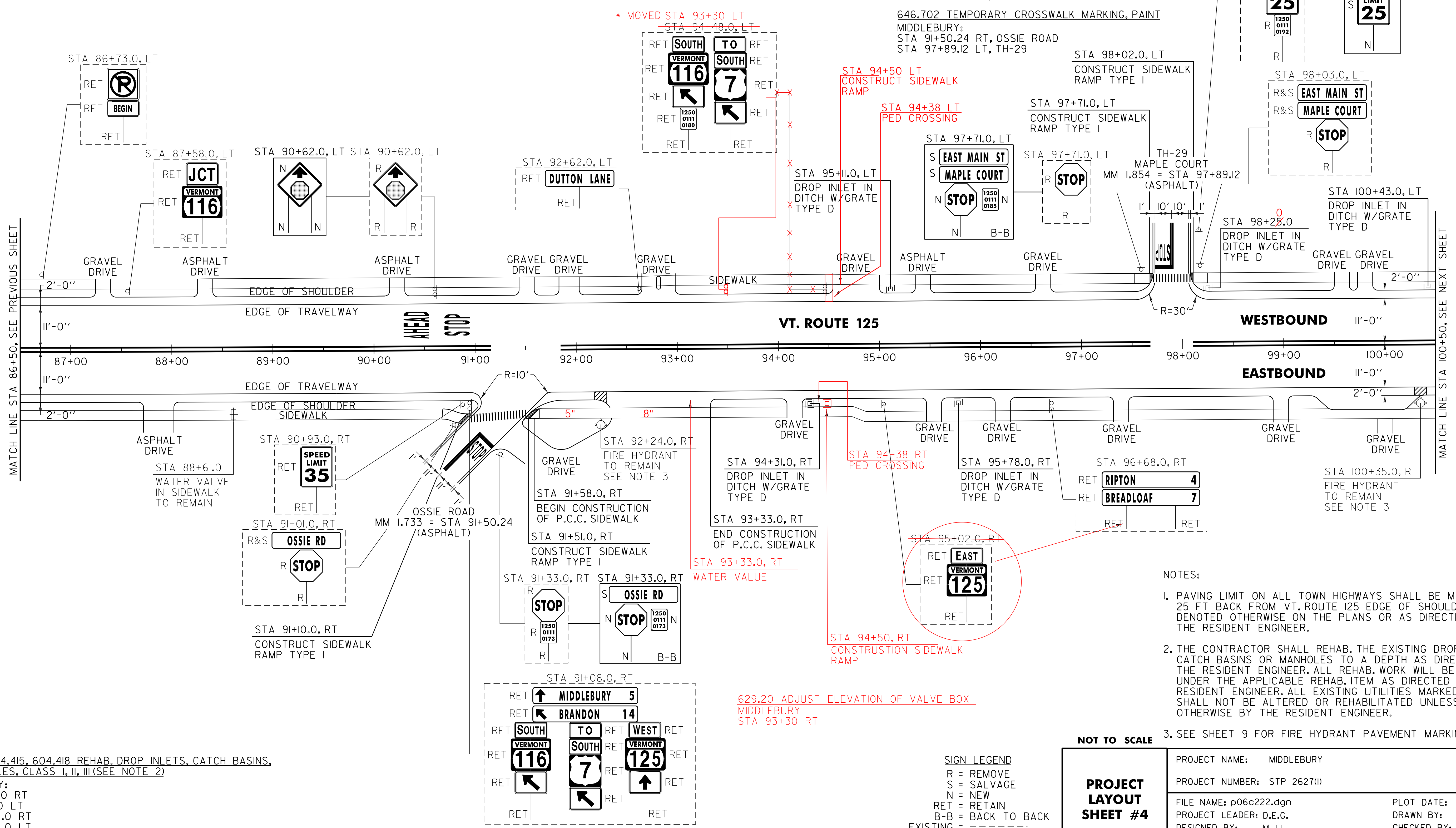
646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 MIDDLEBURY:
 STA 90+41.80 LT, "A,H,E,A,D" (5 EA)
 STA 90+82.20 LT, "S,T,O,P" (4 EA)
 STA 91+50.24 RT, OSSIE ROAD "S,T,O,P" (4 EA)
 STA 97+89.12 LT, TH-29 "S,T,O,P" (4 EA)

646.502 DURABLE CROSSWALK MARKING, THERMOPLASTIC
 MIDDLEBURY:
 STA 91+50.24 RT, OSSIE ROAD
 STA 97+89.12 LT, TH-29

646.702 TEMPORARY CROSSWALK MARKING, PAINT
 MIDDLEBURY:
 STA 91+50.24 RT, OSSIE ROAD
 STA 97+89.12 LT, TH-29

675.50 REMOVING SIGNS AS SHOWN - 12

675.60 ERECTING SALVAGED SIGNS AS SHOWN - 4



604.412, 604.415, 604.418 REHAB, DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, II, III (SEE NOTE 2)
 MIDDLEBURY:
 STA 94+31.0 RT
 STA 95+11.0 LT
 STA 95+78.0 RT
 STA 98+25.0 LT
 STA 100+43.0 LT

- NOTES:
- PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.
 - SEE SHEET 9 FOR FIRE HYDRANT PAVEMENT MARKING DETAIL.

NOT TO SCALE

PROJECT LAYOUT SHEET #4	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 23 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222104.1	

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = _____
 NEW = _____

629.20 ADJUST ELEVATION OF VALVE BOX
 MIDDLEBURY
 STA 93+30 RT

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 100+50.00 - STA 114+50.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY: LT C RT
 STA 100+50.00 - STA 114+50.00 S - S
 STA 110+56.32 DOUBLE SOLID RT, TH-33
 STA 110+61.60 DOUBLE SOLID LT, TH-27

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 100+50.00 - STA 114+50.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY: LT C RT
 STA 100+50.00 - STA 114+50.00 S - S
 STA 110+56.32 DOUBLE SOLID RT, TH-33
 STA 110+61.60 DOUBLE SOLID LT, TH-27

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 MIDDLEBURY:
 STA 110+56.32 RT, TH-33
 STA 110+61.60 LT, TH-27

646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 MIDDLEBURY:
 STA 110+56.32 RT, TH-33
 STA 110+61.60 LT, TH-27

646.442 DURABLE 8 INCH WHITE LINE, THERMOPLASTIC
 MIDDLEBURY:
 STA 110+06.0 - STA 110+18.0 FIRE HYDRANT, NO PARKING, RT

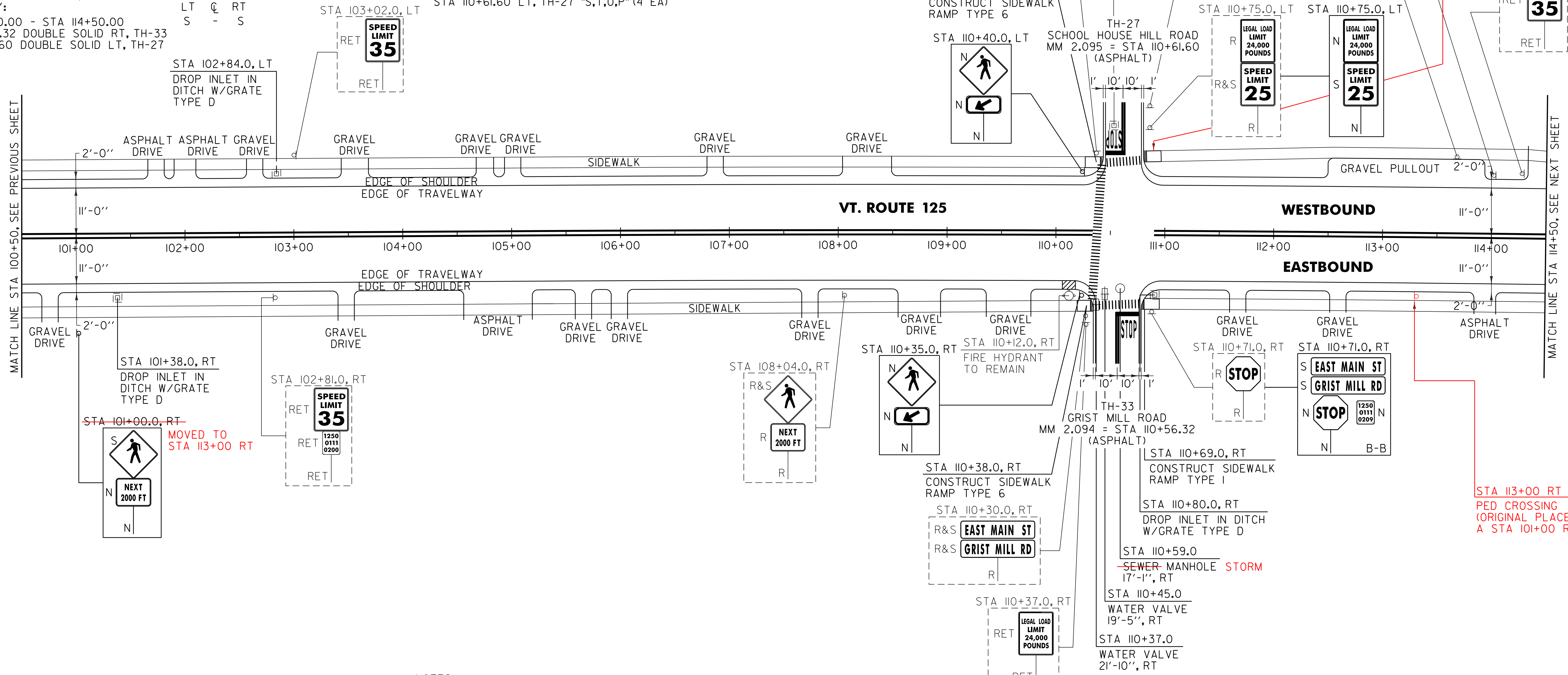
646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 MIDDLEBURY:
 STA 110+56.32 RT, TH-33 "S,T,O,P" (4 EA)
 STA 110+61.60 LT, TH-27 "S,T,O,P" (4 EA)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 MIDDLEBURY:
 STA 110+56.32 RT, TH-33 "S,T,O,P" (4 EA)
 STA 110+61.60 LT, TH-27 "S,T,O,P" (4 EA)

646.502 DURABLE CROSSWALK MARKING, THERMOPLASTIC
 MIDDLEBURY:
 STA 110+41.00 LT & RT
 STA 110+56.32 RT, TH-33
 STA 110+61.60 LT, TH-27

646.702 TEMPORARY CROSSWALK MARKING, PAINT
 MIDDLEBURY:
 STA 110+41.00 LT & RT
 STA 110+56.32 RT, TH-33
 STA 110+61.60 LT, TH-27

675.50 REMOVING SIGNS AS SHOWN - 10
 675.60 ERECTING SALVAGED SIGNS AS SHOWN - 6



604.412, 604.415, 604.418 REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, II, III (SEE NOTE 2)
 MIDDLEBURY:
~~STA 101+38.0 RT - CLEANED OUT ONLY~~
 STA 102+84.0 LT
~~STA 110+80.0 RT - CLEANED OUT ONLY~~
~~604.42 CHANGING ELEVATION OF SEWER MANHOLE -- TO BE DONE BY OTHERS~~
 MIDDLEBURY:
 STA 110+59.0 RT MANHOLE STORM
 629.20 ADJUST ELEVATION OF VALVE BOX
 MIDDLEBURY:
 STA 110+37.0 RT
 STA 110+45.0 RT

- NOTES:
- PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.
 - SEE SHEET 9 FOR FIRE HYDRANT PAVEMENT MARKING DETAIL.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #5	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.L.	SHEET 24 OF 114

IPARM FILE: p06c222105.1

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 114+50.00 - STA 128+50.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 114+50.00 - STA 128+50.00 LT C RT
 STA 123+02.40 DOUBLE SOLID RT, TH-23 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 114+50.00 - STA 128+50.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 114+50.00 - STA 128+50.00 LT C RT
 STA 123+02.40 DOUBLE SOLID RT, TH-23 S - S

646.442 DURABLE 8 INCH WHITE LINE, THERMOPLASTIC
 MIDDLEBURY:
 STA 117+22.0 - STA 117+34.0 FIRE HYDRANT, NO PARKING, RT
 STA 122+54.0 - STA 122+66.0 FIRE HYDRANT, NO PARKING, RT
 STA 128+08.0 - STA 128+20.0 FIRE HYDRANT, NO PARKING, RT

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 MIDDLEBURY:
 STA 123+02.40 RT, TH-23

646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 MIDDLEBURY:
 STA 123+02.40 RT, TH-23

646.502 DURABLE CROSSWALK MARKING, THERMOPLASTIC
 MIDDLEBURY:
 STA 123+02.40 RT, TH-23

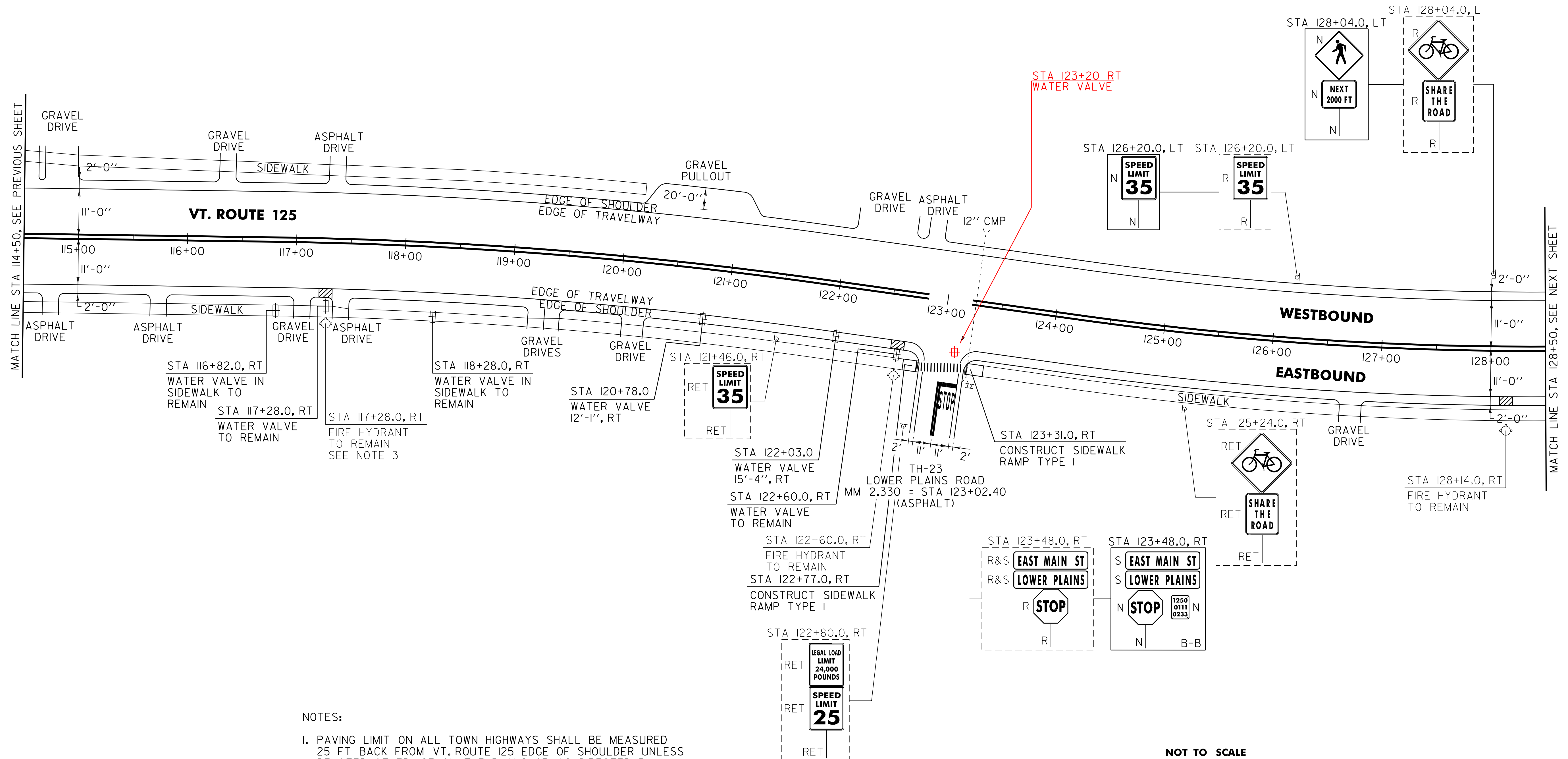
646.702 TEMPORARY CROSSWALK MARKING, PAINT
 MIDDLEBURY:
 STA 123+02.40 RT, TH-23

675.50 REMOVING SIGNS
 AS SHOWN - 6

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 2

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 MIDDLEBURY:
 STA 123+02.40 RT, TH-23 "S,T,O,P" (4 EA)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 MIDDLEBURY:
 STA 123+02.40 RT, TH-23 "S,T,O,P" (4 EA)



NOTES:

- PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
- PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATION RENTAL, TYPE I.
- SEE SHEET 9 FOR FIRE HYDRANT PAVEMENT MARKING DETAIL.

629.20 ADJUST ELEVATION OF VALVE BOX
 MIDDLEBURY:
 STA 120+78.0 RT
~~STA 122+03.0 RT - DID NOT FIND~~
 STA 123+20 RT

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = _____
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #6	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 25 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222106.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 128+50.00 - STA 143+50.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY: LT C RT
 STA 128+50.00 - STA 143+50.00 S - S
 STA 138+44.16 DOUBLE SOLID LT, TH-35

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 128+50.00 - STA 143+50.00 EDGE LINE LT & RT

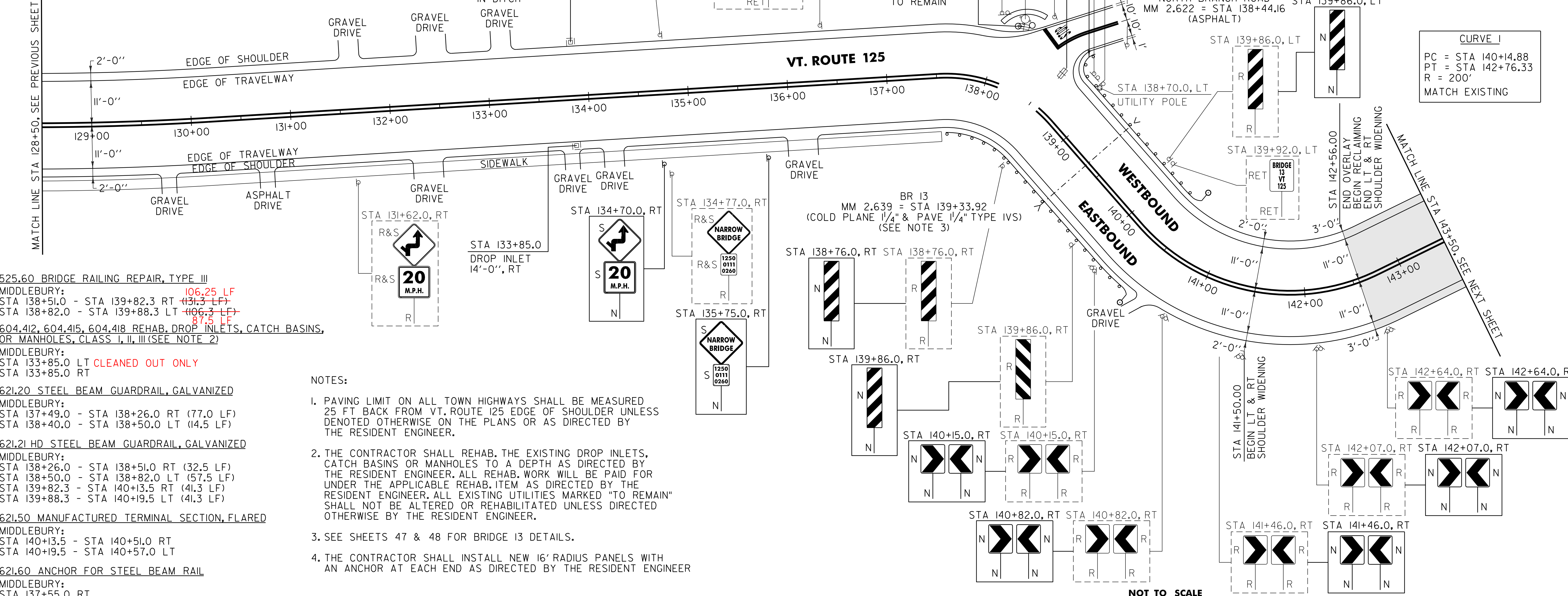
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY: LT C RT
 STA 128+50.00 - STA 143+50.00 S - S
 STA 138+44.16 DOUBLE SOLID LT, TH-35

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 MIDDLEBURY:
 STA 138+44.16 LT, TH-35

646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 MIDDLEBURY:
 STA 138+44.16 LT, TH-35

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 MIDDLEBURY:
 STA 138+44.16 LT, TH-35 "S,T,O,P" (4 EA)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 MIDDLEBURY:
 STA 138+44.16 LT, TH-35 "S,T,O,P" (4 EA)



525.60 BRIDGE RAILING REPAIR, TYPE III
 MIDDLEBURY:
 STA 138+51.0 - STA 139+82.3 RT (106.25 LF)
 STA 138+82.0 - STA 139+88.3 LT (106.3 LF)
 STA 139+88.3 - STA 140+19.5 LT (87.5 LF)

604.412, 604.415, 604.418 REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, II, III (SEE NOTE 2)
 MIDDLEBURY:
 STA 133+85.0 LT CLEANED OUT ONLY
 STA 133+85.0 RT

621.20 STEEL BEAM GUARDRAIL, GALVANIZED
 MIDDLEBURY:
 STA 137+49.0 - STA 138+26.0 RT (77.0 LF)
 STA 138+40.0 - STA 138+50.0 LT (14.5 LF)

621.21 HD STEEL BEAM GUARDRAIL, GALVANIZED
 MIDDLEBURY:
 STA 138+26.0 - STA 138+51.0 RT (32.5 LF)
 STA 138+50.0 - STA 138+82.0 LT (57.5 LF)
 STA 139+82.3 - STA 140+13.5 RT (41.3 LF)
 STA 139+88.3 - STA 140+19.5 LT (41.3 LF)

621.50 MANUFACTURED TERMINAL SECTION, FLARED
 MIDDLEBURY:
 STA 140+13.5 - STA 140+51.0 RT
 STA 140+19.5 - STA 140+57.0 LT

621.60 ANCHOR FOR STEEL BEAM RAIL
 MIDDLEBURY:
 STA 137+55.0 RT
 STA 138+40.0 LT
 STA 138+66.5 LT

- NOTES:
- PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.
 - SEE SHEETS 47 & 48 FOR BRIDGE 13 DETAILS.
 - THE CONTRACTOR SHALL INSTALL NEW 16' RADIUS PANELS WITH AN ANCHOR AT EACH END AS DIRECTED BY THE RESIDENT ENGINEER

629.20 ADJUST ELEVATION OF VALVE BOX
 MIDDLEBURY:
 STA 138+56.0 LT

676.10 DELINEATOR WITH STEEL POST
 MIDDLEBURY:
 STA 140+51.0 RT
 STA 140+57.0 LT

AREA TO BE RECLAIMED

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING =
 NEW =

PROJECT LAYOUT SHEET #7

PROJECT NAME: MIDDLEBURY
 PROJECT NUMBER: STP 2627(1)
 FILE NAME: p06c222.dgn
 PROJECT LEADER: D.E.G.
 DESIGNED BY: M.J.L.
 IPARM FILE: p06c222107.1

PLOT DATE: 18-SEP-2013 17:09
 DRAWN BY: C.A.K.
 CHECKED BY: D.W.E.
 SHEET 26 OF 114

675.50 REMOVING SIGNS AS SHOWN - 24
 675.60 ERECTING SALVAGED SIGNS AS SHOWN - 5

CURVE 1
 PC = STA 140+14.88
 PT = STA 142+76.33
 R = 200'
 MATCH EXISTING

NOT TO SCALE

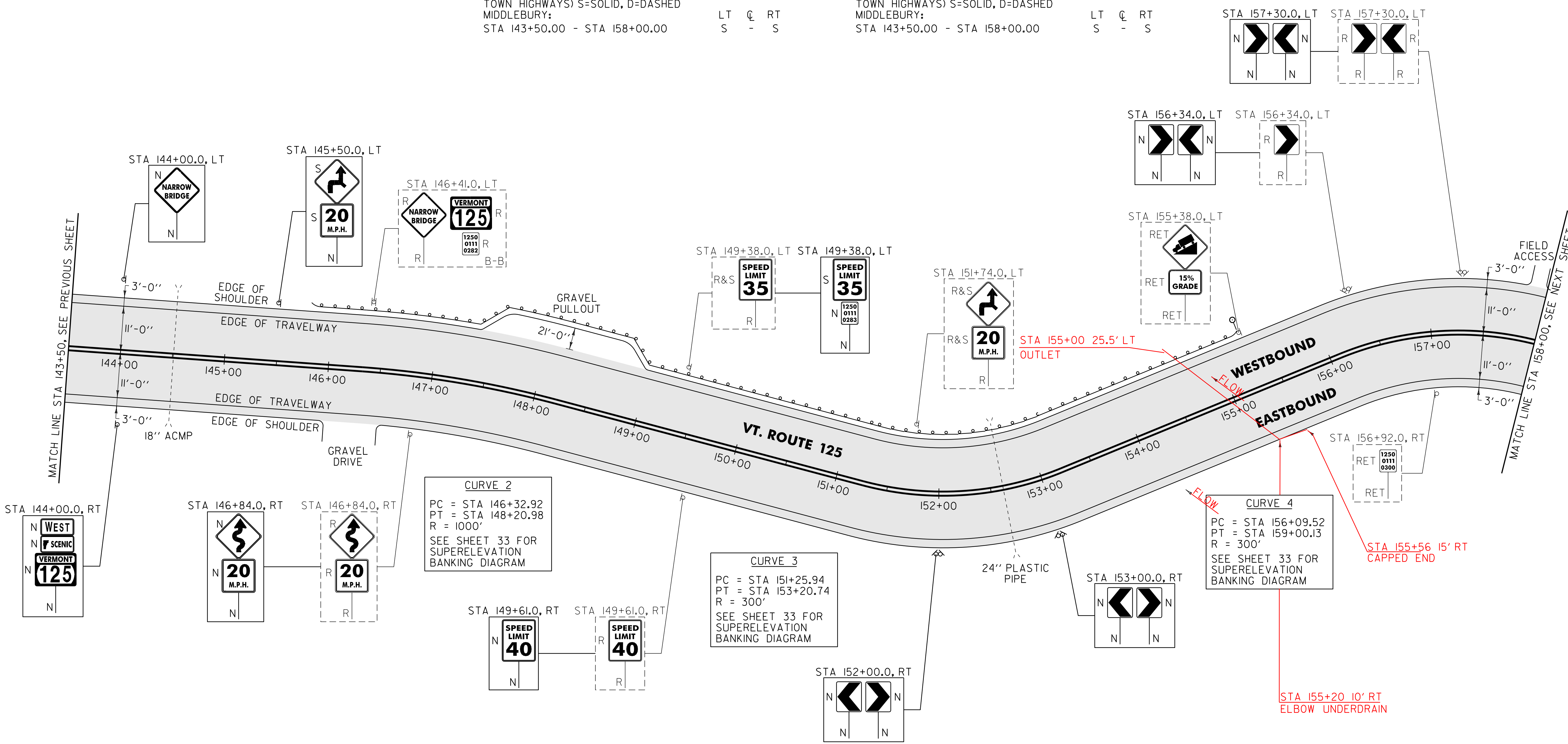
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 143+50.00 - STA 158+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 143+50.00 - STA 158+00.00 LT ☐ RT
 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 143+50.00 - STA 158+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 143+50.00 - STA 158+00.00 LT ☐ RT
 S - S

675.50 REMOVING SIGNS AS SHOWN - 12
 675.60 ERECTING SALVAGED SIGNS AS SHOWN - 3



AREA TO BE RECLAIMED

621.60 ANCHOR FOR STEEL BEAM GUARDRAIL
 MIDDLEBURY:
 STA 145+88.0 LT
 STA 146+48.0 LT

621.20 STEEL BEAM GUARDRAIL, GALVANIZED
 MIDDLEBURY:
 STA 145+88.0 - STA 155+00.5 LT (937.5 LF)

621.50 MANUFACTURED TERMINAL SECTION, FLARED
 MIDDLEBURY:
 STA 155+00.5 - STA 155+38.0 LT

601.0910 15 INCH CPEP
 MIDDLEBURY:
 STA 145+88.0 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 MIDDLEBURY:
 STA 145+90.0 - STA 155+38.0 LT (948.0 LF)

676.10 DELINEATOR WITH STEEL POST
 MIDDLEBURY:
 STA 155+38.0 LT

NOTE:
 1. PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = -----
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #8	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 27 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222108.1	

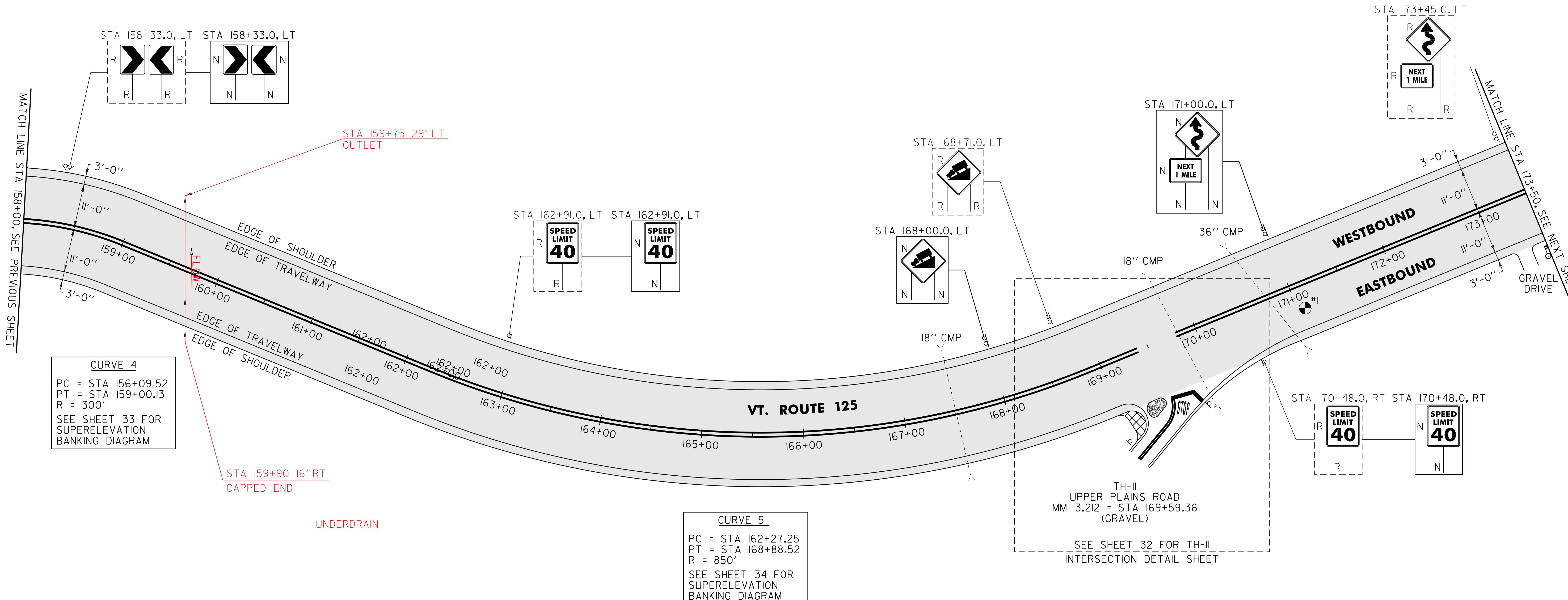
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 158+00.00 - STA 173+50.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 158+00.00 - STA 173+50.00 LT ☉ RT
 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 MIDDLEBURY:
 STA 158+00.00 - STA 173+50.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 158+00.00 - STA 173+50.00 LT ☉ RT
 S - S

675.50 REMOVING SIGNS AS SHOWN - 7



CURVE 4
 PC = STA 156+09.52
 PT = STA 159+00.13
 R = 300'
 SEE SHEET 33 FOR SUPERELEVATION BANKING DIAGRAM

CURVE 5
 PC = STA 162+27.25
 PT = STA 168+88.52
 R = 850'
 SEE SHEET 34 FOR SUPERELEVATION BANKING DIAGRAM

TH-II
 UPPER PLAINS ROAD
 MM 3.212 = STA 169+59.36 (GRAVEL)
 SEE SHEET 32 FOR TH-II INTERSECTION DETAIL SHEET

AREA TO BE RECLAIMED

NOTE:
 I. PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.

621.50 MANUFACTURED TERMINAL SECTION, FLARED
 MIDDLEBURY:
 STA 173+45.0 - STA 173+50.0 RT

676.10 DELINEATOR WITH STEEL POST
 MIDDLEBURY:
 STA 173+45.0 RT

PAVEMENT CORES = # TOTAL PCC COMMENTS
 DEPTH (INCH)
 I 6 3/4" NO

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING =
 NEW =

NOT TO SCALE

PROJECT LAYOUT SHEET #9	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 28 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222109.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC

(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

MIDDLEBURY:

STA 173+50.00 - STA 188+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC

(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

MIDDLEBURY:

STA 173+50.00 - STA 188+00.00 LT C RT

S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT

(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

MIDDLEBURY:

STA 173+50.00 - STA 188+00.00 EDGE LINE LT & RT

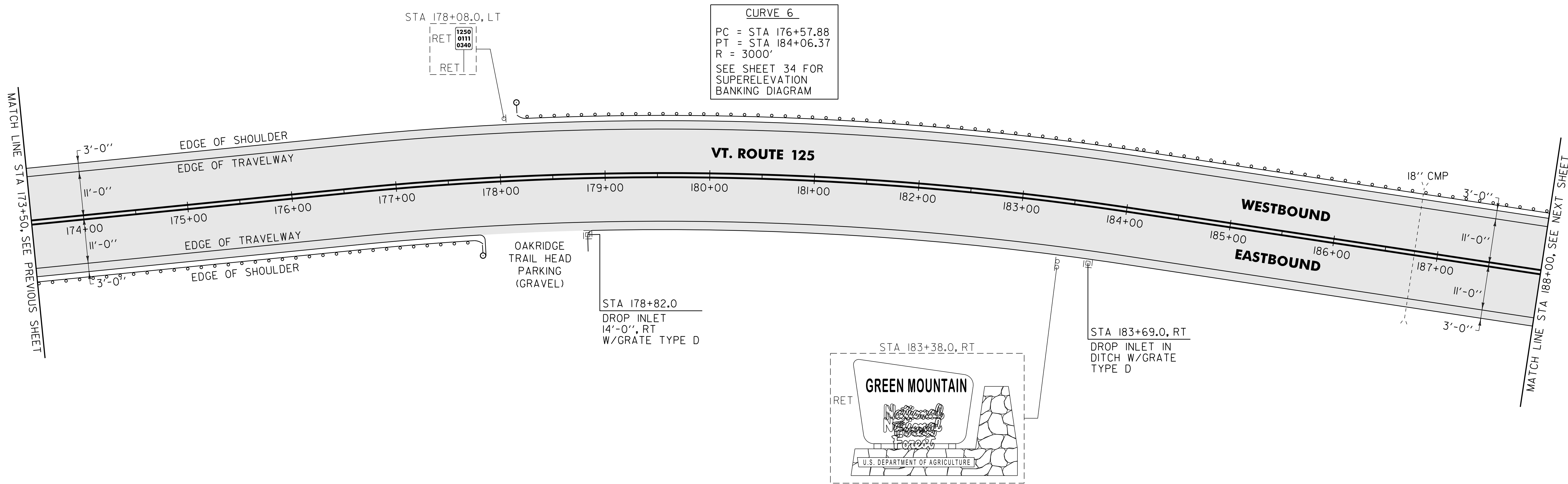
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT

(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

MIDDLEBURY:

STA 173+50.00 - STA 188+00.00 LT C RT

S - S



621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

MIDDLEBURY:
 STA 173+55.0 - STA 177+70.0 RT (415.0 LF)
 STA 178+19.0 - STA 188+00.0 LT (981.0 LF)

676.10 DELINEATOR WITH STEEL POST

MIDDLEBURY:

STA 177+70.0 RT

STA 178+13.5 LT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)

MIDDLEBURY:
 STA 178+13.5 - STA 178+51.0 LT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)

MIDDLEBURY:
 STA 178+51.0 - STA 188+00.0 (949.0 LF)

604.412, 604.415, 604.418 REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, II, III (SEE NOTE 1)

MIDDLEBURY:
 STA 178+82.0 RT
 STA 183+69.0 RT

621.20 STEEL BEAM GUARDRAIL, GALVANIZED

MIDDLEBURY:
 STA 173+82.5 - STA 177+32.5 RT (350.0 LF)

621.50 MANUFACTURED TERMINAL SECTION, FLARED

MIDDLEBURY:
 STA 173+50.0 - STA 173+82.5 RT
 STA 177+32.5 - STA 177+70.0 RT

AREA TO BE RECLAIMED

NOTE:

I. THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.

NOT TO SCALE

PROJECT LAYOUT SHEET #10	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 29 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222110.1	

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = -----
 NEW = _____

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC

(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

MIDDLEBURY:
STA 188+00.00 - STA 202+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC

(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

MIDDLEBURY: LT CL RT
STA 188+00.00 - STA 202+00.00 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT

(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

MIDDLEBURY:
STA 188+00.00 - STA 202+00.00 EDGE LINE LT & RT

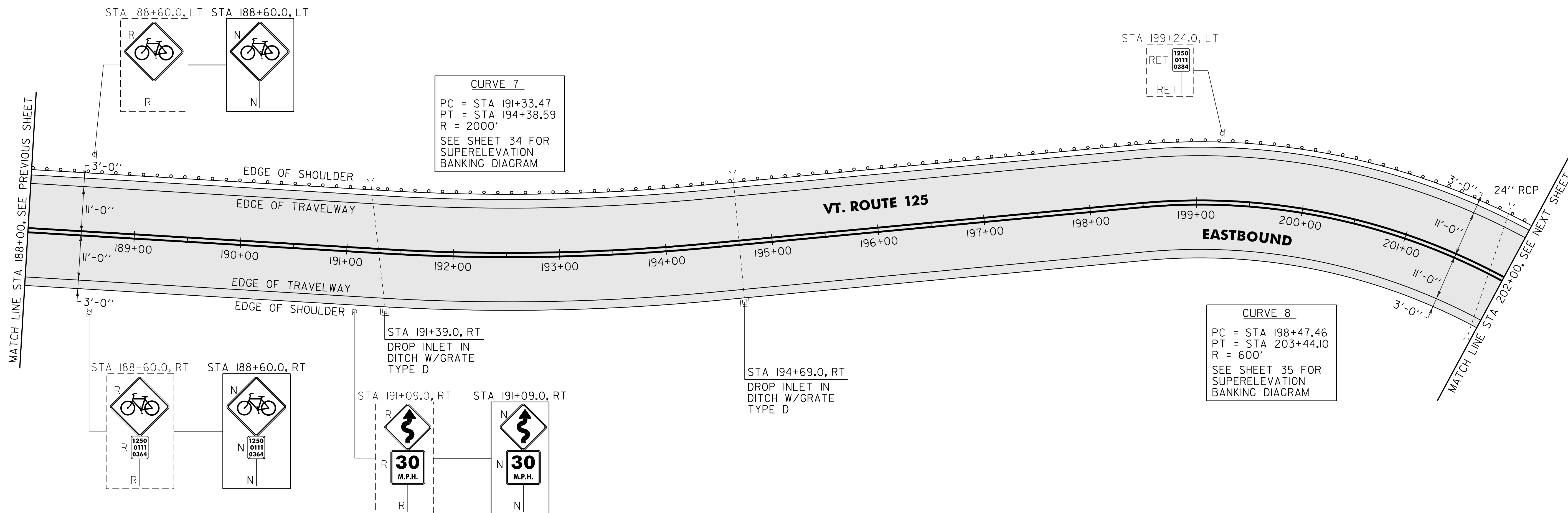
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT

(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

MIDDLEBURY: LT CL RT
STA 188+00.00 - STA 202+00.00 S - S

675.50 REMOVING SIGNS

AS SHOWN - 5



604.412, 604.415, 604.418 REHAB. DROP INLETS, CATCH BASINS

R MANHOLES, CLASS I, II, III (SEE NOTE 1)

MIDDLEBURY:
STA 191+39.0 RT
STA 194+69.0 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

MIDDLEBURY:
STA 188+00.0 - STA 202+00.0 LT (1400.0 LF)

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)

MIDDLEBURY:
STA 188+00.0 - STA 191+26.0 LT (326.0 LF)
STA 199+13.5 - STA 202+00.0 LT (286.5 LF)

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING W/ 8 FEET POSTS)

MIDDLEBURY:
STA 191+26.0 - STA 199+13.5 LT (787.5 LF)

NOTE:

1. THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.

AREA TO BE RECLAIMED

NOT TO SCALE

SIGN LEGEND
R = REMOVE
S = SALVAGE
N = NEW
RET = RETAIN
B-B = BACK TO BACK
EXISTING = - - - - -
NEW = _____

PROJECT LAYOUT SHEET #11	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 30 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222III.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC

(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

MIDDLEBURY:

STA 202+00.00 - STA 216+84.96 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC

(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

MIDDLEBURY:

STA 202+00.00 - STA 216+84.96 LT C RT S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT

(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

MIDDLEBURY:

STA 202+00.00 - STA 216+84.96 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT

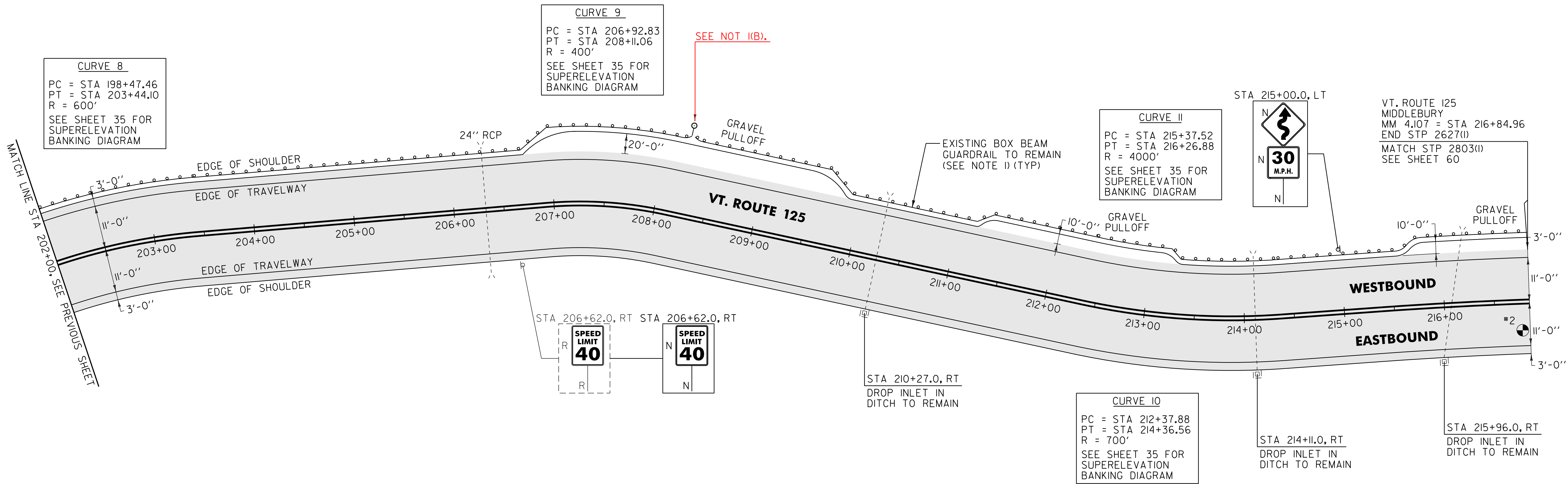
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

MIDDLEBURY:

STA 202+00.00 - STA 216+84.96 LT C RT S - S

675.50 REMOVING SIGNS

AS SHOWN - I



NOTES:

1. THE CONTRACTOR SHALL REPAIR AND/OR MAKE HEIGHT ALTERATIONS PER SUBSECTION 621.13 OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE RESIDENT ENGINEER BETWEEN MIDDLEBURY STA 208+30 AND STA 216+84.96 LT. ALL WORK SHALL BE PAID FOR UNDER ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED IN THE PLAN SET. SEE SHEET 9 FOR AVAILABLE BOX BEAM GUARDRAIL INVENTORY.
2. THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.
3. PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I.

(1B). DISTRICT #9 ENVIRONMENTAL COMMISSION ("COMMISSION") HERBY ISSUES LAND USE ADMINISTRATIVE AMENDMENT PERMIT #9A0221-5 PURSUANT TO AUTHORITY VESTED IN IT BY 10 V.S.A., CHAPTER 151. THIS PERMIT APPLIES TO LAND IDENTIFIED IN THE LAND RECORDS OF MIDDLEBURY & RIPTON, VERMONT, AS THE SUBJECT OF A DEED TO THE PERMITTEE AS GRANTEE. THIS PERMIT SPECIFICALLY AUTHORIZES THE PERMITTEE TO REPLACE APPROXIMATELY 7,186 FEET OF EXISTING GUARDRAIL WITH OXIDIZING W-BEAM GUARDRAIL ALONG SECTION OF VERMONT ROUTE 125 (PROJECT). THE PROJECT LOCATED OFF OF VERMONT ROUTE 125 IN MIDDLEBURY & RIPTON, VERMONT. DATED OCTOBER 12, 2010

STA 208+30 LT TO STA 216+84.96 LT WILL BE PAID UNDER ITEM 900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)

- 621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
MIDDLEBURY:
STA 202+00.0 - STA 208+26.0 LT (626.0 LF) - STA 216+84.96 LT
- 676.10 DELINEATOR WITH STEEL POST
MIDDLEBURY:
STA 208+26.0 LT
- 900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
MIDDLEBURY:
STA 207+88.5 - STA 208+26.0 LT
- 900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
MIDDLEBURY:
STA 202+00.0 - STA 207+88.5 LT (588.5 LF) - STA 216+84.96 LT

AREA TO BE RECLAIMED

PAVEMENT CORES =
TOTAL PCC COMMENTS DEPTH (INCH)
2 7" NO

SIGN LEGEND
R = REMOVE
S = SALVAGE
N = NEW
RET = RETAIN
B-B = BACK TO BACK
EXISTING = -----
NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #12	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 31 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222112.i	

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 169+59.36 DOUBLE SOLID RT, TH-II

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 MIDDLEBURY:
 STA 169+59.36 DOUBLE SOLID RT, TH-II

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 MIDDLEBURY:
 STA 169+59.36 RT, TH-II

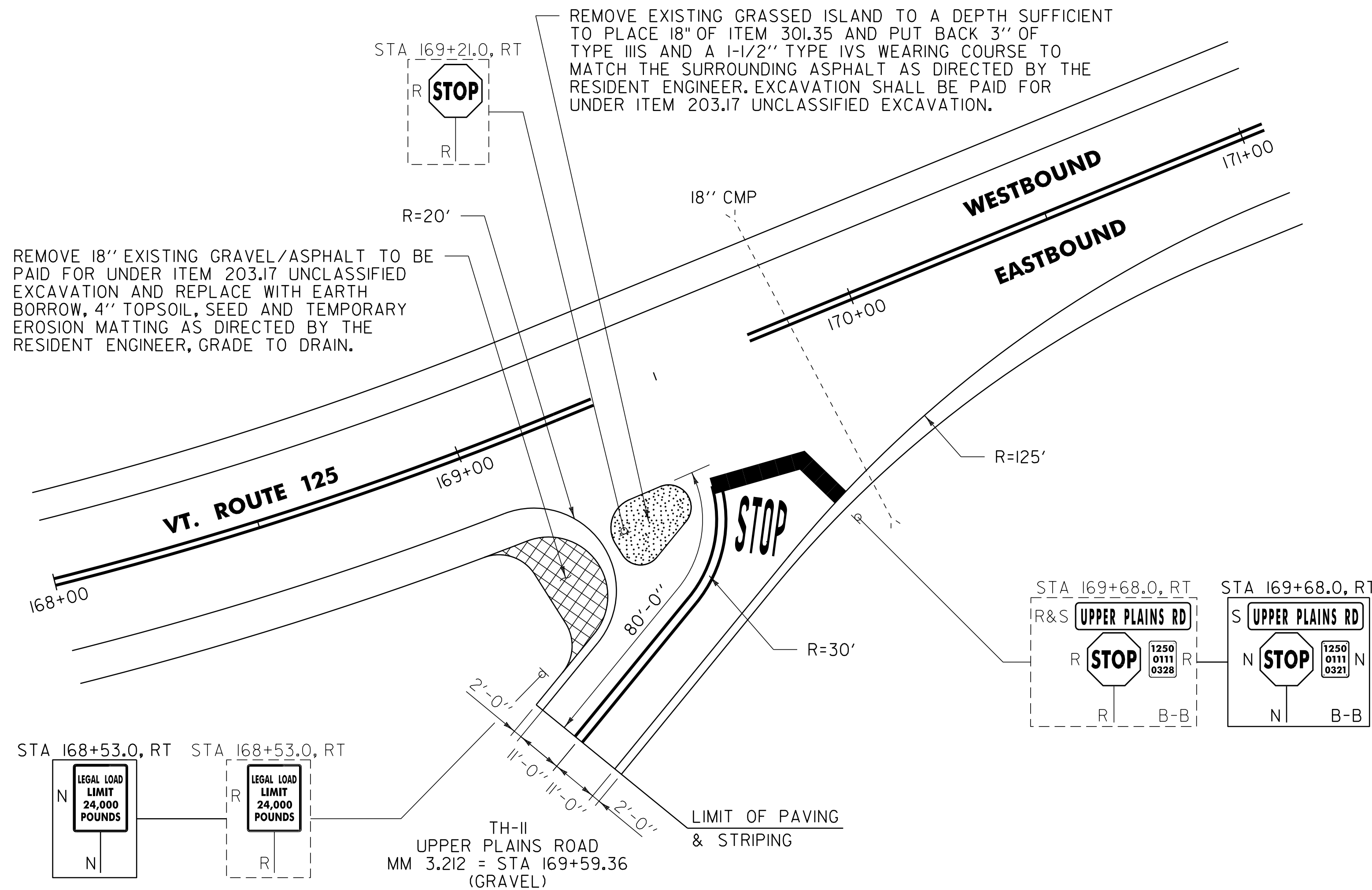
646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 MIDDLEBURY:
 STA 169+59.36 RT, TH-II

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 MIDDLEBURY:
 STA 169+59.36 RT, TH-II "S,T,O,P" (4 EA)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 MIDDLEBURY:
 STA 169+59.36 RT, TH-II "S,T,O,P" (4 EA)

675.50 REMOVING SIGNS
 AS SHOWN - 5

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 1



SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

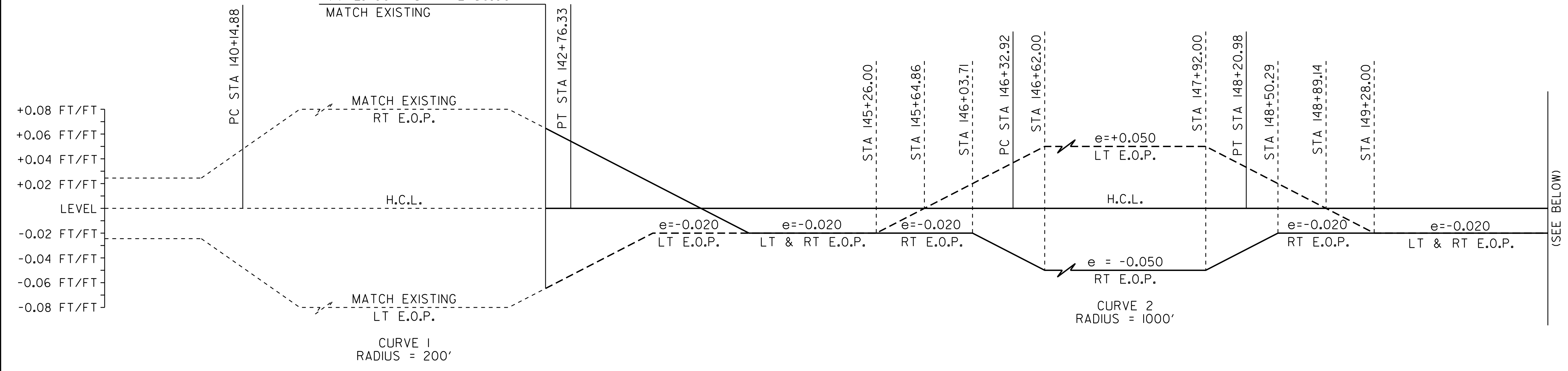
NOT TO SCALE

**TH-11
 INTERSECTION
 DETAIL
 SHEET**

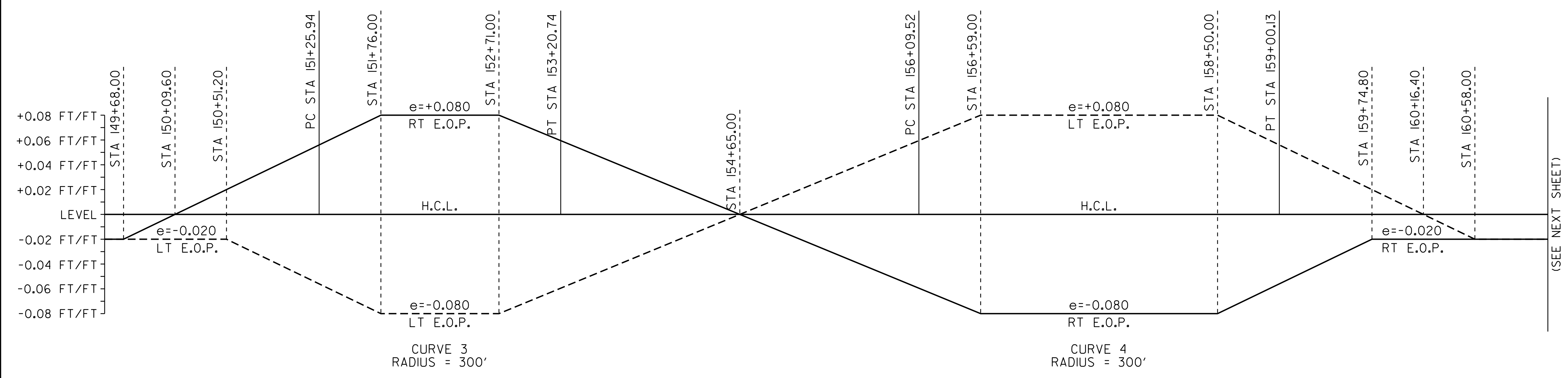
PROJECT NAME: MIDDLEBURY
 PROJECT NUMBER: STP 2627(1)
 FILE NAME: p06c222.dgn
 PROJECT LEADER: D.E.G.
 DESIGNED BY: M.J.L.
 IPARM FILE: p06c222mdt1.i

