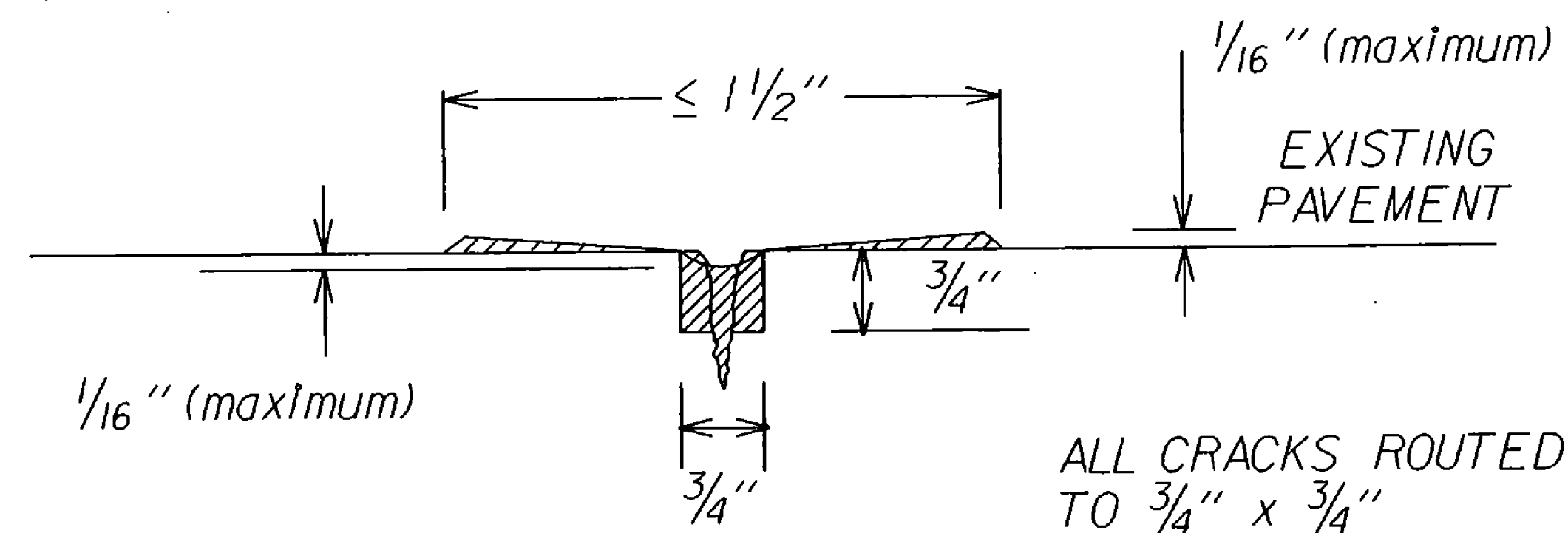
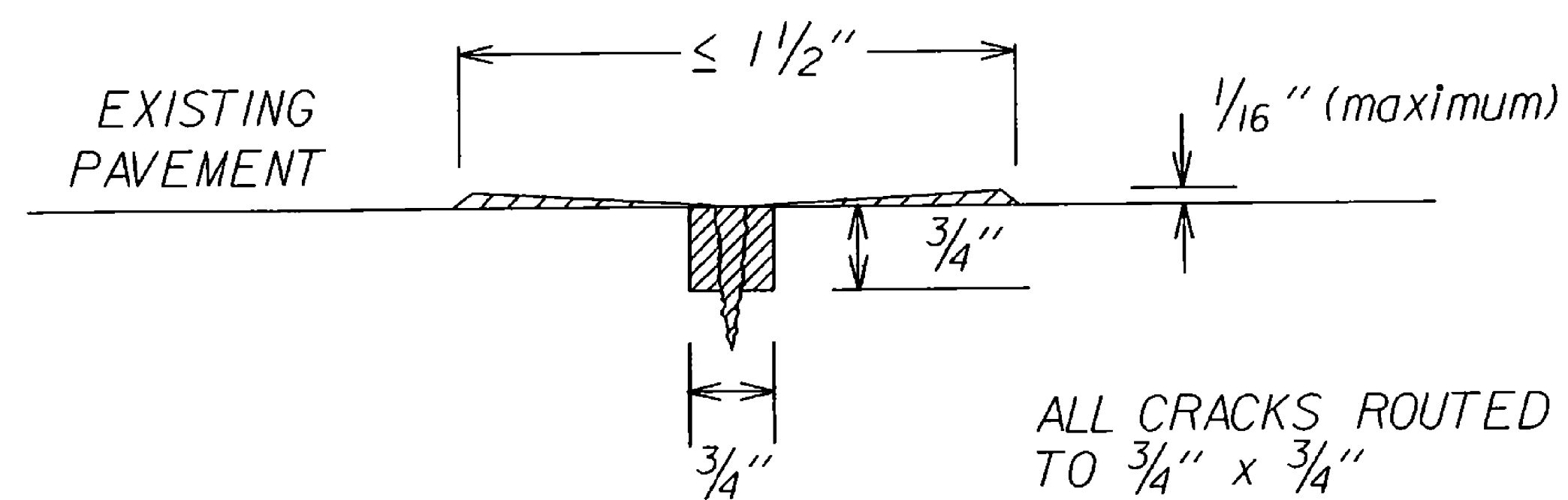


MAXIMUM FILL DEPTH



MINIMUM FILL DEPTH



TARGET FLUSH FILL

CONTRACT PLANS

THESE PLANS DO NOT REFLECT CHANGES MADE ON THE PROJECT.