PLOT DATE: 18-SEP-2013 17:09
 DRAWN BY: C.A.K.
 CHECKED BY: D.W.E.
 SHEET 32 OF 114

BEGIN RECLAIM TYPICAL SECTION
MM 2.700 = STA 142+56.00
MATCH EXISTING



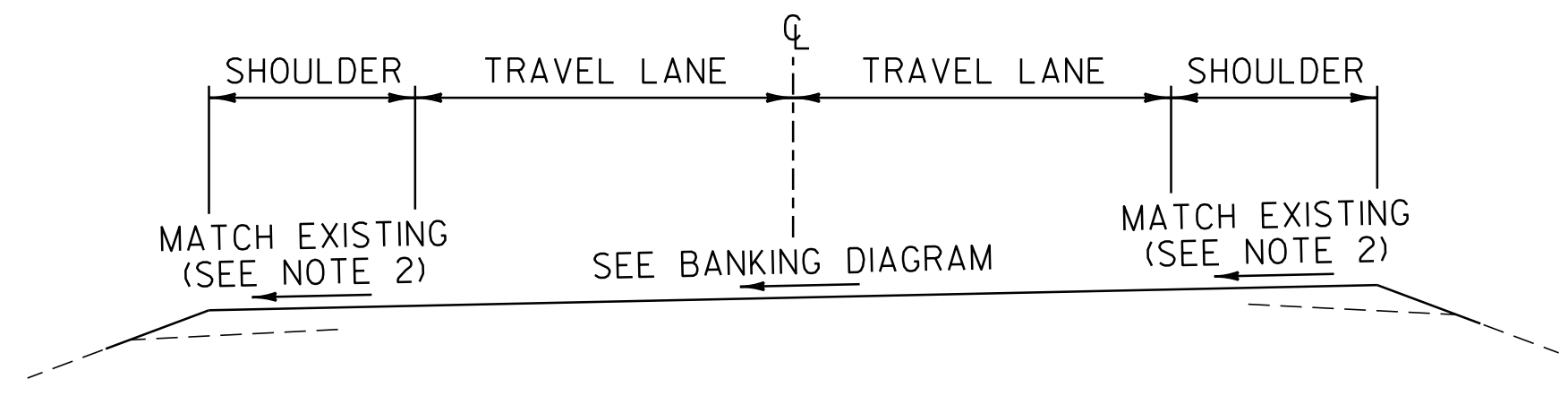
(SEE BELOW)



(SEE NEXT SHEET)

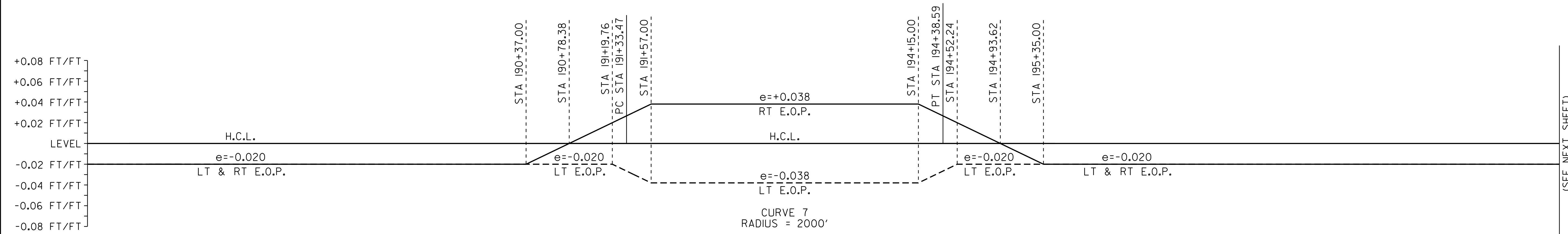
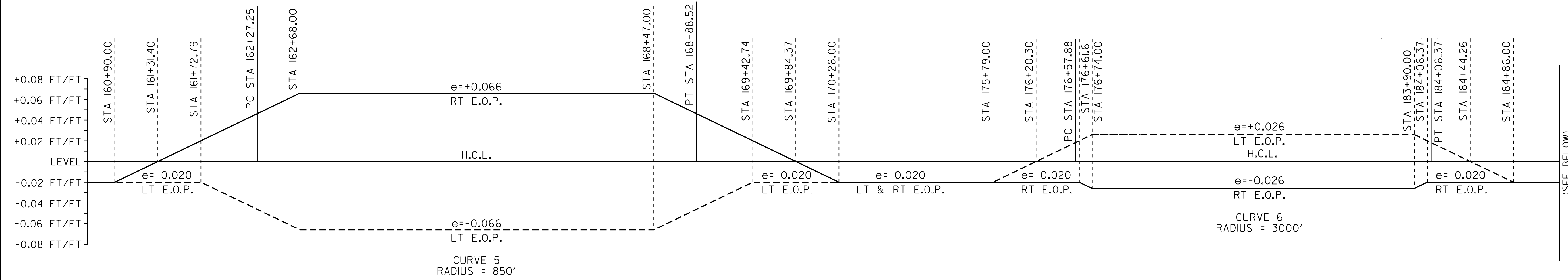
SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



NOT TO SCALE
SUPERELEVATION BANKING DIAGRAMS SHEET #1

PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
DESIGNED BY: M.J.L.	SHEET 33 OF 114
IPARM FILE: p06c222sbd1.i	

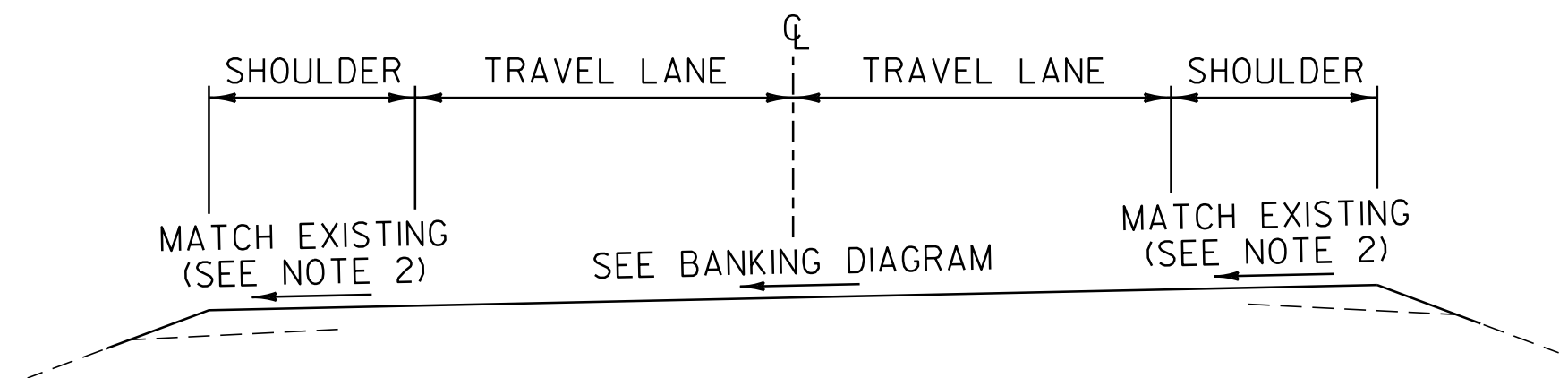


(SEE BELOW)

(SEE NEXT SHEET)

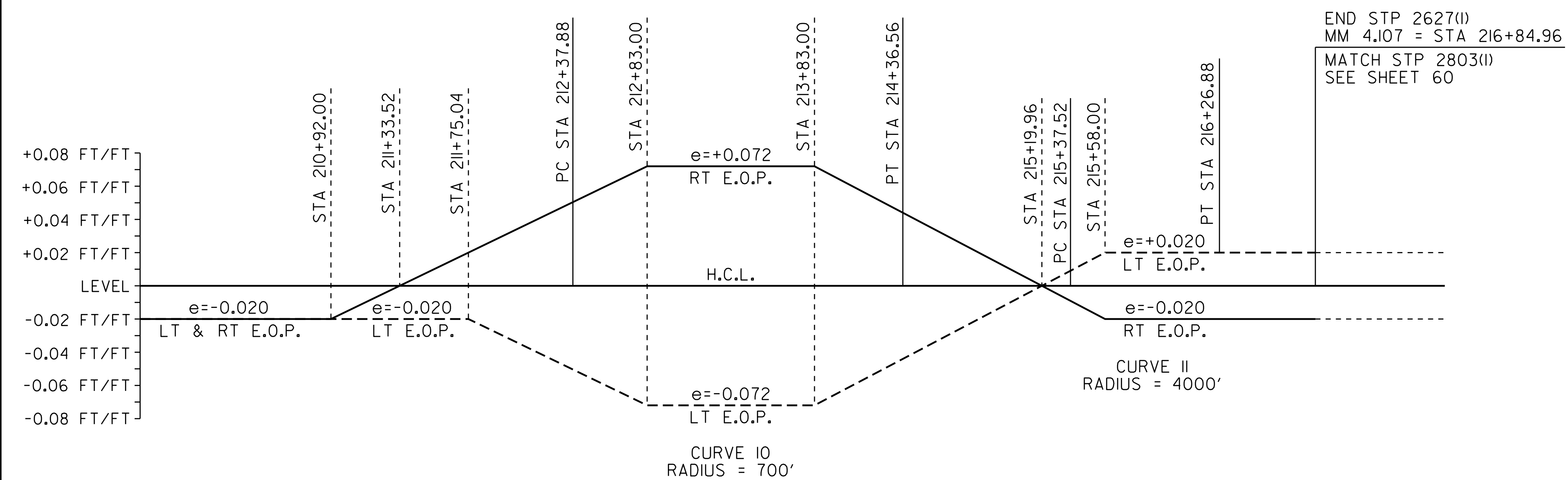
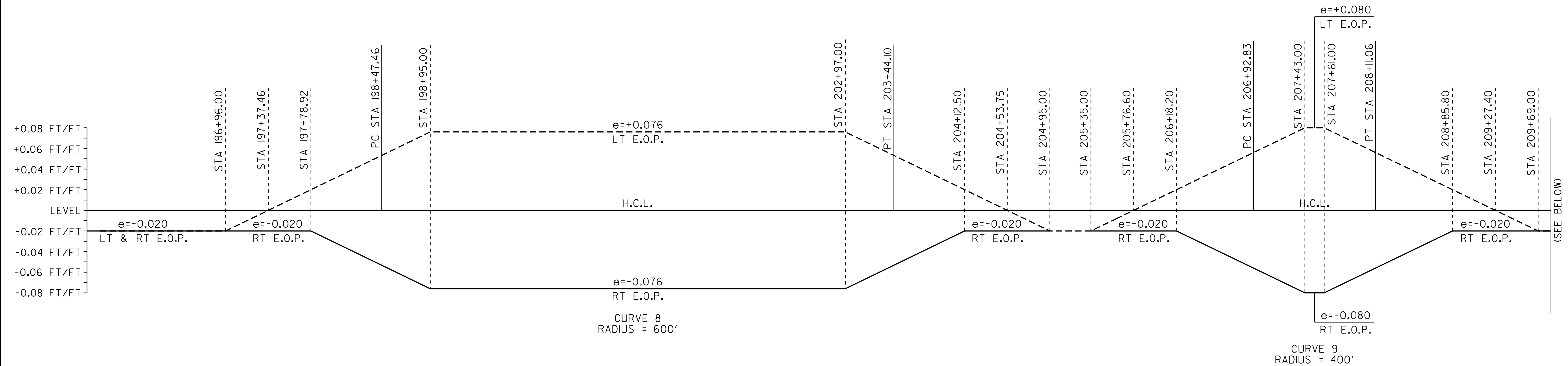
SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



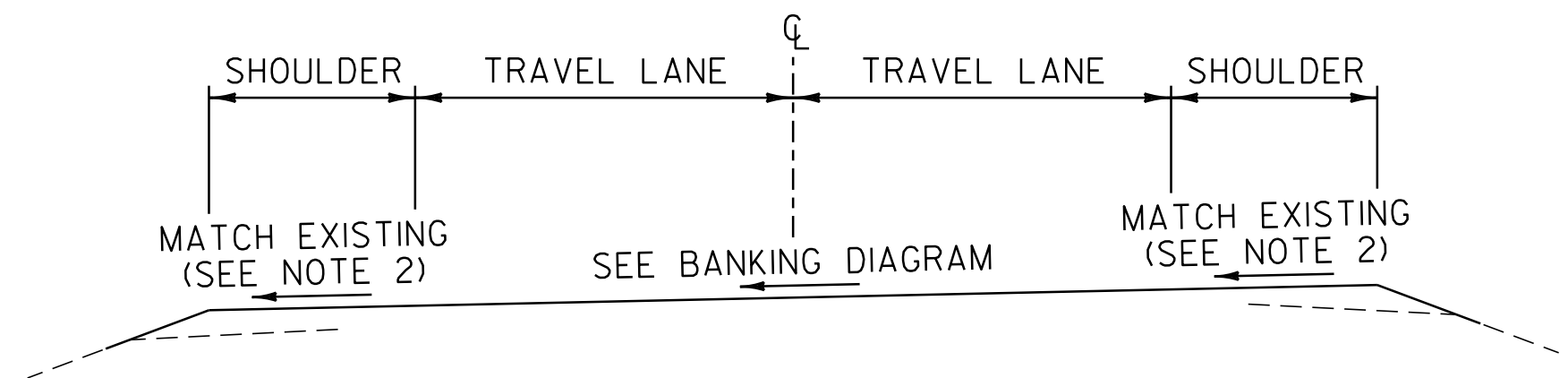
NOT TO SCALE

SUPERELEVATION BANKING DIAGRAMS SHEET #2	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.L.	SHEET 34 OF 114
	IPARM FILE: p06c222sbd2.i	



SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



NOT TO SCALE SUPERELEVATION BANKING DIAGRAMS SHEET #3	PROJECT NAME: MIDDLEBURY	
	PROJECT NUMBER: STP 2627(I)	
	FILE NAME: p06c222.dgn	PLOT DATE: 18-SEP-2013 17:09
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.L.	CHECKED BY: D.W.E.	
IPARM FILE: p06c222sbd3.i	SHEET 35 OF 114	

TRAFFIC SIGN SUMMARY SHEET 1

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST RETAIN	SALVAGED	NO. OF POSTS	NEW SIGN POSTS																REQUIRE SIGNAGE	REMARKS SHS = FHWA STANDARD SIGNS BOOK	SIGN DETAIL			
		EA	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN				SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL				DETAIL ON DWG. NUMBER	STD. SHEET NUMBER		
												1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUNDATION	3.0	3.5	4.0	5.0	FTG. SIZE					WEIGHT	POST SIZE
MIDDLEBURY: 58+05.0, LT	TO	1	24	12	2.00					2																	X	SIGN ID CODE M4-5	E-136B			
	SOUTH NORTH WEST	1	24	12	2.00																							SIGN ID CODE M3-3 SIGN ID CODE M3-1 SIGN ID CODE M3-4	SHS SHS E-136B			
	7 7 VERMONT 125	1	24	24	4.00																							SIGN ID CODE MI-4 SIGN ID CODE MI-4 SIGN ID CODE MI-4	SHS SHS E-136B			
	← → →	1	21	15	2.19																							SIGN ID CODE M6-1 SIGN ID CODE M6-1 SIGN ID CODE M6-1	SHS SHS E-136B			
58+65.0, RT	TO	1	24	12	2.00					2																X	SIGN ID CODE M4-5	E-136B				
	EAST NORTH	1	24	12	2.00																							SIGN ID CODE M3-2 SIGN ID CODE M3-1	E-136B E-136B			
	VERMONT 125 VERMONT 116	1	30	24	5.00																							SIGN ID CODE MI-4 SIGN ID CODE MI-4	E-136B E-136B			
60+19.0, LT	← BRANDON 13 ← RUTLAND 28	1	72	12	6.00					2																		SIGN ID CODE DI-1 SIGN ID CODE DI-1	SHS SHS			
	MIDDLEBURY 4 → NY STATE 21 →	1	72	12	6.00																							SIGN ID CODE DI-1 SIGN ID CODE DI-1	SHS SHS			
61+79.0, RT	E MIDDLEBURY 1 RIPTON 4	1	72	12	6.00					2																		SIGN ID CODE DI-1 SIGN ID CODE DI-1	SHS SHS			
	1250 0111 0120	1	6	10	0.42																							SIGN ID CODE VD-700	E-138			

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS	SF 79.99	SF	EA.	SF		LF	LF 84 95.83	EA.	LB	EA.	LB 211 250.8	EA.	EA.	LB
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TRAFFIC SIGN SUMMARY SHEET #1

PROJECT NAME: MIDDLEBURY
PROJECT NUMBER: STP 2627(1)

FILE NAME: p06c222.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.L.
IPARM FILE: p06c222+ss01.i

PLOT DATE: 18-SEP-2013 17:09
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 36 OF 114

TRAFFIC SIGN SUMMARY SHEET 2

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS														REMARKS	SIGN DETAIL			
		EA	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN			SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)					W-SHAPE STEEL		DETAIL ON DWG. NUMBER	STD. SHEET NUMBER
											1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0	4.0	MOD	3.0	3.5		4.0	5.0		

SHS = FHWA STANDARD SIGNS BOOK

MIDDLEBURY: 62+29.0, LT		1	21	15	2.19					1					16.0	X	X												SIGN ID CODE M2-1		SHS
		1	24	24	4.00																							SIGN ID CODE MI-4		SHS	
64+72.0, LT		1	36	36	9.00					2					14.33	X	X											SIGN ID CODE W3-1		SHS	
68+09.0, RT		1	30	18	3.75					1					12.42	X	X											SIGN ID CODE VD-18		E-128B	
71+94.0, RT		1	36	36	9.00					2					11.67	X	X											SIGN ID CODE W3-1		SHS	
77+15.0, RT		1	24	30	5.00					1					14.33	X	X											SIGN ID CODE VR-017		E-141	
77+53.0, RT										1					13.50	X	X											SALVAGED SIGNS TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-	
		1	30	30	6.25																							BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138	
83+14.0, RT										1					14.33	X	X											SALVAGED SIGNS TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-	
		1	30	30	6.25																							BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138	
		1	18	6	0.75																							SIGN ID CODE RI-4		SHS	

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS	SF 47.03	SF	EA. 3	SF		LF	LF	LF	LF	EA.	LB	LB	LB	EA.	LB	EA.	EA.	LB
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TRAFFIC SIGN SUMMARY SHEET #2

PROJECT NAME: MIDDLEBURY
PROJECT NUMBER: STP 2627(1)

FILE NAME: p06c222.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.L.
IPARM FILE: p06c222+ss02.i

PLOT DATE: 18-SEP-2013 17:09
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 37 OF 114

TRAFFIC SIGN SUMMARY SHEET 3

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST RETAIN	SALVAGE	NO. OF POSTS	NEW SIGN POSTS																REMARKS	SIGN DETAIL					
		E	A	WIDTH (in)	HEIGHT (in)	"A"	"B"				SALV SIGN	SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL		DETAIL ON DWG. NUMBER	STD. SHEET NUMBER				
													1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUNDATION	3.0	3.5	4.0	5.0				FTG. SIZE		WEIGHT	POST SIZE
																														lb/ft	lb/ft		

SHS = FHWA STANDARD SIGNS BOOK

MIDDLEBURY: 83+40.0, LT																								SALVAGED SIGNS TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-
		I	30	30	6.25																			BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138
		I	18	6	0.75																			SIGN ID CODE RI-4		SHS
83+83.0, RT		I	30	30	6.25																			SIGN ID CODE RI-1		SHS
		I	18	6	0.75																			SIGN ID CODE RI-4		SHS
84+32.0, LT		I	30	30	6.25																			SIGN ID CODE RI-1		SHS
		I	18	6	0.75																			SIGN ID CODE RI-4		SHS
85+67.0, LT																								SALVAGED SIGNS TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-
		I	30	30	6.25																			BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138
86+05.0, LT		I	24	30	5.00																			SIGN ID CODE VR-017		E-141
																								SALVAGED SIGN TO BE MOUNTED ON NEW POST.	-	-
		I	6	10	0.42																			SIGN ID CODE VD-700		E-138
90+62.0, LT		I	36	36	9.00																			SIGN ID CODE W3-1		SHS

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS



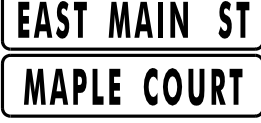









SF	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.	SF	EA.		
42.51		5																																												

TRAFFIC SIGN SUMMARY SHEET #3

PROJECT NAME: MIDDLEBURY
PROJECT NUMBER: STP 2627(1)
FILE NAME: p06c222.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.L.
IPARM FILE: p06c222+ss03.i

PLOT DATE: 18-SEP-2013 17:09
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 38 OF 114

TRAFFIC SIGN SUMMARY SHEET 4

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS														REMARKS	SIGN DETAIL				
		E A	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN			SALV TIS	SALVAGE	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL		DETAIL ON DWG. NUMBER	STD. SHEET NUMBER	
												1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	3.0	3.5	4.0		5.0	24"			30"
MIDDLEBURY: 91+33.0, RT							I																	SALVAGED SIGN TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-			
		I	30	30	6.25																			BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138			
97+71.0, LT							I																	SALVAGED SIGNS TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-			
		I	30	30	6.25																			BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138			
98+00.0, LT		I	24	30	5.00																			SIGN ID CODE VR-017		E-141			
							I																	SALVAGED SIGN TO BE MOUNTED ON NEW POST.	-	-			
101+00.0, RT 113+00 RT		I																						SALVAGED SIGN TO BE MOUNTED ON NEW POST.	-	-			
		I	24	18	3.00																			SIGN ID CODE W16-2 SIGN SHALL BE FLUORESCENT YELLOW GREEN.		SHS			
110+35.0, RT		I	30	30	6.25																			SIGN ID CODE SI-1 SIGN SHALL BE FLUORESCENT YELLOW GREEN.		SHS			
		I	24	12	2.00																			SIGN ID CODE W16-7pL SIGN SHALL BE FLUORESCENT YELLOW GREEN.		SHS			
110+40.0, LT		I	30	30	6.25																			SIGN ID CODE SI-1 SIGN SHALL BE FLUORESCENT YELLOW GREEN.		SHS			
		I	24	12	2.00																			SIGN ID CODE W16-7pL SIGN SHALL BE FLUORESCENT YELLOW GREEN.		SHS			

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS	SF	SF	EA.	SF		LF	LF	LF	EA.	LB	LB	LB	EA.	EA.	LB
	37.84 44.09		5 4												

TRAFFIC SIGN SUMMARY SHEET #4	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.L.	SHEET 39 OF 114

TRAFFIC SIGN SUMMARY SHEET 5

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS																REMARKS	SIGN DETAIL				
		E	A	WIDTH (in)	HEIGHT (in)	"A"	"B"			SALV SIGN	SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL		DETAIL ON DWG. NUMBER	STD. SHEET NUMBER			
												1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	3.0	3.5	4.0	5.0	24"				30"	WEIGHT	POST SIZE
MIDDLEBURY: 110+46.0, LT							I		I																		SALVAGED SIGNS TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-		
		I		30	30	6.25																					BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138		
110+71.0, RT							I		I																		SALVAGED SIGNS TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-		
		I		30	30	6.25																					BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138		
110+75.0, LT		I		24	30	5.00			I																		SIGN ID CODE VR-017		E-141		
							I																				SALVAGED SIGN TO BE MOUNTED ON NEW POST.	-	-		
123+48.0, RT							I		I																		SALVAGED SIGNS TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-		
		I		30	30	6.25																					BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138		
126+20.0, LT		I		24	30	5.00			I																		SIGN ID CODE R2-1		SHS		
128+04.0, LT		I		30	30	6.25			I																		SIGN ID CODE SI-1 SIGN SHALL BE FLUORESCENT YELLOW GREEN.		SHS		
		I		24	18	3.00																					SIGN ID CODE W16-2 SIGN SHALL BE FLUORESCENT YELLOW GREEN.		SHS		

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS	SF 39.26	SF	EA. 7	SF		LF	LF	LF	EA.	LB	LB	LB	EA.	LB	EA.	EA.	LB
---------------	----------	----	-------	----	--	----	----	----	-----	----	----	----	-----	----	-----	-----	----

TRAFFIC SIGN SUMMARY SHEET #5











PROJECT NAME: MIDDLEBURY
PROJECT NUMBER: STP 2627(1)

FILE NAME: p06c222.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.L.
IPARM FILE: p06c222+ss04.i

PLOT DATE: 18-SEP-2013 17:09
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 40 OF 114

TRAFFIC SIGN SUMMARY SHEET 6

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS E A WIDTH HEIGHT (in) (in)		NEW & SALVAGED SIGNS				EXIST POST	NEW SIGN POSTS														REMARKS SHS = FHWA STANDARD SIGNS BOOK	SIGN DETAIL				
				"A"	"B"	SALV SIGN	SALV TIS	NO. OF POSTS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL				DETAIL ON DWG. NUMBER	STD. SHEET NUMBER	
									1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUN-DATION	3.0	3.5	4.0	5.0		FTG. SIZE				WEIGHT

MIDDLEBURY: 134+70.0, RT							I						15.17	X		X												-	-
							I																				-	-	
134+77.0, LT		I	24	30	5.00					I			13.17	X		X											SIGN ID CODE R2-1		SHS
135+75.0, RT										I			13.75	X		X											-	-	
										I																	-	-	
138+12.0, LT		I	48	24	8.00					2			7.58	X		X											SIGN ID CODE WI-6L		SHS
													7.83																
138+15.0, LT										I			9.92	X		X											SALVAGED SIGN TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-
		I	30	30	6.25																						BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138
		I	6	10	0.42																								
138+63.0, LT		I	48	24	8.00					2			10.75	X		X											SIGN ID CODE WI-6R		SHS
													10.75																
138+76.0, RT		I	12	36	3.00					I			13.17	X		X											SIGN ID CODE OM-3R		SHS

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."	LF LF LF LF LF LF EA. EA. LB LB LB LB LB LB LB LB													
	TOTALS	SF 30.67	SF	EA. 5	SF	LF	LF	EA.	LB	EA.	EA.	LB	EA.	EA.
	126 102.09													
	126 102.09													

TRAFFIC SIGN SUMMARY SHEET #6

PROJECT NAME: MIDDLEBURY

PROJECT NUMBER: STP 2627(1)

FILE NAME: p06c222.dgn

PLOT DATE: 18-SEP-2013 17:09

PROJECT LEADER: D.E.G.

DRAWN BY: C.A.K.

DESIGNED BY: M.J.L.

CHECKED BY: D.W.E.

IPARM FILE: p06c222+ss06.l

SHEET 41 OF 114

TRAFFIC SIGN SUMMARY SHEET 7

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST RETAIN	SALVAGE	NO. OF POSTS	NEW SIGN POSTS												REMARKS	SIGN DETAIL					
		E A	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN				SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL		DETAIL ON DWG. NUMBER	STD. SHEET NUMBER	
												1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	3.0	3.5		4.0	5.0	24"			30"

SHS = FHWA STANDARD SIGNS BOOK

MIDDLEBURY: 138+82.0, LT		I	12	36	3.00					1					12.25	X	X										SIGN ID CODE OM-3L		SHS
139+86.0, LT		I	12	36	3.00					1					13.33	X	X										SIGN ID CODE OM-3R		SHS
139+86.0, RT		I	12	36	3.00					1					13.33	X	X										SIGN ID CODE OM-3L		SHS
140+15.0, RT		I I	18 18	24 24	3.00 3.00					2					12.08 X 11.92	X	X										SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
140+82.0, RT		I I	18 18	24 24	3.00 3.00					2					10.82 X 10.83	X	X										SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
141+46.0, RT		I I	18 18	24 24	3.00 3.00					2					12.50 X 12.50	X	X										SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
142+07.0, RT		I I	18 18	24 24	3.00 3.00					2					11.70 X 11.75	X	X										SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
142+64.0, RT		I I	18 18	24 24	3.00 3.00					2					12.0 X 11.90	X	X										SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
144+00.0, LT		I	30	30	6.25					1					13.67	X	X										SIGN ID CODE W5-2		SHS

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

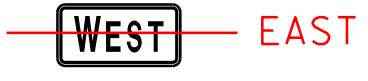









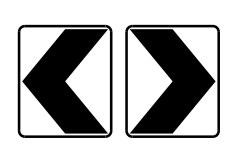
TOTALS	SF 45.25	SF	EA.	SF			LF	LF	LF	LF	LF	LF	EA.	LB	LB	LB	EA.	LB	EA.	EA.	LB

TRAFFIC SIGN SUMMARY SHEET #7	PROJECT NAME: MIDDLEBURY
	PROJECT NUMBER: STP 2627(II)
	FILE NAME: p06c222.dgn PROJECT LEADER: D.E.G. DESIGNED BY: M.J.L. IPARM FILE: p06c222+ss07.i
	PLOT DATE: 18-SEP-2013 17:09 DRAWN BY: C.A.K. CHECKED BY: D.W.E. SHEET 42 OF 114

TRAFFIC SIGN SUMMARY SHEET 8

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST RE TAIN S ALV AGE	NO. OF POST S	NEW SIGN POSTS																REMARKS	SIGN DETAIL		
				"A"	"B"	SALV SIGN	SALV TIS			FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM Ø (in)			TUBULAR STEEL Ø (in)				W-SHAPE STEEL		RE QUIR ED		DETAIL ON DWG. NUMBER	STD. SHEET NUMBER	
										1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	3.0	3.5	4.0	5.0	FTG. SIZE	WEIGHT					POST SIZE

SHS = FHWA STANDARD SIGNS BOOK

MIDDLEBURY: 144+00.0, RT		1	24	12	2.00																							SIGN ID CODE M3-4		E-136B					
		1	30	15	3.13																							SIGN ID CODE VM-064	49						
		1	30	24	5.00																							SIGN ID CODE MI-4		E-136B					
145+50.0, LT									1																					-	-				
																														-	-				
146+84.0, RT		1	30	30	6.25				1																						SIGN ID CODE WI-5R		SHS		
		1	18	18	2.25																											SIGN ID CODE WI3-1		SHS	
149+38.0, LT									1																							SALVAGED SIGN TO BE MOUNTED ON NEW POST.	-	-	
		1	6	10	0.42																											SIGN ID CODE VD-700		E-138	
149+61.0, RT		1	24	30	5.00				1																								SIGN ID CODE R2-1		SHS
152+00.0, RT		1 1	18 18	24 24	3.00 3.00				2																								SIGN ID CODE WI-8L SIGN ID CODE WI-8R		SHS SHS

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

							LF	LF	LF	LF	LF	LF	EA.	LB	LB	LB	LB	LB	EA.	EA.	LB													
TOTALS	SF	SF	EA.	SF			LF		LF			EA.		LB			EA.	EA.	LB															
	30.05		3						98 105.37																									

**TRAFFIC
SIGN
SUMMARY
SHEET #8**

PROJECT NAME: MIDDLEBURY
PROJECT NUMBER: STP 2627(1)
FILE NAME: p06c222.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.L.
IPARM FILE: p05c222+ss08.i
PLOT DATE: 18-SEP-2013 17:09
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 43 OF 114

TRAFFIC SIGN SUMMARY SHEET 9

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS														REMARKS	SIGN DETAIL			
		E A	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN			SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)					W-SHAPE STEEL		DETAIL ON DWG. NUMBER	STD. SHEET NUMBER
											1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUNDATION	3.0	3.5	4.0		5.0	FTG. SIZE		
MIDDLEBURY: 153+00.0, RT		I	18	24	3.00				2				14.0			X										SIGN ID CODE WI-8L SIGN ID CODE WI-8R		SHS SHS
156+34.0, LT		I	18	24	3.00				2				12.0			X										SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
157+30.0, LT		I	18	24	3.00				2				11.7			X										SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
158+33.0, LT		I	18	24	3.00				2				13.83			X										SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
162+91.0, LT		I	24	30	5.00				1				14.75			X										SIGN ID CODE R2-1		SHS
168+00.0, LT		I	36	36	9.00				2				13.80			X										SIGN ID CODE W7-1		SHS
168+53.0, RT		I	24	30	5.00				1				12.0			X										SIGN ID CODE VR-017		E-141
169+68.0, RT									1				11.40			X										SALVAGED SIGN TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-
		I	30	30	6.25																					BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

										LF	LF	LF	LF	LF	LF	EA.	LB	LB	LB	LB	LB	LB	EA.	EA.	LB
TOTALS																									

TRAFFIC SIGN SUMMARY SHEET #9

PROJECT NAME: MIDDLEBURY
PROJECT NUMBER: STP 2627(1)

FILE NAME: p06c222.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.L.
IPARM FILE: p06c222+ss09.i

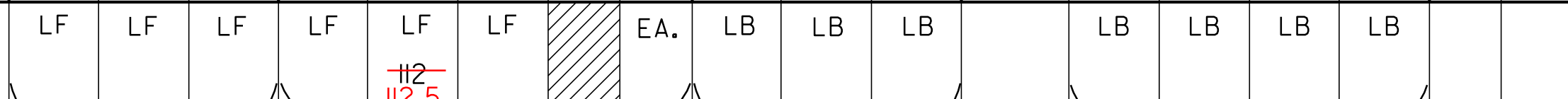
PLOT DATE: 18-SEP-2013 17:09
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 44 OF 114

TRAFFIC SIGN SUMMARY SHEET 10

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST RETAIN	SALVAGE	NO. OF POSTS	NEW SIGN POSTS												REMARKS	SIGN DETAIL	
		EA	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN				SALV TIS	FLANGED CHANNEL	SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)					W-SHAPE STEEL	
		lb/ft					ANCHOR SLEEVE			FOUNDATION						FTG. SIZE		WEIGHT		POST SIZE		SF			
		1.2	2.0	3.0	1.88	2.42	3.35	3.0	4.0	4.0 MOD	3.0	3.5	4.0	5.0	24"	30"	WEIGHT	POST SIZE	SF						

SHS = FHWA STANDARD SIGNS BOOK

MIDDLEBURY: 170+48.0, RT		1	24	30	5.00					1																		SIGN ID CODE R2-1		SHS			
171+00.0, LT		1	36	36	9.00					2																					SIGN ID CODE WI-5L	SHS	
		1	24	18	3.00																										SIGN ID CODE W7-3A	SHS	
188+60.0, LT		1	30	30	6.25					1																					SIGN ID CODE WII-1	SHS	
188+60.0, RT		1	30	30	6.25					1																					SIGN ID CODE WII-1	SHS	
		1	6	10	0.42																										SIGN ID CODE VD-700	E-138	
191+09.0, RT		1	30	30	6.25					1																					SIGN ID CODE WI-5R	SHS	
		1	18	18	2.25																										SIGN ID CODE WI3-1	SHS	
206+62.0, RT		1	24	30	5.00					1																					SIGN ID CODE R2-1	SHS	
215+00.0, LT		1	30	30	6.25					1																						SIGN ID CODE WI-5R	SHS
		1	18	18	2.25																											SIGN ID CODE WI3-1	SHS



FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS	SF	SF	EA.	SF	 	LF	LF	EA.	LB	EA.	EA.	LB
	51.92											

TRAFFIC SIGN SUMMARY SHEET #10

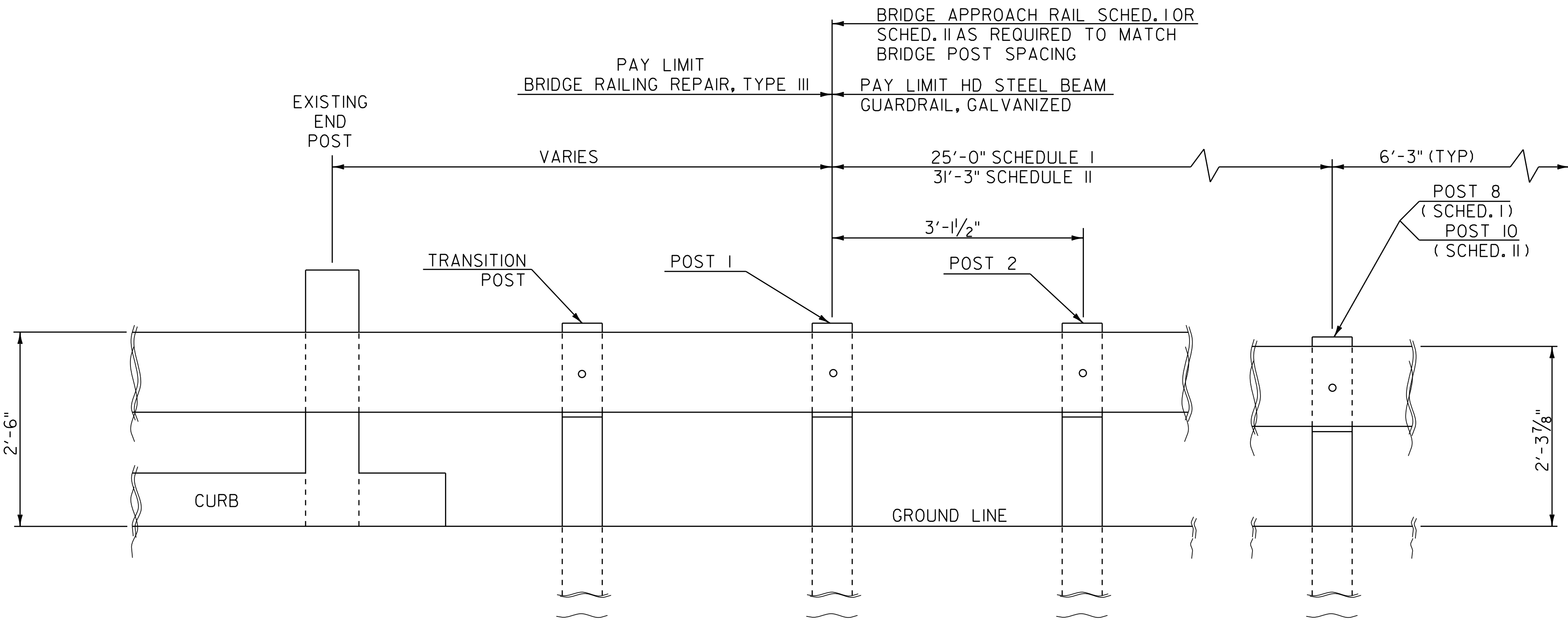
PROJECT NAME: MIDDLEBURY	PROJECT NUMBER: STP 2627(1)
FILE NAME: p06c222.dgn	PLOT DATE: 18-SEP-2013 17:09
PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.L.	CHECKED BY: D.W.E.
IPARM FILE: p06c222+ssl0.i	SHEET 45 OF 114

BRIDGE QUANTITY SHEET

STATION	POS.	BRIDGE NUMBER	OFFSET BLOCK	525.10 REMOVAL OF EXISTING RAIL LF	525.60 BRIDGE RAILING REPAIR, TYPE III LF	REMARKS
MIDDLEBURY:						
138+51.00 - 139+82.30	RT	13	8"	131.3	131.3 106.25	
138+82.00 - 139+88.30	LT	13	8"	106.3	106.3 87.5	
ROUNDING				0.4	0.4 0.25	
TOTAL				238.0	238.0 194	

GENERAL NOTES

- BRIDGE RAIL SHALL BE HD STEEL BEAM RAIL.
- BRIDGE APPROACH RAIL HEIGHT SHALL BE TRANSITIONED TO NORMAL ROADWAY RAIL HEIGHT IN 25'.
- APPROACH RAILING SHALL BE HD STEEL BEAM FOR 25' FROM THE ENDS OF THE BRIDGE UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE RESIDENT ENGINEER.
- FOR BRIDGE RAILING, THE TRANSITION POST SHALL HAVE AN OFFSET BLOCK AND BE LOCATED AS CLOSE AS PRACTICABLE TO THE MIDPOINT BETWEEN THE BRIDGE END POST AND APPROACH RAIL POST 1.
- SPLICES SHALL LAP IN DIRECTION OF TRAFFIC FLOW.
- SEE STANDARD SHEET G-1 FOR DELINEATION DETAILS AND PLACEMENT.
- ERECT DELINEATORS ON EVERY FIFTH POST OR APPROXIMATELY 31'-3" APART. PAYMENT SHALL BE INCIDENTAL TO OTHER ITEMS.
- ALL BRIDGE POSTS, HD STEEL BEAM BRIDGE RAIL, AND RELATED HARDWARE SHALL BE PAID FOR UNDER THE APPROPRIATE BRIDGE RAILING ITEMS AS DENOTED IN THE PLANS.
- SEE STANDARD SHEET G-1 FOR CONNECTION OF STEEL BEAM TO OFFSET BLOCK AND OFFSET BLOCK TO BRIDGE POST.
- NEW BRIDGE RAILING POSTS SHALL BE SET NORMAL TO GRADE.
- ALL WELDING SHALL CONFORM TO THE PROVISIONS OF SUBSECTION 506.10 OF THE STANDARD SPECIFICATIONS.
- THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON LIMITED FIELD INVESTIGATION AND AVAILABLE RECORD DRAWING INFORMATION. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO THE CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH FIELD CONDITIONS AS DIRECTED BY THE RESIDENT ENGINEER.
- ALL LANES SHALL BE OPENED TO TRAFFIC DURING NON-WORKING HOURS. NO GAPS BETWEEN EXISTING RAILING AND NEW RAILING WILL BE PERMITTED. DETAILS FOR TEMPORARY RAIL SHALL BE SUBMITTED FOR APPROVAL IF REQUIRED TO BRIDGE GAPS IN RAILING. PAYMENT FOR TEMPORARY RAIL SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 525.10 REMOVAL OF EXISTING RAILING.



SCHEDULE I		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1 1/2"	1.4 x 12'-6"
2	3'-1 1/2"	
3	3'-1 1/2"	
4	3'-1 1/2"	
5	3'-1 1/2"	
6	4'-2"	1.2 x 12'-6"
7	4'-2"	
8	4'-2"	
9	6'-3" (TYP.)	1.0 (TYP)
PAYMENT LENGTH = 32'-6"		

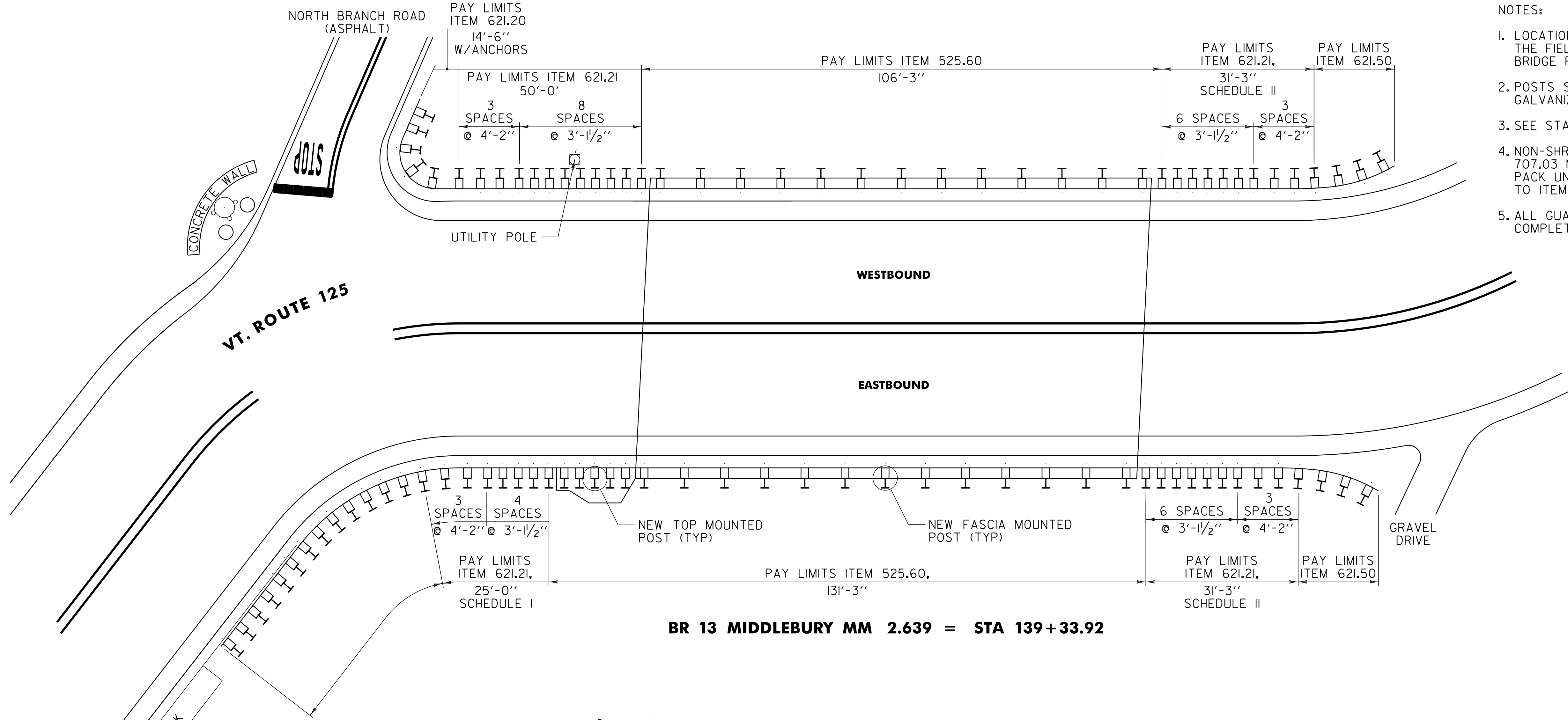
SCHEDULE II		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1 1/2"	1.4 x 18'-9"
2	3'-1 1/2"	
3	3'-1 1/2"	
4	3'-1 1/2"	
5	3'-1 1/2"	
6	3'-1 1/2"	
7	3'-1 1/2"	
8	4'-2"	
9	4'-2"	
10	4'-2"	
11	6'-3" (TYP)	1.0 (TYP)
PAYMENT LENGTH = 41'-3"		

SCHEDULE I APPROACH RAILING SHALL BE USED WHEN A RAIL PANEL SPLICE OCCURS AT POST NO. 1.
 SCHEDULE II APPROACH RAILING SHALL BE USED WHEN A RAIL PANEL SPLICE OCCURS AT THE BRIDGE END POST.

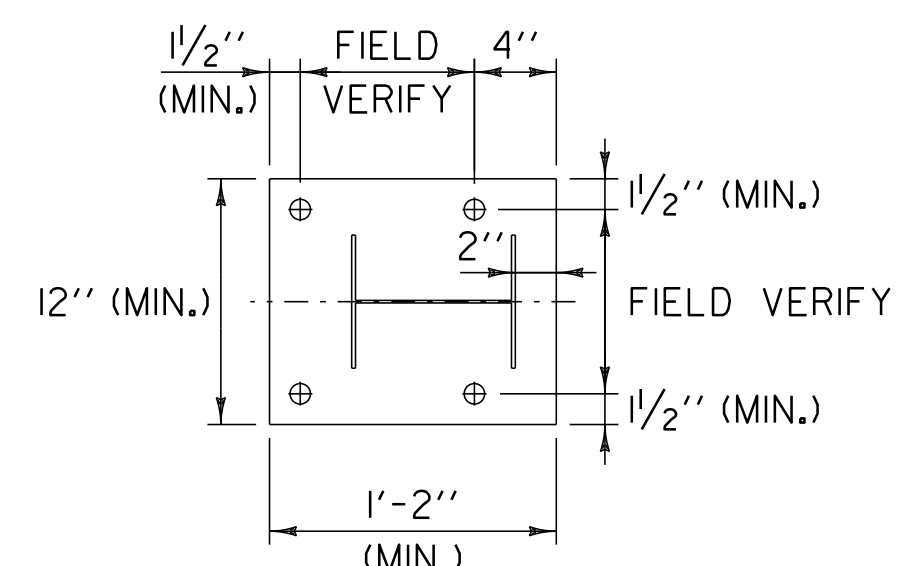
BRIDGE APPROACH RAILING

NOT TO SCALE

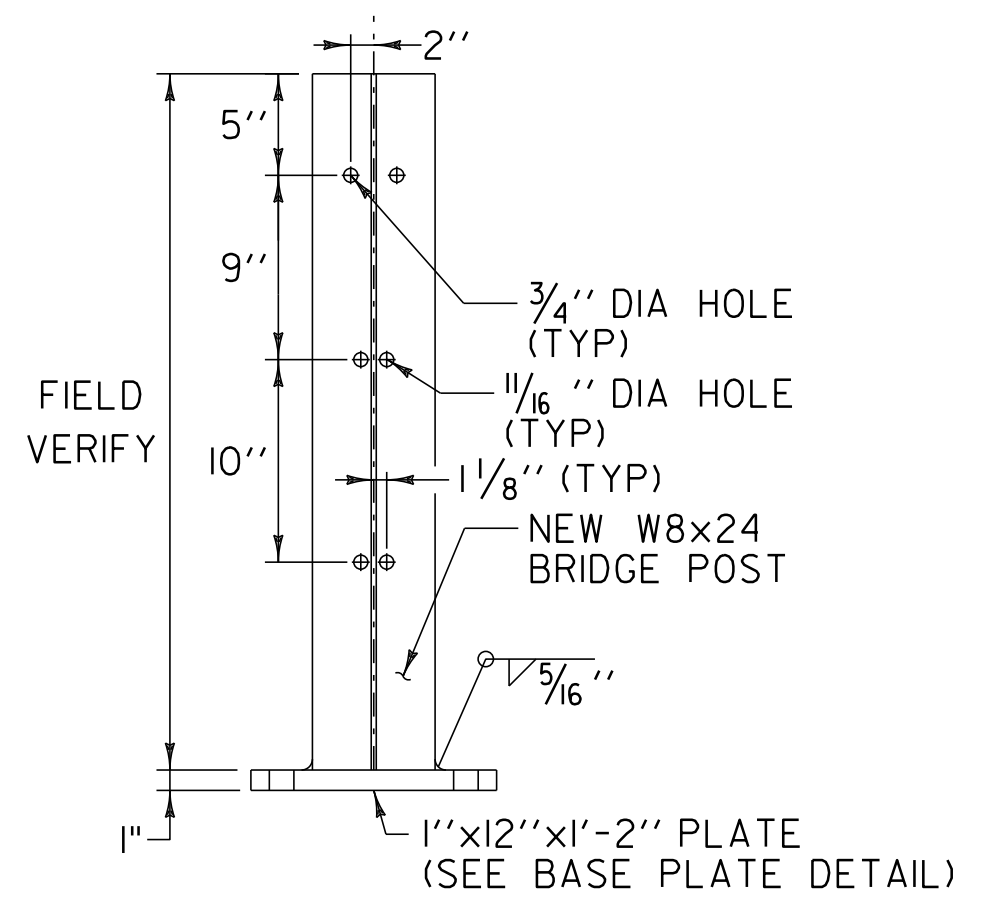
BRIDGE DETAIL SHEET #1	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G. DESIGNED BY: M.J.L. IPARM FILE: p06c222bdl.i	SHEET 47 OF 114



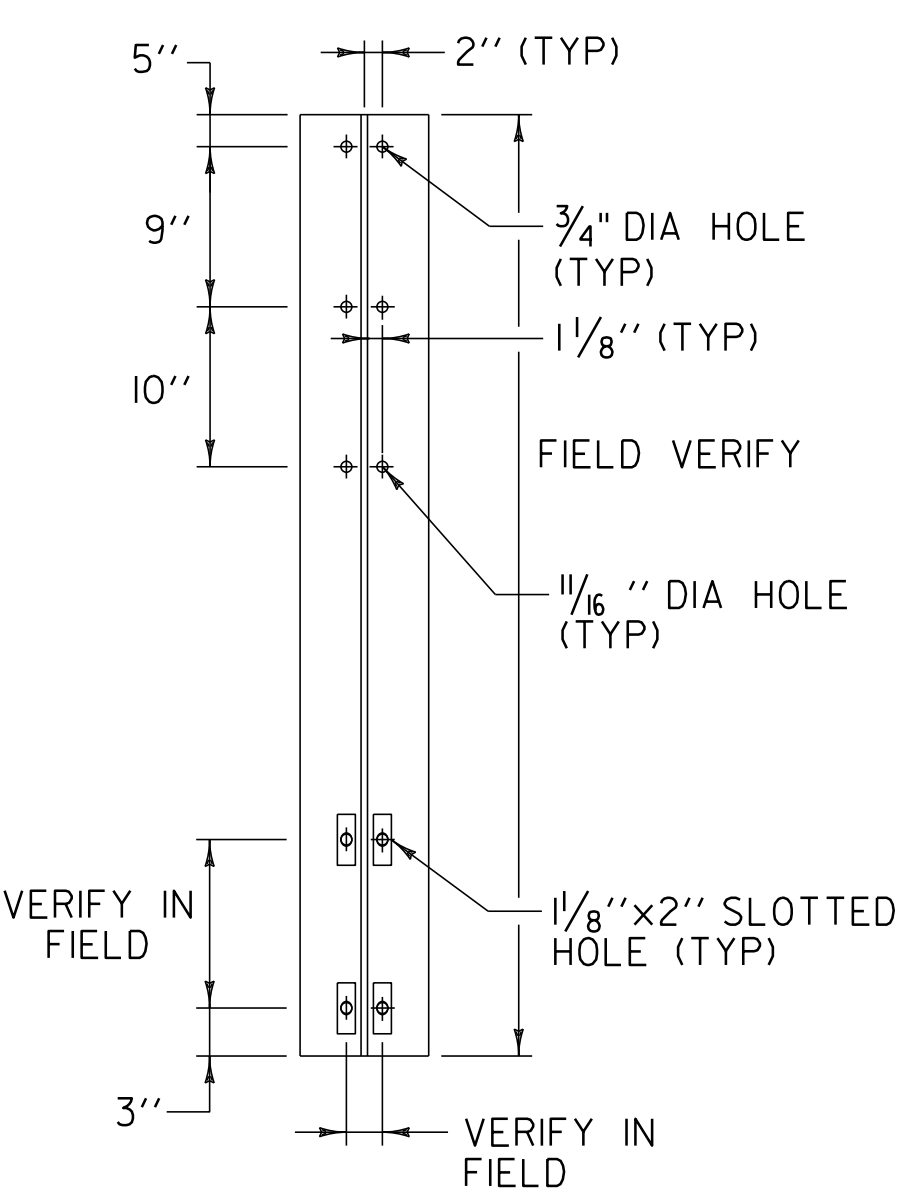
- NOTES:
1. LOCATION OF EXISTING ANCHOR BOLTS TO BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO ORDERING BRIDGE POSTS.
 2. POSTS SHALL BE SHOP CUT AND DRILLED PRIOR TO GALVANIZING.
 3. SEE STANDARD SHEET SB-R6-82 FOR ADDITIONAL DETAILS.
 4. NON-SHRINKING GROUT SHALL CONFORM TO SUBSECTION 707.03 MORTAR TYPE IV OF THE STANDARD SPECIFICATIONS. PACK UNDER BASE PLATE. PAYMENT WILL BE INCIDENTAL TO ITEM 525.60 BRIDGE RAILING REPAIR, TYPE III.
 5. ALL GUARDRAIL WORK TO BE DONE ON THIS BRIDGE SHALL BE COMPLETED ON OR BEFORE JUNE 30, 2010.



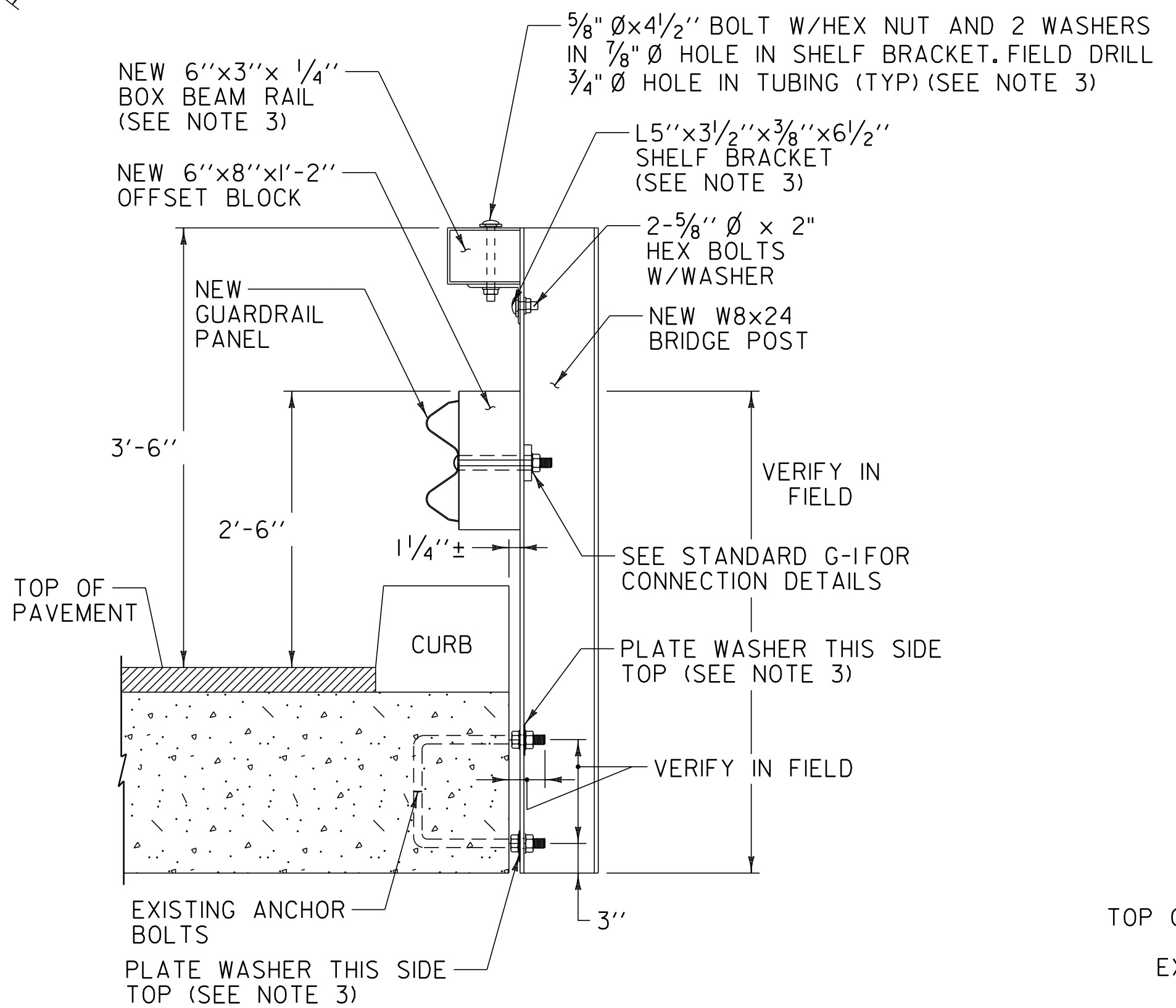
BASE PLATE DETAIL



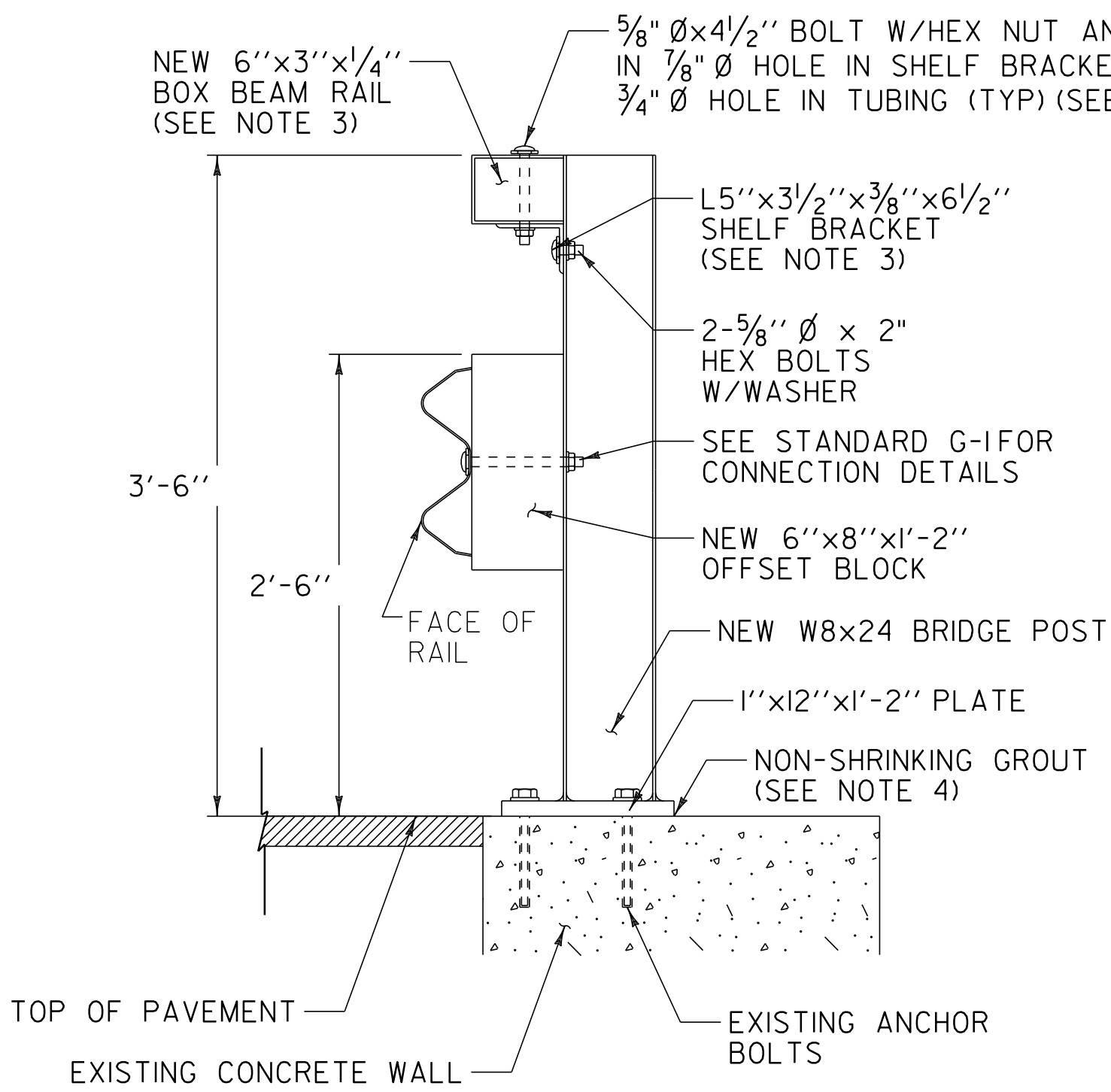
TOP MOUNTED BRIDGE POST DETAIL



FASCIA MOUNTED POST DETAIL



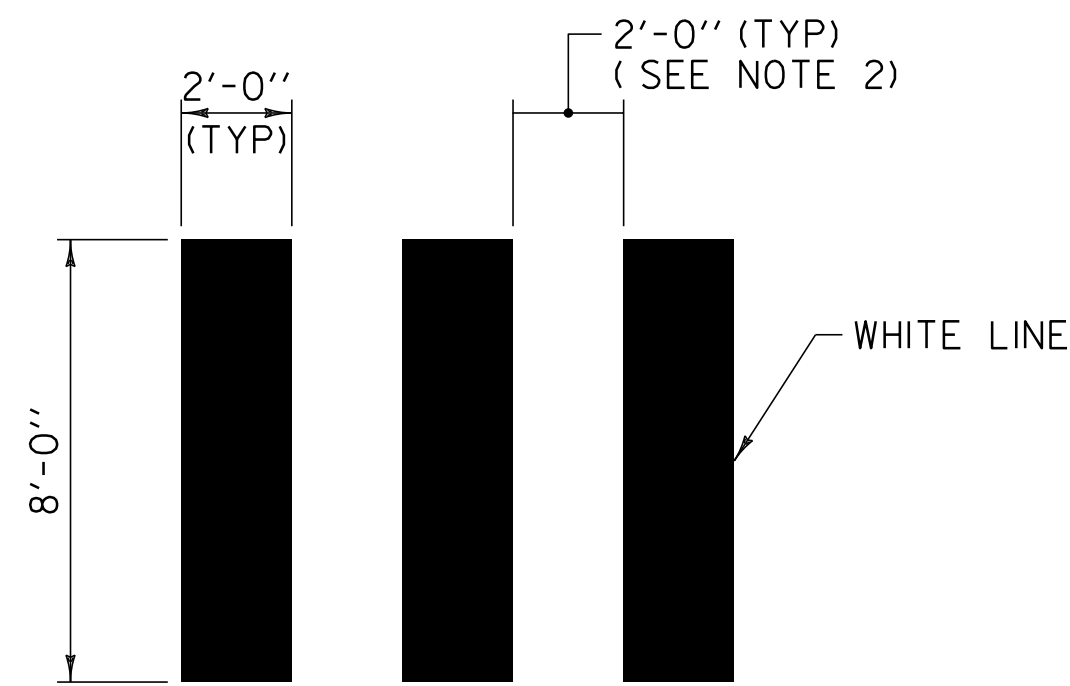
FASCIA MOUNTED STEEL POST W/BOX BEAM HAND RAIL



TOP MOUNTED STEEL POST W/BOX BEAM HAND RAIL

NOT TO SCALE

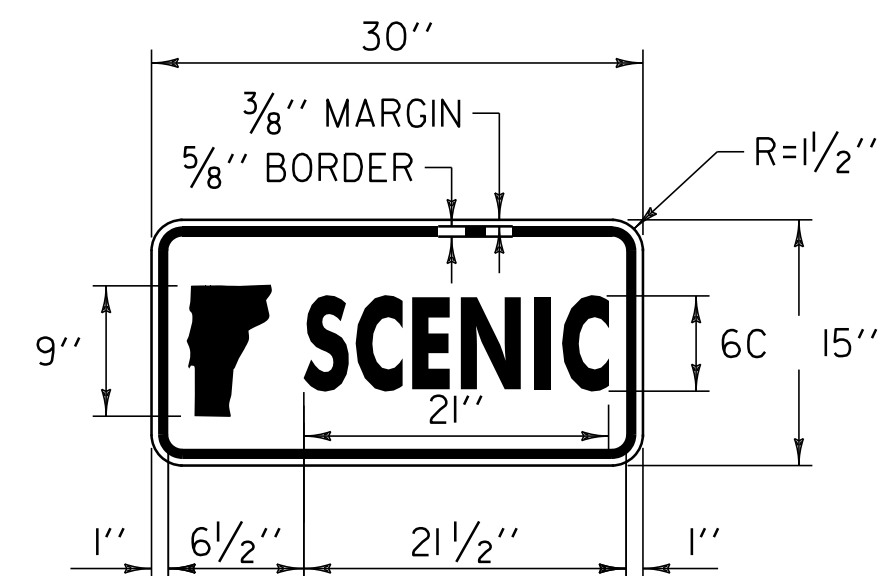
BRIDGE DETAIL SHEET #2	PROJECT NAME: MIDDLEBURY	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2627(1)	DRAWN BY: C.A.K.
	FILE NAME: p06c222.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 48 OF 114
	DESIGNED BY: M.J.L.	
	IPARM FILE: p06c222b3.i	



DETAIL OF CROSSWALK MARKINGS

NOTES:

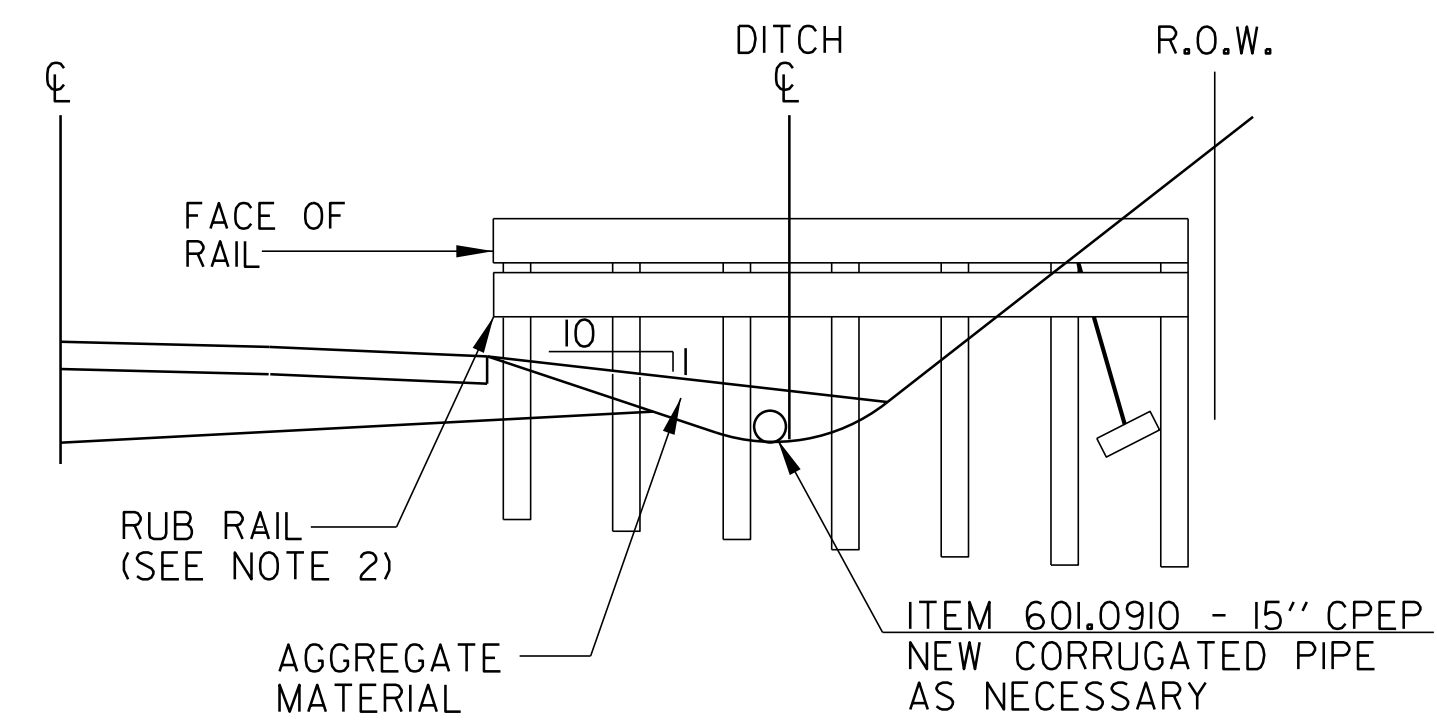
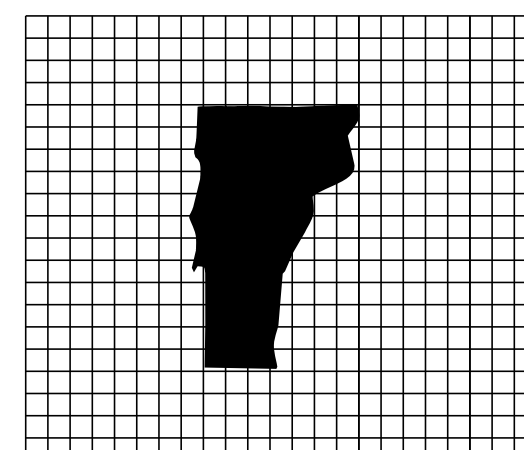
1. THE PER FOOT COST OF INSTALLED CROSSWALK, SHALL BE PAID AS ITEM 646.502 DURABLE CROSSWALK MARKING, THERMOPLASTIC AND ITEM 646.702 TEMPORARY CROSSWALK MARKING, PAINT.
2. ADJUST SPACING (12" - 24") TO AVOID WHEEL PATHS.



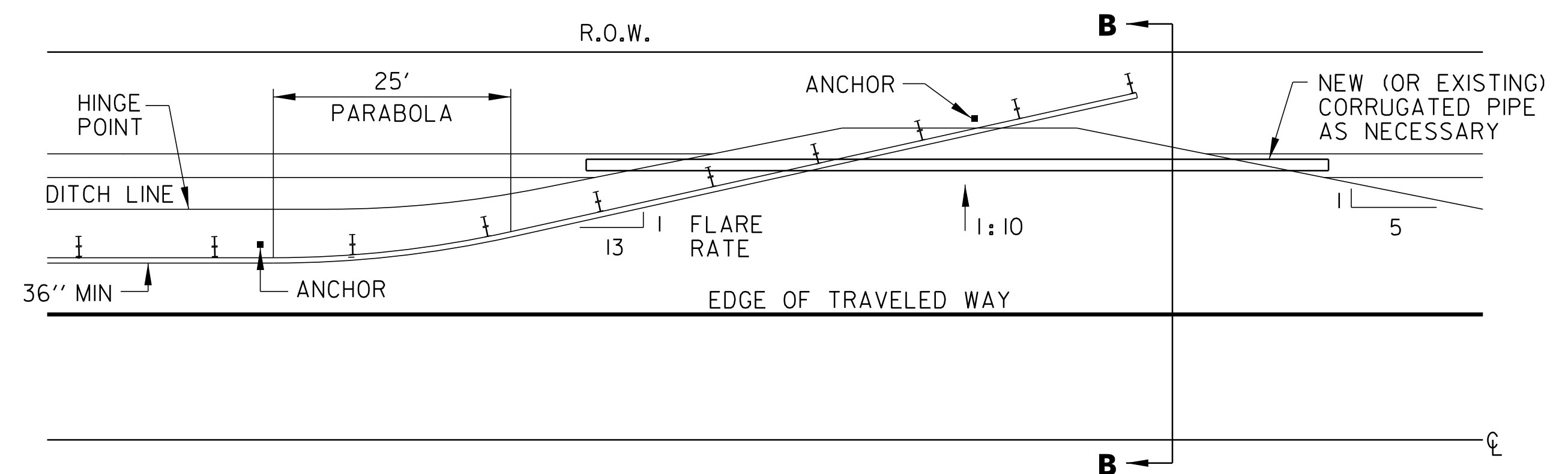
COLOR: GREEN BORDER & LEGEND (REFLECTIVE)
WHITE BACKGROUND (REFLECTIVE)

MATERIAL: PER VAOT STANDARD E-136B

LOCATION: VT, ROUTE 125
MIDDLEBURY:
STA 144+00.0, RT



SECTION B-B



BURIED GUARDRAIL ENDS INTO BACKSLOPES
MIDDLEBURY: STA 145+88.0 LT TO STA 145+50.5 LT

NOTES:

1. PRIMARY RAIL SHALL REMAIN AT A CONSTANT HEIGHT (LEVEL) RELATIVE TO THE HEIGHT OF RAIL AT THE EDGE OF SHOULDER.
2. ADDITION OF RUB RAIL IS REQUIRED WHEN OPENING BENEATH PRIMARY RAIL EXCEEDS 18in. RUB RAIL EXTENDS FROM THE EDGE OF SHOULDER TO THE BACK SLOPE.

NOT TO SCALE

MISCELLANEOUS DETAILS SHEET	PROJECT NAME: MIDDLEBURY	
	PROJECT NUMBER: STP 2627(1)	
	FILE NAME: p06c222.dgn	PLOT DATE: 18-SEP-2013 17:09
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
	DESIGNED BY: M.J.L.	CHECKED BY: D.W.E.
	IPARM FILE: p06c222mdt2.i	SHEET 49 OF 114

STATE OF VERMONT AGENCY OF TRANSPORTATION

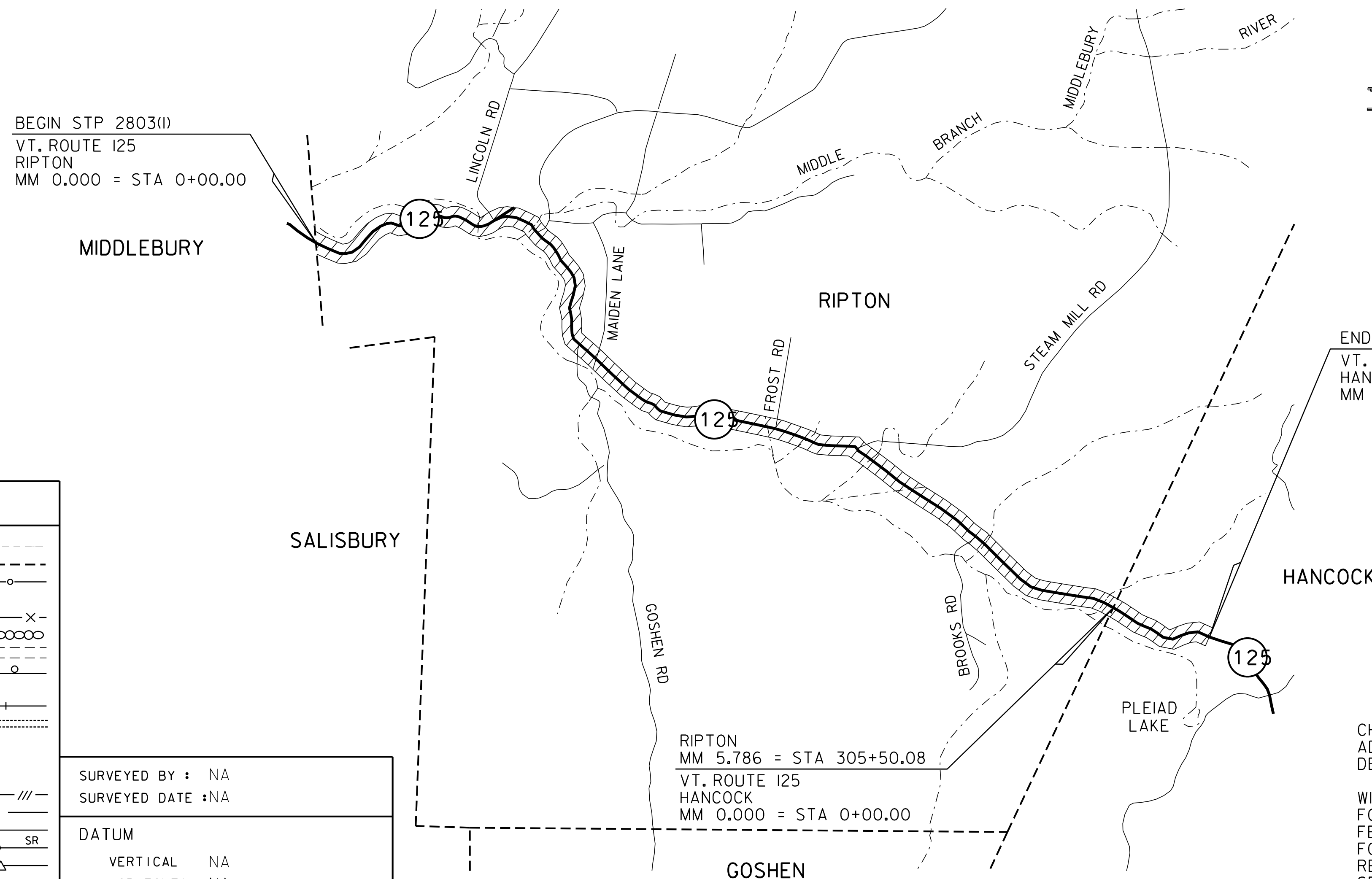
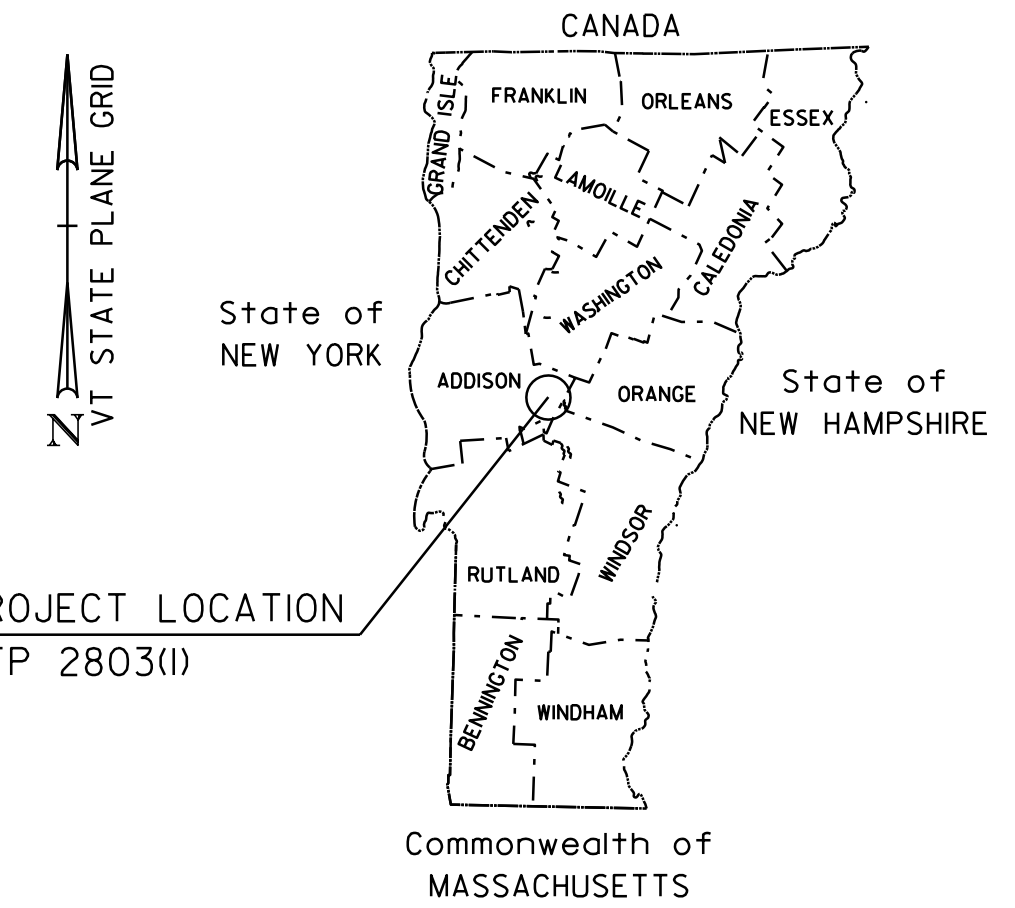


PROPOSED IMPROVEMENT TOWNS OF RIPTON AND HANCOCK COUNTY OF ADDISON VT. ROUTE 125

BEGINNING IN THE TOWN OF RIPTON ON VT. ROUTE 125 AT MILE MARKER 0.000 = STA 0+00.00
AND EXTENDING EASTERLY FOR A DISTANCE OF 34,510.08 FEET (6.536 MILES) TO A
POINT IN THE TOWN OF HANCOCK AT MILE MARKER 0.750 = STA 39+60.00

LENGTH OF ROADWAY 34,510.08 FT (6.536 MILES)
LENGTH OF PROJECT 34,510.08 FT (6.536 MILES)

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES RECLAIMING SEGMENTS OF THE EXISTING HIGHWAY AND
RESURFACING WITH A SHIM/LEVELING COURSE AND WEARING AND/OR OVERLAYING COURSE, NEW PAVEMENT
MARKINGS, GUARDRAIL IMPROVEMENTS, DRAINAGE IMPROVEMENTS, NEW SIGNS AND INCIDENTAL ITEMS.



BITUMINOUS CONCRETE SUPERPAVE MIXTURE DESIGN CRITERIA	
DESIGN LANE/DESIGN LIFE ESAL	368,000
DESIGN NUMBER OF GYRATIONS	65
PERFORMANCE GRADE ASPHALT BINDER	SEE SECTION 490 GENERAL SPECIAL PROVISIONS

END STP 2803(I)
VT. ROUTE 125
HANCOCK
MM 0.750 = STA 39+60.00

TRAFFIC DATA

SECTIONS	ADT		DHV		ESALS	ESALS
	2009	2019	2009	2019	2009-2019	2009-2029
BEGIN PROJECT TO TH-1 (LINCOLN ROAD)	1900	2100	210	240	316,000	736,000
TH-1 (LINCOLN ROAD) TO END PROJECT	1250	1300	140	140	137,000	314,000

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING
CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY
ADMINISTRATION OR THE DIRECTOR OF PROGRAM
DEVELOPMENT.

CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE
WITH THESE PLANS AND THE STANDARD SPECIFICATIONS
FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE
FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006
FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT
REVISIONS AND SUCH REVISED SPECIFICATIONS AND
SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE
PLANS.

CONVENTIONAL SYMBOLS	
COUNTY LINE	---
TOWN LINE	- - - - -
LIMITS OF ACCESS	○ ○ ○ ○ ○
POINT OF ACCESS	X
FENCE LINE	X — X — X — X —
STONE WALL	○ ○ ○ ○ ○
TRAVELED WAY	— — — — —
GUARD RAIL	○ ○ ○ ○ ○
RAILROAD	— + — + — + — + —
SURVEY LINE	— + — + — + — + —
CULVERT	— + — + — + — + —
POWER POLE	⊕
TELEPHONE POLE	⊕
TREES	⊕
CONTROL OF ACCESS	— // — // — // — // —
PROPERTY LINE	— — — — —
R.O.W. TAKING LINE	— SR — SR — SR — SR —
SLOPE RIGHTS	○ — SR — △ — SR — SR — SR —
TOP OF CUT	△ — △ — △ — △ —
TOE OF SLOPE	○ — ○ — ○ — ○ —

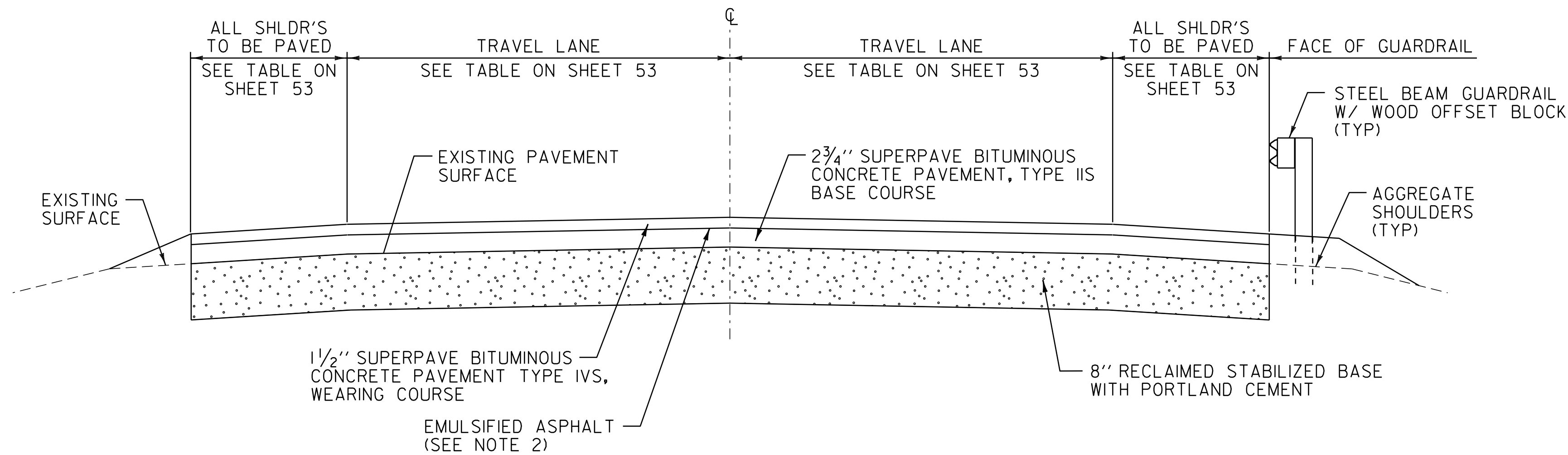
SURVEYED BY : NA
SURVEYED DATE : NA

DATUM
VERTICAL NA
HORIZONTAL NA

PROJECT MANAGER :	KEVIN MARSHIA
PROJECT NAME :	RIPTON - HANCOCK
PROJECT NUMBER :	STP 2803 (I)
SHEET	50 OF 114 SHEETS

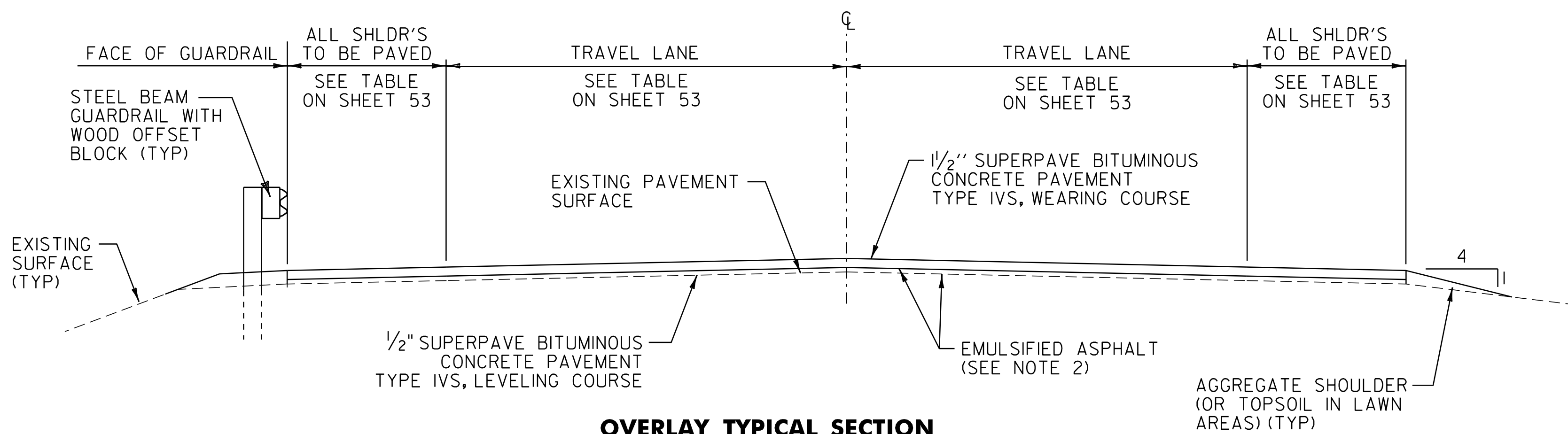
NOTES

1. THE WEARING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. THE BASE COURSE SHALL BE TYPE IIS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. THE LEVELING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT.
2. EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES, BETWEEN ALL COURSES OF PAVEMENT AND ON COLD PLANED SURFACES AT THE RATE OF 0.025 GAL/SY OR AS DIRECTED BY THE RESIDENT ENGINEER.
3. SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = +/- 1/4" (TOTAL PAVEMENT THICKNESS EXCLUDING LEVELING).
4. PRIOR TO RECLAIMING, ANY EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER SHALL BE EXCAVATED TO THE DEPTH OF RECLAIMING OR AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATED MATERIAL WILL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT AS DIRECTED BY THE RESIDENT ENGINEER. THIS WORK WILL BE PAID FOR USING THE APPROPRIATE RENTAL ITEM(S). THE METHOD OF REMOVAL AND THE USE OF RENTAL ITEM(S) SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO ANY WORK BEING DONE. MATERIAL REMOVED SHALL BE REPLACED WITH ITEM 301.28 SUBBASE OF CRUSHED GRAVEL, FINE GRADED. THIS WORK SHALL BE DONE BEFORE THE BASE COURSE IS PAVED TO ALLOW ALL OF THE LIFTS OF PAVEMENT THROUGHOUT THE PROJECT AREA. AN ADDITIONAL QUANTITY OF ITEM 301.28 SUBBASE OF CRUSHED GRAVEL, FINE GRADED HAS BEEN INCLUDED TO CORRECT SUPERELEVATION AND GRADATION DEFICIENCIES WITHIN THE RECLAIMED SECTION. AN ESTIMATED THICKNESS OF 2" HAS BEEN INCLUDED FOR THE ENTIRE RECLAIMED SURFACE AREA.
5. ALL EDGES OF PAVEMENT SHALL BE BACKED UP TO FULL HEIGHT WITH AGGREGATE SHOULDER MATERIAL AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.12 AGGREGATE SHOULDERS.
6. DITCHING MATERIAL OR EARTH BORROW SHALL BE USED FOR THE CONSTRUCTION OF MANUFACTURED TERMINAL FLARES WHICH SHALL BE CAPPED WITH AN ESTIMATED 3" DEPTH OF AGGREGATE SHOULDER MATERIAL AS DIRECTED BY THE RESIDENT ENGINEER. THE QUANTITIES INCLUDED REFLECT 5 TONS OF AGGREGATE SHOULDER MATERIAL AND 25 CUBIC YARDS OF DITCHING MATERIAL OR EARTH BORROW FOR EACH GUARDRAIL TERMINAL.
7. THE PROPOSED GUARDRAIL SHALL BE INSTALLED IN A LOCATION THAT MAXIMIZES THE DISTANCE FROM THE CENTER OF THE ROAD TO THE FACE OF GUARDRAIL. 3' OF BACKING IS REQUIRED BEHIND THE FACE OF GUARDRAIL WITH 6' POSTS. IF THIS CANNOT BE OBTAINED, THEN 8' POSTS SHALL BE USED.
8. ITEM 609.15 DUST AND ICE CONTROL WITH CALCIUM CHLORIDE HAS AN ESTIMATED QUANTITY TO BE USED ON SIDE ROAD APPROACHES WHICH WILL BE PAVED.
9. AN ESTIMATED QUANTITY OF ITEM 619.17 YIELDING MARKER POSTS HAS BEEN INCLUDED TO DELINEATE PIPE INLETS, PIPE OUTLETS AND DROP INLETS LOCATED OUTSIDE OF THE PAVEMENT SURFACE OR AS DIRECTED BY THE RESIDENT ENGINEER.
10. WORK FOR REPAIR OF EXISTING WEATHERING STEEL BOX BEAM GUARDRAIL SHALL BE PERFORMED IN ACCORDANCE WITH GUARDRAIL, WEATHERING OF SECTION 900. PAYMENT WILL BE MADE UNDER PAY ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING).



RECLAIMED STABILIZED TYPICAL SECTION

VT. ROUTE 125 RIPTON STA 0+00.00 TO RIPTON STA 52+80.00
 VT. ROUTE 125 RIPTON STA 84+48.00 TO RIPTON STA 305+50.08
 VT. ROUTE 125 HANCOCK STA 0+00.00 TO HANCOCK STA 39+60.00



OVERLAY TYPICAL SECTION

VT. ROUTE 125 RIPTON STA 52+80.00 TO RIPTON STA 84+48.00

TYPICAL ORDER OF OPERATIONS FOR RECLAIMED STABILIZED BASE WITH PORTLAND CEMENT PAVEMENT REHABILITATION

STEP	DESCRIPTION	PAYMENT ITEM(S)
1	REPAIR EXISTING SHOULDERS AS DIRECTED BY THE RESIDENT ENGINEER	ITEM 301.28 SUBBASE OF CRUSHED GRAVEL, FINE GRADED
2	RECLAIM EXISTING ROADBED 8" USING WATER FOR STABILIZATION AND COMPACTION	ITEM 310.20 RECLAIMED STABILIZED BASE, WATER USED FOR STABILIZATION SHALL BE CONSIDERED INCIDENTAL TO ITEM 310.20 PER SPECIFICATION AS DIRECTED BY THE RESIDENT ENGINEER
3	CORRECT SUPERELEVATION DEFICIENCIES	ITEM 301.28 SUBBASE OF CRUSHED GRAVEL, FINE GRADED
4	PLACE PORTLAND CEMENT ON ROADBED SURFACE AND RECLAIM ROADBED 8", CURE, MICROCRACK, AND APPLY FOG SEAL SURFACE TREATMENT	ITEM 900.675 SPECIAL PROVISION (RECLAIMED STABILIZED BASE, PORTLAND CEMENT), ITEM 900.680 SPECIAL PROVISION (PORTLAND CEMENT FOR BASE STABILIZATION) AND ITEM 900.683 SPECIAL PROVISION (FOG SEAL SURFACE TREATMENT)
5	PLACE 2 3/4" TYPE IIS BASE COURSE	ITEM 490.30 SUPERPAVE BITUMINOUS CONCRETE PAVEMENT
6	PLACE 1 1/2" TYPE IVS WEARING COURSE	ITEM 490.30 SUPERPAVE BITUMINOUS CONCRETE PAVEMENT

NOT TO SCALE

PROJECT SHEET

PROJECT NAME: RIPTON - HANCOCK

PROJECT NUMBER: STP 2803(1)

FILE NAME: p08cl60.dgn

PROJECT LEADER: D.E.G.

DESIGNED BY: M.J.M.

IPARM FILE: p08cl60+yp1.i

PLOT DATE: 18-SEP-2013 17:09

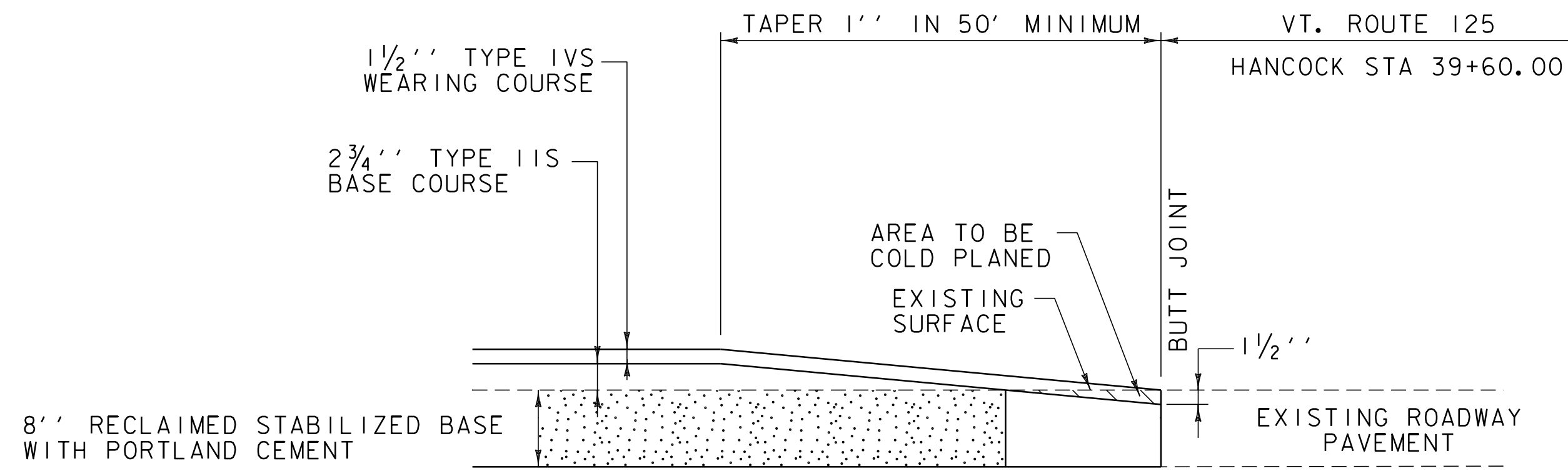
DRAWN BY: C.A.K.

CHECKED BY: D.W.E.

SHEET 51 OF 114

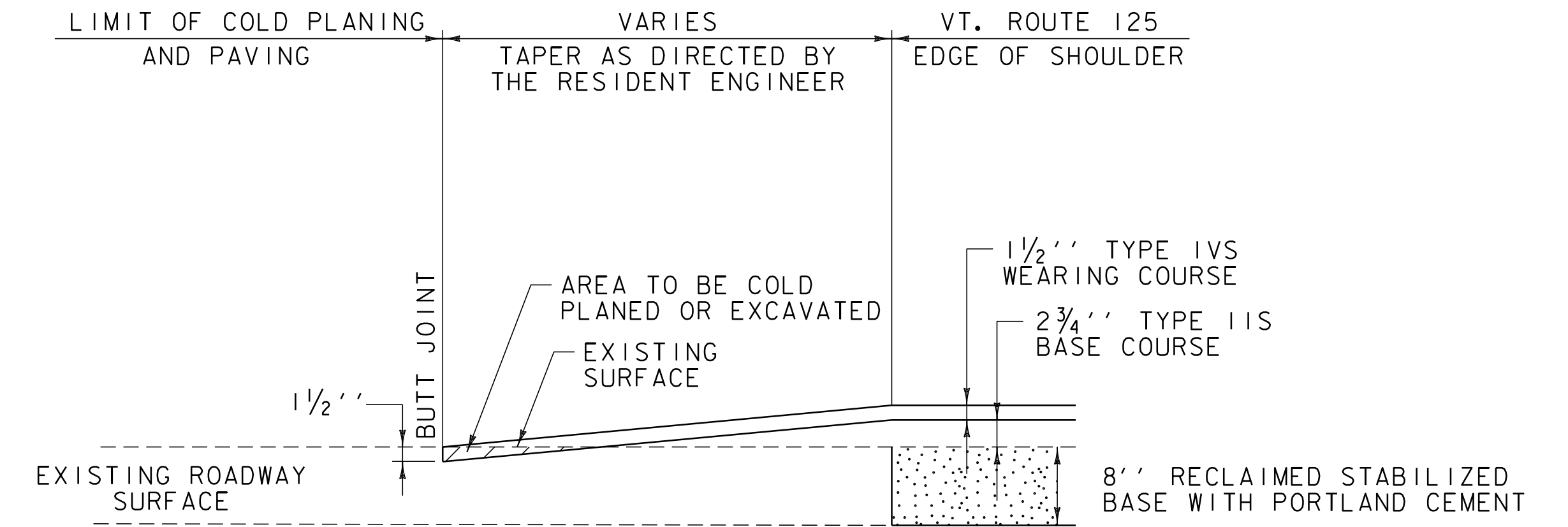
NOTE:

1. THIS LIST OF PROCEDURES FOR REHABILITATING THE EXISTING ROADBED IS PRESENTED FOR INFORMATIONAL PURPOSES ONLY AND SHALL NOT BE CONSIDERED ALL INCLUSIVE. THE CONTRACTOR SHALL PREPARE THEIR OWN SCHEDULE OF OPERATIONS TO COMPLETE THE PROPOSED WORK UTILIZING THE APPROPRIATE PAY ITEMS AS SHOWN IN THE PLANS AS DIRECTED BY THE RESIDENT ENGINEER.



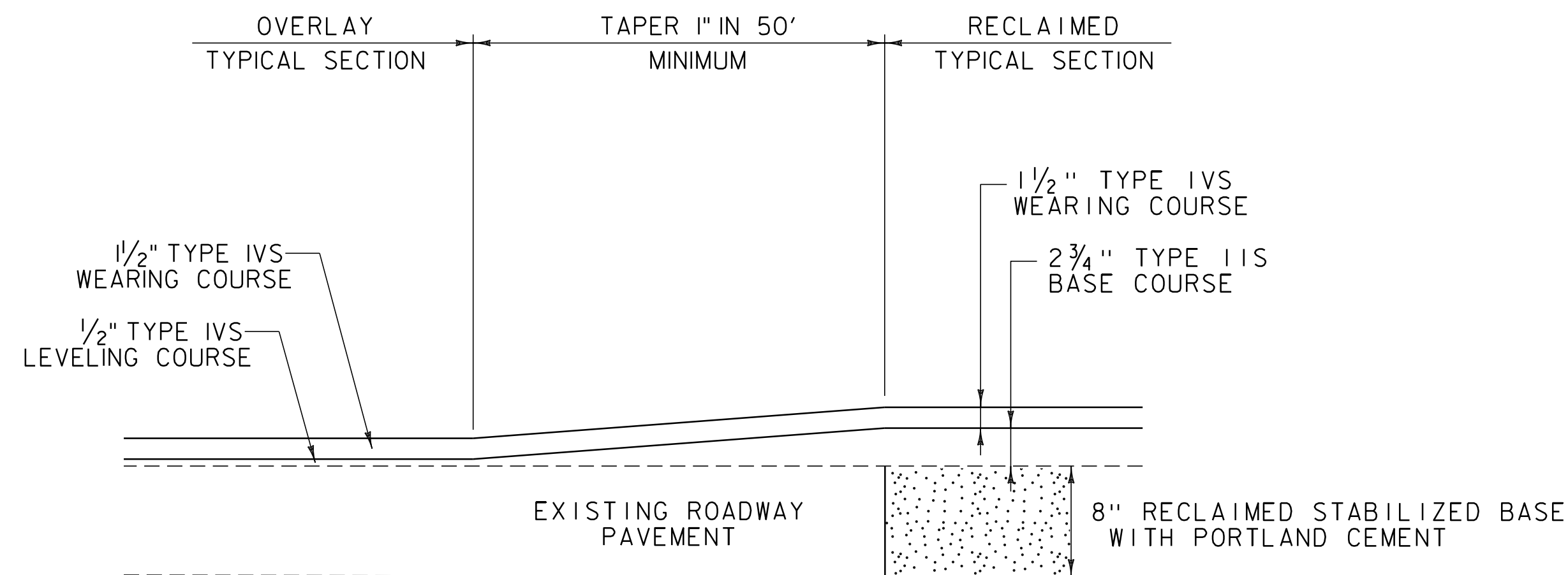
RECLAIMED APPROACH AREA DETAIL

VT. ROUTE 125 HANCOCK STA 39+60.00 - END PROJECT



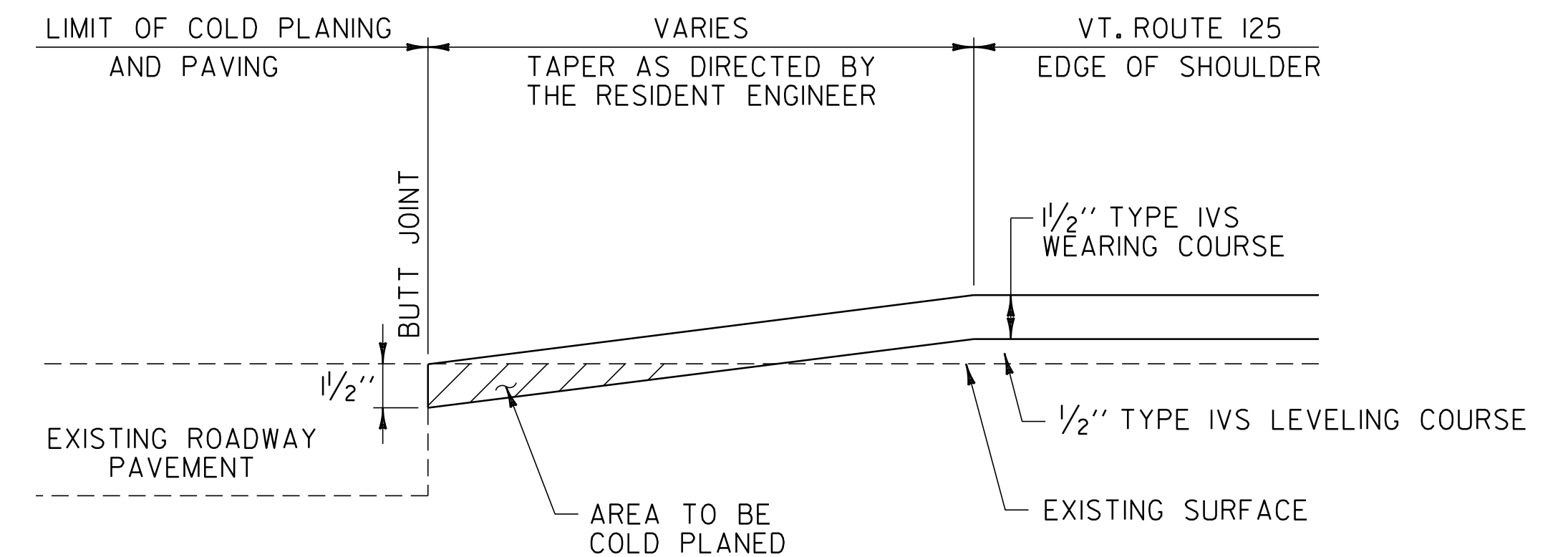
RECLAIMED SIDE ROAD DETAIL

RIPTON: TH-18 STA 116+37.12 RT
 TH-2 STA 124+55.52 LT
 TH-19 STA 184+00.80 LT
 TH-20 STA 211+35.84 LT
 TH-21 STA 256+39.68 RT



RECLAIMED TO OVERLAY TRANSITION DETAIL

VT. ROUTE 125 RIPTON STA 52+80.00
 VT. ROUTE 125 RIPTON STA 84+48.00

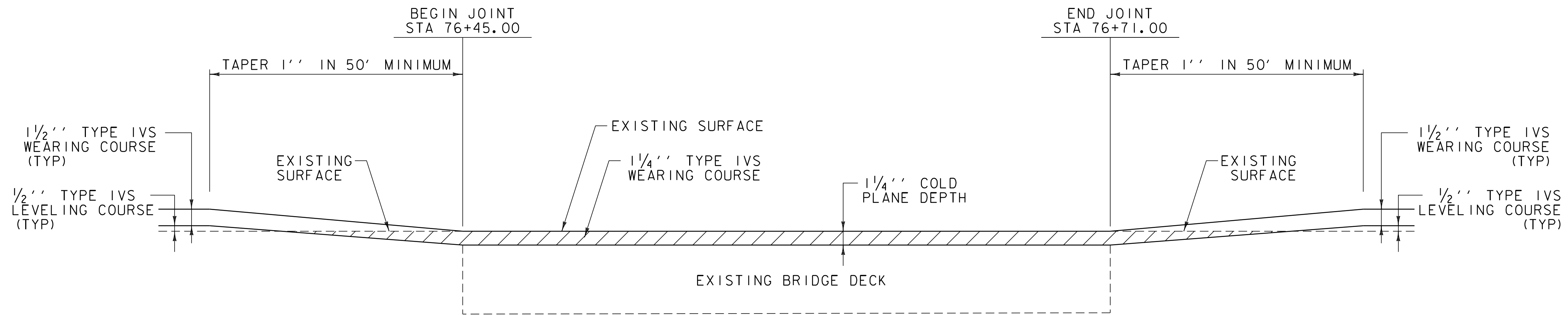


OVERLAYED SIDE ROAD DETAIL

RIPTON: TH-I STA 63+57.12 LT
 TH-II STA 77+56.32 LT

NOT TO SCALE

PAVING DETAILS SHEET #1	PROJECT NAME: RIPTON - HANCOCK	
	PROJECT NUMBER: STP 2803(1)	
	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:09
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.	
IPARM FILE: p08cl60pvd1.i	SHEET 52 OF 114	



BRIDGE TRANSITION AREA DETAIL

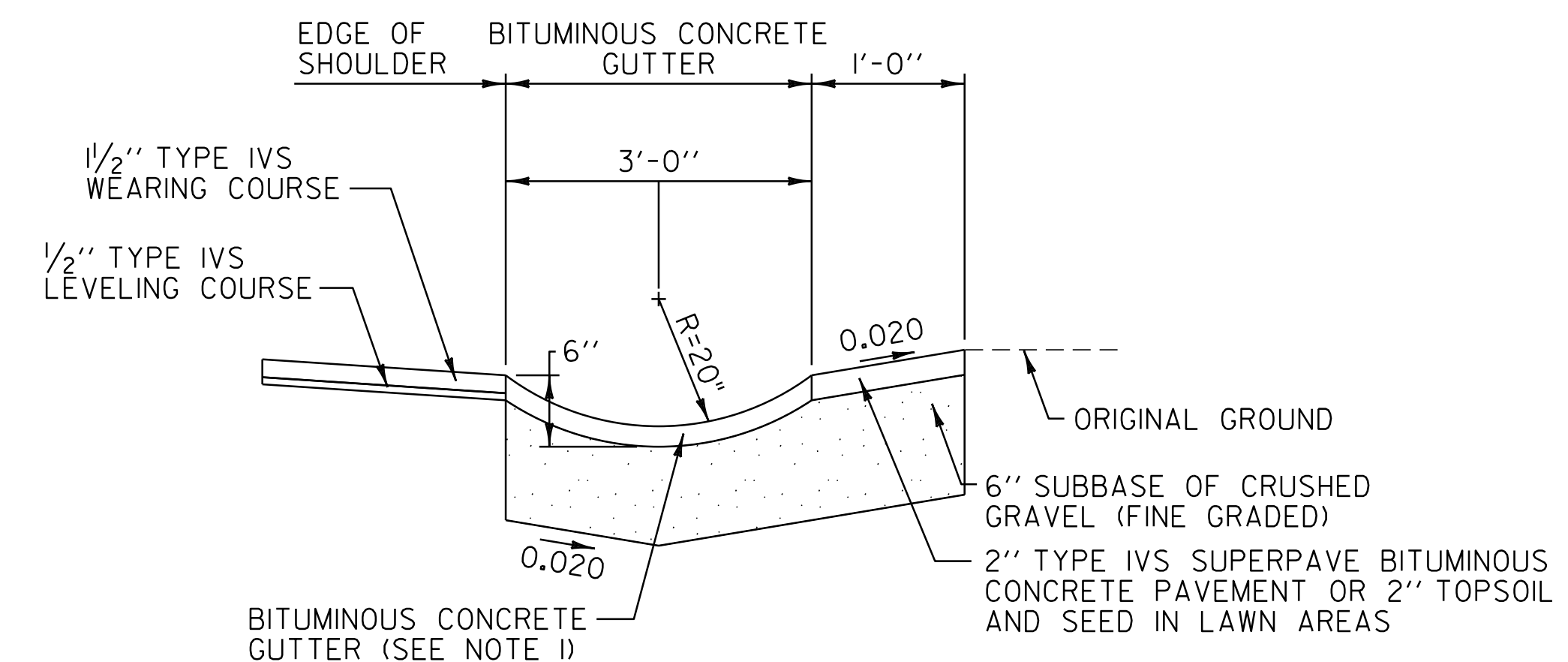
BR 15 VT.ROUTE 125 RIPTON STA 76+45.00 TO STA 76+71.00

SEEDING FORMULA

RATE: DOUBLE IF HYDROSEEDING

% WT.	LBS./A.	NAME	PUR %	GERM %
38	32	CREEPING RED FESCUE	98	90
29	24	SPARTAN HARD FESCUE	95	85
15	12	AZAY SHEEP'S FESCUE	95	87
15	12	ANNUAL RYE GRASS	95	90
3	--	INERTS	--	--
100.0	80 LB/A			

EXISTING WIDTH					
RIPTON			HANCOCK		
STATION	WIDTH	STATION	WIDTH	STATION	WIDTH
8+35.00	27'-4"	150+00.00	26'-0"	0+00.00	29'-5"
12+05.00	23'-6"	161+00.00	28'-4"	2+10.00	35'-11"
24+10.00	27'-5"	173+65.00	27'-1"	4+10.00	43'-4"
35+75.00	26'-0"	185+65.00	26'-2"	5+75.00	32'-9"
48+00.00	27'-2"	196+65.00	27'-7"	12+40.00	29'-5"
59+50.00	34'-7"	208+65.00	27'-0"	17+70.00	33'-3"
61+50.00	25'-7"	217+40.00	26'-3"	22+25.00	31'-1"
72+25.00	24'-6"	224+80.00	25'-9"	27+60.00	33'-2"
76+45.00	19'-10"	230+65.00	26'-8"	29+90.00	31'-5"
80+30.00	24'-2"	240+95.00	26'-8"	31+75.00	32'-0"
90+00.00	24'-0"	248+65.00	27'-1"	39+60.00	32'-4"
102+75.00	27'-2"	260+65.00	26'-5"		
111+20.00	27'-5"	270+65.00	26'-10"		
119+15.00	25'-6"	282+65.00	26'-9"		
130+00.00	26'-10"	288+65.00	26'-10"		
140+00.00	26'-6"	298+65.00	26'-7"		



BITUMINOUS CONCRETE GUTTER DETAIL

RIPTON: STA 56+30.0 - STA 58.06.0 LT (6.9 TON)
STA 60+32.0 - STA 63+45.0 LT (12.2 TON)

HANCOCK: STA 11+05.0 - STA 20+00.0 RT (34.9 TON)

NOTE:

1. BITUMINOUS CONCRETE GUTTER TO BE CONSTRUCTED BY PLACING 1-2" TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT LIFT AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT UNDER ITEM 616.47 BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS.