NOTES

1. A STRIKE OFF FLUSH FILL TECHNIQUE WILL BE USED FOR MATERIAL APPLICATION. STRIKE OFF MAY BE ACCOMPLISHED WITH A SHOE.
2. ALL CRACKS WILL BE ROUTED TO $\frac{3}{4}''$ BY $\frac{3}{4}''$ PRIOR TO SEALING. ROUTED CRACKS SHALL BE SEALED WITHIN THE SAME WORKDAY.
3. CRACKS THAT ARE TO BE ROUTED AND SEALED SHOULD HAVE WIDTHS NO LESS THAN 0.10" AND NO GREATER THAN $\frac{3}{4}''$. ALL OTHERS TO BE LEFT AS IS UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
4. LIMITS OF WORK SHOWN ON PLANS, BEGIN AND END M.M., MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER TO ACCOUNT FOR CONDITIONS IN THE FIELD.
5. AMBIENT TEMPERATURE RANGE: 40°F TO 104°F.
6. PAVEMENT TEMPERATURE RANGE: 50°F TO 140°F
7. RELATIVE HUMIDITY SHOULD BE LESS THAN 80 PERCENT.
8. PAVEMENT SURFACE AND CRACKS MUST BE CLEAN AND DRY PRIOR TO APPLICATION.
9. TEMPERATURE OF SEALANT SHALL BE WITHIN THE MANUFACTURER'S SPECIFIED TEMPERATURE RANGE AT APPLICATION.
10. BACKFLUSHING OF HOSE AND APPLICATOR WAND IS NECESSARY ANYTIME THE APPLICATION OF SEALANT HAS BEEN DELAYED FOR A PERIOD GREATER THAN 15 MINUTES.
11. THE DISTANCE BETWEEN APPLICATOR AND SOUEEGEE SHOULD BE LESS THAN 3 FEET, BUT IN NO CASE SHOULD IT BE GREATER THAN 6 FEET.
12. THE TIME DELAY BETWEEN THE HOT AIR LANCE TREATMENT AND THE APPLICATION OF THE SEALANT SHOULD BE LESS THAN TWO MINUTES, BUT IN NO CASE GREATER THAN FIVE MINUTES.
13. THE ACCEPTABLE SEALANT THICKNESS SHALL BE IN THE RANGE OF + $\frac{1}{16}''$ ABOVE SURFACE TO - $\frac{1}{16}''$ BELOW SURFACE. THICKNESSES ABOVE THE SURFACE GREATER THAN $\frac{1}{16}''$ AND RECESSES GREATER THAN $\frac{1}{16}''$ BELOW THE SURFACE SHALL REFER TO SECTION 417 - BITUMINOUS CRACK SEALING, SUBSECTION 417.06 PLACING OF SEALER, OF THE STANDARD SPECIFICATIONS.
14. THE MANUFACTURER'S RECOMMENDATIONS ON CURING OF MATERIAL WILL BE SUPPLIED IN ADVANCE OF ACTIVITIES. THE CURE TIMES MAY BE SHORTENED OR EXTENDED TO MEET CONDITION IN THE FIELD AS DIRECTED BY THE ENGINEER.
15. ANY MATERIAL HEATED ABOVE THE MANUFACTURERS RECOMMENDED MAXIMUM WILL NOT BE USED ON PROJECT.
16. ALL CRACKS AT THE COLCHESTER AND ROCKINGHAM - SPRINGFIELD SITES SHALL NOT BE ROUTED. WORK AT THESE LOCATIONS SHALL BE DONE IN ACCORDANCE WITH SPECIAL PROVISION (BITUMINOUS CRACK SEALING, BLOW AND GO METHOD).