GENERAL NOTES

SEED MIXTURE:
SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS WEED SEED.

SEED:
TO BE APPLIED PER SEEDING FORMULA OR AS DIRECTED BY THE RESIDENT ENGINEER.

FERTILIZER:
FORMULA 10-20-10 TO BE USED WITH SEED APPLIED AT THE RATE OF 500 LBS/ACRE (HYDRO SEEDERS MAY USE 19-19-19 FORMULA).

AGRICULTURAL LIMESTONE:
TO BE APPLIED AT THE RATE OF 2 TONS/ACRE OR AS DIRECTED BY THE RESIDENT ENGINEER.

TOPSOIL:
TO BE USED WITH SEED AS INDICATED ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.

PROJECT PAVING LIMITS

TOWN & ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING TON	NOTES
RIPTON:						
VT. ROUTE 125	0+00.00	52+80.00	3'-0" - 11'-0" - 11'-0" - 3'-0"	1/2"	-	RECLAIM 8", PAVE WITH 2 3/4" TYPE IIS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	52+80.00	62+50.00	3'-0" - 11'-0" - 11'-0" - 3'-0"	1/2"	85	LEVEL 1/2" TYPE IVS & PAVE 1/4" TYPE IVS
VT. ROUTE 125	62+50.00	76+45.00	2'-0" - 11'-0" - 11'-0" - 2'-0"	1/2"	117	LEVEL 1/2" TYPE IVS & PAVE 1/4" TYPE IVS
VT. ROUTE 125	76+45.00	76+75.00	10'-0" - 10'-0"	1/2"	-	BR 15 COLD PLANE 1/4", PAVE WITH 1/4" TYPE IVS
VT. ROUTE 125	76+75.00	84+48.00	2'-0" - 11'-0" - 11'-0" - 2'-0"	1/2"	64	LEVEL 1/2" TYPE IVS & PAVE 1/4" TYPE IVS
VT. ROUTE 125	84+48.00	102+75.00	2'-0" - 11'-0" - 11'-0" - 2'-0"	1/2"	-	RECLAIM 8", PAVE WITH 2 3/4" TYPE IIS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	102+75.00	222+15.00	3'-0" - 11'-0" - 11'-0" - 3'-0"	1/2"	-	RECLAIM 8", PAVE WITH 2 3/4" TYPE IIS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	222+15.00	229+65.00	4'-0" - 11'-0" - 11'-0" - 3'-0"	1/2"	-	RECLAIM 8", PAVE WITH 2 3/4" TYPE IIS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	229+65.00	305+50.08	3'-0" - 11'-0" - 11'-0" - 3'-0"	1/2"	-	RECLAIM 8", PAVE WITH 2 3/4" TYPE IIS & PAVE WITH 1/2" TYPE IVS
HANCOCK:						
VT. ROUTE 125	0+00.00	2+50.00	3'-0" - 11'-0" - 11'-0" - 3'-0"	1/2"	-	RECLAIM 8", PAVE WITH 2 3/4" TYPE IIS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	2+50.00	4+10.00	4'-0" - 12'-0" - 12'-0" - 10'-0"	1/2"	-	RECLAIM 8", PAVE WITH 2 3/4" TYPE IIS & PAVE WITH 1/2" TYPE IVS
VT. ROUTE 125	4+10.00	39+60.00	4'-0" - 12'-0" - 12'-0" - 4'-0"	1/2"	-	RECLAIM 8", PAVE WITH 2 3/4" TYPE IIS & PAVE WITH 1/2" TYPE IVS

NOT TO SCALE

**PAVING
DETAILS
SHEET #2**

PROJECT NAME: RIPTON - HANCOCK

PROJECT NUMBER: STP 2803(1)

FILE NAME: p08cl60.dgn

PROJECT LEADER: D.E.G.

DESIGNED BY: M.J.M.

IPARM FILE: p08cl60pvd2.i

PLOT DATE: 18-SEP-2013 17:09

DRAWN BY: C.A.K.

CHECKED BY: D.W.E.

SHEET 53 OF 114

QUANTITY SHEET 1

SUMMARY OF ESTIMATED QUANTITIES

BRIDGE	FULL C.E.	ROADWAY	QUANTITIES GRAND TOTAL	UNIT	ITEMS	ITEM NO.	ROUNDING
		575	575	CY	EARTH BORROW	203.30	-
		4460	4460	CY	TRENCH EXCAVATION OF EARTH	204.20	53
		I	I	CY	TRENCH EXCAVATION OF EARTH, EXPLORATORY (N.A.B.I.)	204.22	EST.
		75	75	CY	•• GRANULAR BACKFILL FOR STRUCTURES	204.30	EST.
		600	600	SY	COLD PLANING, BITUMINOUS PAVEMENT	210.10	3I
		15,000	15,000	TON	SUBBASE OF CRUSHED GRAVEL, FINE GRADED	301.28	16I
		60	60	TON	SUBBASE, RAP	301.40	EST.
		101,000	101,000	SY	RECLAIMED STABILIZED BASE	310.20	769
		2700	2700	TON	AGGREGATE SHOULDERS	402.12	4I
		245	245	CWT	EMULSIFIED ASPHALT	404.65	3
		I	I	LU	PRICE ADJUSTMENT, ASPHALT CEMENT (N.A.B.I.)	406.50	EST.
					BEGIN OPTION AA		
		26,500	26,500	TON	SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (0.0% RAP CONTENT)	490.30	295
		26,500	26,500	TON	SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (0.0% < RAP CONTENT < 15.0%)	490.30	295
		26,500	26,500	TON	SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (15.0% <= RAP CONTENT < 25.0%)	490.30	295
		26,500	26,500	TON	SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (25.0% <= RAP CONTENT <= 50.0%)	490.30	295
					END OPTION AA		
		I	I	LU	AIR VOIDS PAY ADJUSTMENT (N.A.B.I.)	490.31	EST.
		I	I	LU	MAT DENSITY PAY ADJUSTMENT (N.A.B.I.)	490.32	EST.
		I	I	LU	SURFACE TOLERANCE PAY ADJUSTMENT (N.A.B.I.)	490.33	EST.
		I	I	LU	LONGITUDINAL JOINT COMPACTION PAY ADJUSTMENT (N.A.B.I.)	490.34	EST.
50			50	LF	BRIDGE EXPANSION JOINT, ASPHALTIC PLUG	516.10	10.3
50			50	CF	RAPID SETTING CONCRETE REPAIR MATERIAL WITH COARSE AGGREGATE	580.20	EST.
					BEGIN OPTION BB		
		315	315	LF	18" CSP .064 (2-2/3 x 1/2)	601.0015	4
		315	315	LF	18" RCPES CLASS III	601.0815	4
		315	315	LF	18" CPEP (SL)	601.2615	4
					END OPTION BB		
					BEGIN OPTION CC		
		I	I	EA	18" CSPES .064 (2-2/3 X 1/2)	601.6015	-
		I	I	EA	18" RCPES CLASS III	601.6815	-
		I	I	EA	18" CPEPES	601.7015	-
					END OPTION CC		
		3	3	EA	PRECAST REINFORCED CONCRETE DROP INLET WITH CAST IRON GRATE	604.18	-
		2	2	EA	REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I	604.412	-
		I	I	EA	REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS II	604.415	-
		I	I	EA	REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS III	604.418	-
		8100	8100	LF	6 INCH UNDERDRAIN PIPE	605.10	-
		80	80	EA	UNDERDRAIN FLUSHING BASIN	605.95	-
		500	500	HR	POWER GRADER RENTAL	608.15	EST.

DETAILED SUMMARY OF QUANTITIES

QUANTITIES	UNIT	ITEMS
		RECLAIMED STABILIZED BASE
100,231 769	SY	MAINLINE ROUNDING
101,000	SY	TOTAL
		SPECIAL PROVISION (RECLAIMED STABILIZED BASE, PORTLAND CEMENT)
100,231 769	SY	MAINLINE ROUNDING
101,000	SY	TOTAL
		COLD PLANING, BITUMINOUS PAVEMENT
67 502 31	SY	MAINLINE SIDEROADS ROUNDING
600	SY	TOTAL
		SUPERPAVE BITUMINOUS CONCRETE PAVEMENT
9571 16,078 266 88 202 290	TON	WEARING COURSE (TYPE IVS) BASE COURSE (TYPE IIS) TYPE IVS LEVELING SIDE ROADS PULLOUTS ROUNDING
26,500	TON	TOTAL
		AGGREGATE SHOULDERS
115 2544 41	TON	CAPPING FLARED GUARDRAIL END SECTIONS BACK UP SHOULDERS TO FULL HEIGHT ROUNDING
2700	TON	TOTAL
		RAP MATERIAL GENERATED FROM PROJECT
60 60	TON	COLD PLANE GRINDINGS GENERATED SUBBASE, RAP

DETAILED SUMMARY OF QUANTITIES

QUANTITIES	UNIT	ITEMS

•• REVISED 02-16-10 BY SEH
ADDED ITEM 204.30, GRANULAR BACKFILL FOR STRUCTURES

QUANTITY SHEET #1	PROJECT NAME: RIPTON - HANCOCK	
	PROJECT NUMBER: STP 2803(1)	
	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:09
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.	
IPARM FILE: p08cl60quanl.i	SHEET 54 OF 114	

ITEM DETAIL SUMMARY SHEET #1

LOCATION			GUARDRAIL												MISCELLANEOUS								REMARKS																												
BEGIN STATION	END STATION	POS.	203.30	402.12	616.35	621.79	621.80	653.20	900.620	900.620	900.640	900.640	900.640	900.640	900.640	900.640	204.20	604.412	604.415	604.418	605.10	605.95		616.47	676.10																										
			EARTH BORROW CY	AGG. SHLDRS TON	TREATED TIMBER CURB LF	ADJUST HEIGHT OF G.R. LF	REMOVAL & DISPOSAL LF	TEMP. EROSION MATTE SY	MANUF. TERM. SECT., FLARED/ WEATHERING EA	MANUF. TERM. SECT., TANGENT/ WEATHERING EA	S.B. G.R. WEATH. LF	HD S.B. G.R. WEATHERING/ NESTED EA	HD S.B. G.R. WEATHER/ NESTED W/8 FEET POST EA	HD S.B. G.R. WEATHER/ NESTED W/8 FEET POST EA	BOX BEAM REPAIRS, WEATH. (NEW MTRLS) LF	BOX BEAM REPAIRS, WEATH. (SALVGD MTRLS) LF	TREN. EXCAV. EARTH CY	REHABING CLASS I EA	D'S CB'S CLASS II EA	OR MH'S CLASS III EA	6 INCH UNDER DRAIN LF	UNDERDRAIN FLUSHING BASIN EA		BIT. CONC. GUT. & TRAF. ISLANDS TON	DELIN. W/STEEL POST EA																										
RIPTON:																																																			
0+00.0	305+50.1	LT/RT	1000																																																
0+00.0	62+50.0	LT/RT												500												1000	10																								
0+00.0	110+00.0	LT/RT																																																	
0+00.0	305+50.08	LT/RT																																																	
16+35.5	17+48.0	RT	50	10				50.0	2			12.5	25.0																																						
56+30.0	58+06.0	LT																																																	
58+00.0	60+00.0	LT						200																																											
60+32.0	63+45.0	LT																																																	
84+48.0	305+50.0	LT/RT												2500													5000		50																						
143+59.5	147+22.0	RT	50	10				354.0	50.0	2			287.5																																						
164+75.0	168+50.0	RT	50	10				122.0	50.0	2			275.0	25.0																																					
165+50.0	169+25.0	LT	50	10				78.0	50.0	2			275.0	25.0																																					
190+80.0	193+80.0	LT	50	10				130.0	50.0	2			200.0	25.0																																					
191+67.5	194+55.0	RT	50	10				127.0	50.0	2			187.5	25.0																																					
257+62.0	258+99.5	RT	50	10				130.0	50.0	2			62.5																																						
258+01.0	259+76.0	LT	50	10				167.0	50.0	2			100.0																																						
298+11.5	303+99.0	RT	25	5	140				483.0	25.0	1	1		500.0																																					
301+99.5	303+74.5	LT	50	10				166.0	50.0	2			100.0																																						
305+05.0	305+50.1	RT	25	5				40.0	25.0	1			7.6																																						
HANCOCK:																																																			
0+00.0	39+60.0	LT/RT						254																			500		1												1000		10								
0+00.0	17+79.9	RT	25	5	350				1784.0	25.0	1			1742.4																																					
11+05.0	17+95.0	RT																																																	
26+00.0	31+50.0	LT/RT												550													1100		10																						
26+31.0	34+56.0	RT	50	10				823.0	50.0	2			750.0																																						
SHEET SUBTOTALS:			575.0	2659	690	1000	4404.0	575.0	23	1		4500.0	75.0	50.0	900	900	4050	2	1	1	8100	80	46.0	23	<p>•• REVISED 02-16-10 BY SEH ADDED ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING) (NEW MATERIAL) AND ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING) (SALVAGED MATERIAL). REMOVED ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). ADDED ITEM 621.79 ADJUST HEIGHT OF GUARDRAIL.</p>																										
ROUNDINGS:			-	41	10	-	96	10.0	-	-	50.0	-	-	-	-	50	-	-	-	-	-	9	-																												
TOTALS:			575	2700	700	1000	4500	585	23	1		4550	75	50	900	900	4100	2	1	1	8100	80	55	23																											

ITEM DETAIL SUMMARY SHEET #1	PROJECT NAME: RIPTON - HANCOCK
	PROJECT NUMBER: STP 2803(1)
	FILE NAME: p08cl60.dgn
	PLOT DATE: 18-SEP-2013 17:09
	PROJECT LEADER: D.E.G.
	DRAWN BY: C.A.K.
	DESIGNED BY: M.J.M.
	CHECKED BY: D.W.E.
	IPARM FILE: p08cl60ids.i
	SHEET 57 OF 114

DRAINAGE SUMMARY SHEET DD-1

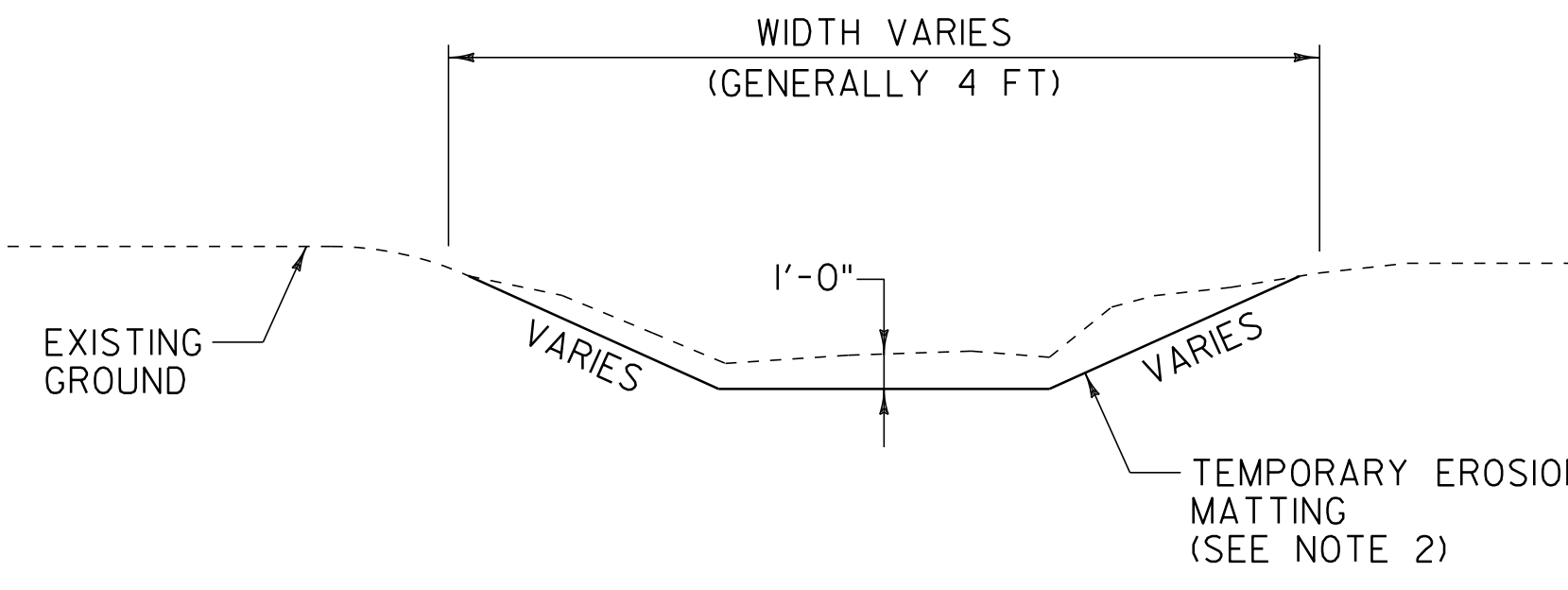
DRAINAGE STRUCTURES

STA.	STA.	POSITION	ASKEW	TYPE IN/OUT	DITCH			PIPE ARCH			PIPE		ALLOWABLE OPTIONS							PIPE ELBOW NO-DEG	E S	P R C D I	DEPTH DI FT	CONC CL B CY	REIN STEEL LBS	DI GRATE TYPE	CHAN ELEV	CRM CY	TR EARTH CY	EXCAV ROCK CY	COMM EXC CY	UNC CHAN EXC CY	STRUC EXCAV CY	GRAN BKF STRUC CY	GRAN BORR CY	GEO UNDER DRAIN SY	STONE CY	FILL TYPE	MRKR POSTS LT	6" UNDER DRAIN LF	6" CARRIER PIPE LF	REMARKS					
					IN	OUT	S IN	R IN	L FT	D IN	L FT	PCCSP TH	CAAP TH	RCP CL	CSP TH	CPEP SL	PCCSP PI TH	RCP CL	CSP TH																								CPEP SL	PCCSP PI TH			
RIPTON: 60+14.0	60+32.0	LT				X				18	17																											INSTALL 4 FT x 4 FT DIW/GRATE. INSTALL 18 INCH PIPE AND OUTLET TO STONE PAD.									
60+32.0	61+56.0	LT								18	118																										INSTALL 4 FT x 4 FT DIW/GRATE. INSTALL 18 INCH PIPE AND CONNECT INTO PROPOSED STRUCTURE.										
61+56.0	63+35.0	LT								18	176																										INSTALL 4 FT x 4 FT DIW/GRATE. INSTALL 18 INCH PIPE AND CONNECT INTO PROPOSED STRUCTURE.										
ROUNDING											4																																				
TOTALS										18	315				III	0.064	X			1	3	15								E											360						75

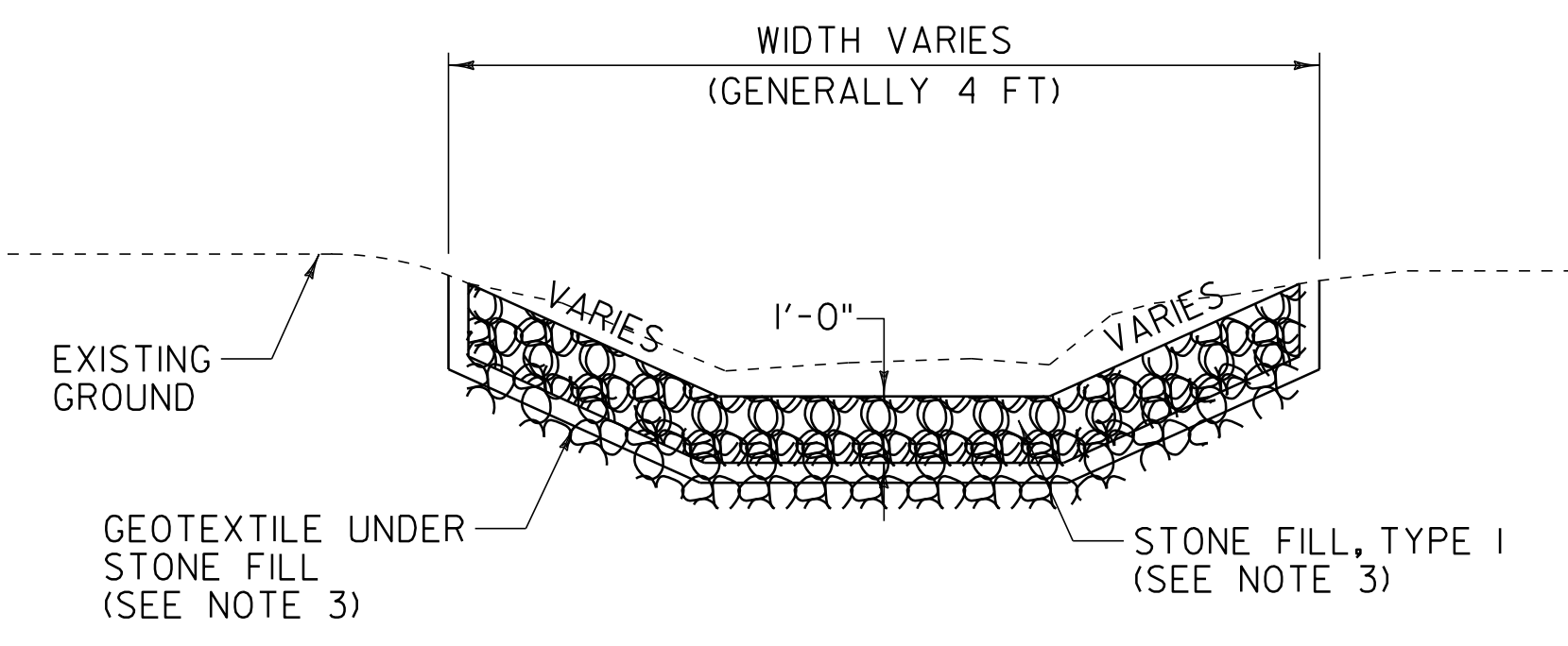
•• REVISED 02-16-10 BY SEH
ADDED ITEM 204.30, GRANULAR BACKFILL FOR STRUCTURES

DRAINAGE SUMMARY SHEET	PROJECT NAME: RIPTON - HANCOCK
	PROJECT NUMBER: STP 2803(1)
	FILE NAME: p08cl60.dgn
	DESIGNED BY: M.J.M.
IPARM FILE: p08cl60dss.i	PLOT DATE: 18-SEP-2013 17:09
	DRAWN BY: C.A.K.
	CHECKED BY: D.W.E.
	SHEET 58 OF 114

LOCATION				FEET OF DITCHING				MISCELLANEOUS ITEMS				REMARKS	LOCATION				FEET OF DITCHING				MISCELLANEOUS ITEMS				REMARKS															
SITE	STATION	STATION	POS.	PERCENT GRADE				STONE FILL, TYPE I	GEOT. UNDER STONE FILL	SEED	TEMP. EROS. MATT.		SITE	STATION	STATION	POS.	PERCENT GRADE				STONE FILL, TYPE I	GEOT. UNDER STONE FILL	SEED	TEMP. EROS. MATT.																
				0-1	1-2.5	2.5-10	>10										0-1	1-2.5	2.5-10	>10						CY	SY	LB	SY	CY	SY	LB	SY							
VT. ROUTE 125																																								
RIPTON:																																								
1	0+00	51+00	RT			5100		756	3400																															
2	24+40	27+55	LT		315																																			
3	78+00	91+00	LT		1300																																			
4	94+00	97+40	RT		340																																			
5	96+50	100+00	LT		350																																			
6	104+00	108+00	LT		400																																			
7	115+00	136+00	LT			2100		311	1400																															
8	120+00	124+00	RT		400																																			
9	142+00	153+00	LT		1100																																			
10	159+00	165+00	LT		600																																			
11	186+00	189+00	LT		300																																			
12	196+25	204+90	LT		865																																			
13	196+25	200+50	RT		425																																			
14	235+25	238+00	LT		275																																			
15	274+50	282+00	LT		550																																			
16	302+40	305+50	LT		310																																			
HANCOCK:																																								
17	0+00	3960	LT		3960			587	2640																															
ROUNDINGS					70	40		36	60	-	61																													
TOTALS					7600	11,200		1690	7500	61	3,415																													



DITCH DETAIL



STONE DITCH DETAIL

NOTES:

- PIPE INLET AND OUTLET AREAS AND DITCH CLEANING THROUGHOUT THE PROJECT SHALL BE PERFORMED AT LOCATIONS IDENTIFIED ON THIS SHEET AND AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT WILL BE UNDER THE APPLICABLE EQUIPMENT RENTAL ITEM(S).
- ESTIMATED QUANTITIES OF TEMPORARY EROSION MATTING AND SEED HAVE BEEN INCLUDED AS NEEDED OR AS DIRECTED BY THE RESIDENT ENGINEER.
- ESTIMATED QUANTITIES OF STONE FILL TYPE I AND GEOTEXTILE UNDER STONE FILL HAVE BEEN INCLUDED AS NEEDED. STONE FILL TYPE I SHALL BE USED IN ALL DITCHES WITH A GRADE BETWEEN 2.5 AND 10 PERCENT OR AS DIRECTED BY THE RESIDENT ENGINEER.

DITCH CLEANING DETAIL SHEET	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 59 OF 114

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 0+00.00 - STA 11+00.00 EDGE LINE LT & RT

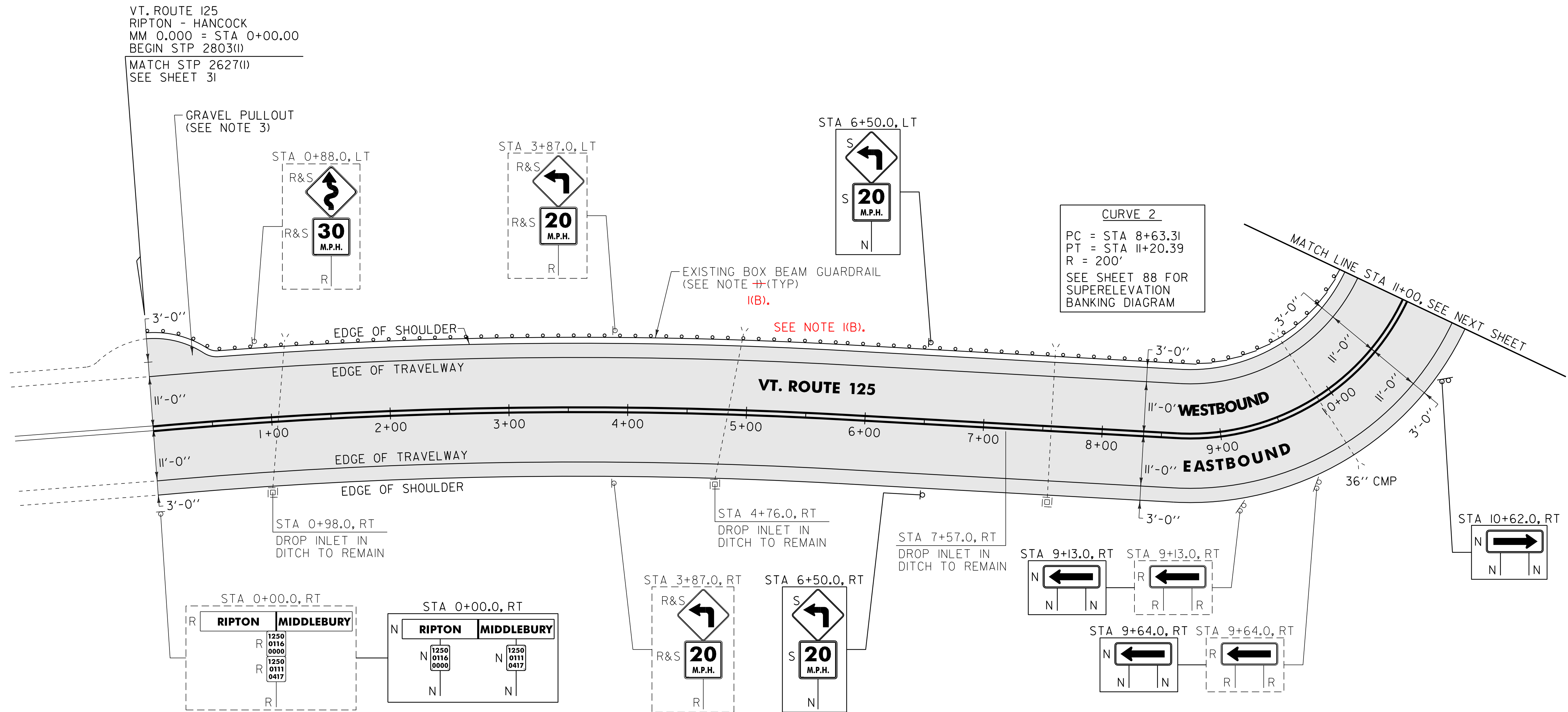
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 0+00.00 - STA 11+00.00 LT ☉ RT
 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 0+00.00 - STA 11+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 0+00.00 - STA 11+00.00 LT ☉ RT
 S - S

675.50 REMOVING SIGNS
 AS SHOWN - 11

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 4



NOTES:

1. THE CONTRACTOR SHALL REPAIR AND/OR MAKE HEIGHT ALTERATIONS PER SUBSECTION 621.13 OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE RESIDENT ENGINEER BETWEEN RIPTON STA 0+00 AND STA 11+00 LT. ALL WORK SHALL BE PAID FOR UNDER ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED IN THE PLAN SET. SEE SHEET 9 FOR AVAILABLE BOX BEAM GUARDRAIL INVENTORY.
2. THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.
3. PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I.

CURVE 1
 PC = STA 0+00.00
 PT = STA 5+80.57
 R = 4000'

SEE SHEET 88 FOR SUPERELEVATION BANKING DIAGRAM

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #1

PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:09
PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
PROJECT LEADER: D.E.G.	SHEET 60 OF 114
DESIGNED BY: M.J.M.	
IPARM FILE: p08cl60I01.i	

AREA TO BE RECLAIMED

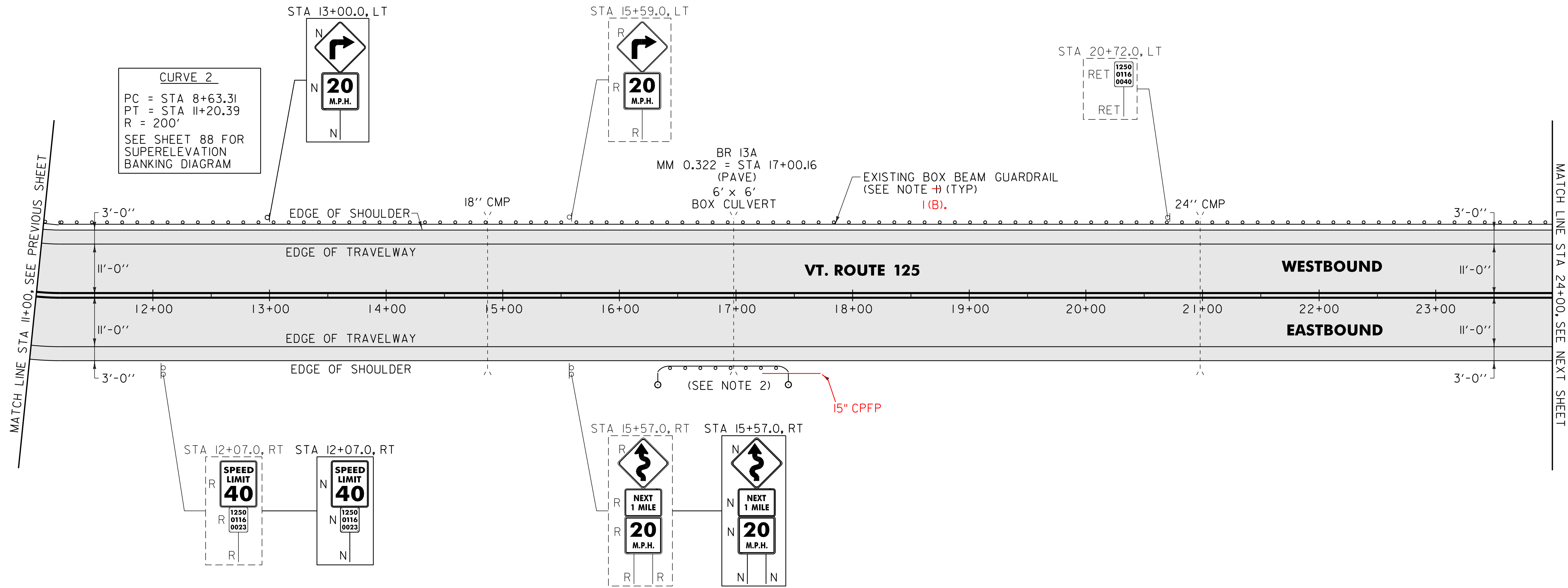
646.402 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 11+00.00 - STA 24+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON: LT CL RT
 STA 11+00.00 - STA 24+00.00 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 11+00.00 - STA 24+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON: LT CL RT
 STA 11+00.00 - STA 24+00.00 S - S

675.50 REMOVING SIGNS
 AS SHOWN - 7



676.10 DELINEATOR WITH STEEL POSTS
 TYPE I WHITE
 RIPTON:
 STA 16+35.5 RT
 STA 17+48.0 RT

ITEM 621.60 ANCHOR FOR STEEL BEAM RAIL

~~900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)~~
 RIPTON:
 STA 16+35.5 - STA 16+73.0 RT
 STA 17+10.5 - STA 17+48.0 RT

~~900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)~~
 RIPTON:
 STA 16+73.0 - STA 16+85.5 RT (12.5 LF)

900.640 SPECIAL PROVISION (HD STEEL BEAM GUARDRAIL, WEATHERING/NESTED)
 RIPTON:
 STA 16+85.5 - STA 17+10.5 RT (25.0 LF)

(B). REFERENCE SHEET 31 OF 114 NOTE (B).
 STA 11+00 TO 24+00 LT

NOTES:

- THE CONTRACTOR SHALL REPAIR AND/OR MAKE HEIGHT ALTERATIONS PER SUBSECTION 621.13 OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE RESIDENT ENGINEER BETWEEN RIPTON STA 11+00 AND STA 24+00 LT. ALL WORK SHALL BE PAID FOR UNDER ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED IN THE PLAN SET. SEE SHEET 9 FOR AVAILABLE BOX BEAM GUARDRAIL INVENTORY.
- SEE SHEET 114 FOR NESTED GUARDRAIL DETAIL.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = -----
 NEW = _____

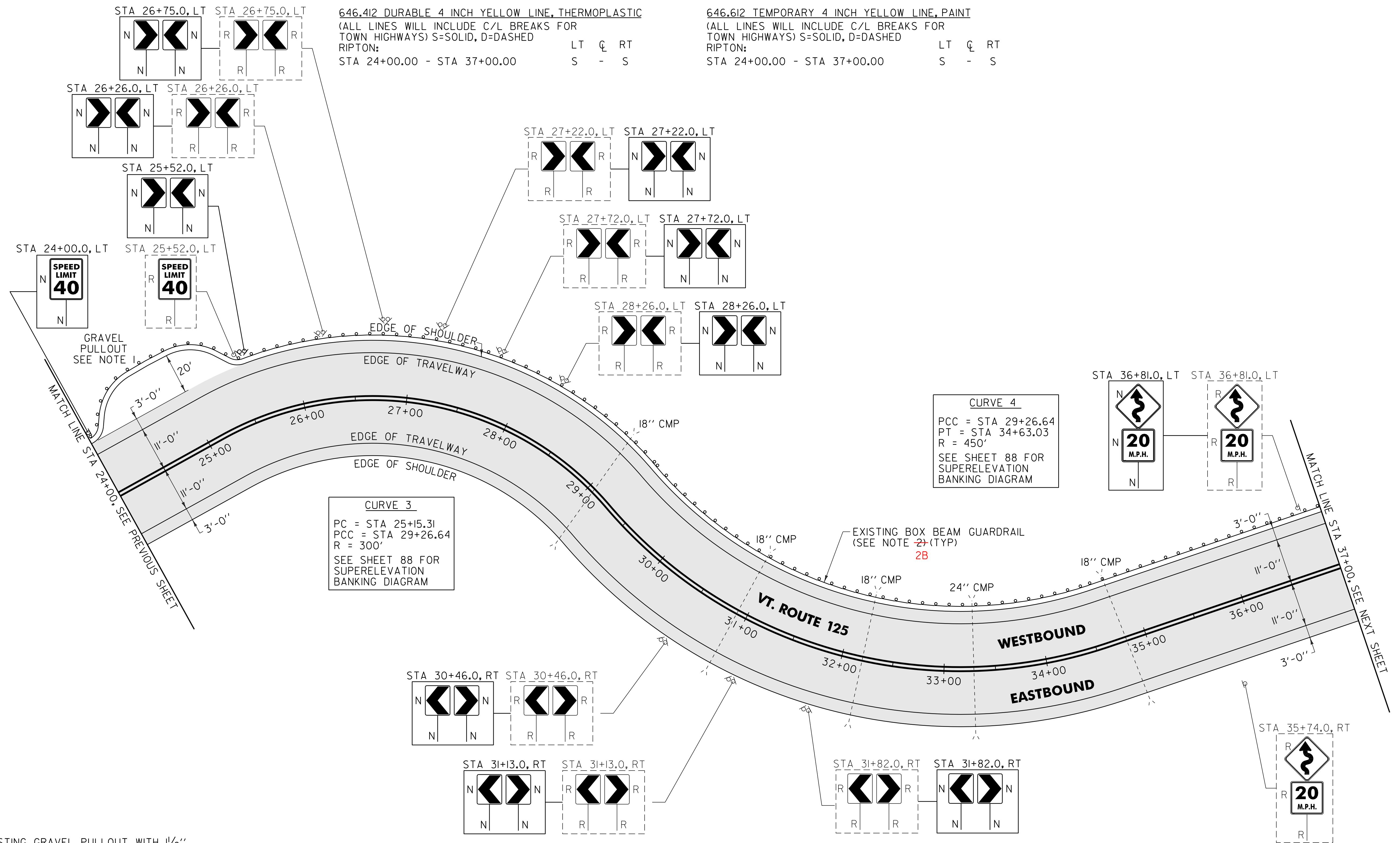
NOT TO SCALE

PROJECT LAYOUT SHEET #2	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08c160.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 61 OF 114

AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 24+00.00 - STA 37+00.00 EDGE LINE LT & RT
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON:
STA 24+00.00 - STA 37+00.00 LT CL RT
S - S

646.602 TEMPORARY 4 INCH YELLOW LINE, PAINT
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 24+00.00 - STA 37+00.00 EDGE LINE LT & RT
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON:
STA 24+00.00 - STA 37+00.00 LT CL RT
S - S



CURVE 3
PC = STA 25+15.31
PCC = STA 29+26.64
R = 300'
SEE SHEET 88 FOR SUPERELEVATION BANKING DIAGRAM

CURVE 4
PCC = STA 29+26.64
PT = STA 34+63.03
R = 450'
SEE SHEET 88 FOR SUPERELEVATION BANKING DIAGRAM

NOTES:
1. PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I. THE CONTRACTOR SHALL REPAIR AND/OR MAKE HEIGHT ALTERATIONS PER SUBSECTION 621.13 OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE RESIDENT ENGINEER BETWEEN RIPTON STA 24+00 AND STA 37+00 LT. ALL WORK SHALL BE PAID FOR UNDER ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED IN THE PLAN SET. SEE SHEET 9 FOR AVAILABLE BOX BEAM GUARDRAIL INVENTORY.

2. B. REFERENCE SHEET 31 OF I14 NOTE (KB).
STA 24+00 LT TO STA 37+00 LT

SIGN LEGEND
R = REMOVE
S = SALVAGE
N = NEW
RET = RETAIN
B-B = BACK TO BACK
EXISTING = _____
NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #3	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:09
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 62 OF 114
	DESIGNED BY: M.J.M.	
	IPARM FILE: p08cl60i03.i	

AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 50+00.00 - STA 63+00.00 EDGE LINE LT & RT

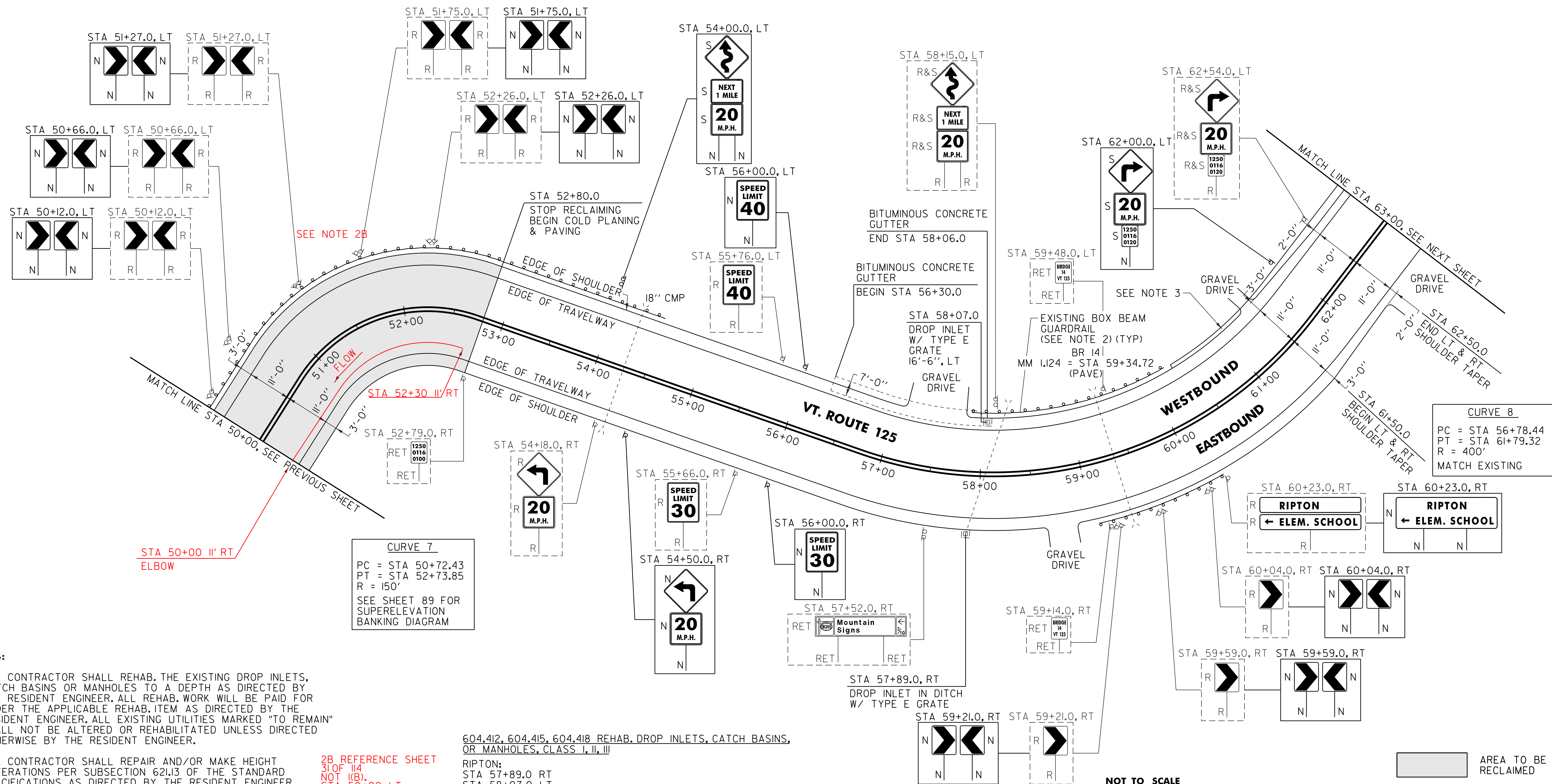
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 50+00.00 - STA 63+00.00 LT C RT S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 50+00.00 - STA 63+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 50+00.00 - STA 63+00.00 LT C RT S - S

675.50 REMOVING SIGNS
 AS SHOWN - 25

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 6



CURVE 7
 PC = STA 50+72.43
 PT = STA 52+73.85
 R = 150'
 SEE SHEET 89 FOR SUPERELEVATION BANKING DIAGRAM

CURVE 8
 PC = STA 56+78.44
 PT = STA 61+79.32
 R = 400'
 MATCH EXISTING

- NOTES:
- THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.
 - THE CONTRACTOR SHALL REPAIR AND/OR MAKE HEIGHT ALTERATIONS PER SUBSECTION 621.13 OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE RESIDENT ENGINEER BETWEEN RIPTON STA 50+00 AND STA 63+00 LT. ALL WORK SHALL BE PAID FOR UNDER ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED IN THE PLAN SET. SEE SHEET 9 FOR AVAILABLE BOX BEAM GUARDRAIL INVENTORY.
 - SEE SHEET 87 FOR NEW DRAINAGE PLAN FROM STA 59+55.0 TO STA 63+45.0 LT.

2B REFERENCE SHEET
 31 OF 114
 NOT (K.B).
 STA 50+00 LT

604.412, 604.415, 604.418 REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, II, III
 RIPTON:
 STA 57+89.0 RT
 STA 58+07.0 LT

616.35 TREATED TIMBER CURB
 RIPTON:
~~STA 58+00.0 - STA 60+00.0 LT (200.0 LF) - NOT PLACED USED EXISTING~~

616.47 BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS
 RIPTON:
 STA 56+30.0 - STA 58+06.0 LT (6.9 TON)

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

PROJECT LAYOUT SHEET #5

PROJECT NAME: RIPTON - HANCOCK
 PROJECT NUMBER: STP 2803(1)
 FILE NAME: p08cl60.dgn
 PROJECT LEADER: D.E.G.
 DESIGNED BY: M.J.M.
 IPARM FILE: p08cl60105.i

PLOT DATE: 18-SEP-2013 17:10
 DRAWN BY: C.A.K.
 CHECKED BY: D.W.E.
 SHEET 64 OF 114

NOT TO SCALE

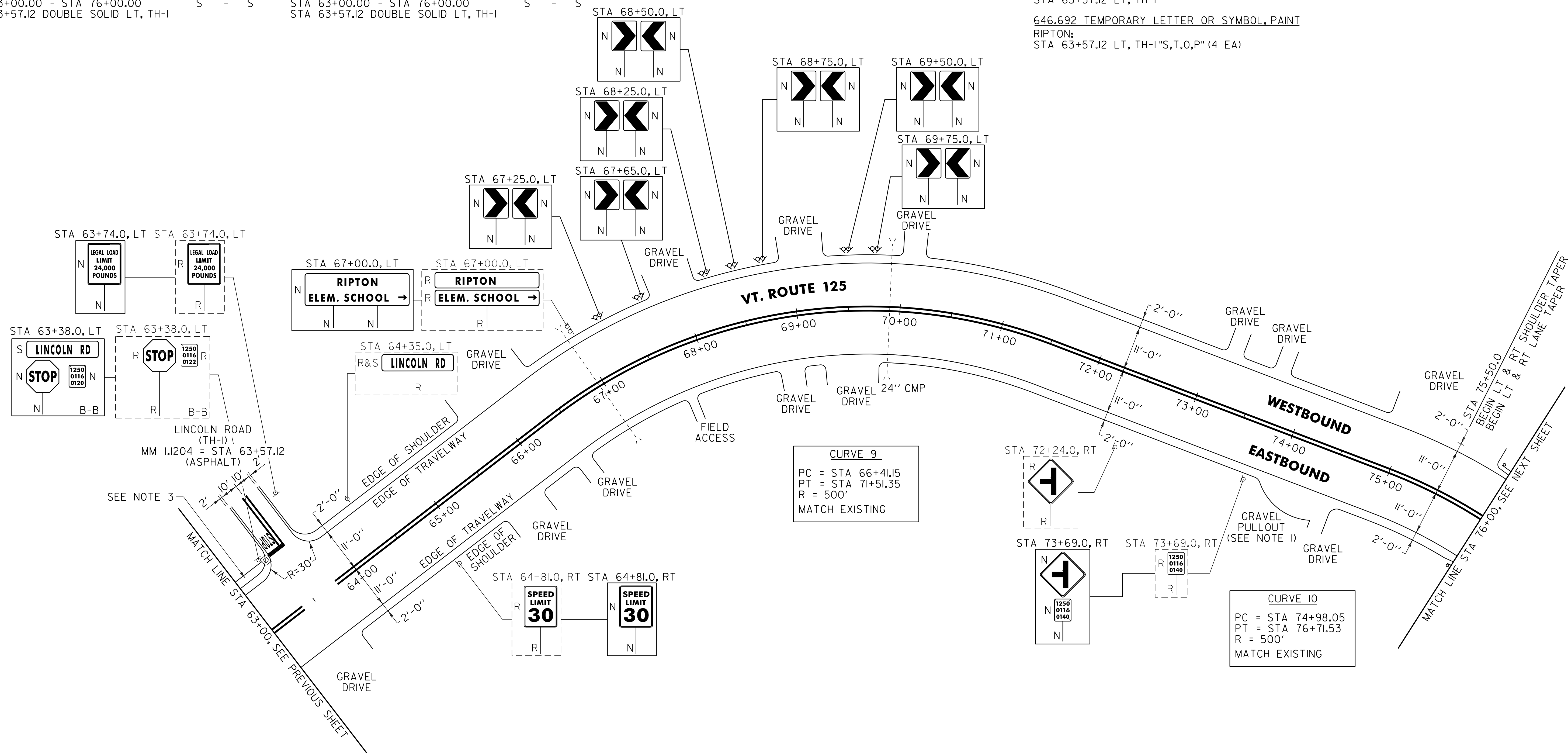
AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND
 RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 63+00.00 - STA 76+00.00 EDGE LINE LT & RT
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR
 TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 63+00.00 - STA 76+00.00 LT CL RT
 STA 63+57.12 DOUBLE SOLID LT, TH-I S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND
 RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 63+00.00 - STA 76+00.00 EDGE LINE LT & RT
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR
 TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 63+00.00 - STA 76+00.00 LT CL RT
 STA 63+57.12 DOUBLE SOLID LT, TH-I S - S

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 RIPTON:
 STA 63+57.12 LT, TH-I
 646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 RIPTON:
 STA 63+57.12 LT, TH-I "S,T,O,P" (4 EA)
 646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 RIPTON:
 STA 63+57.12 LT, TH-I
 646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 RIPTON:
 STA 63+57.12 LT, TH-I "S,T,O,P" (4 EA)

675.50 REMOVING SIGNS
 AS SHOWN - 9
 675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 1

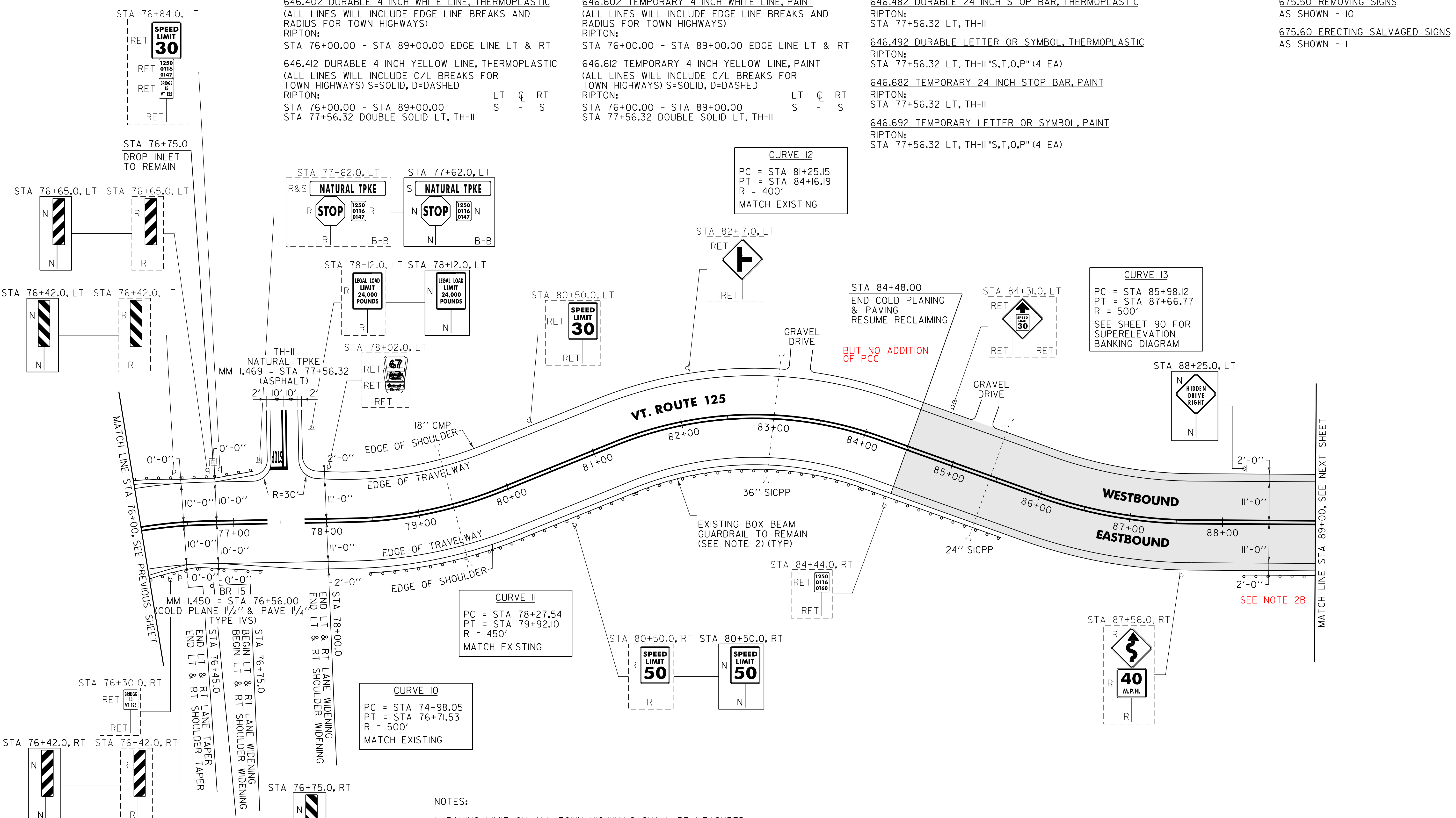


- NOTES:
- PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I.
 - PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - SEE SHEET 87 FOR NEW DRAINAGE PLAN FROM STA 59+55.0 TO STA 63+45.0 LT.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #6	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 65 OF 114



646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 76+00.00 - STA 89+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 76+00.00 - STA 89+00.00 LT C RT S - S
 STA 77+56.32 DOUBLE SOLID LT, TH-II

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 76+00.00 - STA 89+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 76+00.00 - STA 89+00.00 LT C RT S - S
 STA 77+56.32 DOUBLE SOLID LT, TH-II

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 RIPTON:
 STA 77+56.32 LT, TH-II

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 RIPTON:
 STA 77+56.32 LT, TH-II "S,T,O,P" (4 EA)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 RIPTON:
 STA 77+56.32 LT, TH-II

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 RIPTON:
 STA 77+56.32 LT, TH-II "S,T,O,P" (4 EA)

675.50 REMOVING SIGNS
 AS SHOWN - 10

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 1

CURVE 12
 PC = STA 81+25.15
 PT = STA 84+16.19
 R = 400'
 MATCH EXISTING

CURVE 13
 PC = STA 85+98.12
 PT = STA 87+66.77
 R = 500'
 SEE SHEET 90 FOR SUPERELEVATION BANKING DIAGRAM

CURVE 11
 PC = STA 78+27.54
 PT = STA 79+92.10
 R = 450'
 MATCH EXISTING

CURVE 10
 PC = STA 74+98.05
 PT = STA 76+71.53
 R = 500'
 MATCH EXISTING

NOTES:

- PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL REPAIR AND/OR MAKE HEIGHT ALTERATIONS PER SUBSECTION 621.13 OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE RESIDENT ENGINEER BETWEEN RIPTON STA 78+45 TO STA 85+75 RT AND STA 88+20 TO STA 89+00 RT. ALL WORK SHALL BE PAID FOR UNDER ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED IN THE PLAN SET. SEE SHEET 9 FOR AVAILABLE BOX BEAM GUARDRAIL INVENTORY.

2B. REFERENCE SHEET 31 OF 114, NOTE 1(B).
 STA 88+200 TO STA 89+00 LT

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = -----
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #7	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 66 OF 114
	DESIGNED BY: M.J.M.	
	IPARM FILE: p08cl60107.i	

AREA TO BE RECLAIMED

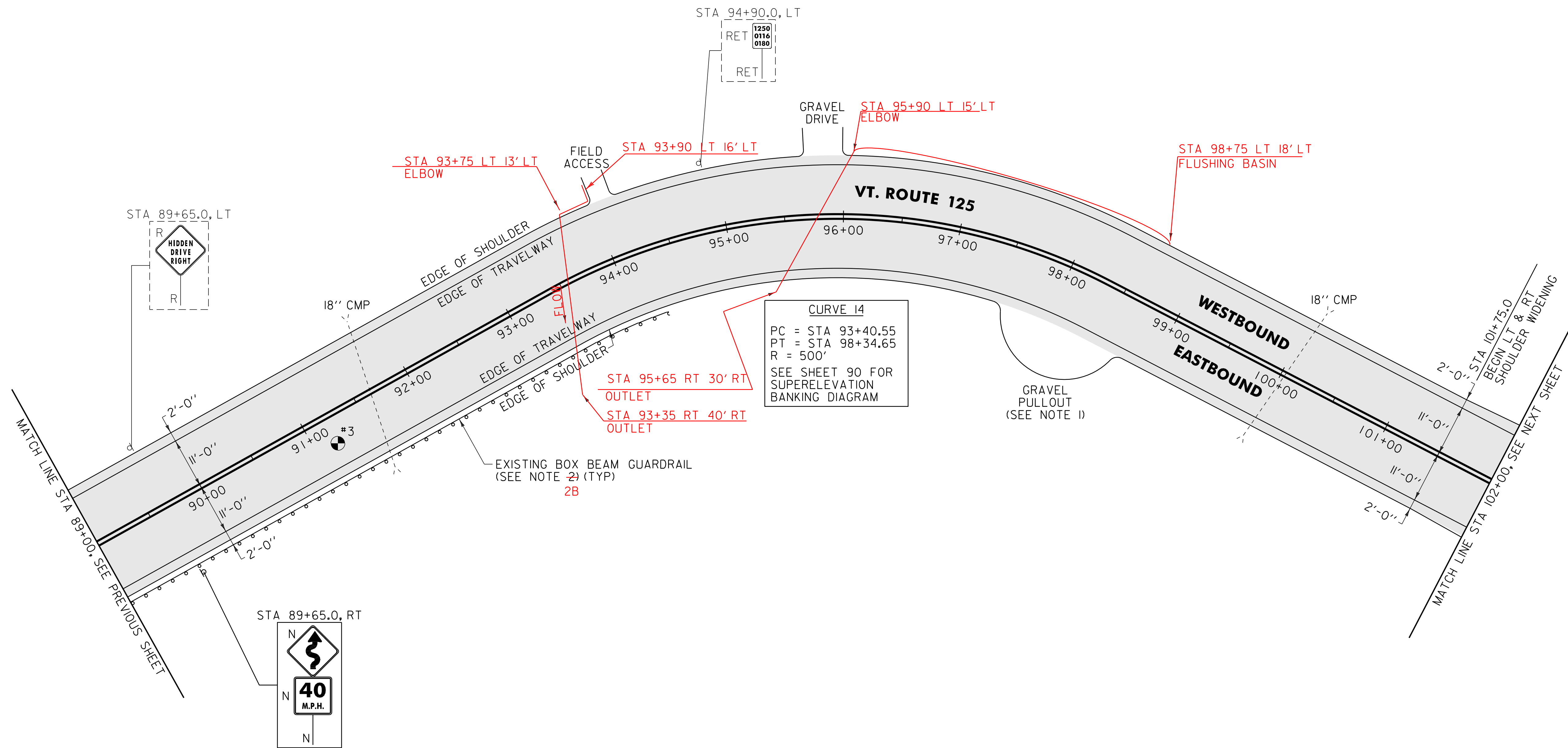
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 89+00.00 - STA 102+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 89+00.00 - STA 102+00.00 LT CL RT
 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 89+00.00 - STA 102+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 89+00.00 - STA 102+00.00 LT CL RT
 S - S

675.50 REMOVING SIGNS AS SHOWN - I



CURVE 14
 PC = STA 93+40.55
 PT = STA 98+34.65
 R = 500'
 SEE SHEET 90 FOR
 SUPERELEVATION
 BANKING DIAGRAM

PAVEMENT CORES =

#	TOTAL PCC COMMENTS	DEPTH (INCH)
3	8"	NO

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #8

PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
FILE NAME: p08c160.dgn	CHECKED BY: D.W.E.
PROJECT LEADER: D.E.G.	SHEET 67 OF 114
DESIGNED BY: M.J.M.	
IPARM FILE: p08c160i08.i	

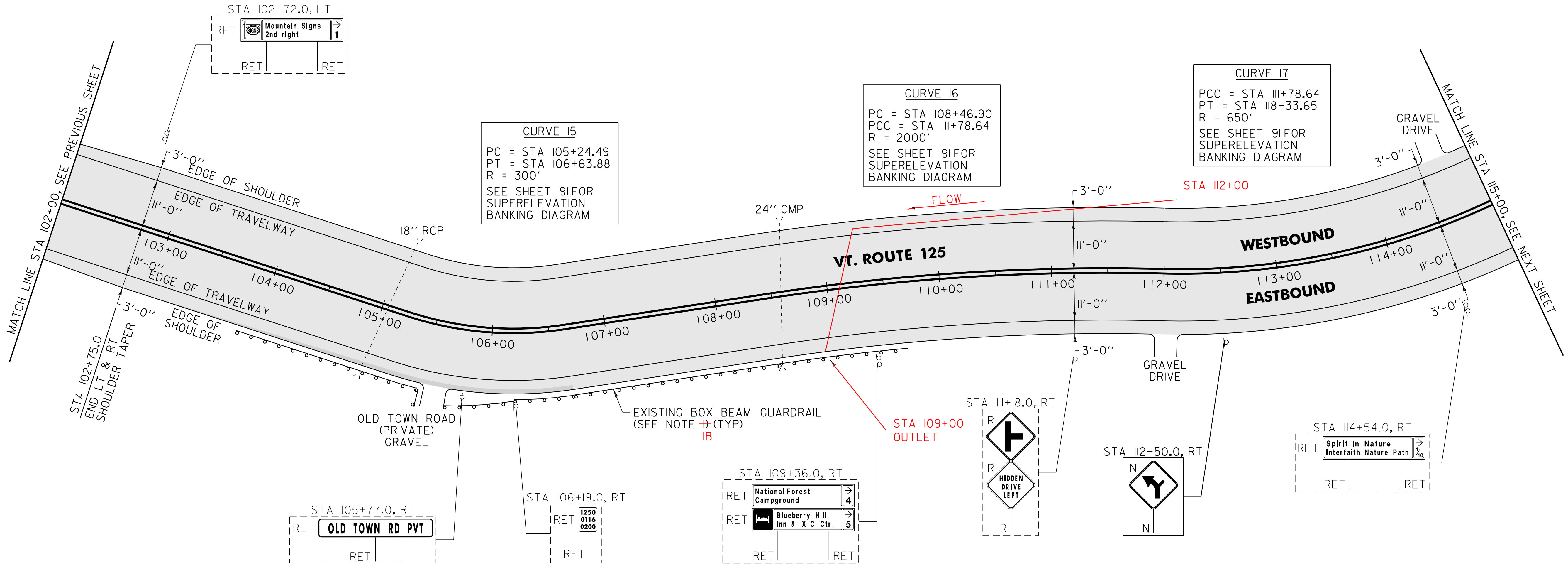
- NOTES:
- PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I.
 - THE CONTRACTOR SHALL REPAIR AND/OR MAKE HEIGHT ALTERATIONS PER SUBSECTION 621.13 OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE RESIDENT ENGINEER BETWEEN RIPTON STA 89+00 AND STA 94+27 RT. ALL WORK SHALL BE PAID FOR UNDER ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED IN THE PLAN SET. SEE SHEET 9 FOR AVAILABLE BOX BEAM GUARDRAIL INVENTORY.

2B. REFERENCE SHEET 31 OF 114 NOTE 1(B). STA 89+00 TO 94+27 LT

AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 102+00.00 - STA 115+00.00 EDGE LINE LT & RT
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON:
STA 102+00.00 - STA 115+00.00 LT C RT
S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 102+00.00 - STA 115+00.00 EDGE LINE LT & RT
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON:
STA 102+00.00 - STA 115+00.00 LT C RT
S - S



NOTE:

~~THE CONTRACTOR SHALL REPAIR AND/OR MAKE HEIGHT ALTERATIONS PER SUBSECTION 621.13 OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE RESIDENT ENGINEER BETWEEN RIPTON STA 103+83 TO STA 105+50 RT AND STA 106+19 TO STA 109+67. ALL WORK SHALL BE PAID FOR UNDER ITEM 900.640 SPECIAL PROVISION (BOX BEAM GUARDRAIL REPAIRS, WEATHERING). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED IN THE PLAN SET. SEE SHEET 9 FOR AVAILABLE BOX BEAM GUARDRAIL INVENTORY.~~

IB REFERENCE SHEET 31 OF 114 NOTE (IB).
STA 103+83 LT TO 105+50 LT
STA 105+75 LT TO 109+67 LT

SIGN LEGEND
R = REMOVE
S = SALVAGE
N = NEW
RET = RETAIN
B-B = BACK TO BACK
EXISTING = _____
NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #9	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 68 OF 114
	IPARM FILE: p08cl60109.i	

AREA TO BE RECLAIMED

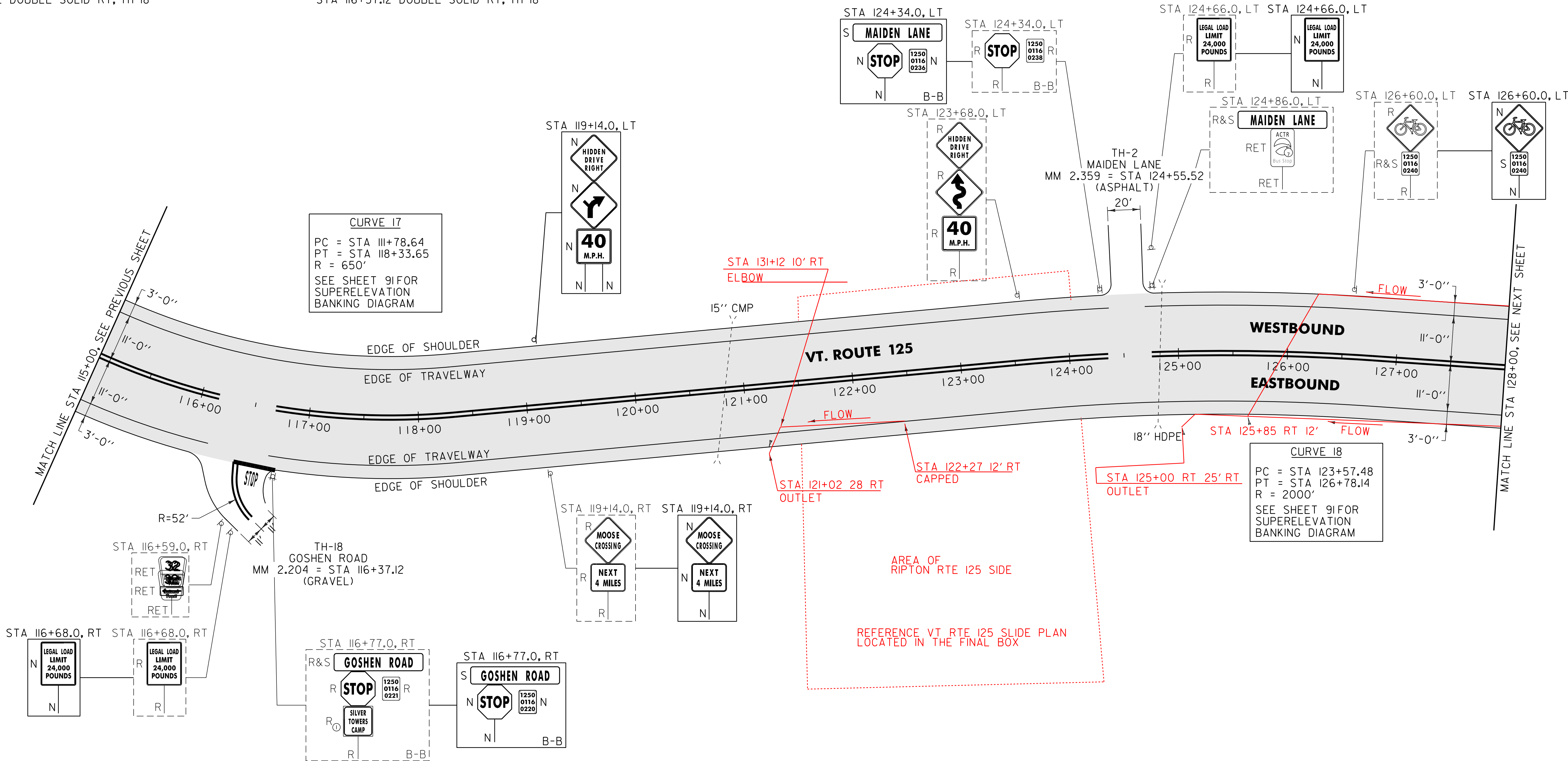
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 115+00.00 - STA 128+00.00 EDGE LINE LT & RT
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 115+00.00 - STA 128+00.00 LT C RT S - S
 STA 116+37.12 DOUBLE SOLID RT, TH-18

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 115+00.00 - STA 128+00.00 EDGE LINE LT & RT
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 115+00.00 - STA 128+00.00 LT C RT S - S
 STA 116+37.12 DOUBLE SOLID RT, TH-18

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 RIPTON:
 STA 116+37.12 RT, TH-18
 646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 RIPTON:
 STA 116+37.12 RT, TH-18 "S,T,O,P" (4 EA)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 RIPTON:
 STA 116+37.12 RT, TH-18
 646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 RIPTON:
 STA 116+37.12 RT, TH-18 "S,T,O,P" (4 EA)

675.50 REMOVING SIGNS AS SHOWN - 16
 675.60 ERECTING SALVAGED SIGNS AS SHOWN - 3



CURVE 17
 PC = STA 111+78.64
 PT = STA 118+33.65
 R = 650'
 SEE SHEET 91 FOR SUPERELEVATION BANKING DIAGRAM

CURVE 18
 PC = STA 123+57.48
 PT = STA 126+78.14
 R = 2000'
 SEE SHEET 91 FOR SUPERELEVATION BANKING DIAGRAM

NOTE:
 1. PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = _____
 NEW = _____
 R⊙ = RETURN TO THE TOWN OF RIPTON

NOT TO SCALE

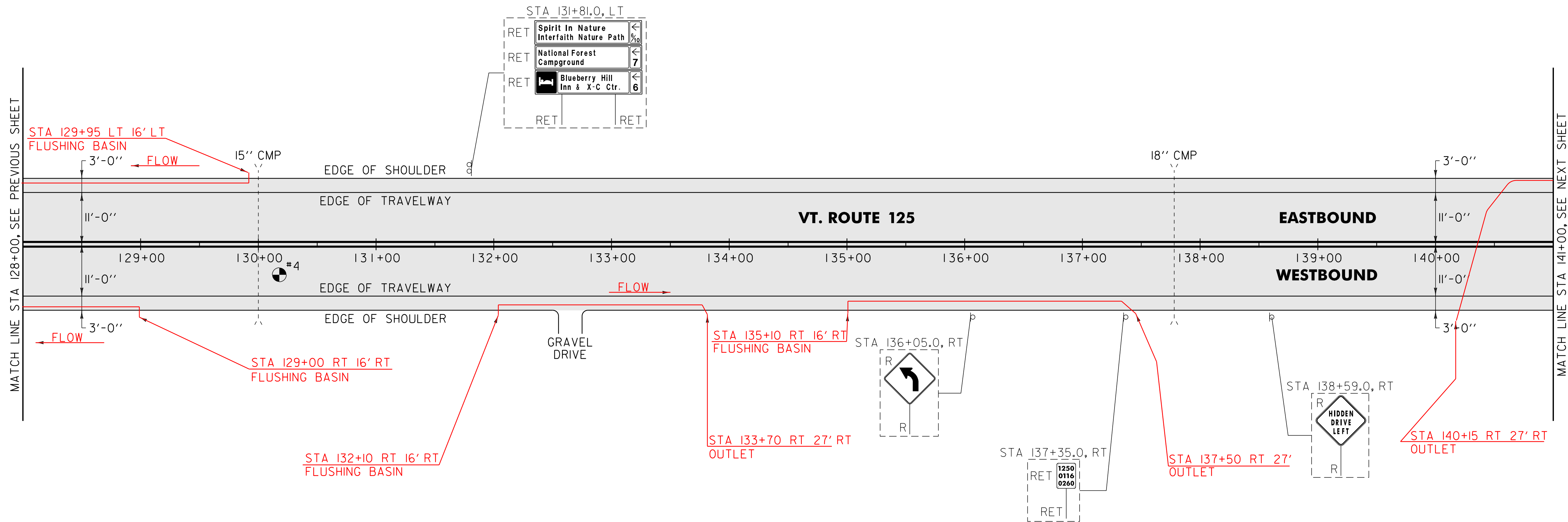
PROJECT LAYOUT SHEET #10	PROJECT NAME: RIPTON - HANCOCK	PLT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G. DESIGNED BY: M.J.M. IPARM FILE: p08cl60i10.i	SHEET 69 OF 114

AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 128+00.00 - STA 141+00.00 EDGE LINE LT & RT
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON: LT C RT
 STA 128+00.00 - STA 141+00.00 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 128+00.00 - STA 141+00.00 EDGE LINE LT & RT
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON: LT C RT
 STA 128+00.00 - STA 141+00.00 S - S

675.50 REMOVING SIGNS AS SHOWN - 2



MATCH LINE STA 128+00, SEE PREVIOUS SHEET

MATCH LINE STA 141+00, SEE NEXT SHEET

PAVEMENT CORES = ●

#	TOTAL PCC COMMENTS DEPTH (INCH)
4	9" NO

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #11	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08c160.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 70 OF 114
	IPARM FILE: p08c160i11.i	

AREA TO BE RECLAIMED

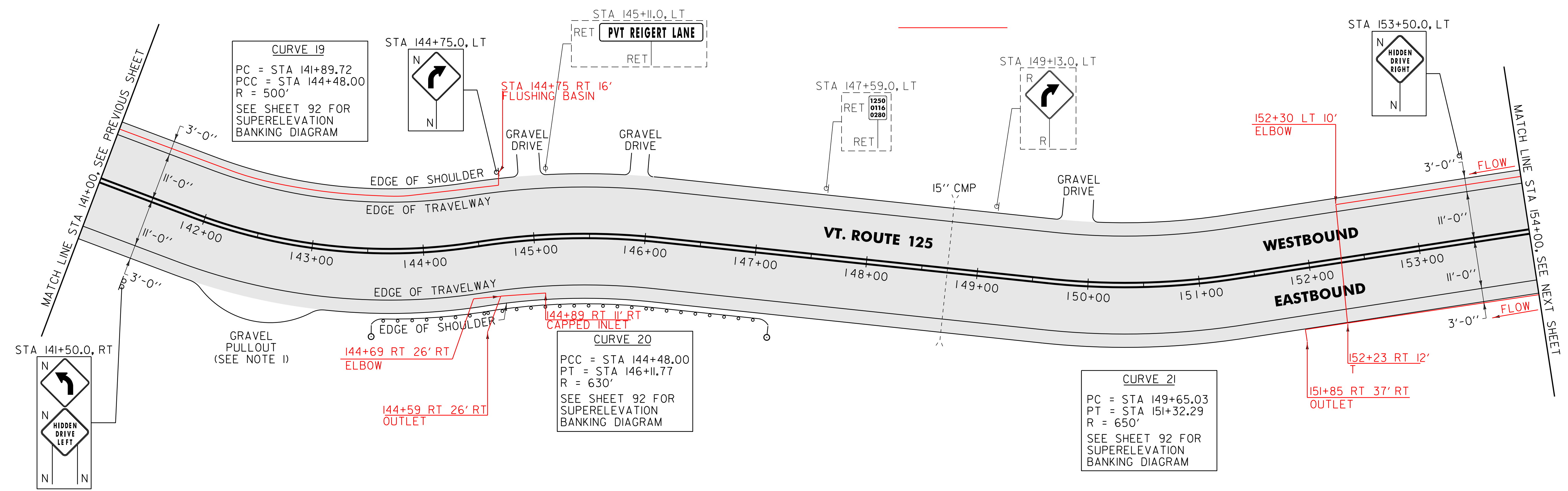
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 141+00.00 - STA 154+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 141+00.00 - STA 154+00.00 LT C RT S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 141+00.00 - STA 154+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 141+00.00 - STA 154+00.00 LT C RT S - S

675.50 REMOVING SIGNS AS SHOWN - 1



621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 RIPTON:
 STA 143+64.0 - STA 147+18.0 RT (354.0 LF)

676.10 DELINEATOR WITH STEEL POST
 ♀ TYPE 1 WHITE
 RIPTON:
 STA 143+59.5 RT
 STA 147+22.0 RT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
 RIPTON:
 STA 143+59.5 - STA 143+97.0 RT
 STA 146+84.5 - STA 147+22.0 RT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
 RIPTON:
 STA 143+97.0 - STA 146+84.5 RT (287.5 LF)

NOTE:
 1. PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1 1/2" TYPE IVS.
 ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE 1.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #12

PROJECT NAME: RIPTON - HANCOCK	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:10
PROJECT NUMBER: STP 2803(1)	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
	DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.
	IPARM FILE: p08cl60i12.i	SHEET 71 OF 114

AREA TO BE RECLAIMED

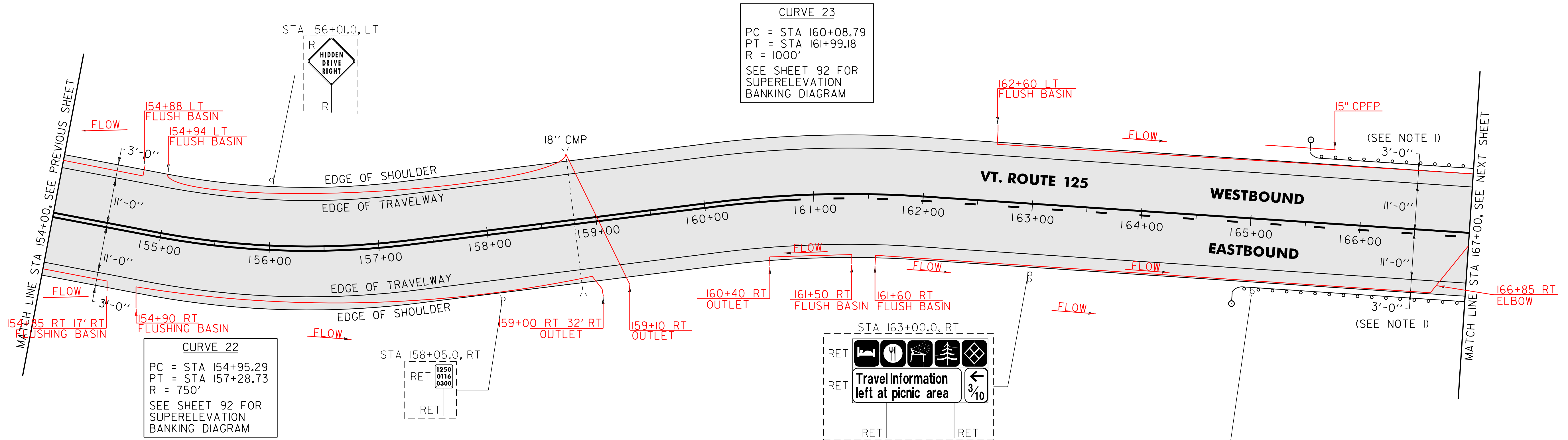
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 154+00.00 - STA 167+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 154+00.00 - STA 160+78.00 LT C RT
 STA 160+78.00 - STA 167+00.00 S - D

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 154+00.00 - STA 167+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 154+00.00 - STA 160+78.00 LT C RT
 STA 160+78.00 - STA 167+00.00 S - D

675.50 REMOVING SIGNS
 AS SHOWN - I



621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 RIPTON:
 STA 166+52.0 - STA 167+00.0 RT (48.0 LF)
 STA 166+62.0 - STA 167+00.0 LT (38.0 LF)

676.10 DELINEATOR WITH STEEL POST
 TYPE I WHITE
 RIPTON:
 STA 164+75.0 RT
 STA 165+50.0 LT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
 RIPTON:
 STA 164+75.0 - STA 165+12.5 RT
 STA 165+50.0 - STA 165+87.5 LT

900.640 SPECIAL PROVISION (HD STEEL BEAM GUARDRAIL, WEATHERING/NESTED)
 RIPTON:
 STA 166+87.5 - STA 167+00.0 LT (12.5 LF)
 STA 166+87.5 - STA 167+00.0 RT (12.5 LF)

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
 RIPTON:
 STA 165+12.5 - STA 166+87.5 RT (175.0 LF)
 STA 165+87.5 - STA 166+87.5 LT (100.0 LF)

NOTE:
 I. SEE SHEET 114 FOR STEEL BEAM GUARDRAIL AT SMALL CULVERT DETAIL.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #13

PROJECT NAME: RIPTON - HANCOCK	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:10
PROJECT NUMBER: STP 2803(1)	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
	DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.
	IPARM FILE: p08cl60i13.i	SHEET 72 OF 114

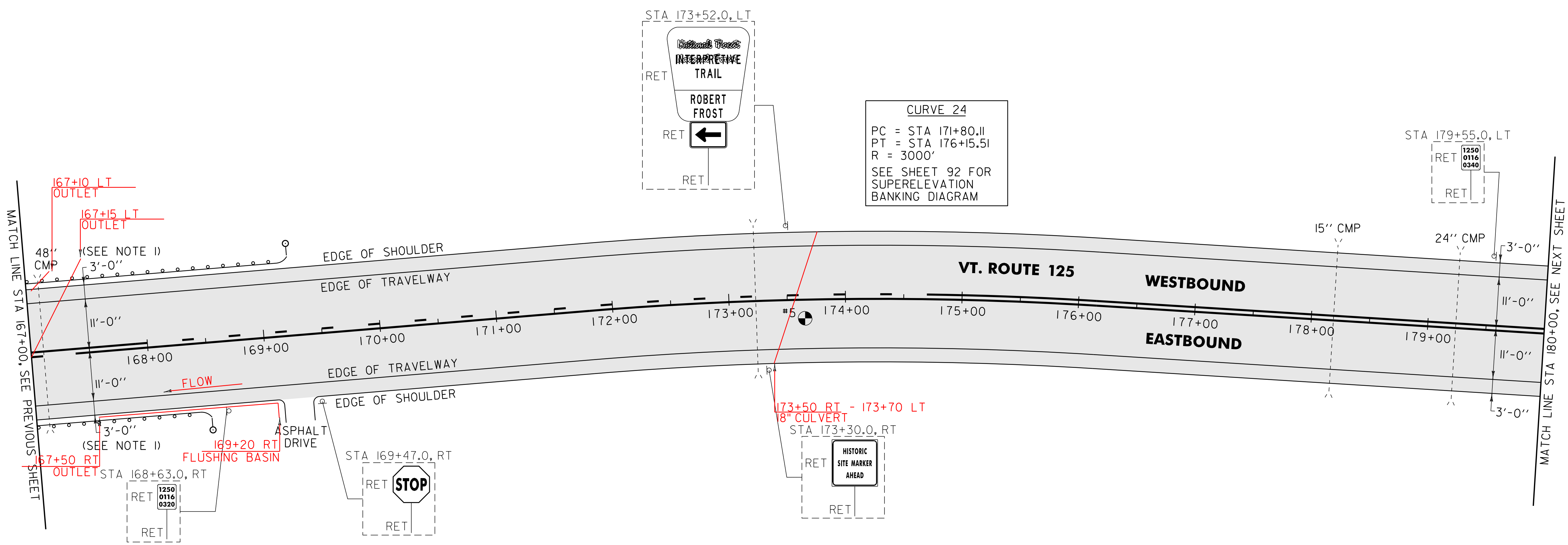
AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 167+00.00 - STA 180+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 167+00.00 - STA 167+38.00 S - D
 STA 167+38.00 - STA 167+90.00 S - S
 STA 167+90.00 - STA 174+77.00 D - S
 STA 174+77.00 - STA 180+00.00 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 167+00.00 - STA 180+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 167+00.00 - STA 167+38.00 S - D
 STA 167+38.00 - STA 167+90.00 S - S
 STA 167+90.00 - STA 174+77.00 D - S
 STA 174+77.00 - STA 180+00.00 S - S



621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 RIPTON:
 STA 167+00.0 - STA 167+40.0 LT (40.0 LF)
 STA 167+00.0 - STA 167+74.0 RT (74.0 LF)

676.10 DELINEATOR WITH STEEL POST
 TYPE 1 WHITE
 RIPTON:
 STA 168+50.0 RT
 STA 169+25.0 LT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
 RIPTON:
 STA 168+12.5 - STA 168+50.0 RT
 STA 168+87.5 - STA 169+25.0 LT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
 RIPTON:
 STA 167+12.5 - STA 168+12.5 RT (100.0 LF)
 STA 167+12.5 - STA 168+87.5 LT (175.0 LF)

900.640 SPECIAL PROVISION (HD STEEL BEAM GUARDRAIL, WEATHERING/NESTED)
 RIPTON:
 STA 167+00.0 - STA 167+12.5 LT (12.5 LF)
 STA 167+00.0 - STA 167+12.5 RT (12.5 LF)

PAVEMENT CORES =

TOTAL PCC COMMENTS DEPTH (INCH)

5 6 1/4" NO

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING =
 NEW =

NOT TO SCALE

PROJECT LAYOUT SHEET #14

NOTE:
 I. SEE SHEET 114 FOR NESTED GUARDRAIL DETAIL.

PROJECT NAME: RIPTON - HANCOCK	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:10
PROJECT NUMBER: STP 2803(1)	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
	DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.
	IPARM FILE: p08cl60i14.i	SHEET 73 OF 114

AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 180+00.00 - STA 193+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 180+00.00 - STA 187+97.00 S - S
 STA 187+97.00 - STA 193+00.00 S - D
 STA 184+80.00 DOUBLE SOLID LT, TH-19

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 180+00.00 - STA 193+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 180+00.00 - STA 187+97.00 S - S
 STA 187+97.00 - STA 193+00.00 S - D
 STA 184+80.00 DOUBLE SOLID LT, TH-19

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 RIPTON:
 STA 184+80.00 LT, TH-19

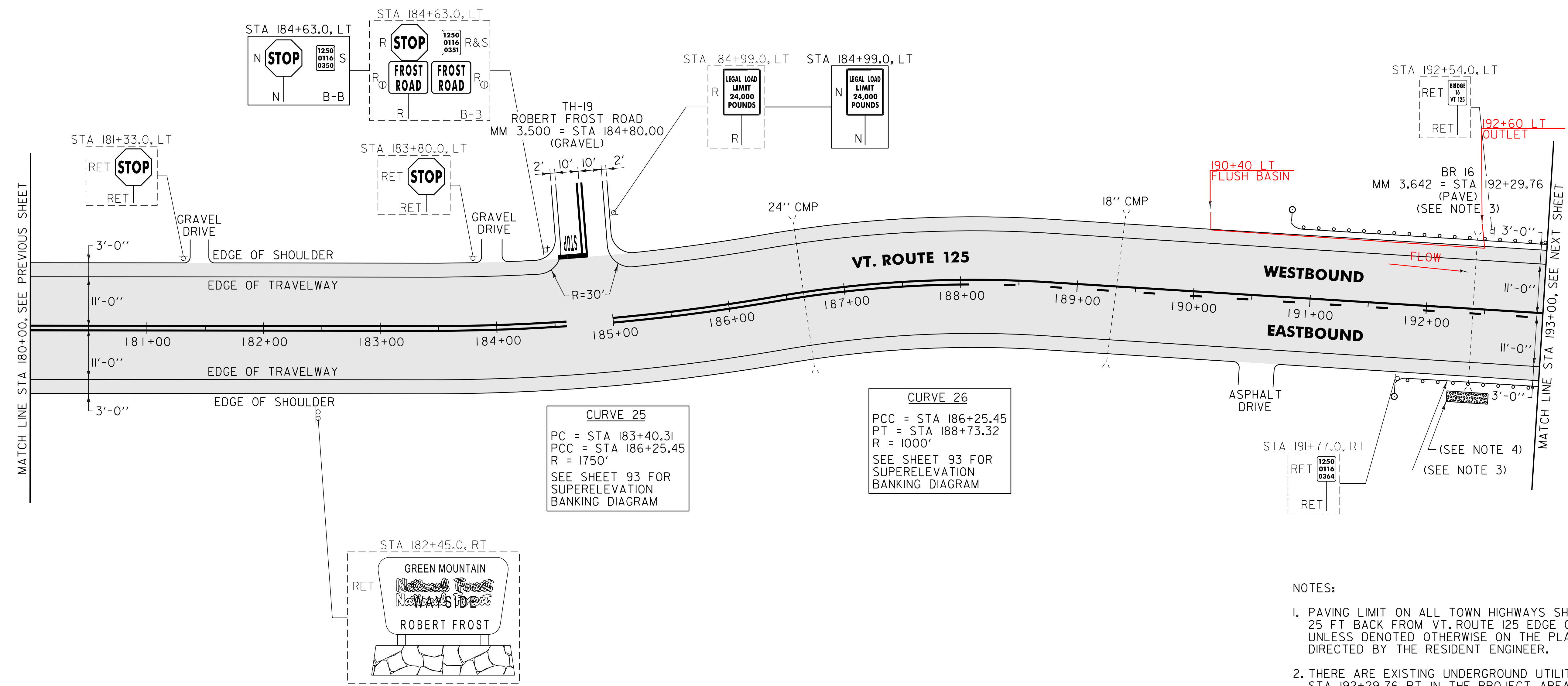
646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 RIPTON:
 STA 184+80.00 LT, TH-19 "S,T,O,P" (4 EA)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 RIPTON:
 STA 184+80.00 LT, TH-19

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 RIPTON:
 STA 184+80.00 LT, TH-19 "S,T,O,P" (4 EA)

675.50 REMOVING SIGNS AS SHOWN - 5

675.60 ERECTING SALVAGED SIGNS AS SHOWN - 1



CURVE 25
 PC = STA 183+40.31
 PCC = STA 186+25.45
 R = 1750'
 SEE SHEET 93 FOR SUPERELEVATION BANKING DIAGRAM

CURVE 26
 PCC = STA 186+25.45
 PT = STA 188+73.32
 R = 1000'
 SEE SHEET 93 FOR SUPERELEVATION BANKING DIAGRAM

- NOTES:
- PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - THERE ARE EXISTING UNDERGROUND UTILITIES LOCATED AT STA 192+29.76 RT IN THE PROJECT AREA. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING "DIG SAFE" AT (88) 344-7233 TO DETERMINE THE LOCATIONS OF THESE UTILITIES PRIOR TO PERFORMING ANY SUBSURFACE OPERATIONS.
 - SEE SHEET 114 FOR STEEL BEAM GUARDRAIL AT SMALL CULVET DETAIL.
 - ITEM 613.10 STONE FILL, TYPE I SHALL BE PLACED ALONG THE EXISTING SIDE SLOPE AS DIRECTED BY THE RESIDENT ENGINEER.

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 RIPTON:
 STA 191+74.0 - STA 193+00.0 LT (126.0 LF)
 STA 191+77.0 - STA 193+00.0 RT (123.0 LF)

676.10 DELINEATOR WITH STEEL POST
 TYPE I WHITE
 RIPTON:
 STA 190+80.0 LT
 STA 191+67.5 RT

613.10 STONE FILL, TYPE I
 RIPTON:
 STA 192+40.0 RT (2.5 CY)

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
 RIPTON:
 STA 190+80.0 - STA 191+17.5 LT
 STA 191+67.5 - STA 192+05.0 RT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
 RIPTON:
 STA 191+17.5 - STA 192+17.5 LT (100.0 LF)
 STA 192+05.0 - STA 192+17.5 RT (12.5 LF)
 STA 192+42.5 - STA 193+00.0 LT (57.5 LF)
 STA 192+42.5 - STA 193+00.0 RT (57.5 LF)

900.640 SPECIAL PROVISION (HD STEEL BEAM GUARDRAIL, WEATHERING/NESTED W/8 FEET POST)
 RIPTON:
 STA 192+17.5 - STA 192+42.5 LT (25.0 LF)
 STA 192+17.5 - STA 192+42.5 RT (25.0 LF)

AREA TO BE RECLAIMED

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____
 ⊕ = RETURN TO THE TOWN OF RIPTON

NOT TO SCALE

PROJECT LAYOUT SHEET #15

PROJECT NAME: RIPTON - HANCOCK	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:10
PROJECT NUMBER: STP 2803(1)	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
	DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.
	IPARM FILE: p08cl6015.1	SHEET 74 OF 114

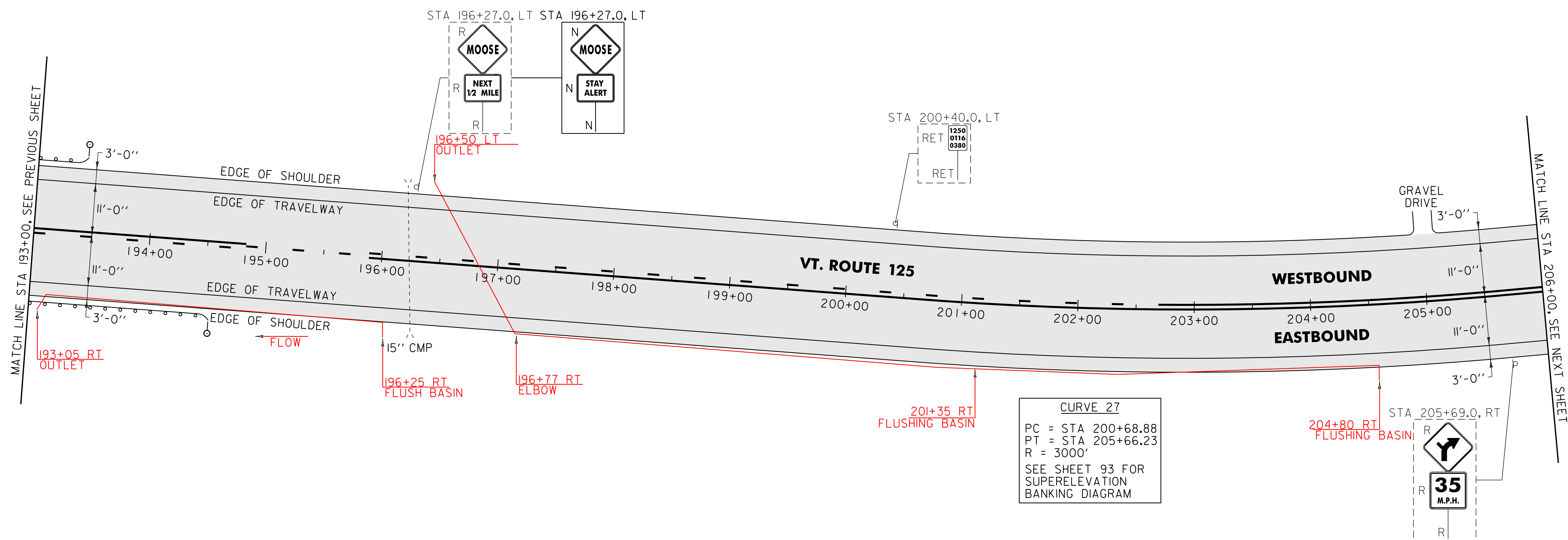
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 193+00.00 - STA 206+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 193+00.00 - STA 194+83.00 LT C RT S - D
 STA 194+83.00 - STA 195+89.00 - D D -
 STA 195+89.00 - STA 202+75.00 D - S
 STA 202+75.00 - STA 206+00.00 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 193+00.00 - STA 206+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 193+00.00 - STA 194+83.00 LT C RT S - D
 STA 194+83.00 - STA 195+89.00 - D D -
 STA 195+89.00 - STA 202+75.00 D - S
 STA 202+75.00 - STA 206+00.00 S - S

675.50 REMOVING SIGNS
 AS SHOWN - 4



621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 RIPTON:
 STA 193+00.0 - STA 193+04.0 LT (4.0 LF)
 STA 193+00.0 - STA 193+04.0 RT (4.0 LF)

676.10 DELINEATOR WITH STEEL POST
 ♀ TYPE I WHITE
 RIPTON:
 STA 193+80.0 LT
 STA 194+55.0 RT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
 RIPTON:
 STA 193+42.5 - STA 193+80.0 LT
 STA 194+17.5 - STA 194+55.0 RT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
 RIPTON:
 STA 193+00.0 - STA 194+17.5 RT (17.5 LF)
 STA 193+00.0 - STA 193+42.5 LT (42.5 LF)

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #16	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 75 OF 114
	DESIGNED BY: M.J.M.	
	IPARM FILE: p08cl60i16.i	

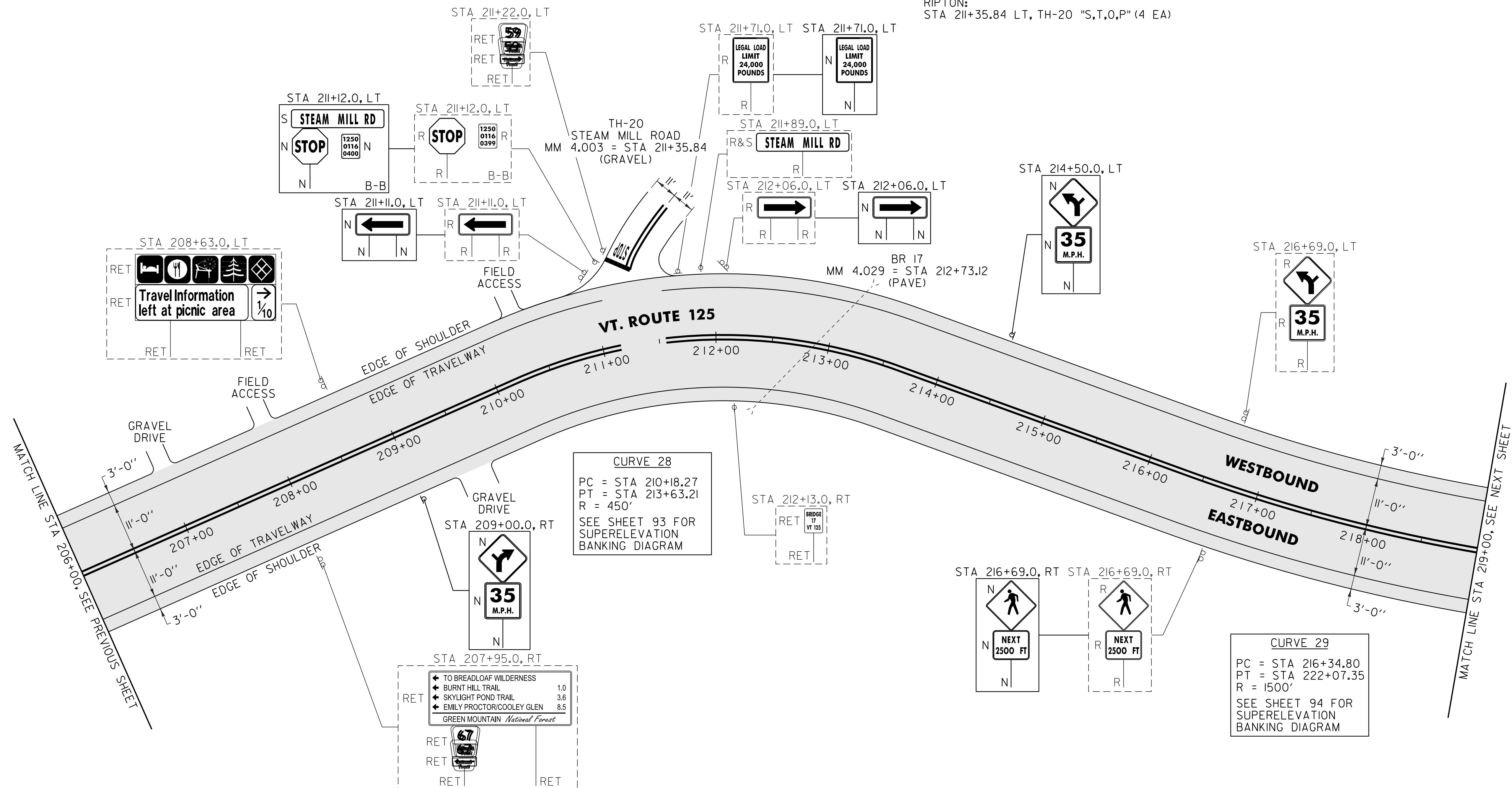
AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 206+00.00 - STA 219+00.00 EDGE LINE LT & RT
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 206+00.00 - STA 219+00.00 LT C RT
 STA 206+00.00 - STA 219+00.00 S - S
 STA 211+35.84 DOUBLE SOLID LT, TH-20

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 206+00.00 - STA 219+00.00 EDGE LINE LT & RT
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 206+00.00 - STA 219+00.00 LT C RT
 STA 206+00.00 - STA 219+00.00 S - S
 STA 211+35.84 DOUBLE SOLID LT, TH-20

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 RIPTON:
 STA 211+35.84 LT, TH-20
 646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 RIPTON:
 STA 211+35.84 LT, TH-20 "S,T,O,P" (4 EA)
 646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 RIPTON:
 STA 211+35.84 LT, TH-20
 646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 RIPTON:
 STA 211+35.84 LT, TH-20 "S,T,O,P" (4 EA)

675.50 REMOVING SIGNS
 AS SHOWN - 10
 675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 1



NOTE:
 1. PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

AREA TO BE RECLAIMED

NOT TO SCALE

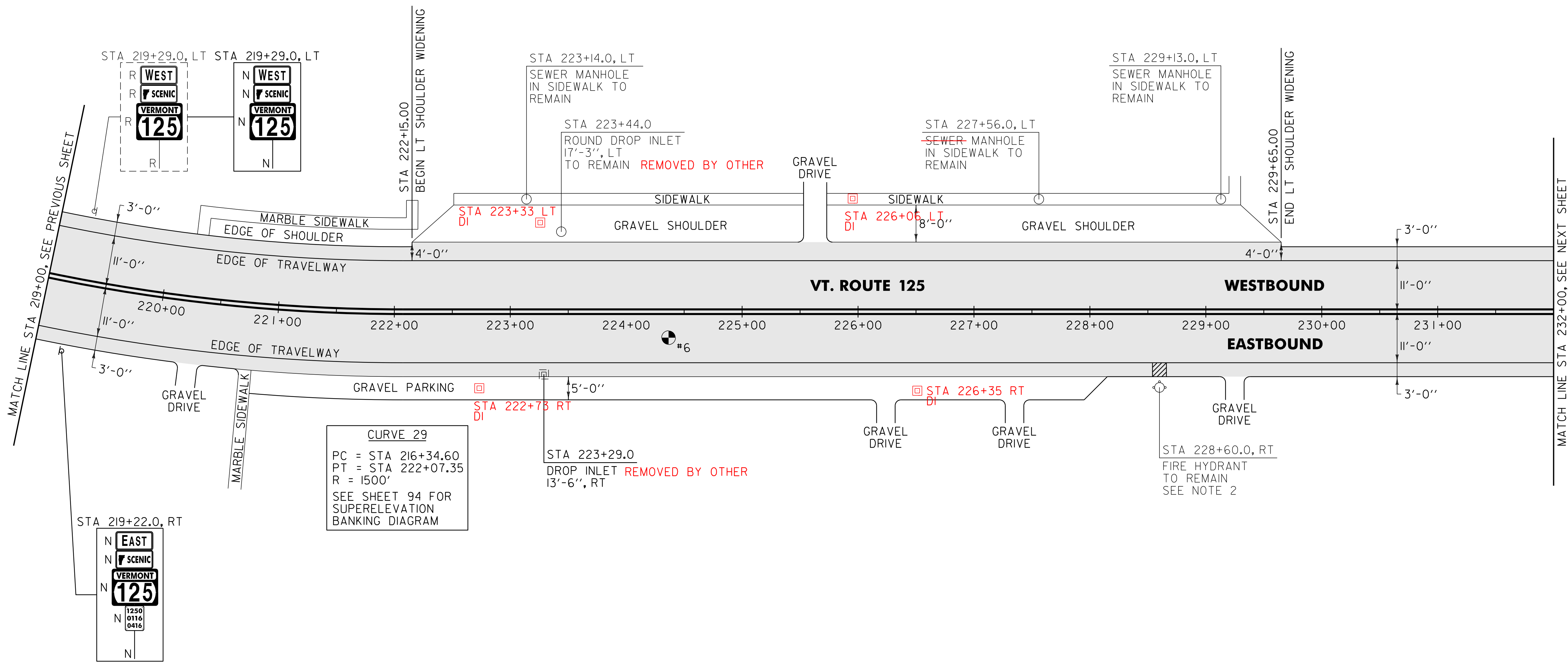
PROJECT LAYOUT SHEET #17	PROJECT NAME: RIPTON - HANCOCK	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
		DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.
		IPARM FILE: p08cl60i17.i	SHEET 76 OF 114

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 219+00.00 - STA 232+00.00 EDGE LINE LT & RT
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 219+00.00 - STA 232+00.00 LT ☉ RT
 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 219+00.00 - STA 232+00.00 EDGE LINE LT & RT
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 219+00.00 - STA 232+00.00 LT ☉ RT
 S - S
 616.28 CAST-IN PLACE CONCRETE CURB TYPE B
 223+21.00 - 225+71.00 LT 250 LF
 225+93.00 - 229+93 LT 400 LF

646.442 DURABLE 8 INCH WHITE LINE, THERMOPLASTIC
 RIPTON:
 STA 228+54.0 - STA 228+66.0 FIRE HYDRANT, NO PARKING, RT

675.50 REMOVING SIGNS
 AS SHOWN - 3



- NOTES:
- SEE SHEET 9 FOR FIRE HYDRANT PAVEMENT MARKING DETAIL.
 - THE CONTRACTOR SHALL REHAB. THE EXISTING DROP INLETS, CATCH BASINS OR MANHOLES TO A DEPTH AS DIRECTED BY THE RESIDENT ENGINEER. ALL REHAB. WORK WILL BE PAID FOR UNDER THE APPLICABLE REHAB. ITEM AS DIRECTED BY THE RESIDENT ENGINEER. ALL EXISTING UTILITIES MARKED "TO REMAIN" SHALL NOT BE ALTERED OR REHABILITATED UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.

604.412, 604.415, 604.418 REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, II, III (SEE NOTE 2)
 RIPTON: STA 222+73 RT
 STA 223+29.0 RT STA 223+33 LT
 STA 226+06 LT

PAVEMENT CORES =
 * TOTAL PCC COMMENTS
 DEPTH (INCH)
 6 3 1/2" NO

AREA TO BE RECLAIMED

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #18

PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
PROJECT LEADER: D.E.G.	SHEET 77 OF 114
DESIGNED BY: M.J.M.	
IPARM FILE: p08cl60i18.i	

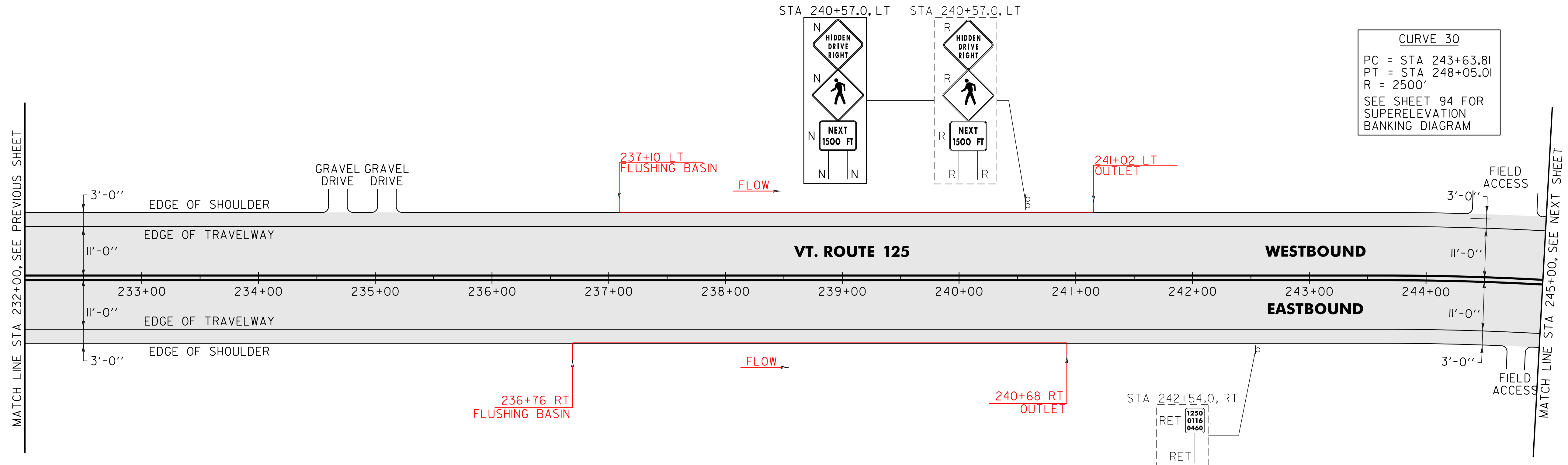
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 232+00.00 - STA 245+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 232+00.00 - STA 245+00.00 LT CL RT
 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 232+00.00 - STA 245+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 232+00.00 - STA 245+00.00 LT CL RT
 S - S

675.50 REMOVING SIGNS
 AS SHOWN - 3



CURVE 30
 PC = STA 243+63.81
 PT = STA 248+05.01
 R = 2500'
 SEE SHEET 94 FOR
 SUPERELEVATION
 BANKING DIAGRAM

MATCH LINE STA 232+00, SEE PREVIOUS SHEET

MATCH LINE STA 245+00, SEE NEXT SHEET

AREA TO BE RECLAIMED

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #19	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 78 OF 114
	DESIGNED BY: M.J.M.	
	IPARM FILE: p08cl60119.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 245+00.00 - STA 258+00.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON: LT C RT
 STA 245+00.00 - STA 258+00.00 S - S
 STA 256+39.68 DOUBLE SOLID RT, TH-2I

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 245+00.00 - STA 258+00.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON: LT C RT
 STA 245+00.00 - STA 258+00.00 S - S
 STA 256+39.68 DOUBLE SOLID RT, TH-2I

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
 RIPTON:
 STA 256+39.68 RT, TH-2I

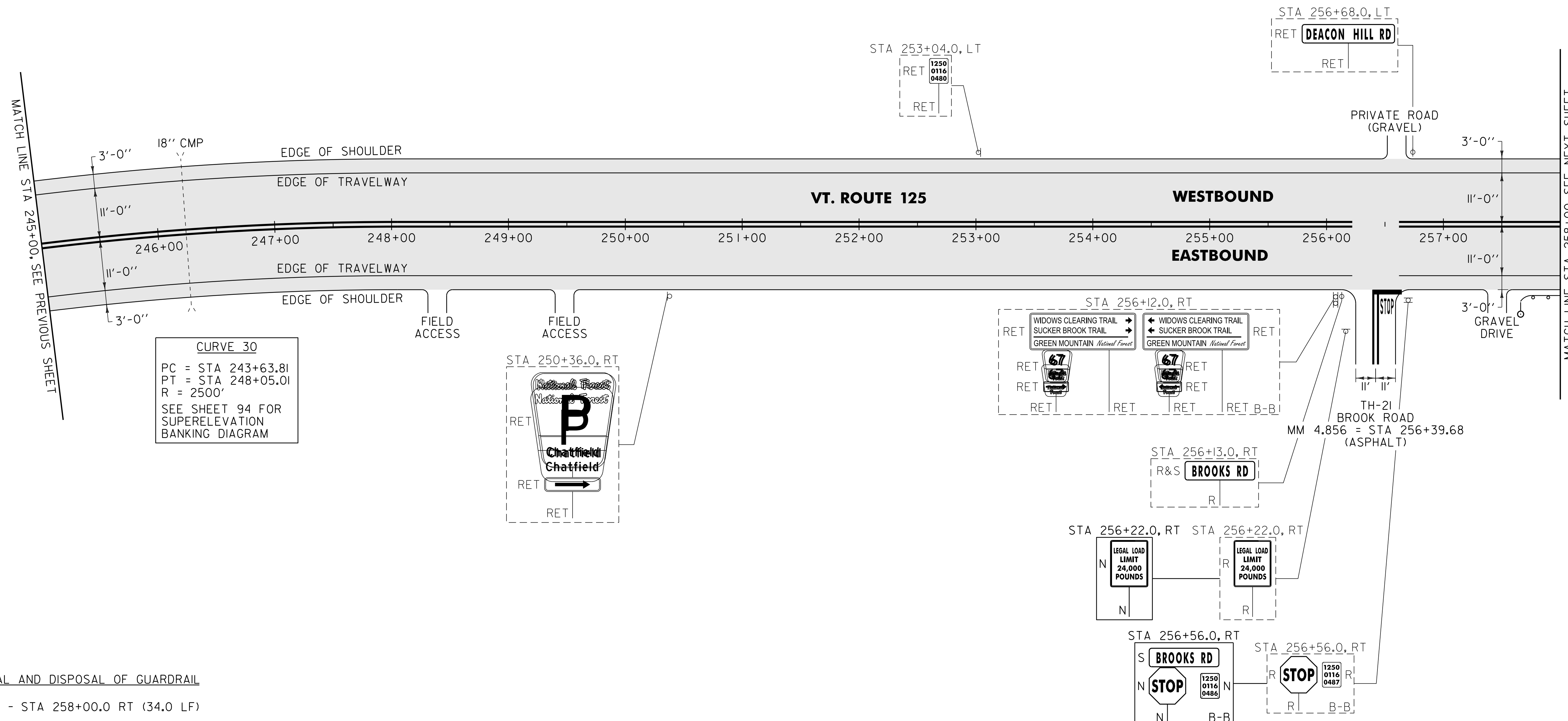
646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 RIPTON:
 STA 256+39.68 RT, TH-2I "S,T,O,P" (4 EA)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT
 RIPTON:
 STA 256+39.68 RT, TH-2I

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 RIPTON:
 STA 256+39.68 RT, TH-2I "S,T,O,P" (4 EA)

675.50 REMOVING SIGNS
 AS SHOWN - 4

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 1



621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 RIPTON:
 STA 257+66.0 - STA 258+00.0 RT (34.0 LF)

676.10 DELINEATOR WITH STEEL POST
 TYPE 1 WHITE
 RIPTON:
 STA 257+62.0 RT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, ~~FLARED/WEATHERING~~)
 RIPTON:
 STA 257+62.0 - STA 257+99.5 RT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
 RIPTON:
 STA 257+99.5 - STA 258+00.0 RT (0.5 LF)

NOTE:
 1. PAVING LIMIT ON ALL TOWN HIGHWAYS SHALL BE MEASURED 25 FT BACK FROM VT. ROUTE 125 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #20	PROJECT NAME: RIPTON - HANCOCK
	PROJECT NUMBER: STP 2803(1)
	FILE NAME: p08cl60.dgn
	PLOT DATE: 18-SEP-2013 17:10
	PROJECT LEADER: D.E.G.
	DRAWN BY: C.A.K.
	DESIGNED BY: M.J.M.
	CHECKED BY: D.W.E.
	IPARM FILE: p08cl60i20.i
	SHEET 79 OF 114

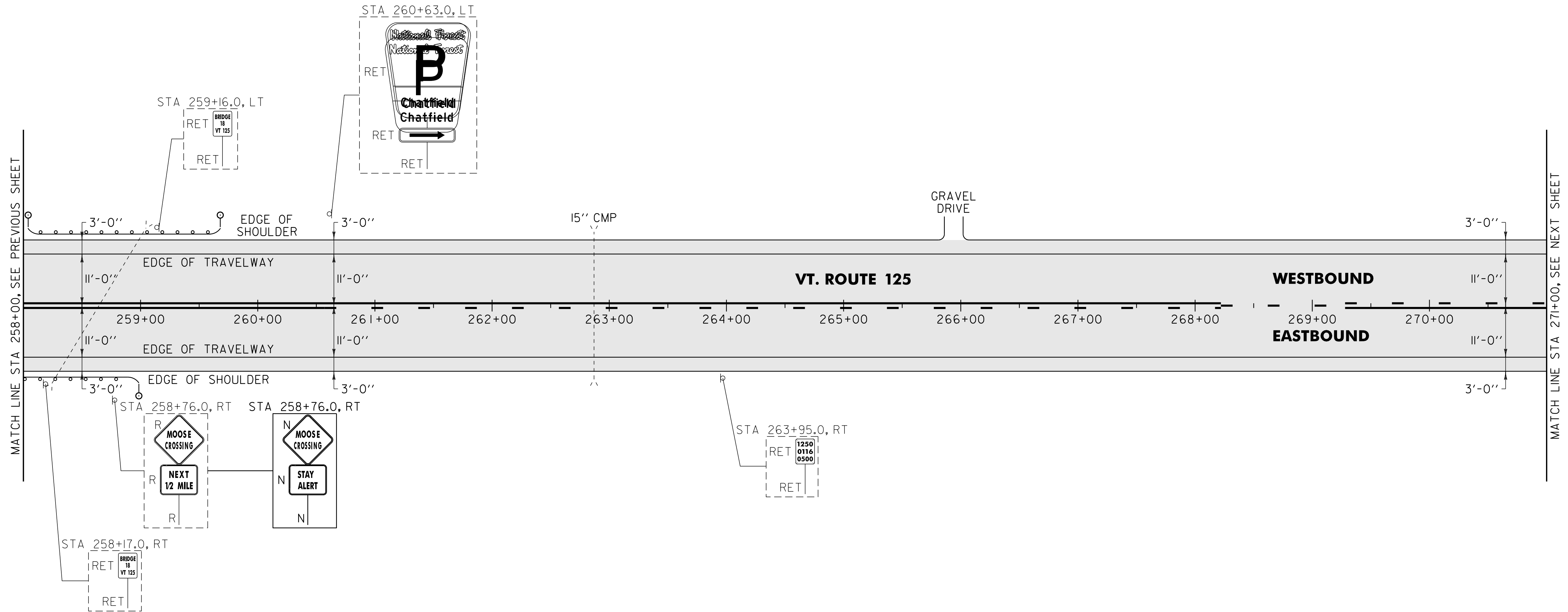


646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 258+00.00 - STA 271+00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON:
STA 258+00.00 - STA 260+57.00 S - S
STA 260+57.00 - STA 268+22.00 S - D
STA 268+22.00 - STA 269+28.00 - D -
STA 269+28.00 - STA 271+00.00 D - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 258+00.00 - STA 271+00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON:
STA 258+00.00 - STA 260+57.00 S - S
STA 260+57.00 - STA 268+22.00 S - D
STA 268+22.00 - STA 269+28.00 - D -
STA 269+28.00 - STA 271+00.00 D - S



621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
RIPTON:
STA 258+00.0 - STA 258+96.0 RT (96.0 LF)
STA 258+01.0 - STA 259+68.0 LT (167.0 LF)

676.10 DELINEATOR WITH STEEL POST
♀ TYPE 1 WHITE
RIPTON:
STA 258+01.0 LT
STA 258+99.5 RT
STA 259+76.0 LT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
RIPTON:
~~STA 258+01.0 - STA 258+38.5 LT~~ TERMINAL SECTION, TANGENT
STA 258+62.0 - STA 258+99.5 RT STA 258+01.0 TO STA 258+38.5 LT
STA 259+38.5 - STA 259+76.0 LT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
RIPTON:
STA 258+00.0 - STA 258+62.0 RT (62.0 LF)
STA 258+38.5 - STA 259+38.5 LT (100.0 LF)

SIGN LEGEND
R = REMOVE
S = SALVAGE
N = NEW
RET = RETAIN
B-B = BACK TO BACK
EXISTING = - - - - -
NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #21

PROJECT NAME: RIPTON - HANCOCK
PROJECT NUMBER: STP 2803(i)
FILE NAME: p08cl60.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.M.
IPARM FILE: p08cl60i21.i

PLOT DATE: 18-SEP-2013 17:10
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 80 OF 114

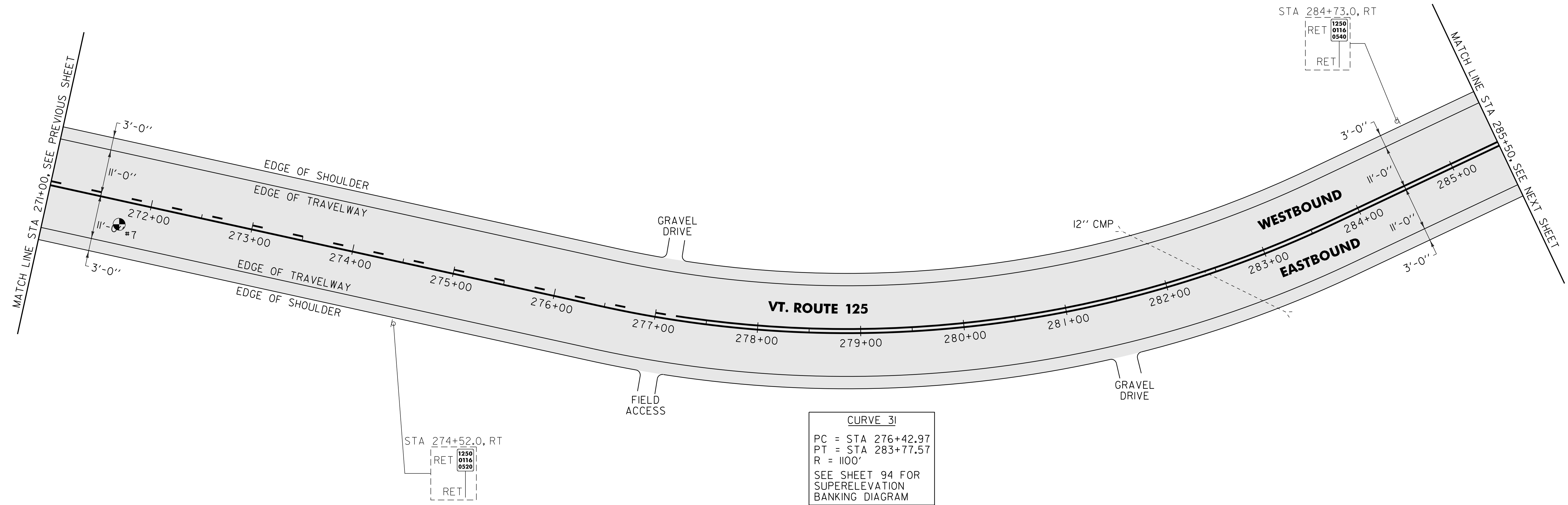
AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 271+00.00 - STA 285+50.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 271+00.00 - STA 277+20.00 LT ☒ RT
 STA 277+20.00 - STA 285+50.00 D - S
 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 RIPTON:
 STA 271+00.00 - STA 285+50.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 RIPTON:
 STA 271+00.00 - STA 277+20.00 LT ☒ RT
 STA 277+20.00 - STA 285+50.00 D - S
 S - S



CURVE 31
 PC = STA 276+42.97
 PT = STA 283+77.57
 R = 1100'
 SEE SHEET 94 FOR SUPERELEVATION BANKING DIAGRAM

PAVEMENT CORES = ●

#	TOTAL DEPTH (INCH)	PCC COMMENTS
7	6"	NO

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

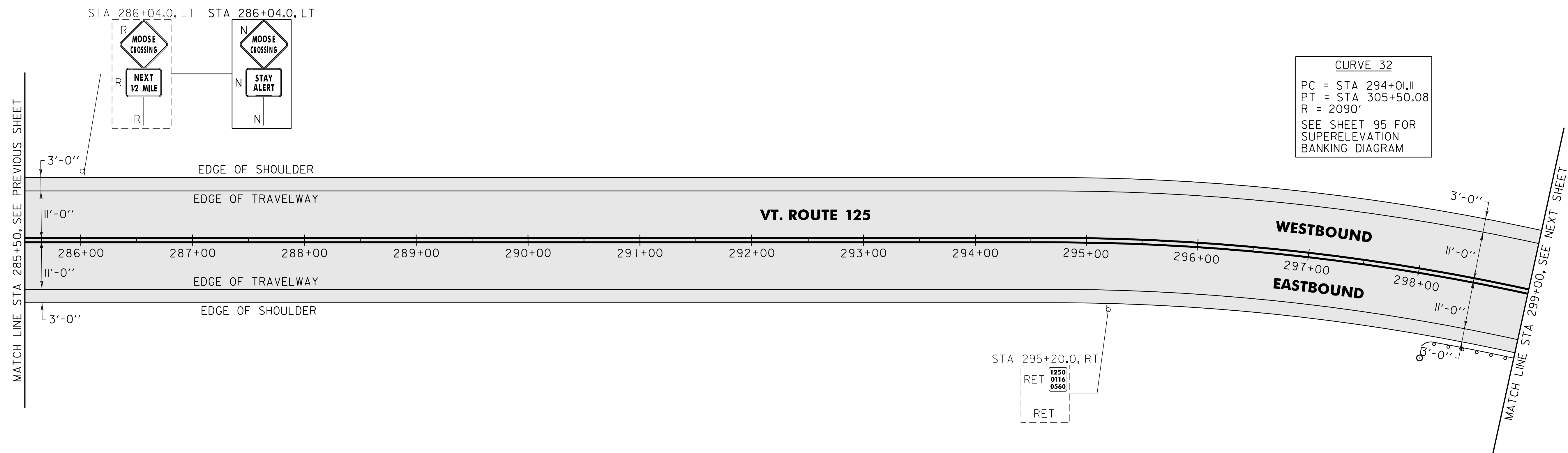
NOT TO SCALE

AREA TO BE RECLAIMED

PROJECT LAYOUT SHEET #22	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 81 OF 114
	DESIGNED BY: M.J.M.	
	IPARM FILE: p08cl60i22.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND
RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 285+50.00 - STA 299+00.00 EDGE LINE LT & RT
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE C/L BREAKS FOR
TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON: LT C RT
STA 285+50.00 - STA 299+00.00 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND
RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 285+50.00 - STA 299+00.00 EDGE LINE LT & RT
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
(ALL LINES WILL INCLUDE C/L BREAKS FOR
TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON: LT C RT
STA 285+50.00 - STA 299+00.00 S - S



CURVE 32
PC = STA 294+01.11
PT = STA 305+50.08
R = 2090'
SEE SHEET 95 FOR
SUPERELEVATION
BANKING DIAGRAM

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
RIPTON:
STA 298+14.0 - STA 299+00.0 RT (86.0 LF)

676.10 DELINEATOR WITH STEEL POST
Ø TYPE 1 WHITE
RIPTON:
STA 298+11.5 RT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
RIPTON:
STA 298+11.5 - STA 298+49.0 RT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
RIPTON:
STA 298+49.0 - STA 299+00.0 RT (51.0 LF)

SIGN LEGEND
R = REMOVE
S = SALVAGE
N = NEW
RET = RETAIN
B-B = BACK TO BACK
EXISTING = - - - - -
NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #23	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 82 OF 114
	DESIGNED BY: M.J.M.	
	IPARM FILE: p08cl60i23.i	

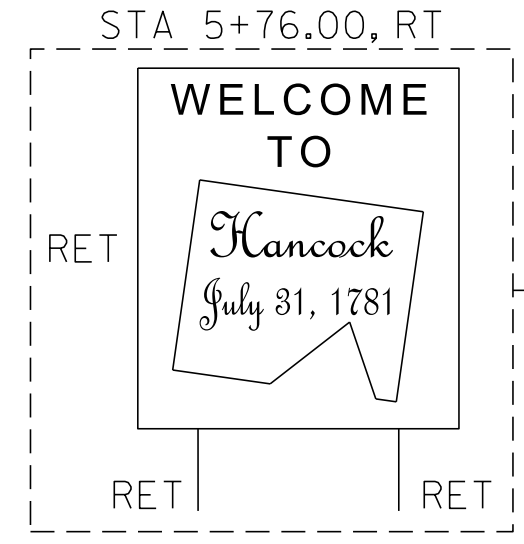
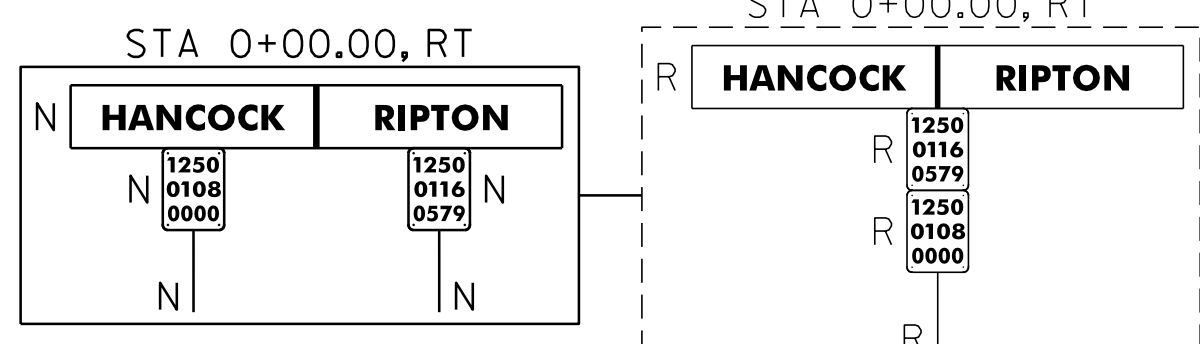
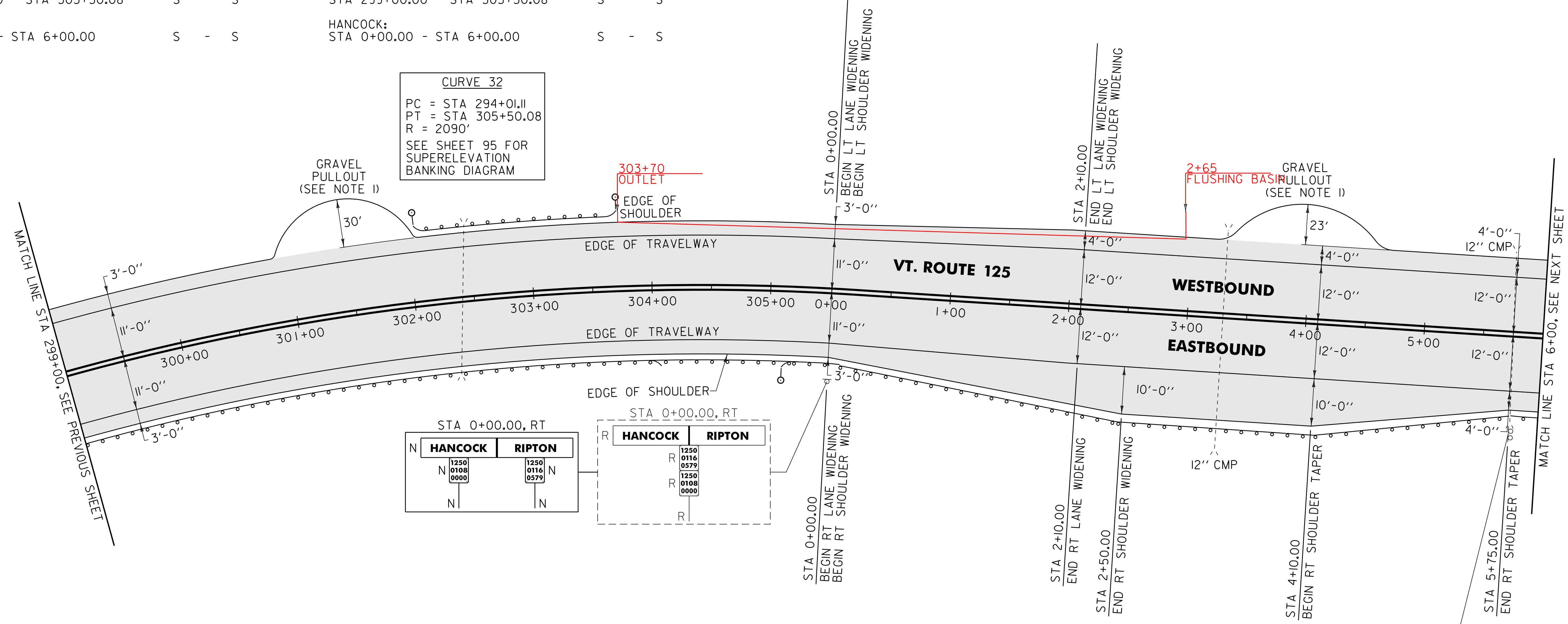
AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 299+00.00 - STA 305+50.08 EDGE LINE LT & RT
HANCOCK:
STA 0+00.00 - STA 6+00.00 EDGE LINE LT & RT
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON:
STA 299+00.00 - STA 305+50.08 LT & RT S - S
HANCOCK:
STA 0+00.00 - STA 6+00.00 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
RIPTON:
STA 299+00.00 - STA 305+50.08 EDGE LINE LT & RT
HANCOCK:
STA 0+00.00 - STA 6+00.00 EDGE LINE LT & RT
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
RIPTON:
STA 299+00.00 - STA 305+50.08 LT & RT S - S
HANCOCK:
STA 0+00.00 - STA 6+00.00 S - S

MM 5.786 = STA 305+50.08
MM 0.000 = STA 0+00.00
RIPTON HANCOCK

CURVE 32
PC = STA 294+01.11
PT = STA 305+50.08
R = 2090'
SEE SHEET 95 FOR SUPERELEVATION BANKING DIAGRAM



616.35 TREATED TIMBER CURB
RIPTON:
STA 299+20.0 - STA 300+60.0 RT (140.0 LF)
621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
RIPTON:
STA 299+00.0 - STA 303+97.0 RT (397.0 LF)
STA 302+04.0 - STA 303+70.0 LT (166.0 LF)
STA 305+10.1 - STA 305+50.1 RT (40.0 LF)
HANCOCK:
STA 0+00.0 - STA 6+00.0 RT (600.0 LF)
676.10 DELINEATOR WITH STEEL POST
TYPE I WHITE
RIPTON:
STA 301+99.5 LT
STA 303+74.5 LT
STA 305+05.0 RT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
RIPTON:
STA 301+99.5 - STA 302+37.0 LT
STA 303+37.0 - STA 303+74.5 LT
~~STA 305+05.0 - STA 305+42.5 RT~~
900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, TANGENT/WEATHERING)
~~RIPTON:
STA 303+49.0 - STA 303+99.0 RT~~
900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
RIPTON:
STA 299+00.0 - STA 303+49.0 RT (449.0 LF)
STA 302+37.0 - STA 303+37.0 LT (100.0 LF)
STA 305+42.5 - STA 305+50.1 RT (7.6 LF)
HANCOCK:
STA 0+00.0 - STA 6+00.0 RT (600.0 LF)

AREA TO BE RECLAIMED

SIGN LEGEND
R = REMOVE
S = SALVAGE
N = NEW
RET = RETAIN
B-B = BACK TO BACK
EXISTING = - - - - -
NEW = _____

NOTE:
I. PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I.

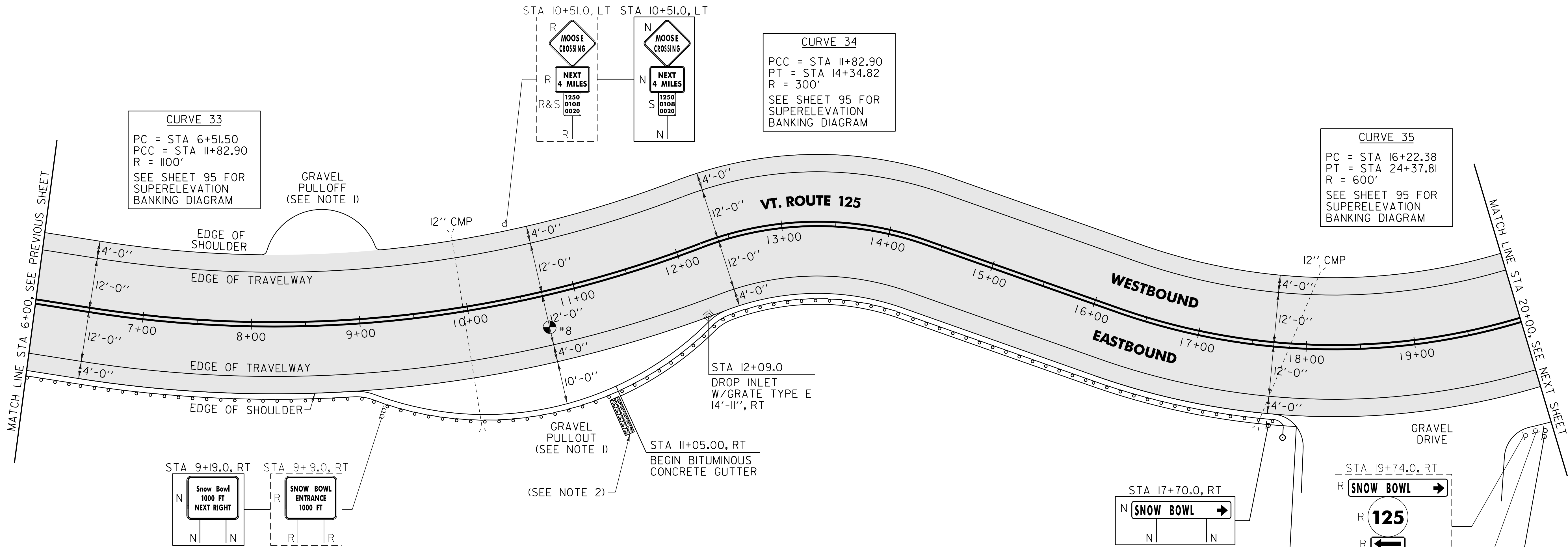
NOT TO SCALE

PROJECT LAYOUT SHEET #24	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 83 OF 114
	DESIGNED BY: M.J.M.	
	IPARM FILE: p08cl60124.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 HANCOCK:
 STA 6+00.00 - STA 20+00.00 EDGE LINE LT & RT
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 HANCOCK:
 STA 6+00.00 - STA 20+00.00 LT CL RT
 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 HANCOCK:
 STA 6+00.00 - STA 20+00.00 EDGE LINE LT & RT
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 HANCOCK:
 STA 6+00.00 - STA 20+00.00 LT CL RT
 S - S

675.50 REMOVING SIGNS
 AS SHOWN - 7
 675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 1



604.412, 604.415, 604.418 REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, II, III

HANCOCK:
 STA 12+09.0 RT
 616.35 TREATED TIMBER CURB
 HANCOCK:
 STA 12+10.0 - STA 15+60.0 RT (350.0 LF)

616.47 BITUMINOUS CONCRETE GUTTER AND TRAFFIC ISLANDS
 HANCOCK:
 STA 10+05.0 - STA 17+95.0 RT (34.9 TON)
 621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 HANCOCK:
 STA 6+00.0 - STA 17+84.0 RT (1184.0 LF)

676.10 DELINEATOR WITH STEEL POST
 TYPE I WHITE
 HANCOCK:
 STA 17+79.9 RT

613.10 STONE FILL, TYPE I
 HANCOCK:
 STA 11+05.0 RT (2.5 CY)

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
 HANCOCK:
 STA 17+42.4 - STA 17+79.9 RT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
 HANCOCK:
 STA 6+00.0 - STA 17+42.4 RT (1142.4 LF)

PAVEMENT CORES =

#	TOTAL PCC COMMENTS	DEPTH (INCH)
8	3 1/2"	NO

AREA TO BE RECLAIMED

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

- NOTES:
- PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I.
 - ITEM 631.10 STONE FILL, TYPE I SHALL BE PLACED ALONG THE EXISTING SIDE SLOPE AS DIRECTED BY THE RESIDENT ENGINEER.

NOT TO SCALE

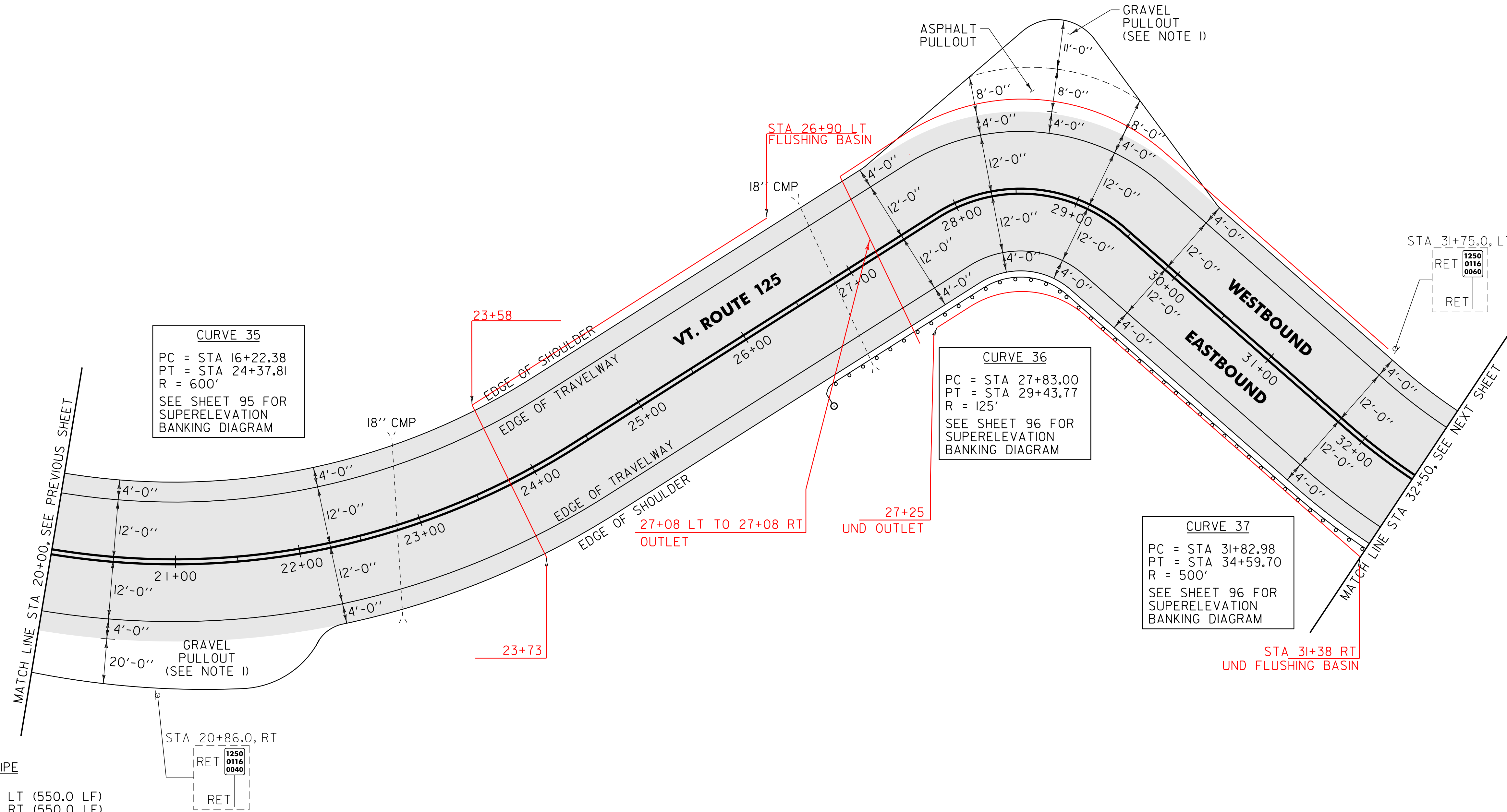
PROJECT LAYOUT SHEET #25	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 84 OF 114
	DESIGNED BY: M.J.M.	
	IPARM FILE: p08cl60125.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 HANCOCK:
 STA 20+00.00 - STA 32+50.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 HANCOCK:
 STA 20+00.00 - STA 32+50.00 LT C RT S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 HANCOCK:
 STA 20+00.00 - STA 32+50.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 HANCOCK:
 STA 20+00.00 - STA 32+50.00 LT C RT S - S



CURVE 35
 PC = STA 16+22.38
 PT = STA 24+37.81
 R = 600'
 SEE SHEET 95 FOR SUPERELEVATION BANKING DIAGRAM

CURVE 36
 PC = STA 27+83.00
 PT = STA 29+43.77
 R = 125'
 SEE SHEET 96 FOR SUPERELEVATION BANKING DIAGRAM

CURVE 37
 PC = STA 31+82.98
 PT = STA 34+59.70
 R = 500'
 SEE SHEET 96 FOR SUPERELEVATION BANKING DIAGRAM

605.10 6 INCH UNDERDRAIN PIPE
 HANCOCK:
 STA 26+00.0 - STA 31+50.0 LT (550.0 LF)
 STA 26+00.0 - STA 31+50.0 RT (550.0 LF)

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 HANCOCK:
 STA 26+31.0 STA 32+50.0 RT (619.0 LF)

676.10 DELINEATOR WITH STEEL POST
 TYPE I WHITE
 HANCOCK:
 STA 26+31.0 RT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
 HANCOCK:
 STA 26+31.0 - STA 26+68.5 RT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
 HANCOCK:
 STA 26+68.5 - STA 32+50.0 RT (581.5 LF)

NOTE:

1. PAVE THE ENTIRE EXISTING GRAVEL PULLOUT WITH 1/2" TYPE IVS. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID FOR UNDER ITEM 608.25 ALL PURPOSE EXCAVATOR RENTAL, TYPE I.

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = - - - - -
 NEW = _____

NOT TO SCALE

PROJECT LAYOUT SHEET #26	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	PROJECT LEADER: D.E.G.	SHEET 85 OF 114
	DESIGNED BY: M.J.M.	
	IPARM FILE: p08cl60126.i	

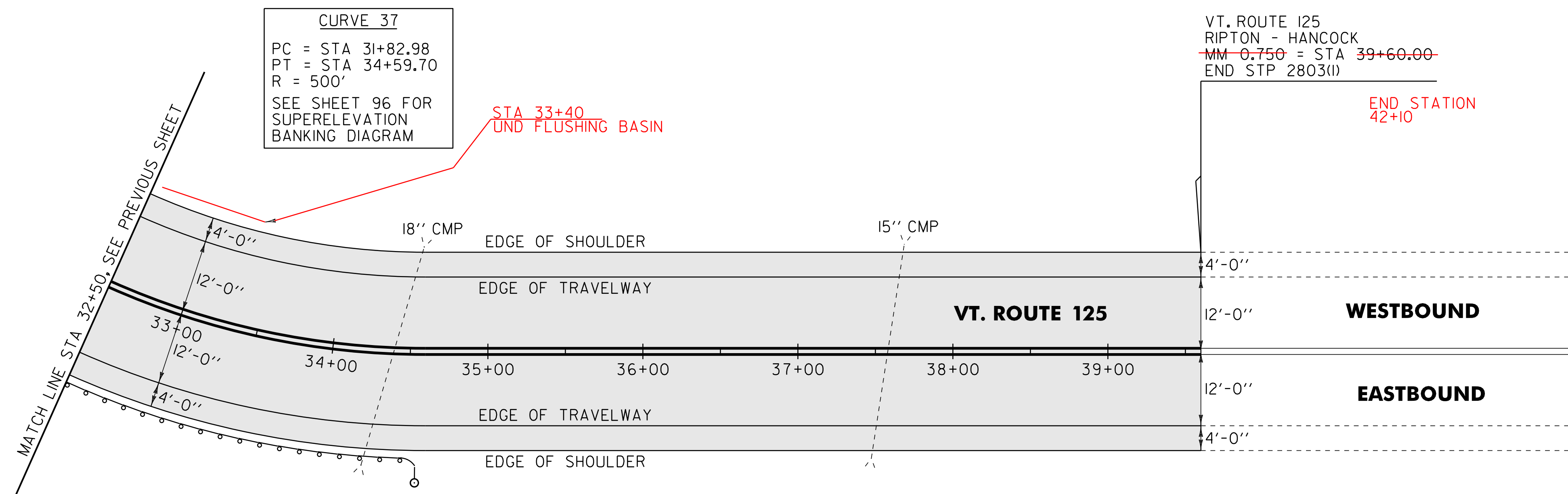
AREA TO BE RECLAIMED

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND
 RADIUS FOR TOWN HIGHWAYS)
 HANCOCK:
 STA 32+50.00 - STA 39+60.00 EDGE LINE LT & RT

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 (ALL LINES WILL INCLUDE C/L BREAKS FOR
 TOWN HIGHWAYS) S=SOLID, D=DASHED
 HANCOCK: LT C RT
 STA 32+50.00 - STA 39+60.00 S - S

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND
 RADIUS FOR TOWN HIGHWAYS)
 HANCOCK:
 STA 32+50.00 - STA 39+60.00 EDGE LINE LT & RT

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 (ALL LINES WILL INCLUDE C/L BREAKS FOR
 TOWN HIGHWAYS) S=SOLID, D=DASHED
 HANCOCK: LT C RT
 STA 32+50.00 - STA 39+60.00 S - S



CURVE 37
 PC = STA 31+82.98
 PT = STA 34+59.70
 R = 500'
 SEE SHEET 96 FOR
 SUPERELEVATION
 BANKING DIAGRAM

VT. ROUTE 125
 RIPTON - HANCOCK
~~MM 0.750 = STA 39+60.00~~
 END STP 2803(I)

END STATION
 42+10

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 HANCOCK:
 STA 32+50.0 - STA 34+54.0 RT (204.0 LF)

676.10 DELINEATOR WITH STEEL POST
 TYPE 1 WHITE
 HANCOCK:
 STA 34+56.0 RT

900.620 SPECIAL PROVISION (MANUFACTURED TERMINAL SECTION, FLARED/WEATHERING)
 HANCOCK:
 STA 34+18.5 - STA 34+56.0 RT

900.640 SPECIAL PROVISION (STEEL BEAM GUARDRAIL, WEATHERING)
 HANCOCK:
 STA 32+50.0 - STA 34+18.5 RT (168.5 LF)

AREA TO BE RECLAIMED

NOT TO SCALE

SIGN LEGEND
 R = REMOVE
 S = SALVAGE
 N = NEW
 RET = RETAIN
 B-B = BACK TO BACK
 EXISTING = -----
 NEW = _____

PROJECT LAYOUT SHEET #27	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(I)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 86 OF 114
	IPARM FILE: p08cl60i27.i	

601.0015, 601.0815 & 601.2615 PIPE OPTIONS

RIPTON:
 STA 59+82.0 - STA 60+32.0 LT (50.0 LF)
 STA 60+32.0 - STA 61+56.0 LT (118.0 LF)
 STA 61+56.0 - STA 63+35.0 LT (176.0 LF)

601.6015, 601.6815 & 601.7015 PIPE END SECTION OPTION

RIPTON:
 STA 59+55.0 LT

613.10 STONE FILL, TYPE I

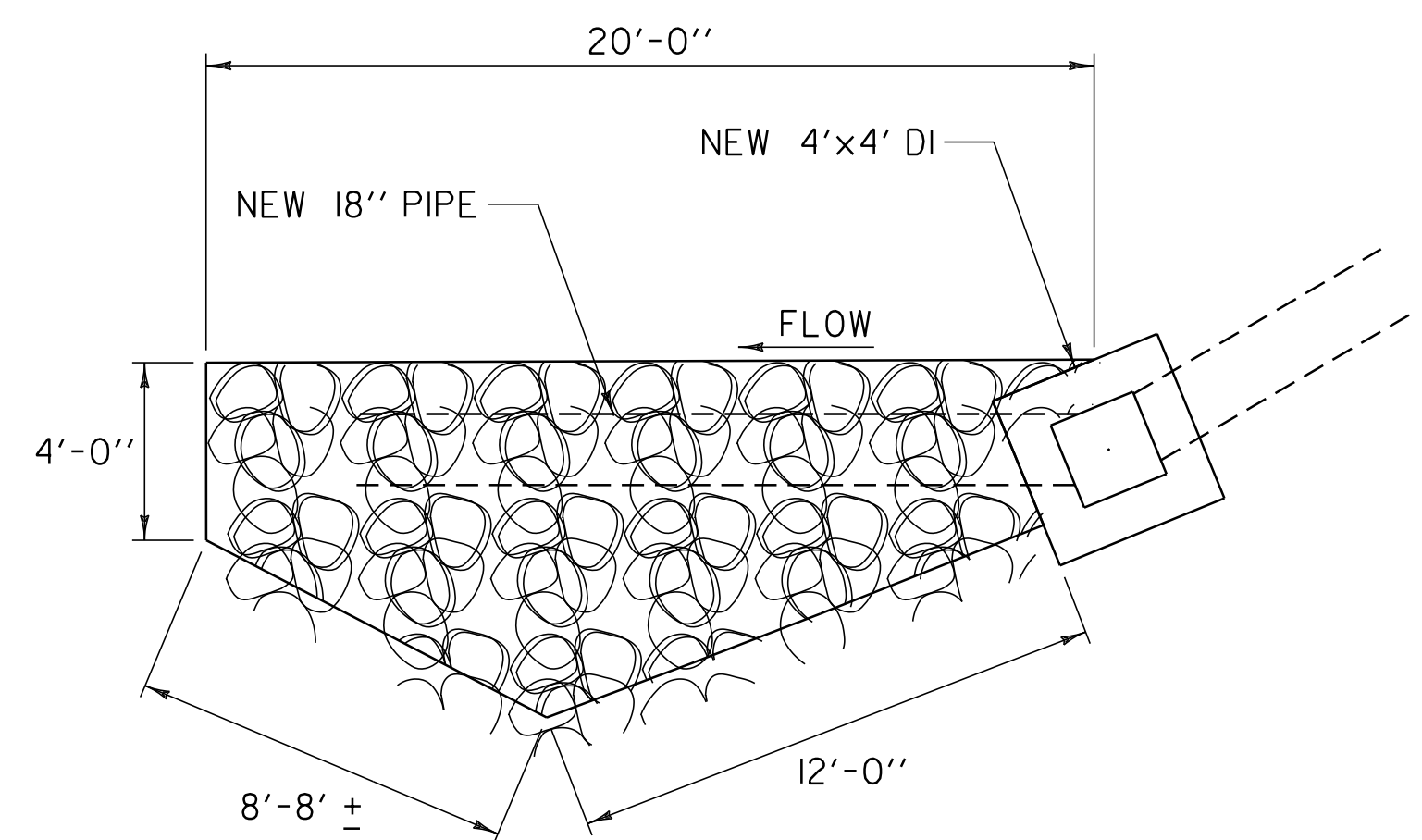
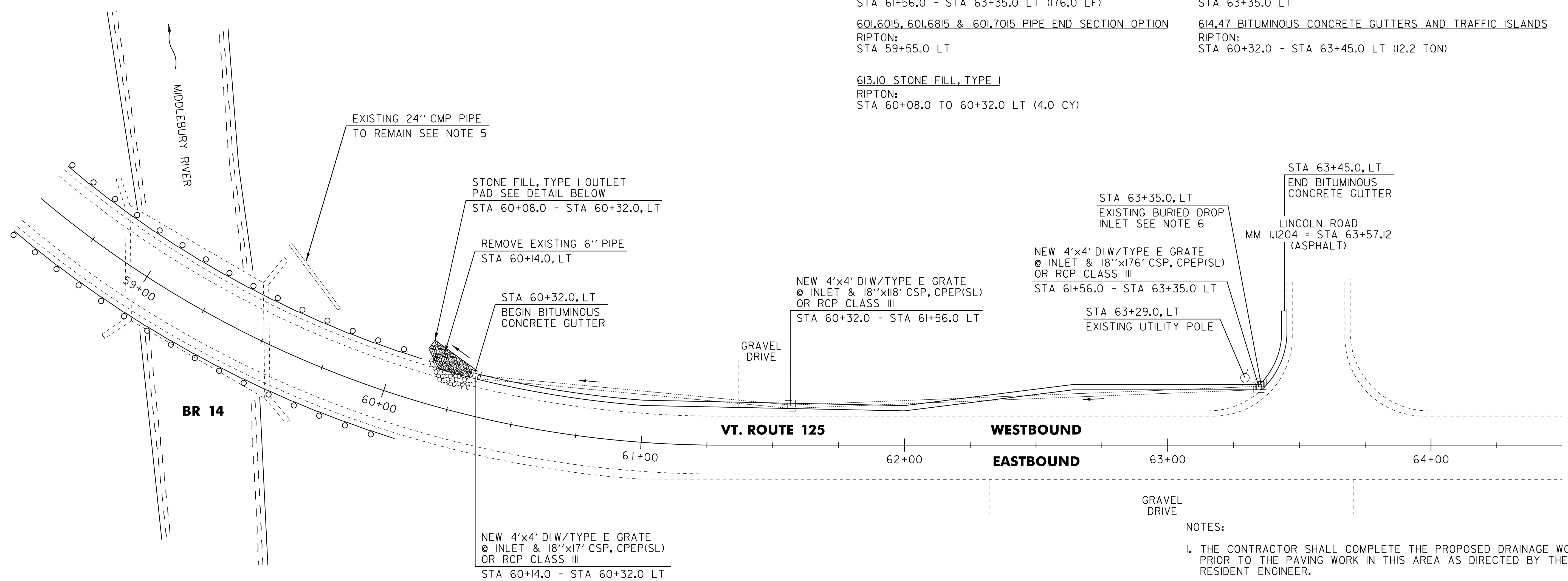
RIPTON:
 STA 60+08.0 TO 60+32.0 LT (4.0 CY)

604.18 PRECAST REINFORCED CONCRETE DROP INLET WITH CAST IRON GRATE

RIPTON:
 STA 60+32.0 LT
 STA 61+56.0 LT
 STA 63+35.0 LT

614.47 BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS

RIPTON:
 STA 60+32.0 - STA 63+45.0 LT (12.2 TON)



TYPE I STONE OUTLET PAD

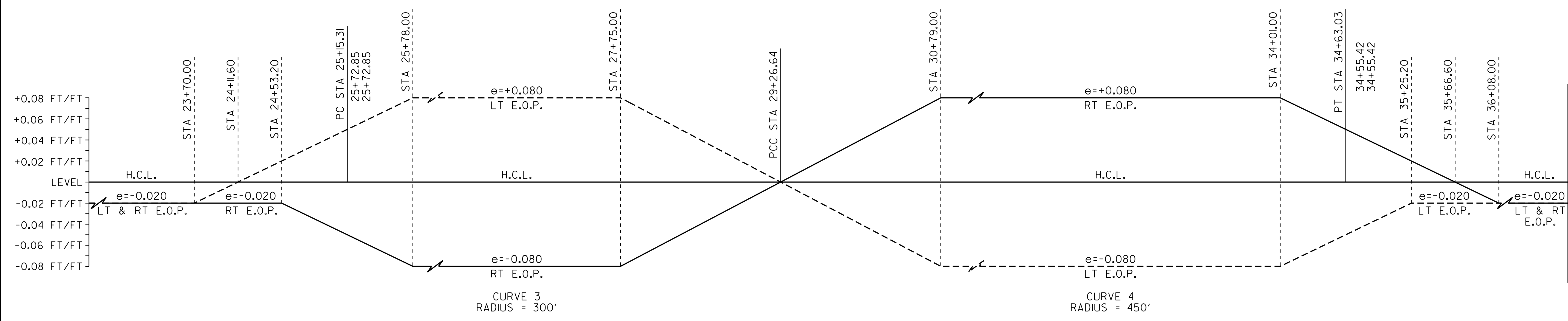
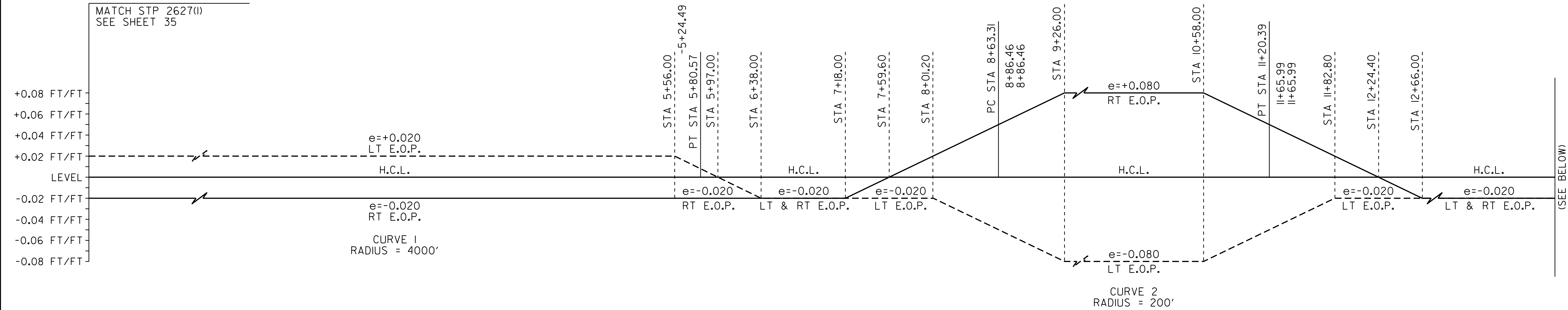
NOTES:

1. THE CONTRACTOR SHALL COMPLETE THE PROPOSED DRAINAGE WORK PRIOR TO THE PAVING WORK IN THIS AREA AS DIRECTED BY THE RESIDENT ENGINEER.
2. THIS PLAN WAS PREPARED USING LIMITED SURVEY DATA AND VISUAL OBSERVATIONS. THE CONTRACTOR SHALL FIELD VERIFY THE EXISTING SITE CONDITIONS PRIOR TO ORDERING MATERIALS AS DIRECTED BY THE RESIDENT ENGINEER.
3. ALL NEW PIPES SHALL HAVE A MINIMUM COVER OF 3' TO THE TOP OF PIPE UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
4. ALL NEW 4' X 4' DROP INLETS SHALL BE MANUFACTURED IN ACCORDANCE WITH STANDARD SHEET D-6 FOR THE APPROPRIATE PIPE DEPTHS.
5. THE EXISTING 24" CMP HAS AN UNKNOWN INLET POINT. THE CONTRACTOR SHALL PROTECT AND MAINTAIN THIS PIPE THROUGHOUT THE CONSTRUCTION OF THIS PROJECT AS DIRECTED BY THE RESIDENT ENGINEER. IF THE PIPE IS DAMAGED DURING CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL REPAIR THE PIPE AT THEIR OWN COST AS DIRECTED BY THE RESIDENT ENGINEER.
6. THE CONTRACTOR SHALL REMOVE OR ABANDON THE EXISTING BURIED DI AND ANY ASSOCIATED PIPES THAT ARE NO LONGER BEING UTILIZED AS DIRECTED BY THE RESIDENT ENGINEER. EXISTING PIPES THAT GO UNDER THE ROADWAY SHALL BE FILLED WITH ITEM 900.608 (SPECIAL PROVISION (CONTROLLED DENSITY (FLOWABLE) FILL) AS DIRECTED BY THE RESIDENT ENGINEER. SEE SHEET 114 FOR PIPE ABANDONMENT AND DRAINAGE STRUCTURE ABANDONMENT DETAILS.
7. STONE FILL, TYPE I SHALL BE USED AS DIRECTED BY THE RESIDENT ENGINEER AND SHALL BE PAID FOR UNDER ITEM 613.10 STONE FILL, TYPE I.

NOT TO SCALE

DRAINAGE PLAN	PROJECT NAME: RIPTON - HANCOCK	
	PROJECT NUMBER: STP 2803(1)	
	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:10
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.	
IPARM FILE: p08cl610dp.1	SHEET 87 OF 114	

BEGIN STP 2803(I)
MM 0.00 = STA 0+00.00
MATCH STP 2627(I)
SEE SHEET 35

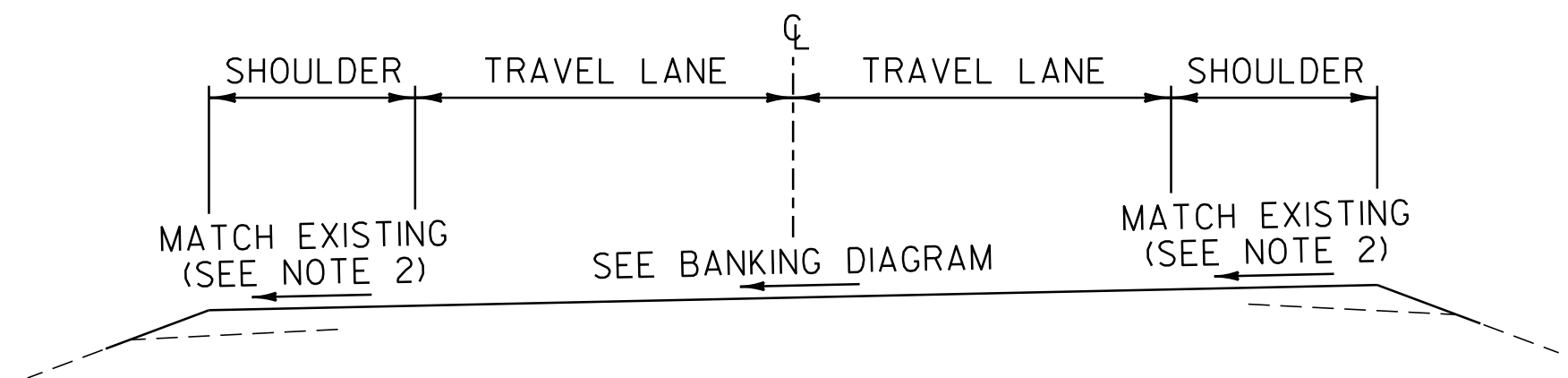


(SEE BELOW)

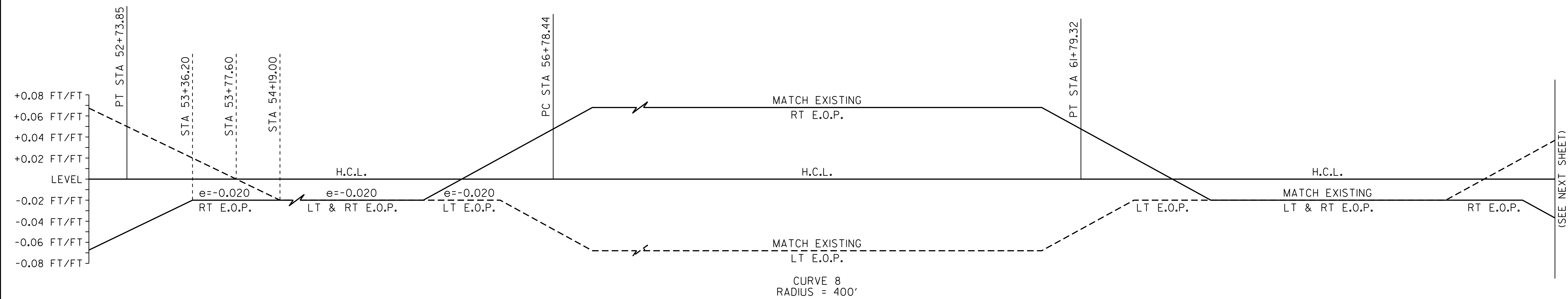
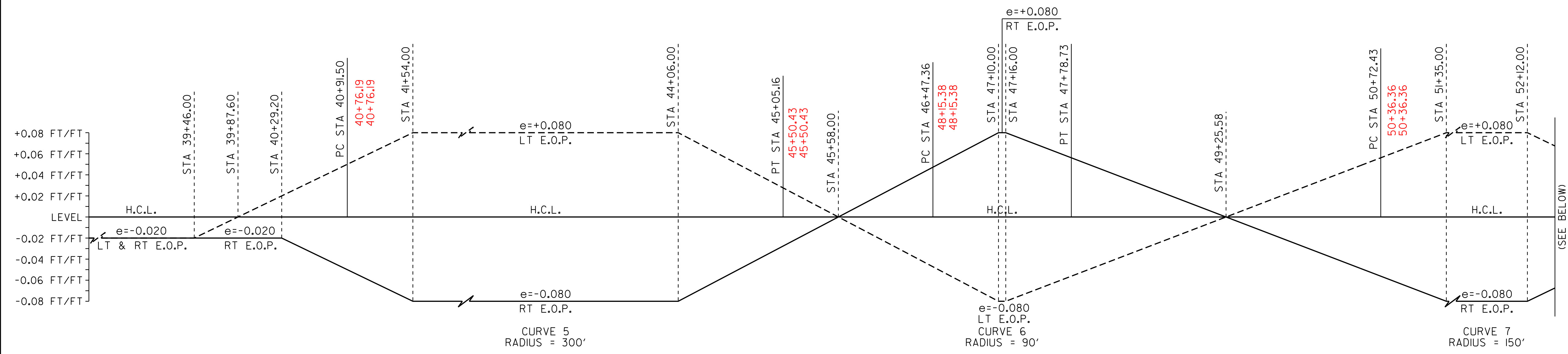
(SEE NEXT SHEET)

SUPERELEVATION BANKING NOTES:

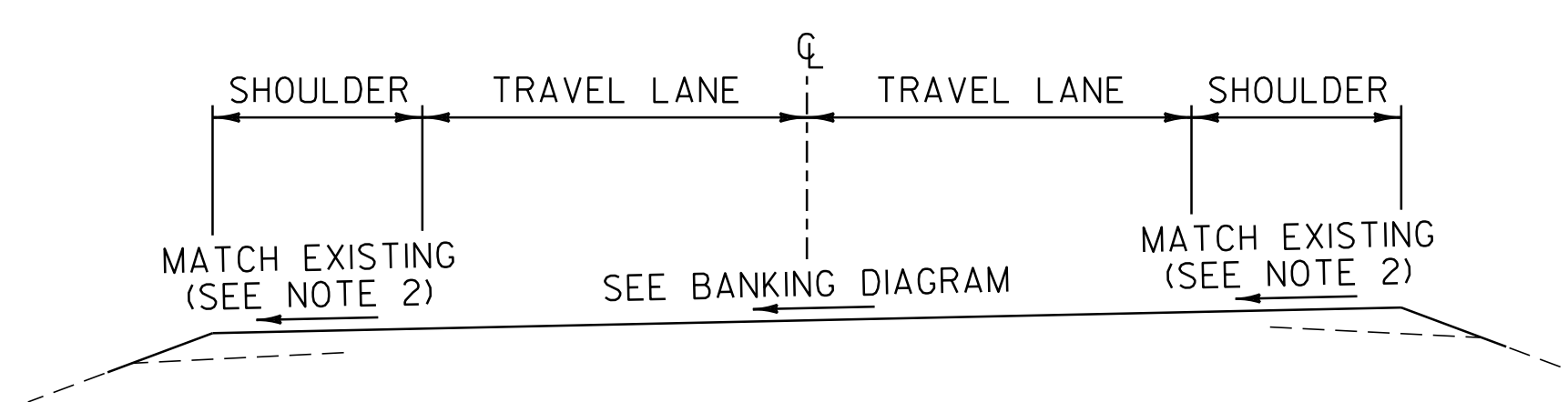
1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



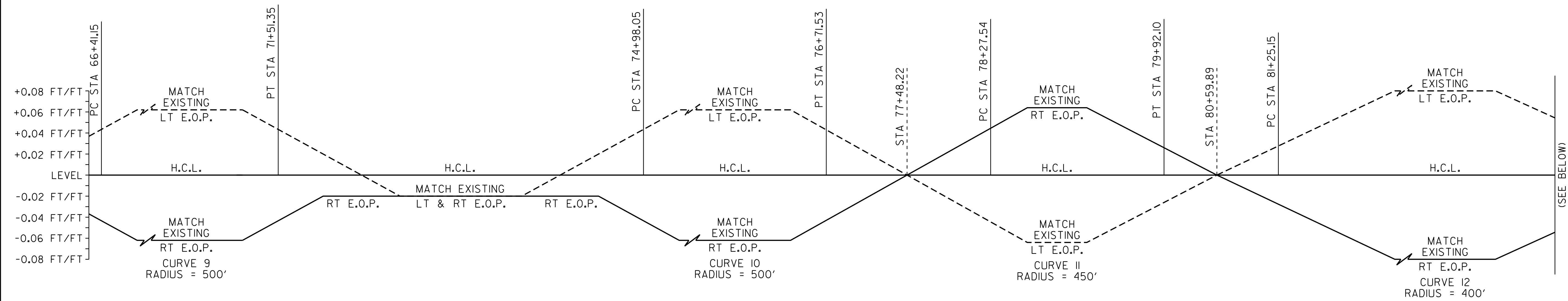
SUPERELEVATION BANKING DIAGRAMS SHEET #1	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(I)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	DESIGNED BY: M.J.M.
	IPARM FILE: p08cl60sbd1.i	CHECKED BY: D.W.E.
		SHEET 88 OF 114



- SUPERELEVATION BANKING NOTES:**
1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
 2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
 3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.

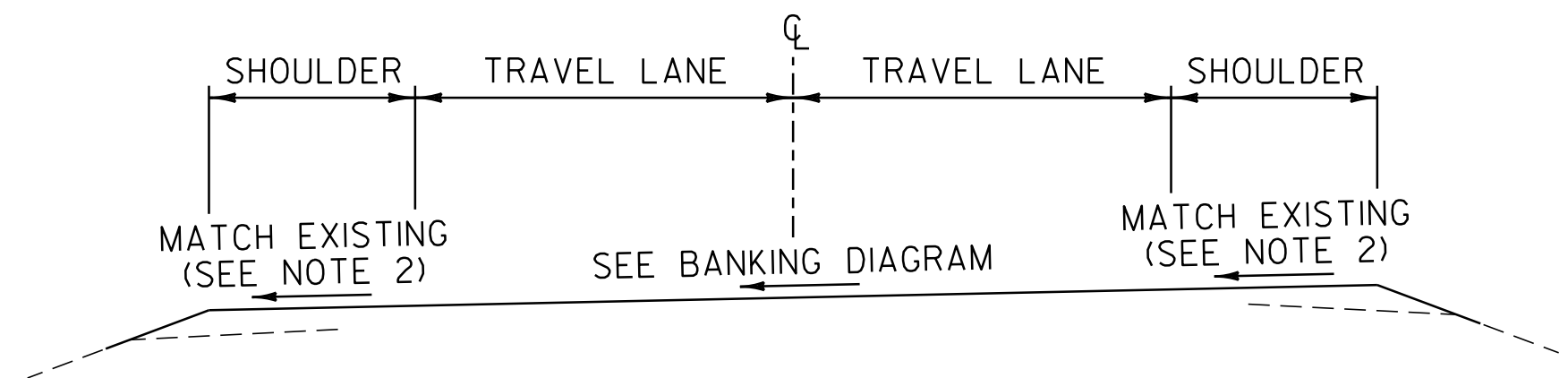


SUPERELEVATION BANKING DIAGRAMS SHEET #2	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 89 OF 114
	IPARM FILE: p08cl60sbd2.i	

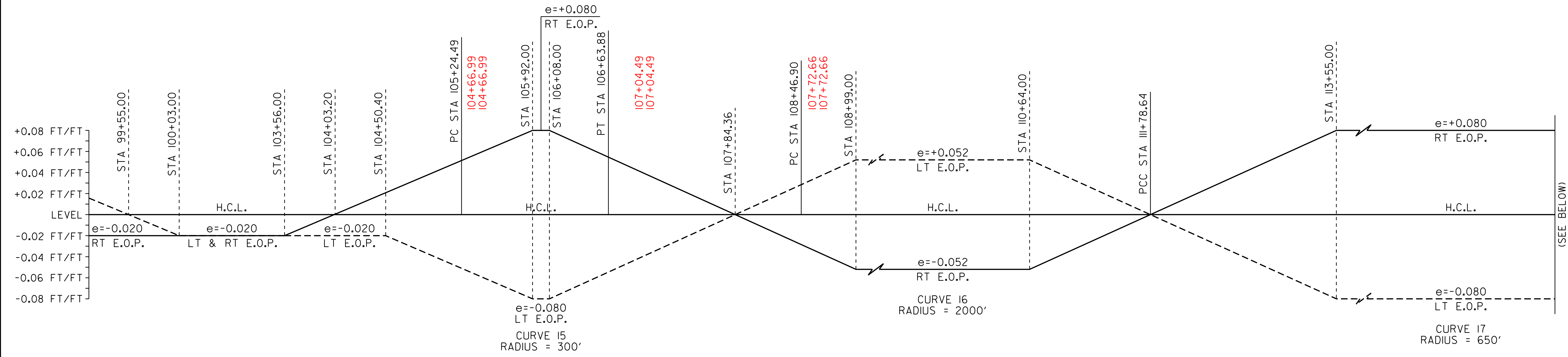


SUPERELEVATION BANKING NOTES:

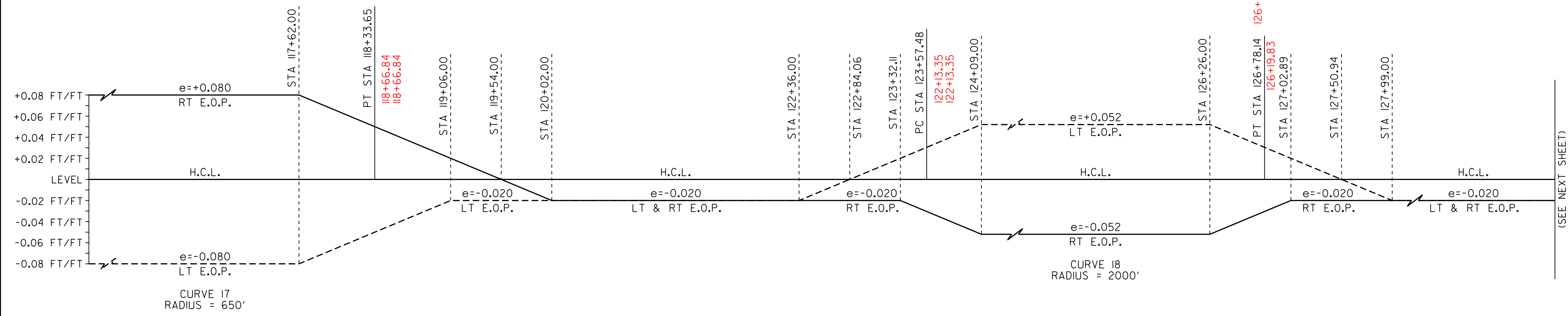
1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



SUPERELEVATION BANKING DIAGRAMS SHEET #3	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	DESIGNED BY: M.J.M.
	IPARM FILE: p08cl60sbd3.1	CHECKED BY: D.W.E.
		SHEET 90 OF 114



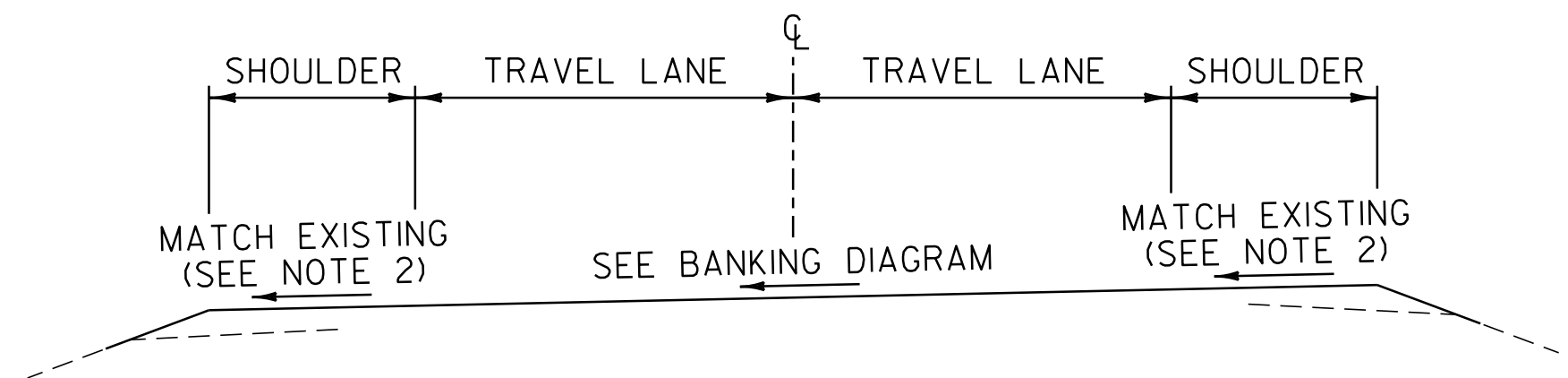
(SEE BELOW)



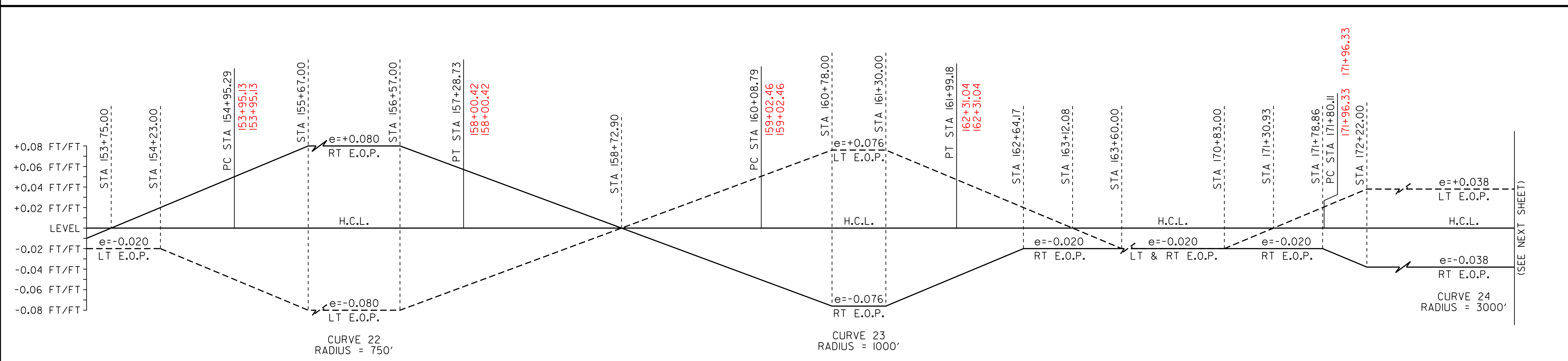
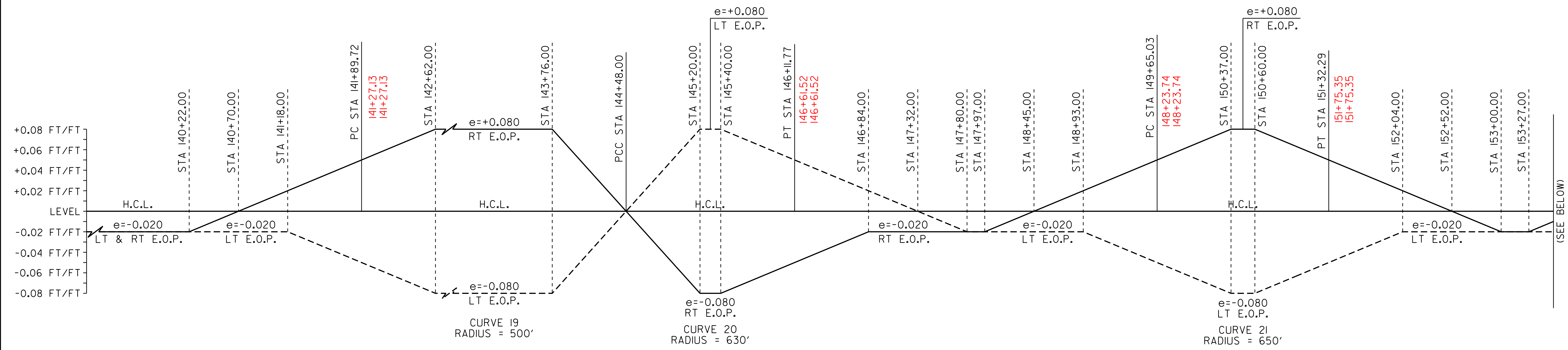
(SEE NEXT SHEET)

SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
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3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.

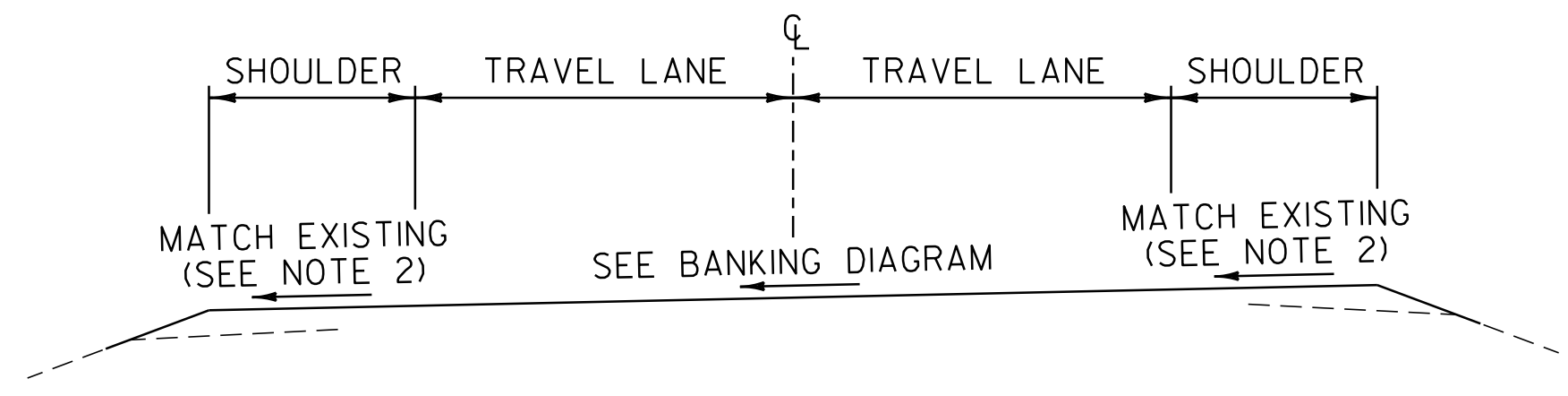


SUPERELEVATION BANKING DIAGRAMS SHEET #4	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 91 OF 114
	IPARM FILE: p08cl60sbd4.i	

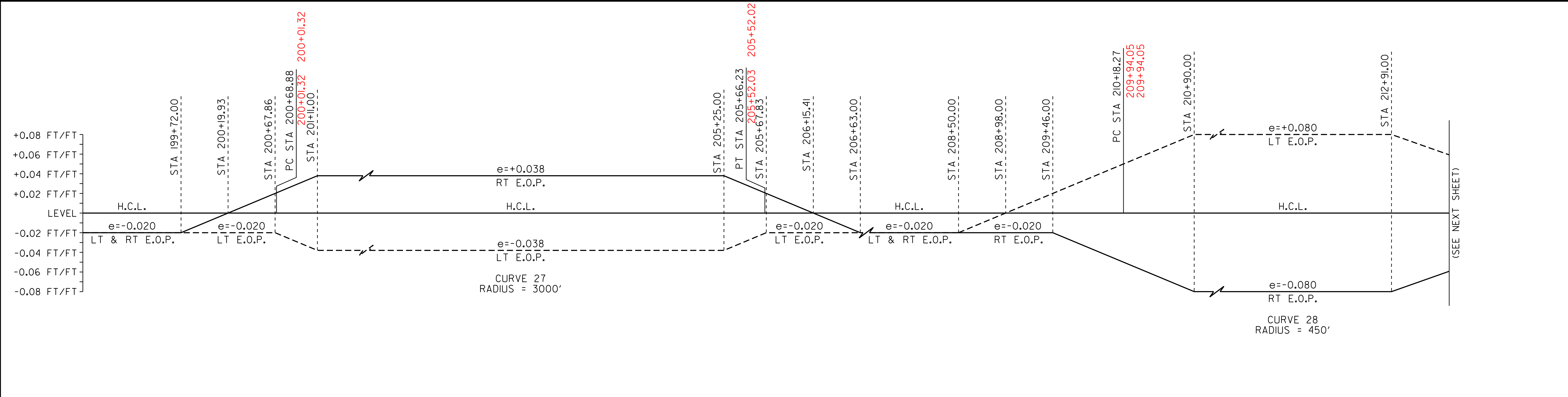
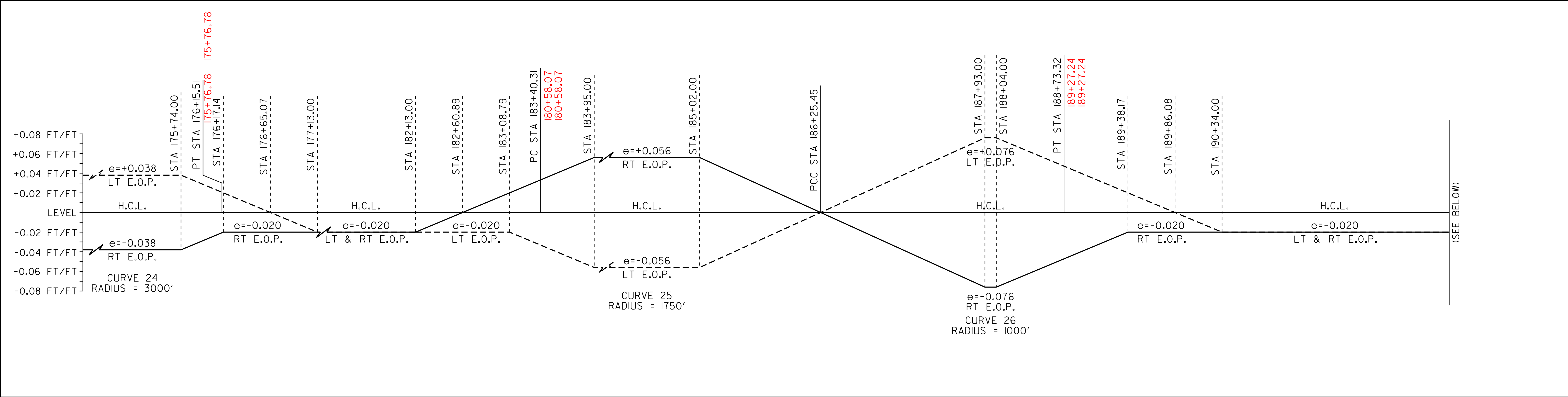


SUPERELEVATION BANKING NOTES:

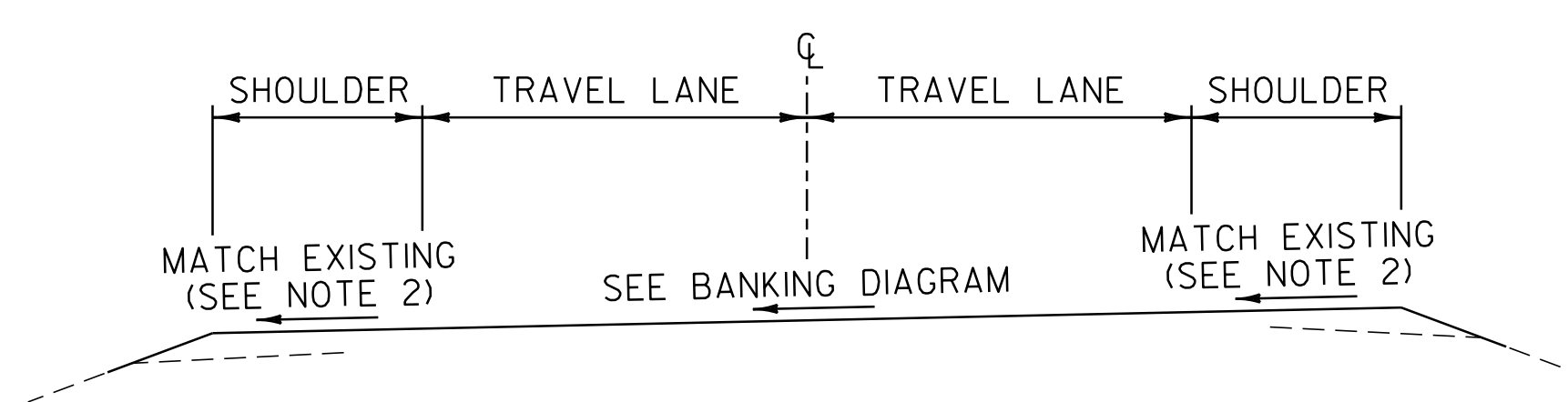
1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



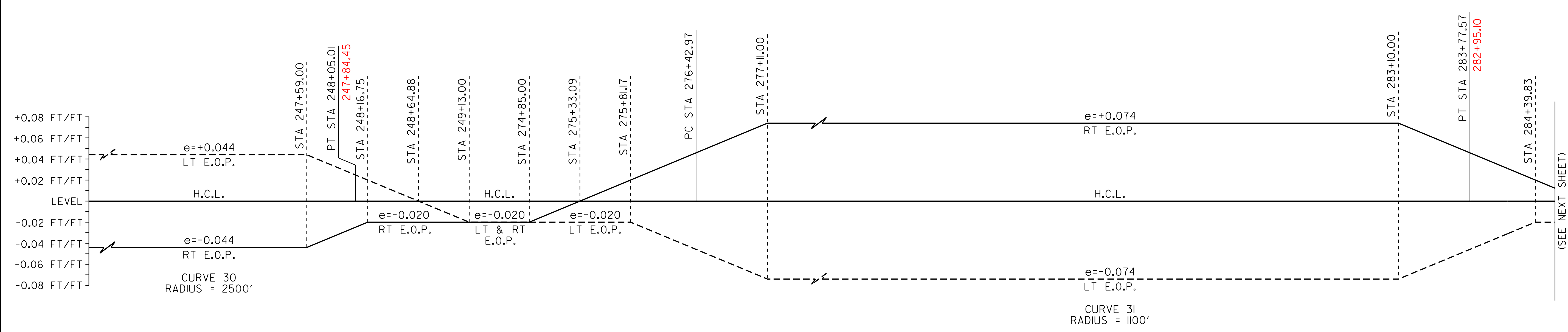
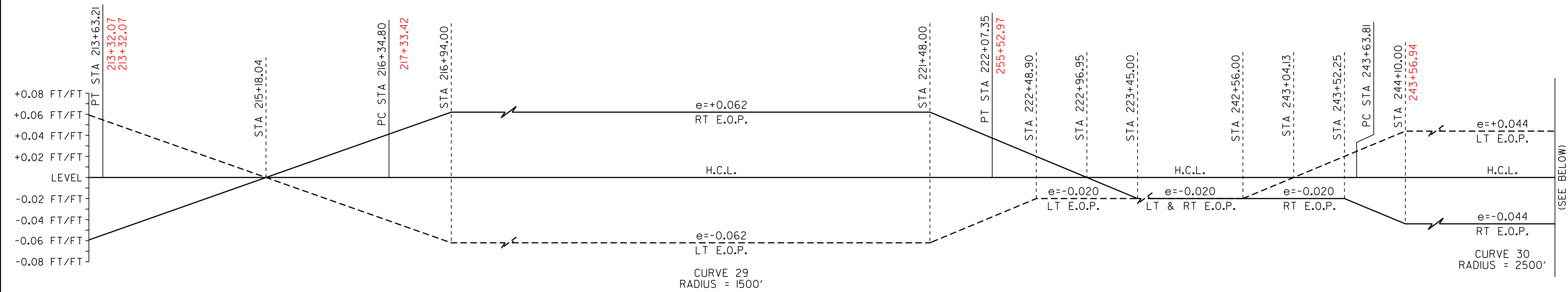
SUPERELEVATION BANKING DIAGRAMS SHEET #5	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	DESIGNED BY: M.J.M.
	IPARM FILE: p08cl60sbd5.i	CHECKED BY: D.W.E.
		SHEET 92 OF 114



- SUPERELEVATION BANKING NOTES:**
1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
 2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
 3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.

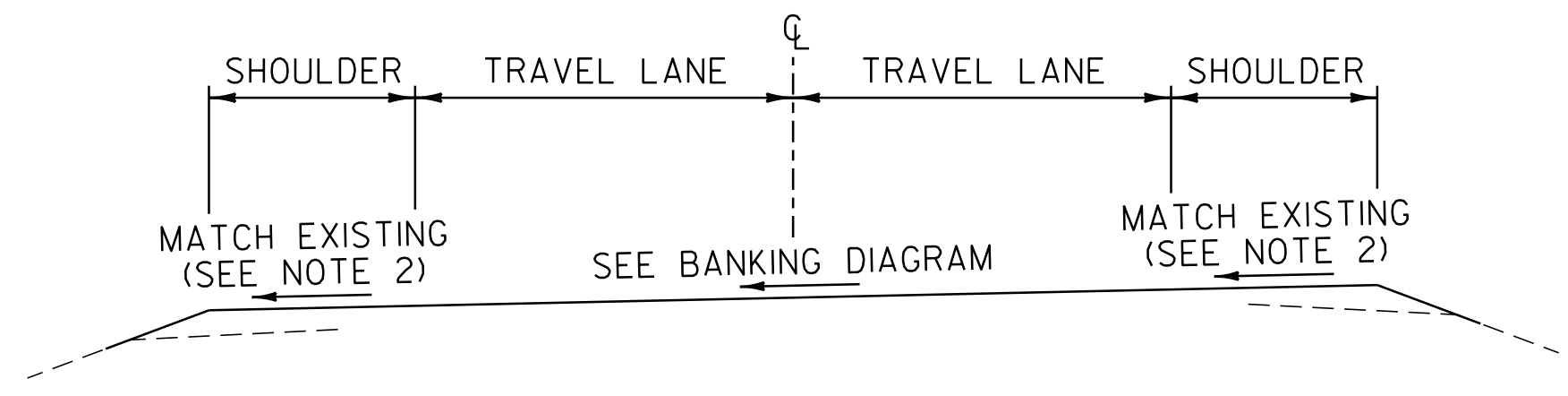


SUPERELEVATION BANKING DIAGRAMS SHEET #6	PROJECT NAME: RIPTON - HANCOCK	
	PROJECT NUMBER: STP 2803(1)	
	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:10
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.	
IPARM FILE: p08cl60sbd6.i		SHEET 93 OF 114



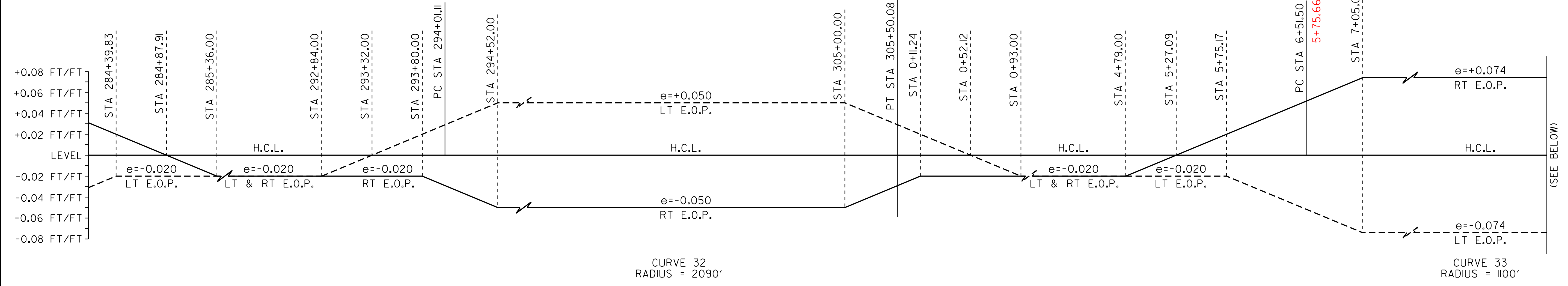
SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
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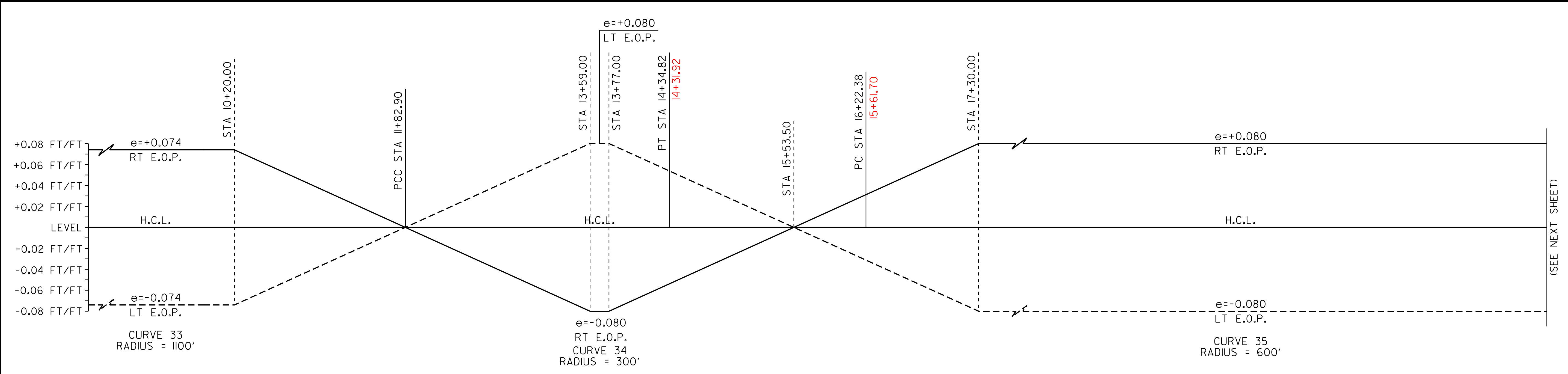


SUPERELEVATION BANKING DIAGRAMS SHEET #7	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	DESIGNED BY: M.J.M.
	IPARM FILE: p08cl60sbd7.i	CHECKED BY: D.W.E.
		SHEET 94 OF 114

END RIPTON
MM 5.786 = STA 305+50.08
BEGIN HANCOCK
MM 0.000 = STA 0+00.00



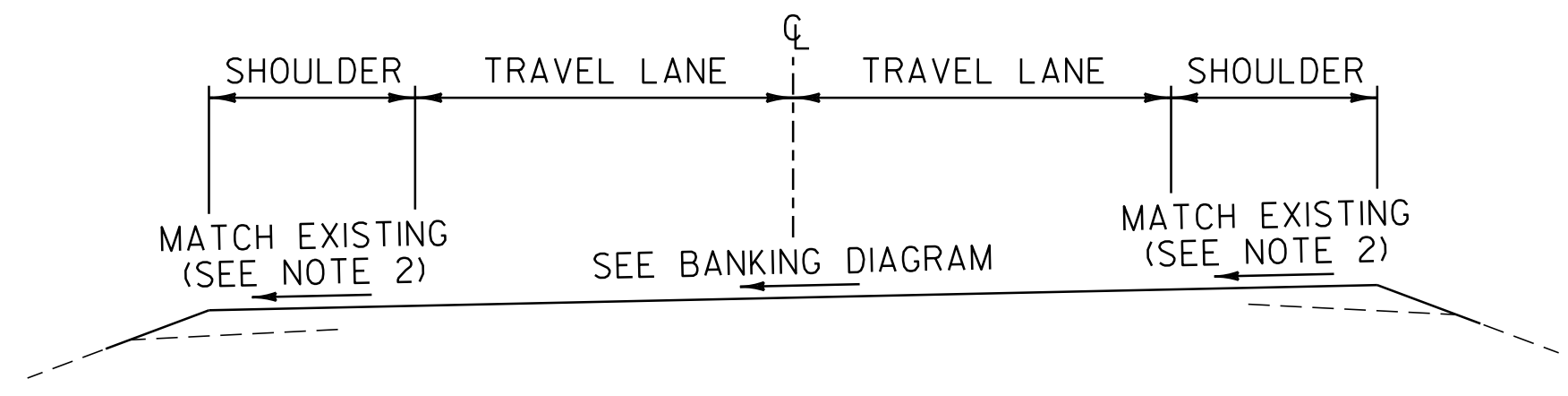
(SEE BELOW)



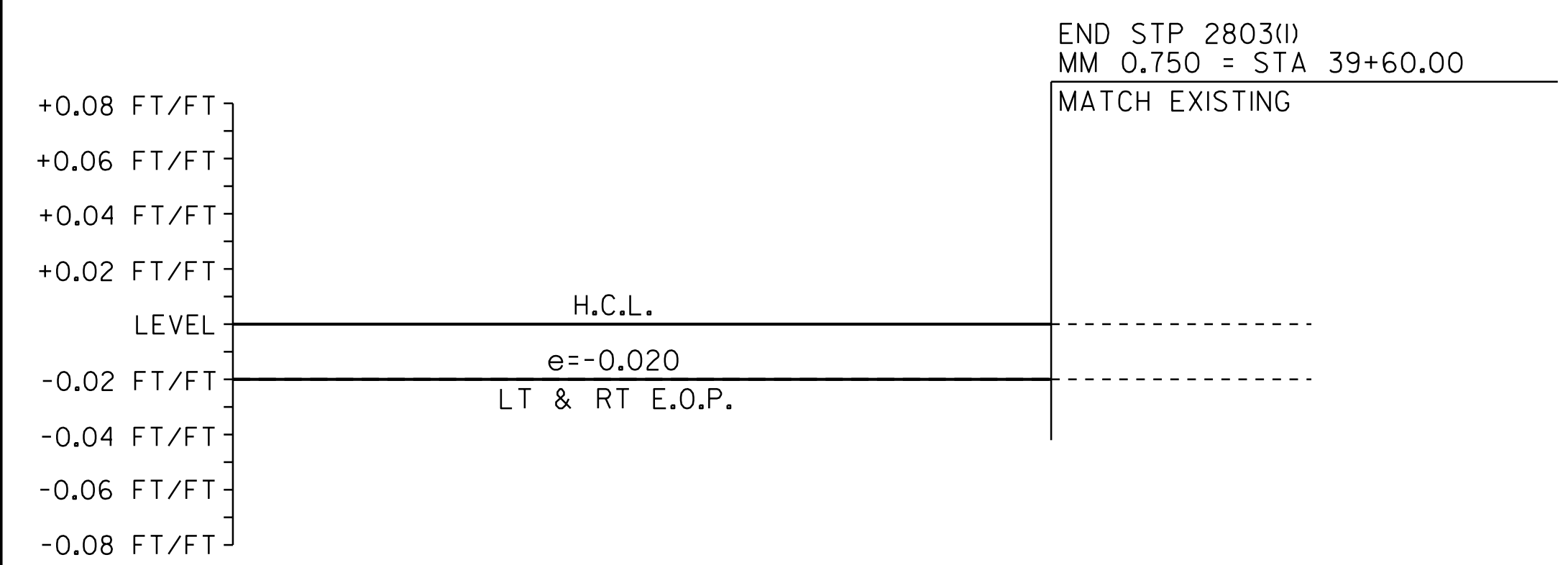
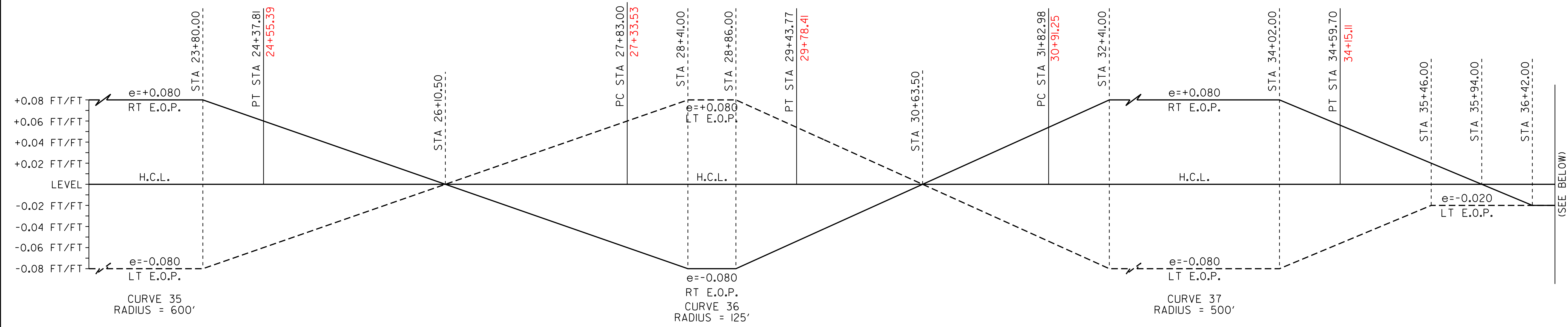
(SEE NEXT SHEET)

SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.

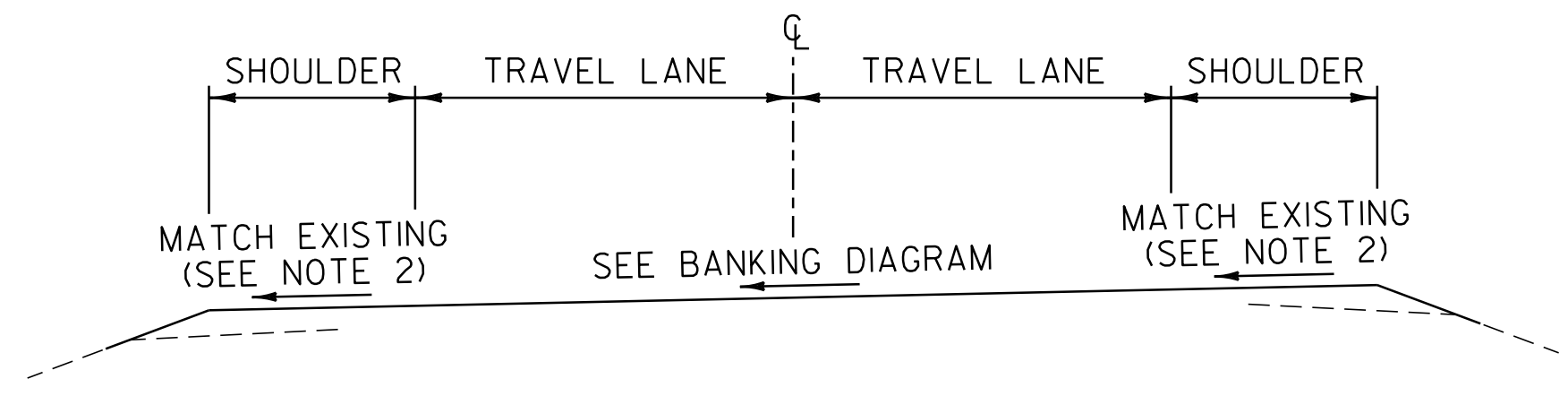


SUPERELEVATION BANKING DIAGRAMS SHEET #8	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	DESIGNED BY: M.J.M.
	IPARM FILE: p08cl60sbd8.i	CHECKED BY: D.W.E.
		SHEET 95 OF 114



SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



SUPERELEVATION BANKING DIAGRAMS SHEET #9	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:10
	PROJECT NUMBER: STP 2803(I)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 96 OF 114
	IPARM FILE: p08cl60sbd9.i	

TRAFFIC SIGN SUMMARY SHEET 3

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS			NEW & SALVAGED SIGNS				EXIST POST SALVAGE RETAIN	NO. OF POSTS	NEW SIGN POSTS														REMARKS	SIGN DETAIL							
		E A	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN	SALV TIS			FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM Ø (in)			TUBULAR STEEL Ø (in)				W-SHAPE STEEL			FR GAIN E	DETAIL ON DWG. NUMBER	STD. SHEET NUMBER				
											1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0	MOD	FOUND- ATION	3.0	3.5	4.0		5.0				FTG. SIZE		WEIGHT	POST SIZE
																														lb/ft	lb/ft		

SHS = FHWA STANDARD SIGNS BOOK

RIPTON: 27+22.0, LT		I	18	24	3.00					2				X		X											SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
27+72.0, LT		I	18	24	3.00					2				X		X											SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
28+26.0, LT		I	18	24	3.00					2				X		X											SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
30+46.0, RT		I	18	24	3.00					2				X		X											SIGN ID CODE WI-8L SIGN ID CODE WI-8R		SHS SHS
31+13.0, RT		I	18	24	3.00					2				X		X											SIGN ID CODE WI-8L SIGN ID CODE WI-8R		SHS SHS
31+82.0, RT		I	18	24	3.00					2				X		X											SIGN ID CODE WI-8L SIGN ID CODE WI-8R		SHS SHS
36+81.0, LT		I	30	30	6.25					1				X		X											SIGN ID CODE WI-5L		SHS
		I	18	18	2.25																						SIGN ID CODE WI3-I		SHS
38+00.0, RT		I	30	30	6.25					1				X		X											SIGN ID CODE WI-5L		SHS
		I	18	18	2.25																						SIGN ID CODE WI3-I		SHS

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

											LF	LF	LF	LF	LF	LF	EA.	LB	LB	LB	LB	LB	LB	EA.	EA.	LB			
TOTALS	SF	SF	EA.	SF							LF	LF 196				EA.	LB	LB				EA.	EA.	LB					
	53.00																												

TRAFFIC SIGN SUMMARY SHEET #3

PROJECT NAME: RIPTON - HANCOCK
PROJECT NUMBER: STP 2803(i)
FILE NAME: p08cl60.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.M.
IPARM FILE: p08cl60+ss03.i
PLOT DATE: 18-SEP-2013 17:10
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 99 OF 114

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS			NEW & SALVAGED SIGNS				EXIST POST RETAIN	NO. OF POSTS	NEW SIGN POSTS														REMARKS	SIGN DETAIL							
		E	A	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN			SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM Ø (in)			TUBULAR STEEL Ø (in)					W-SHAPE STEEL			FRAMING	DETAIL ON DWG. NUMBER	STD. SHEET NUMBER		
												1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0	MOD	3.0	3.5	4.0		5.0	FTG. SIZE					WEIGHT	POST SIZE
																											1.3	1.7					

SHS = FHWA STANDARD SIGNS BOOK








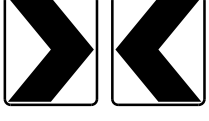
RIPTON: 47+68.0, RT		1	18	24	3.00						2				X		X														SIGN ID CODE WI-8L SIGN ID CODE WI-8R		SHS SHS
48+37.0, RT		1	36	36	9.00						2				X		X														SIGN ID CODE W3-5		SHS
50+12.0, LT		1	18	24	3.00						2				X		X														SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
50+66.0, LT		1	18	24	3.00						2				X		X														SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
51+27.0, LT		1	18	24	3.00						2				X		X														SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
51+75.0, LT		1	18	24	3.00						2				X		X														SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS
52+26.0, LT		1	18	24	3.00						2				X		X														SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

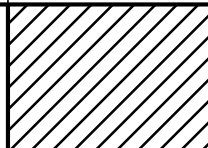
TOTALS	SF	SF	EA.	SF		LF	LF	EA.	LB	EA.	LB	EA.	EA.	LB
	45.00						196							

TRAFFIC SIGN SUMMARY SHEET #5	PROJECT NAME: RIPTON - HANCOCK
	PROJECT NUMBER: STP 2803(1)
	FILE NAME: p08cl60.dgn
	PLOT DATE: 18-SEP-2013 17:10
	PROJECT LEADER: D.E.G.
	DRAWN BY: C.A.K.
	DESIGNED BY: M.J.M.
	CHECKED BY: D.W.E.
	IPARM FILE: p08cl60+ss05.i
	SHEET 101 OF 114

TRAFFIC SIGN SUMMARY SHEET 6

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS			NEW & SALVAGED SIGNS				EXIST POST RETAIN	NO. OF POSTS	NEW SIGN POSTS															REMARKS	SIGN DETAIL				
		EA	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN	SALV TIS			FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM Ø (in)			TUBULAR STEEL Ø (in)				W-SHAPE STEEL			FRUIT ME	DETAIL ON DWG. NUMBER	STD. SHEET NUMBER		
											1.12	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	3.0	3.5	4.0	5.0	FTG. SIZE						WEIGHT	POST SIZE
																								lb/ft	lb/ft						
RIPTON: 54+00.0, LT							I			2				X		X											-	-			
							I																				-	-			
							I																				-	-			
54+50.0, RT		I	30	30	6.25					I																		SIGN ID CODE WI-IL	SHS		
		I	18	18	2.25																							SIGN ID CODE WI3-I	SHS		
56+00.0, LT		I	24	30	5.00					I			X		X													SIGN ID CODE R2-1	SHS		
56+00.0, RT		I	30	36	7.50					I			X		X													SIGN ID CODE R2-1	I13		
59+21.0, RT		I I	18 18	24 24	3.00 3.00					2			X		X													SIGN ID CODE WI-8R SIGN ID CODE WI-8L	SHS SHS		

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS	SF 27.00	SF	EA. 3	SF		LF 98	LF 98	EA.	LB	EA.	LB	EA.	EA.	LB
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TRAFFIC SIGN SUMMARY SHEET #6

PROJECT NAME: RIPTON - HANCOCK
PROJECT NUMBER: STP 2803(1)

FILE NAME: p08cl60.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.M.
IPARM FILE: p08cl60+ss06.i

PLOT DATE: 18-SEP-2013 17:10
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 102 OF 114

TRAFFIC SIGN SUMMARY SHEET 7

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS			NEW & SALVAGED SIGNS								NEW SIGN POSTS																REMARKS	SIGN DETAIL		
		E.A.	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN	SALV TIS	EXIST POST	NO. OF POSTS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL				REQUIRE SIGN NAME		DETAIL ON DWG. NUMBER	STD. SHEET NUMBER	
											1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUND-ATION	3.0	3.5	4.0	5.0	FTG. SIZE		WEIGHT					POST SIZE
																									lb/ft	lb/ft						
																				1.3	1.7	1.7	7.6	9.0	10.8	14.6						
																					SHTS = FHWA STANDARD SIGNS BOOK											
RIPTON: 59+59.0, RT		I	18	24	3.00					2				X		X												SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS		
60+04.0, RT		I	18	24	3.00					2				X		X												SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS		
60+23.0, RT		I	72	20	10.00					2				X		X												SIGN ID CODE DI-2L		E-123		
62+00.0, LT							I			I				X		X												-	-			
							I																					-	-			
							*																					-	-			
63+38.0, LT							I			I				X		X												SALVAGED SIGN TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-		
		I	30	30	6.25																							BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138		
63+74.0, LT		I	24	30	5.00					I				X		X												SIGN ID CODE VR-017		E-141		
64+81.0, RT		I	24	30	5.00					I				X		X												SIGN ID CODE R2-1		SHS		
											LF	LF	LF	LF	LF	LF	EA.	LB	LB	LB	LB	EA.	EA.	EA.	EA.	EA.	EA.					
											LF	140			EA.	LB	LB	LB	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.						
											LF	140			EA.	LB	LB	LB	EA.	EA.	EA.	EA.	EA.	EA.	EA.							
											LF	140			EA.	LB	LB	LB	EA.	EA.	EA.	EA.	EA.	EA.								

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS

SF 38.67 SF EA. 34 SF

TRAFFIC SIGN SUMMARY SHEET #7

PROJECT NAME: RIPTON - HANCOCK
PROJECT NUMBER: STP 2803(1)
FILE NAME: p08cl60.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.M.
IPARM FILE: p08cl60+ss07.i

PLOT DATE: 18-SEP-2013 17:11
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET 103 OF 114

TRAFFIC SIGN SUMMARY SHEET 8

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS						EXIST POST RETAI SALVAGE	NEW SIGN POSTS													REMARKS	SIGN DETAIL							
		E A	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN	SALV TIS	NO. OF POSTS		FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)					W-SHAPE STEEL		FRUIT SFRUIT NE	DETAIL ON DWG. NUMBER	STD. SHEET NUMBER			
											1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUND-ATION	3.0	3.5	4.0		5.0	24"				30"	WEIGHT	POST SIZE

SHS = FHWA STANDARD SIGNS BOOK

RIPTON: 67+00.0, LT		1	72	20	10.00					2				X		X														SIGN ID CODE DI-2R		E-123
	* 67+25 ~ 69+75 LT CHEVRONS REPLACED BY 6" WHITE DELINEATORS																															
67+25.0, LT		1	6 18 6	8 24 8	0.33 3.00 0.33					2				X		X													SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS	
67+65.0, LT		1	6 18 6	8 24 8	0.33 3.00 0.33					2				X		X													SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS	
68+25.0, LT		1	6 18 6	8 24 8	0.33 3.00 0.33					2				X		X													SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS	
68+50.0, LT		1	6 18 6	8 24 8	0.33 3.00 0.33					2				X		X													SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS	
68+75.0, LT		1	6 18 6	8 24 8	0.33 3.00 0.33					2				X		X													SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS	
69+50.0, LT		1	6 18 6	8 24 8	0.33 3.00 0.33					2				X		X													SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS	
69+75.0, LT		1	6 18 6	8 24 8	0.33 3.00 0.33					2				X		X													SIGN ID CODE WI-8R SIGN ID CODE WI-8L		SHS SHS	

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS	52.00	SF	EA.	SF																															

TRAFFIC SIGN SUMMARY SHEET #8

PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:11
PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
DESIGNED BY: M.J.M.	SHEET 104 OF 114
IPARM FILE: p08cl60+ss08.i	

TRAFFIC SIGN SUMMARY SHEET 9

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS			NEW & SALVAGED SIGNS					EXIST POST RETAIN	NO. OF POSTS	NEW SIGN POSTS												REQUIREMENTS	REMARKS	SIGN DETAIL						
		EA	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN	SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL				DETAIL ON DWG. NUMBER	STD. SHEET NUMBER					
									1.2			2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUND-ATION	3.0	3.5	4.0					5.0	FTG. SIZE		WEIGHT	POST SIZE
																													lb/ft	lb/ft		

SHS = FHWA STANDARD SIGNS BOOK

RIPTON: 73+69.0, RT		1	30	30	6.25						1				X		X												SIGN ID CODE W2-2L		SHS
		1	6	10	0.42																								SIGN ID CODE VD-700		E-138
76+42.0, LT		1	12	36	3.00						1				X		X												SIGN ID CODE OM-3L		SHS
76+42.0, RT		1	12	36	3.00						1				X		X												SIGN ID CODE OM-3R		SHS
76+65.0, LT		1	12	36	3.00						1				X		X												SIGN ID CODE OM-3R		SHS
76+75.0, RT		1	12	36	3.00						1				X		X												SIGN ID CODE OM-3L		SHS
77+62.0, LT	NATURAL TPKE										1				X		X												SALVAGED SIGN TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-
		1	30	30	6.25																								BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138
78+12.0, LT		1	24	30	5.00						1				X		X												SIGN ID CODE VR-017		E-141

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS	SF	SF	EA.	SF		LF	LF	EA.	LB	EA.	LB	EA.	EA.	LB
	30.34		1			98								

TRAFFIC SIGN SUMMARY SHEET #9	PROJECT NAME: RIPTON - HANCOCK
	PROJECT NUMBER: STP 2803(1)
	FILE NAME: p08cl60.dgn
	PLOT DATE: 18-SEP-2013 17:11
DESIGNED BY: M.J.M.	DRAWN BY: C.A.K.
CHECKED BY: D.W.E.	
IPARM FILE: p08cl60+ss09.i	SHEET 105 OF 114

TRAFFIC SIGN SUMMARY SHEET 13

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS														REMARKS	SIGN DETAIL				
		E	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN			SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM Ø (in)			TUBULAR STEEL Ø (in)					W-SHAPE STEEL			DETAIL ON DWG. NUMBER	STD. SHEET NUMBER
											1.12	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	3.0	3.5	4.0	5.0		24"	30"	WEIGHT		




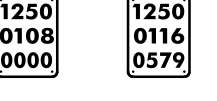



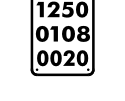


RIPTON: 211+12.0, LT							I		I					X	X												SALVAGED SIGN TO BE MOUNTED ON NEW POST. TOP MOUNTING BRACKETS REQUIRED TO INSTALL SALVAGED SIGNS ON NEW POST. COSTS ASSOCIATED WITH THIS BRACKET ARE INCIDENTAL TO THE SIGN POST.	-	-
		I	30	30	6.25																						BACK-TO-BACK SIGN ID CODE RI-1 SIGN ID CODE VD-700		SHS E-138
		I	24	30	5.00				I					X	X												SIGN ID CODE VR-017		E-141
		I	48	24	8.00				2					X	X												SIGN ID CODE WI-6R		SHS
		I	30	30	6.25				I					X	X												SIGN ID CODE WI-10L		SHS
		I	18	18	2.25																						SIGN ID CODE WI3-1		SHS
		I	30	30	6.25				I					X	X												SIGN ID CODE WII-2 SIGN SHALL BE FLUORESCENT YELLOW GREEN.		SHS
		I	24	18	3.00																						SIGN ID CODE WI6-4 SIGN SHALL BE FLUORESCENT YELLOW GREEN.		SHS
		I	24	12	2.00				I					X	X												SIGN ID CODE M3-2		E-136B
		I	30	15	3.13																						SIGN ID CODE VM-064	113	
		I	30	24	5.00																						SIGN ID CODE MI-4		E-136B
		I	6	10	0.42																						SIGN ID CODE VD-700		E-138

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

TOTALS	SF	SF	EA.	SF		LF	LF	EA.	LB	EA.	EA.	LB
	47.97		I			98						

TRAFFIC SIGN SUMMARY SHEET #13	PROJECT NAME: RIPTON - HANCOCK	PLOT DATE: 18-SEP-2013 17:11
	PROJECT NUMBER: STP 2803(1)	DRAWN BY: C.A.K.
	FILE NAME: p08cl60.dgn	CHECKED BY: D.W.E.
	DESIGNED BY: M.J.M.	SHEET 109 OF 114
	IPARM FILE: p08cl60+ssi3.i	

TRAFFIC SIGN SUMMARY SHEET 15

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS						EXIST POST SALVAGED RETAIN	NO. OF POSTS	NEW SIGN POSTS																REMARKS SHS = FHWA STANDARD SIGNS BOOK	SIGN DETAIL			
		E A	WIDTH (in)	HEIGHT (in)	"A"	"B"	SALV SIGN	SALV TIS	FLANGED CHANNEL			SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL		FRUIT NAME	DETAIL ON DWG. NUMBER	STD. SHEET NUMBER						
									1.2			2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUNDA-TION	3.0	3.5	4.0				5.0		FTG. SIZE		WEIGHT	POST SIZE
																													lb/ft	lb/ft		
RIPTON: 286+04.0, LT		I	30	30	6.25						1				X	X												SIGN ID CODE VW-000		E-153		
		I	18	18	2.25																								113			
HANCOCK: 0+00.0, RT		I	72	10	5.00						2				X	X													113			
		I I	6 6	10 10	0.42 0.42																							SIGN ID CODE VD-700 SIGN ID CODE VD-700		E-138 E-138		
9+19.0, RT		I	54	36	13.50						2				X	X													113			
10+51.0, LT		I	30	30	6.25						1				X	X												SIGN ID CODE VW-000		E-153		
		I	24	18	3.00																							SIGN ID CODE W7-3a		SHS		
																													-	-		
17+70.0, RT		I	72	12	6.00						2				X	X												SIGN ID CODE DI-1		E-123		
19+90.0, RT		I	48	24	8.00						2				X	X												SIGN ID CODE WI-6L		SHS		

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE VTRANS "SIGN POST DESIGN GUIDELINE."

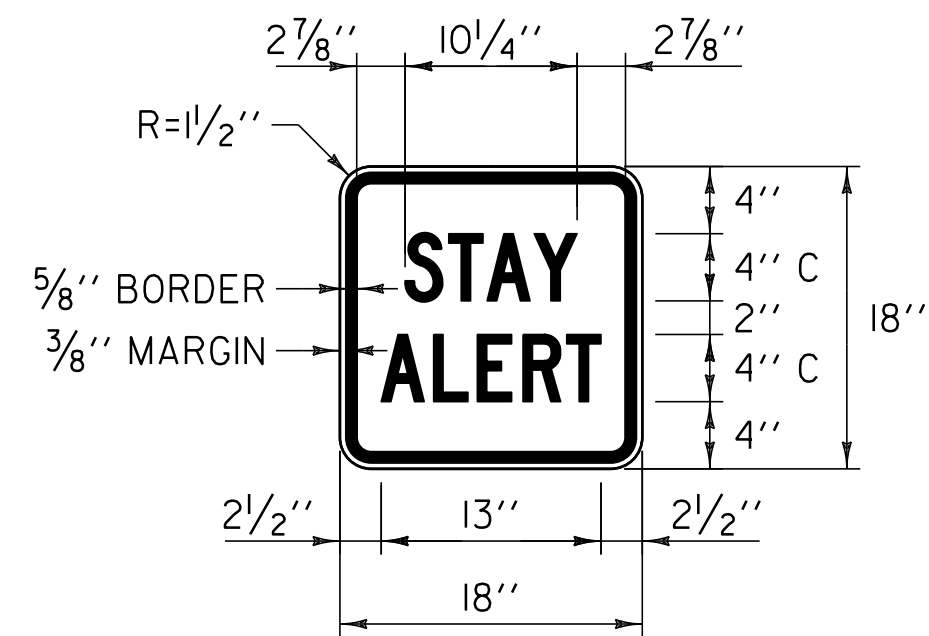
TOTALS	SF	SF	EA.	SF		LF	LF	LF	EA.	LB	EA.	EA.	LB
	51.09		1			140	140						

TRAFFIC SIGN SUMMARY SHEET #15

PROJECT NAME: RIPTON - HANCOCK
PROJECT NUMBER: STP 2803(1)

FILE NAME: p08cl60.dgn
PROJECT LEADER: D.E.G.
DESIGNED BY: M.J.M.
IPARM FILE: p08cl60+ssl5.i

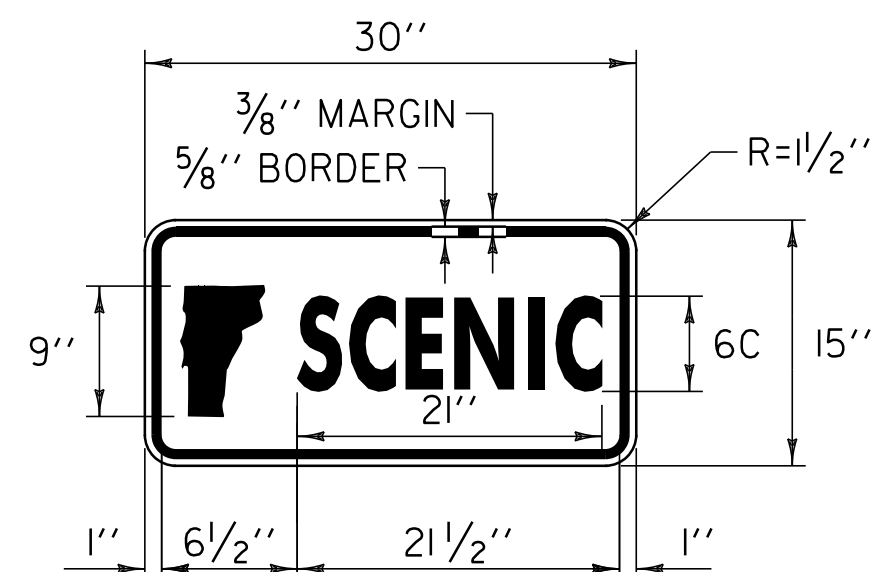
PLOT DATE: 18-SEP-2013 17:11
DRAWN BY: C.A.K.
CHECKED BY: D.W.E.
SHEET III OF 114



COLOR: BLACK BORDER & LEGEND
YELLOW BACKGROUND (RETROREFLECTIVE)

MATERIAL: PER VAOT STANDARD E-155

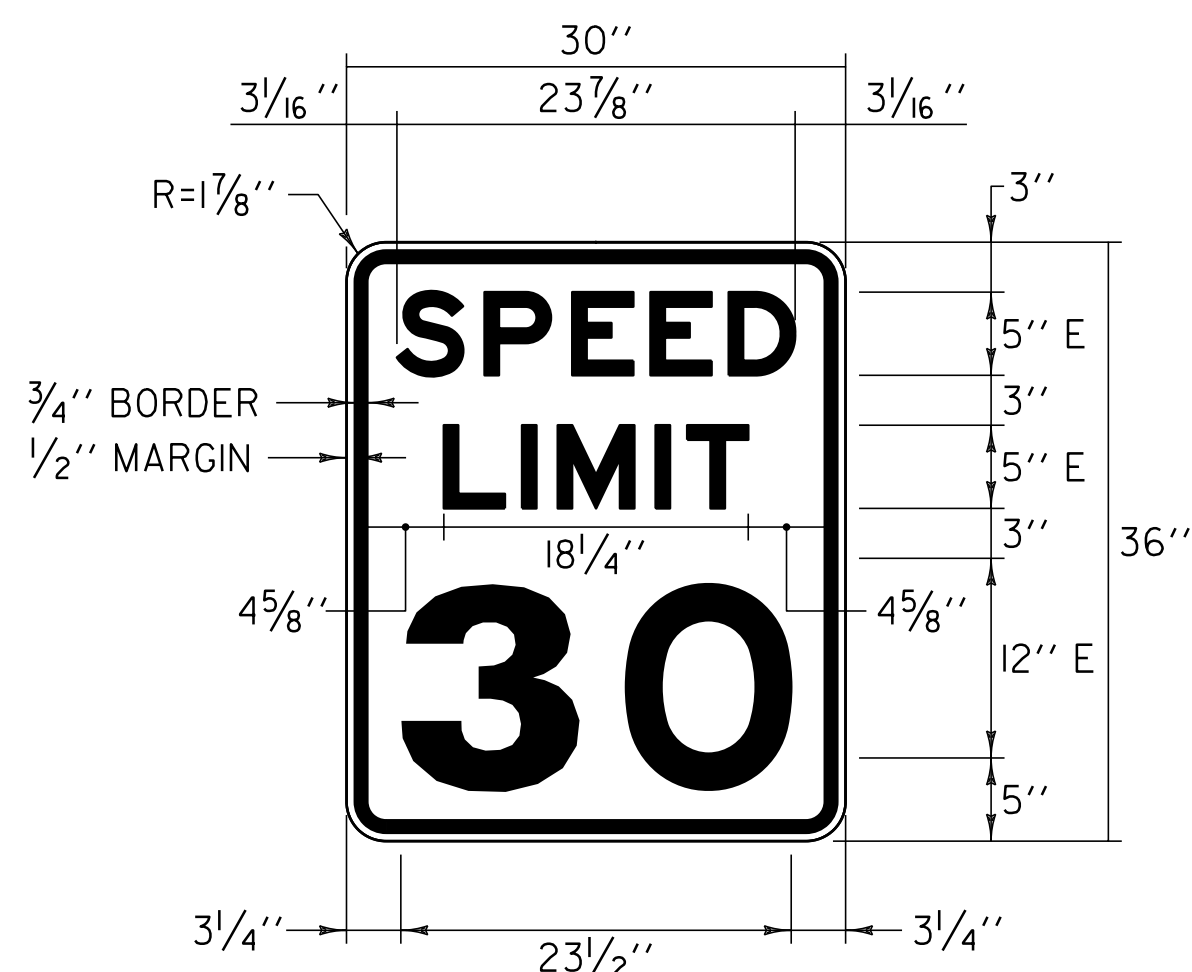
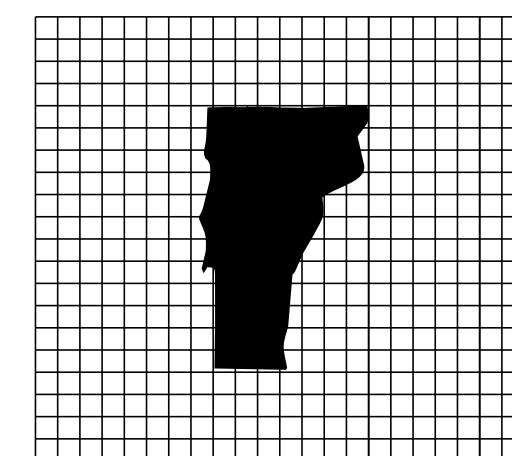
LOCATION: VT. ROUTE 125
RIPTON:
STA 196+27.0, LT
STA 258+76.0, RT
STA 286+04.0, LT



COLOR: GREEN BORDER & LEGEND (REFLECTIVE)
WHITE BACKGROUND (REFLECTIVE)

MATERIAL: PER VAOT STANDARD E-136B

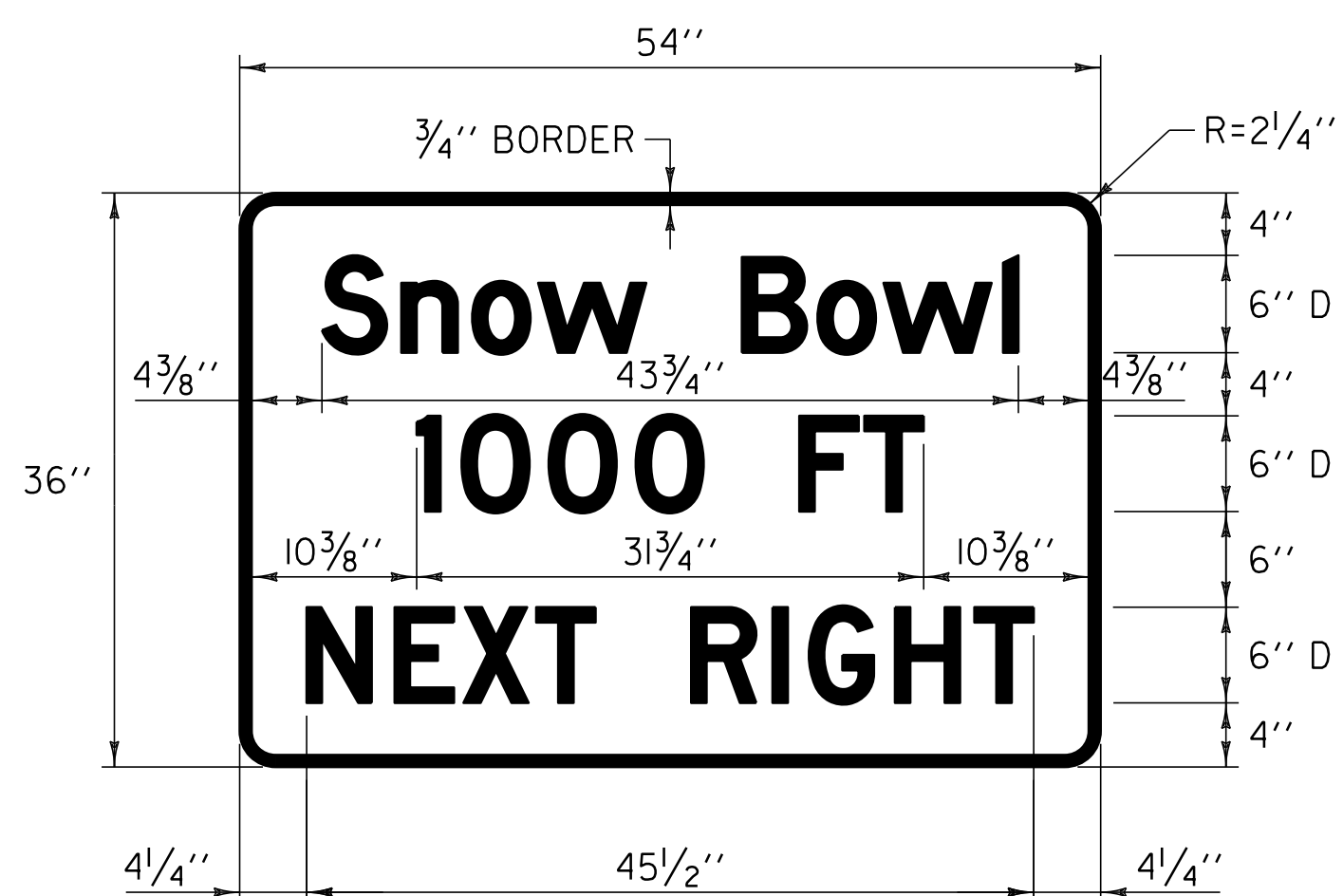
LOCATION: VT. ROUTE 125
RIPTON:
STA 219+22.0, RT
STA 219+29.0, RT



COLOR: BLACK BORDER & TEXT
WHITE BACKGROUND (RETROREFLECTIVE)

MATERIAL: PER VAOT STANDARD E-142

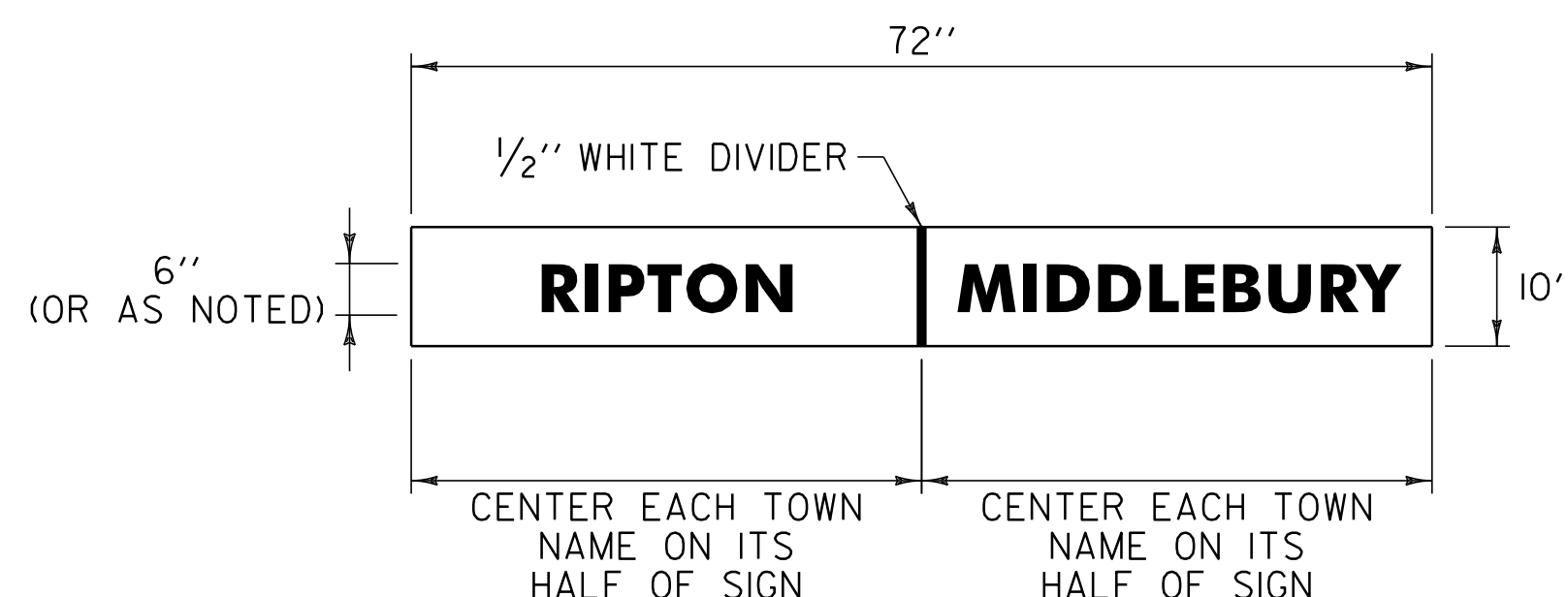
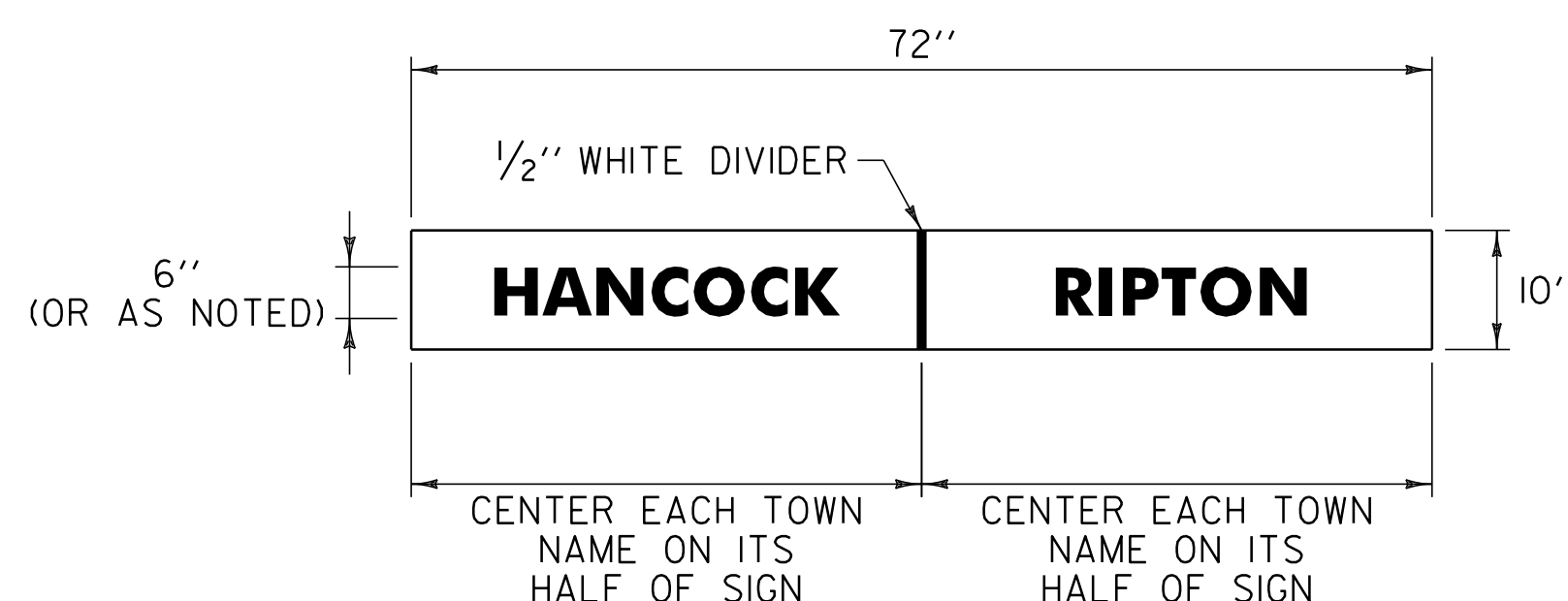
LOCATIONS: VT. ROUTE 125
RIPTON:
STA 56+00.0, RT



COLOR: WHITE BORDER & LEGEND
GREEN BACKGROUND (RETROREFLECTIVE)

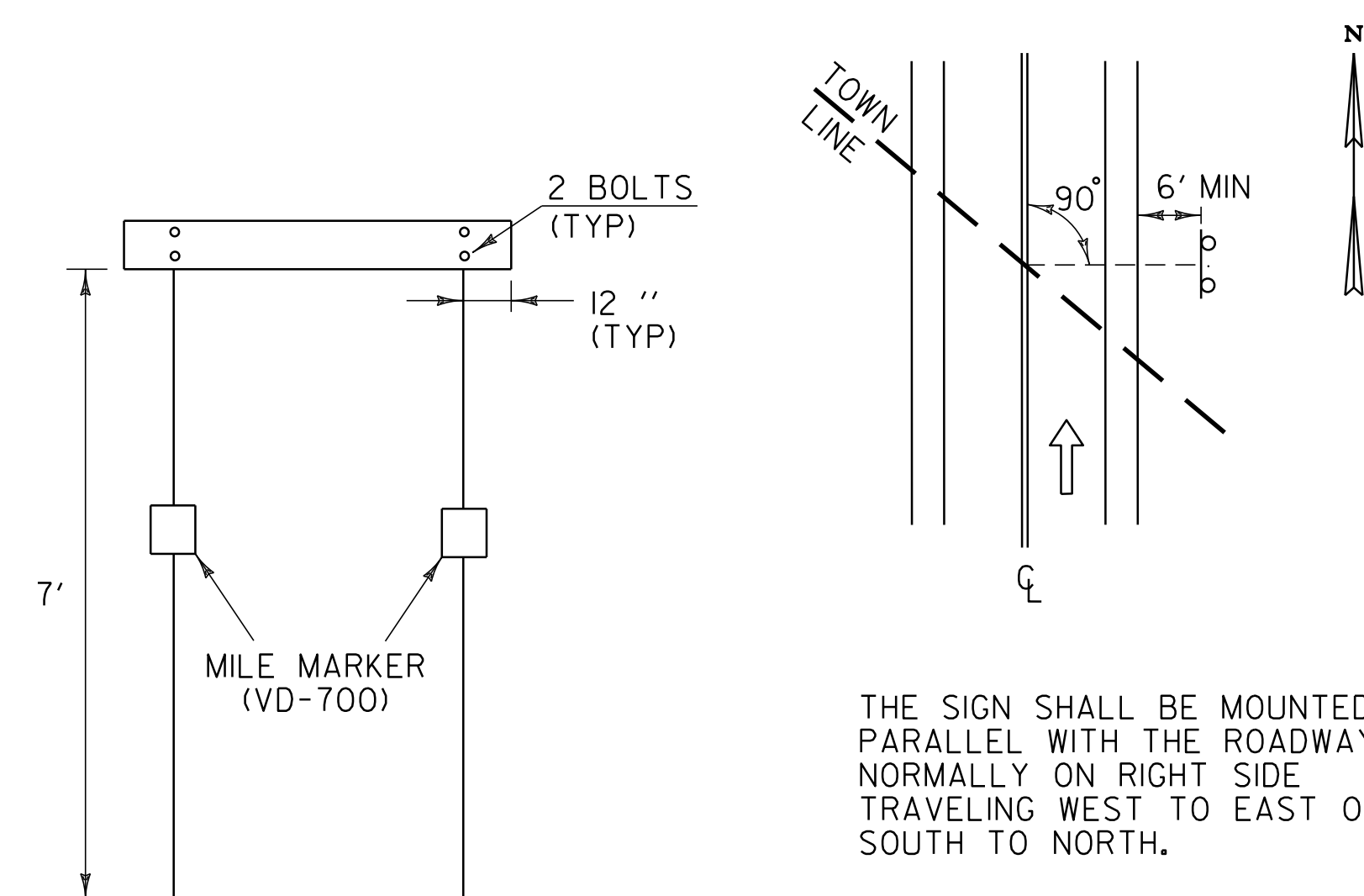
MATERIAL: PER VAOT STANDARD E-131

LOCATION: VT. ROUTE 125
HANCOCK:
STA 9+19.0, RT



GENERAL NOTES:

1. THE SIGN BASE MATERIAL SHALL BE HIGH DENSITY OVERLAID PLYWOOD 5/8 INCH THICK OR ALUMINUM SHEETING 0.125" MINIMUM THICKNESS. THE RETROREFLECTIVE MATERIAL SHALL BE SHEETING APPLIED TO THE ENTIRE BACKGROUND OF THE SIGN. THE LEGEND SHALL BE CUT-OUT RETROREFLECTIVE LEGEND. ALL RETROREFLECTIVE MATERIAL SHALL BE EQUAL TO OR EXCEEDING THE "THE AMERICAN SOCIETY OF TESTING AND MATERIALS" (ASTM) TYPE III.
2. SIGNS SHALL HAVE A RETROREFLECTIVE WHITE LEGEND ON A RETROREFLECTIVE GREEN BACKGROUND.
3. LEGEND SHALL CONFORM WITH THE STANDARD ALPHABETS FOR HIGHWAY SIGNS APPROVED BY THE NATIONAL JOINT COMMITTEE ON UNIFORM TRAFFIC CONTROL DEVICES.
4. ALL SIGNS SHALL BE IN ACCORDANCE WITH "THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES: (MUTCD) AND THE "STANDARD HIGHWAY SIGNS" BOOK (SHS) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION (FHWA).



THE SIGN SHALL BE MOUNTED PARALLEL WITH THE ROADWAY. NORMALLY ON RIGHT SIDE TRAVELING WEST TO EAST OR SOUTH TO NORTH.

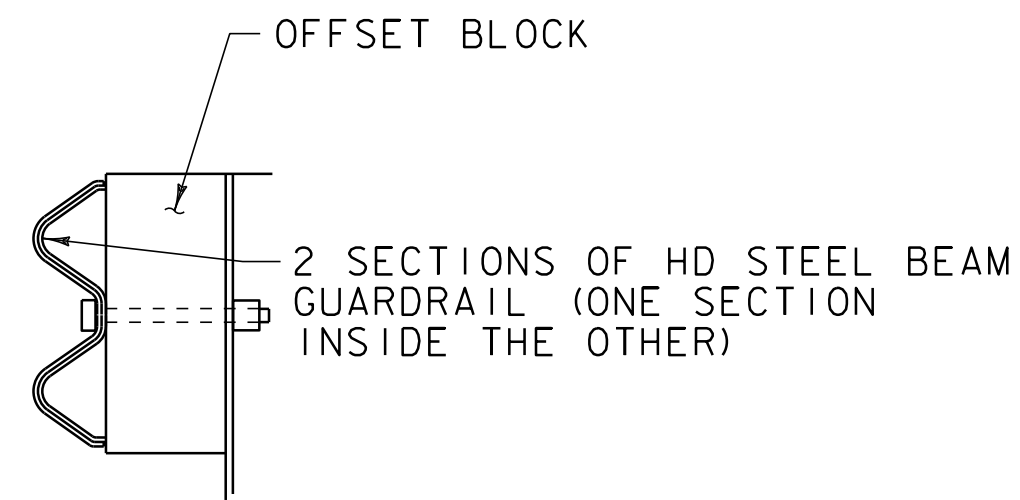
WHEN CROSSING COUNTY BOUNDARIES AN ADDITIONAL COUNTY LINE SIGN SHALL BE MOUNTED BELOW THE TOWN LINE SIGN MAINTAINING A SEVEN FOOT CLEARANCE.

NOTES:

1. WORD TO WORD SPACING FOR MULTIPLE TEXT TOWN NAMES IS ADJUSTABLE TO ACCOMMODATE DIMENSION REQUIREMENTS.
2. DIMENSIONS SHOWN ARE APPROXIMATE AND MAY BE ADJUSTED SLIGHTLY TO ACCOMMODATE VARIOUS MANUFACTURER'S LETTER WIDTHS.
3. ALL DIMENSIONS ARE IN INCHES EXCEPT WHERE NOTED.

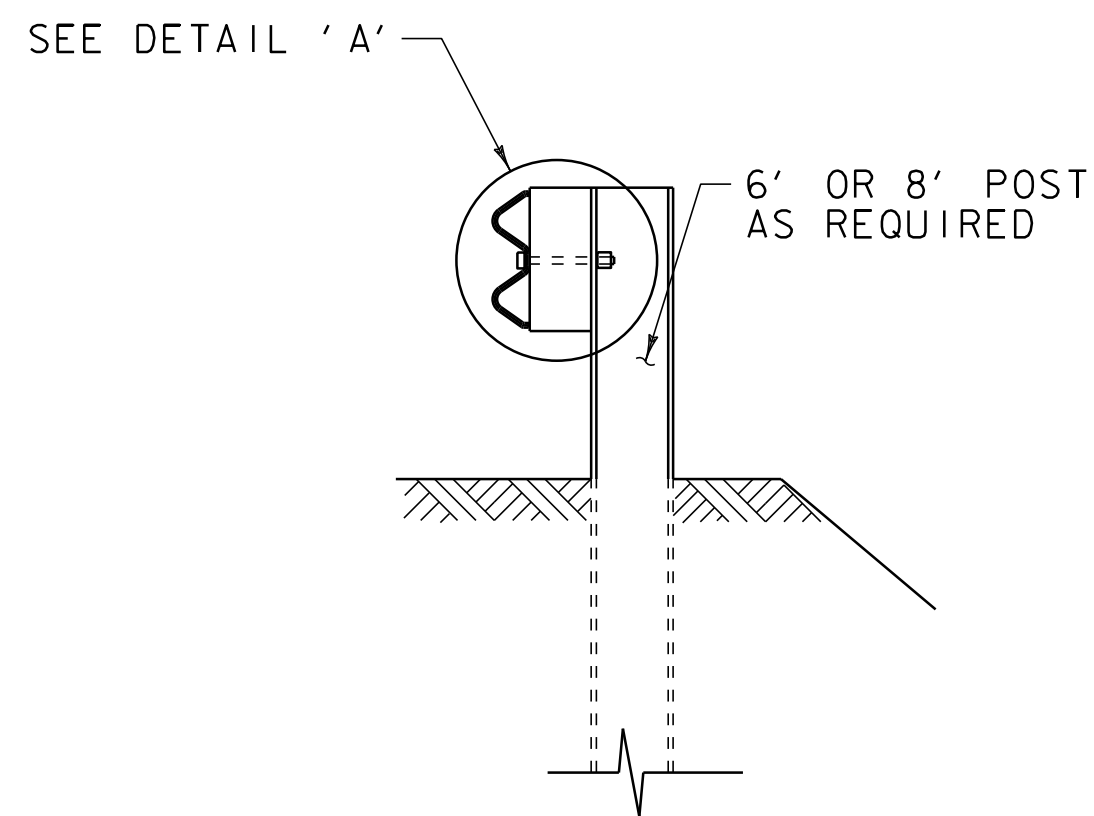
NOT TO SCALE

TRAFFIC SIGN DETAILS SHEET	PROJECT NAME: RIPTON - HANCOCK	
	PROJECT NUMBER: STP 2803(1)	
	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:11
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.	
IPARM FILE: p08cl60+tsd.i	SHEET 113 OF 114	

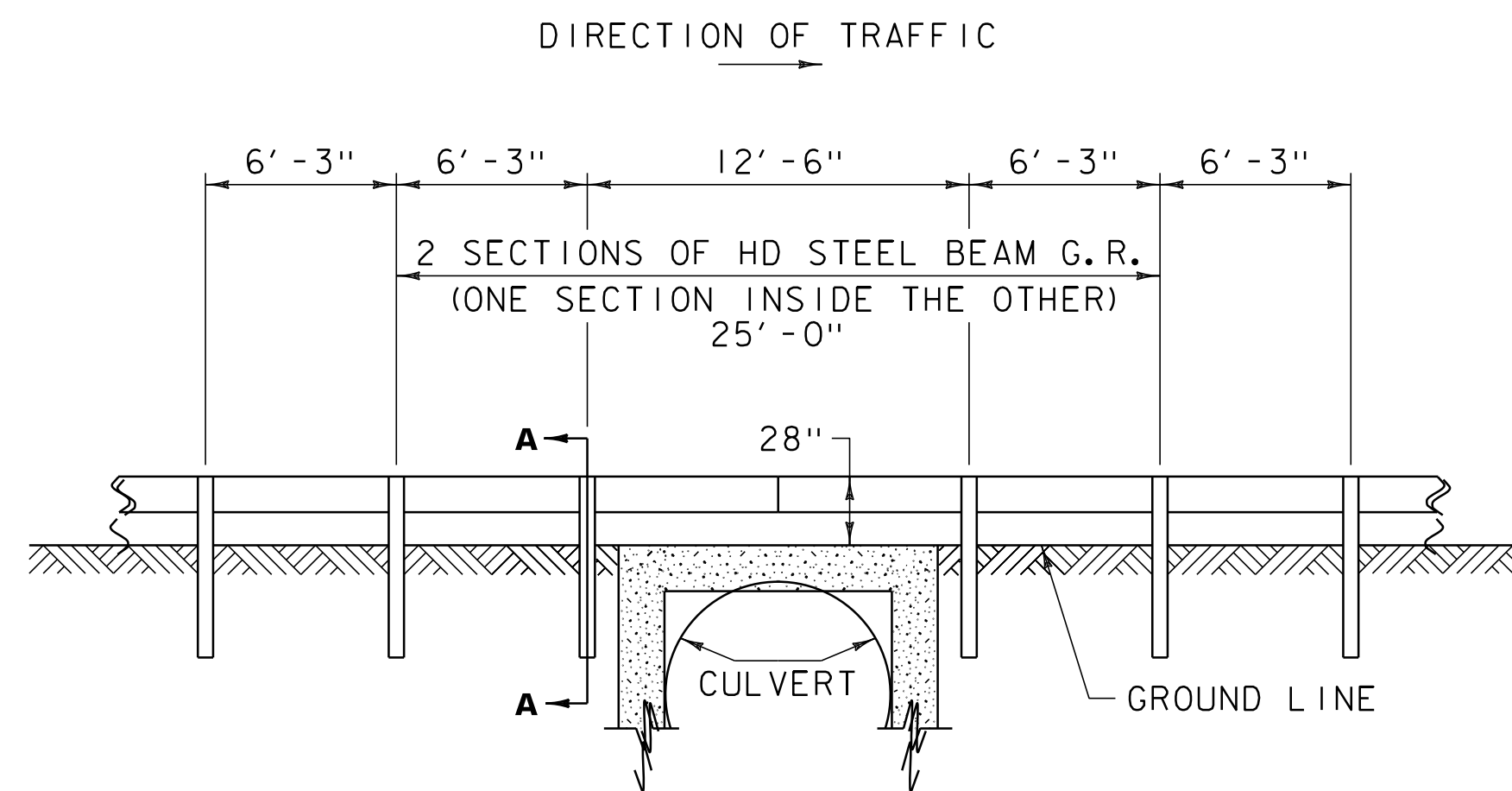


DETAIL A

1. SEE STANDARD G-1 FOR STEEL BEAM GUARDRAIL DETAILS.
2. ITEM 900.640 SPECIAL PROVISION (HD STEEL BEAM GUARDRAIL, WEATHERING/NESTED) OR ITEM 900.640 SPECIAL PROVISION (HD STEEL BEAM GUARDRAIL, WEATHERING/NESTED W/8 FEET POSTS)
3. THIS DETAIL TO BE USED AS INDICATED ON THE ITEM DETAIL SUMMARY SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.

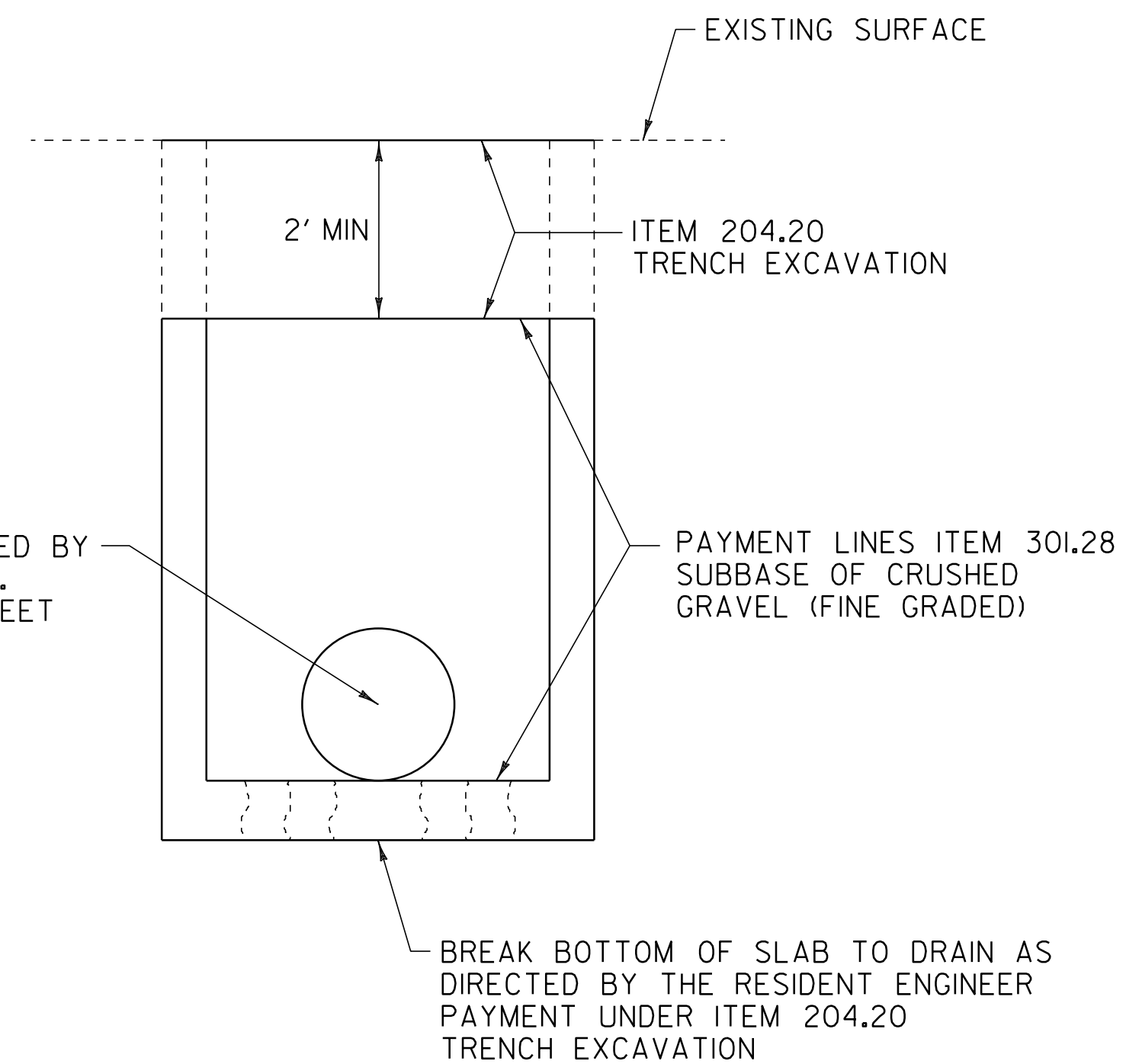


SECTION A-A

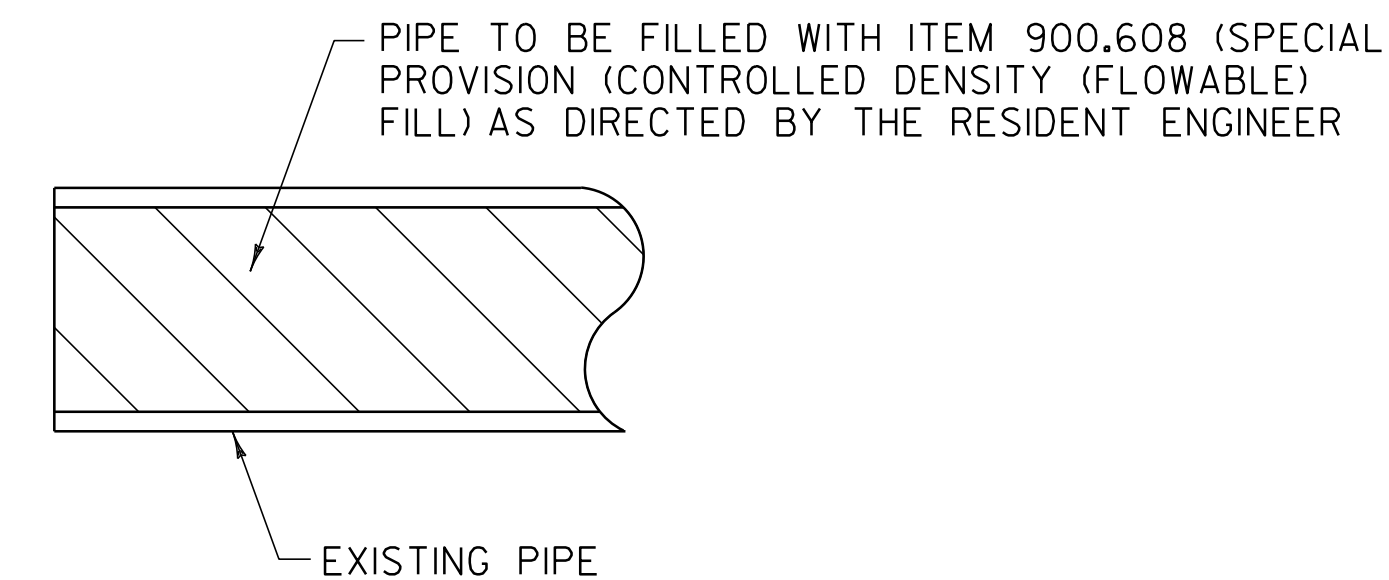


STEEL BEAM GUARDRAIL AT SMALL CULVERT DETAIL

VT. ROUTE 125 BR 13A RIPTON STA 16+85.5 - STA 17+10.5 RT
 VT. ROUTE 125 48" CMP RIPTON STA 166+87.5 - STA 167+12.5 LT
 VT. ROUTE 125 48" CMP RIPTON STA 166+87.5 - STA 167+12.5 RT
 VT. ROUTE 125 BR 16 RIPTON STA 192+27.5 - STA 192+52.5 LT
 VT. ROUTE 125 BR 16 RIPTON STA 192+27.5 - STA 192+52.5 RT



ABANDONMENT OF EXISTING DRAINAGE STRUCTURE



ABANDONING PIPE

NOT TO SCALE

MISCELLANEOUS DETAILS SHEET	PROJECT NAME: RIPTON - HANCOCK	
	PROJECT NUMBER: STP 2803(1)	
	FILE NAME: p08cl60.dgn	PLOT DATE: 18-SEP-2013 17:11
	PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.M.	CHECKED BY: D.W.E.	
IPARM FILE: p08cl60mdt.i	SHEET 114 OF 114	