CRACK SEALING TYPICAL

PROJECT NAME:	STATEWIDE
PROJECT NUMBER:	STP CRAK(26)
FILE NAME:	p07ki78.dgn
PROJECT LEADER:	DOMEY
DESIGNED BY:	LOCKE
p07ki78+yp.l	
PLOT DATE:	20-MAR-2008
DRAWN BY:	LOCKE
CHECKED BY:	PAVT MGMT
SHEET	2 OF 11

VT 109 CAMBRIDGE - BELVIDERE

189 SB COLCHESTER - GEORGIA

189 NB & SB COLCHESTER
(SEE NOTE 16 ON SHEET 2)

VT 62 BERLIN - BARRE CITY

US 7 MIDDLEBURY - NEW HAVEN

191 SB BRADFORD - NEWBURY

191 NB FAIRLEE - NEWBURY

US 7 NB WALLINGFORD - RUTLAND TOWN

VT 103 LUDLOW - MT. HOLLY

US 7 SB WALLINGFORD - RUTLAND TOWN

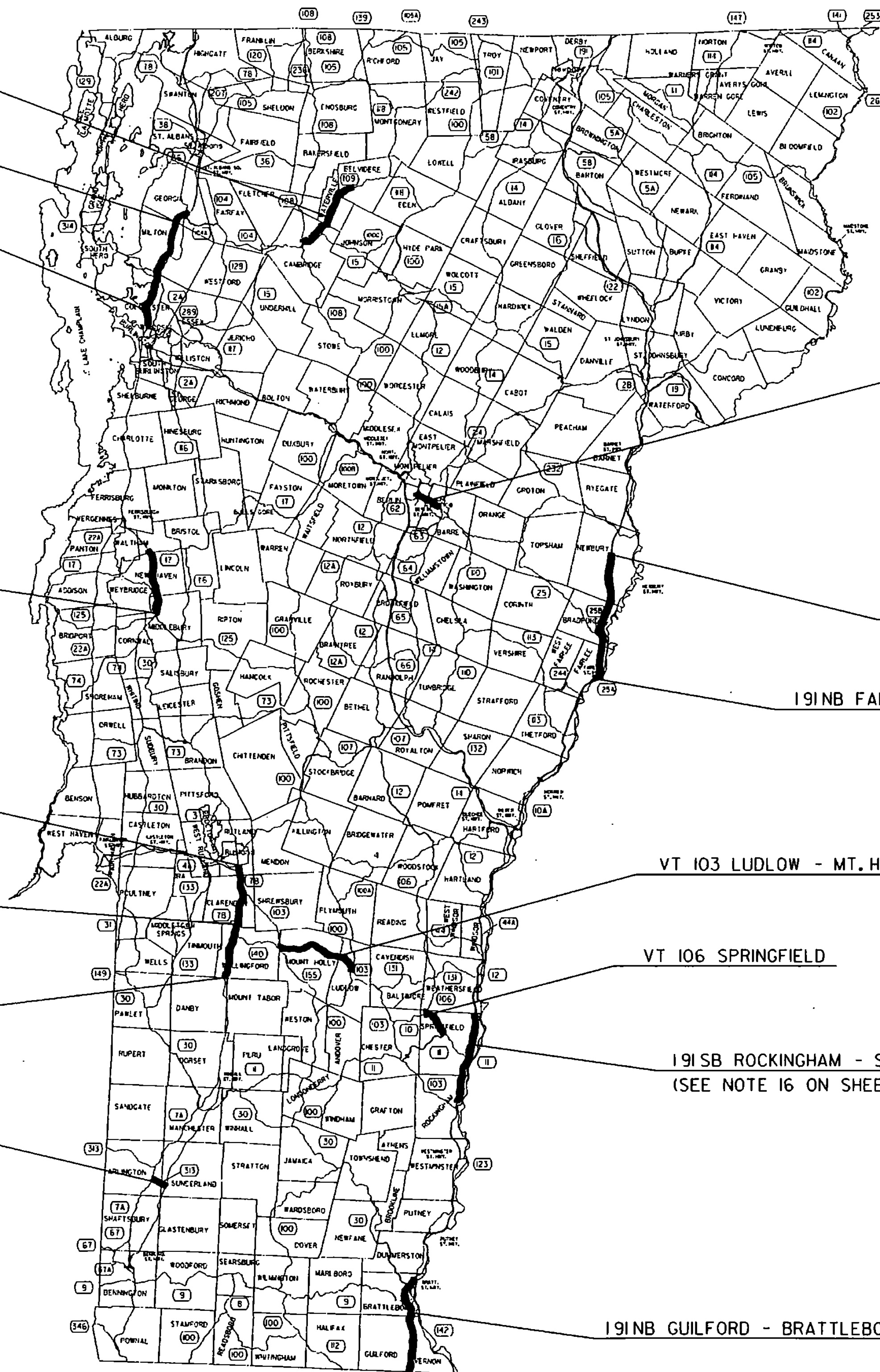
VT 106 SPRINGFIELD

US 7 WALLINGFORD

191 SB ROCKINGHAM - SPRINGFIELD
(SEE NOTE 16 ON SHEET 2)

VT 313 ARLINGTON - SUNDERLAND

191 NB GUILFORD - BRATTLEBORO



**LOCATION
LAYOUT**

PROJECT NAME: STATEWIDE
PROJECT NUMBER: STP CRAK(26)

FILE NAME: p07k178.dgn
PROJECT LEADER: DOMEY
DESIGNED BY: LOCKE
p07k17801.I

PLOT DATE: 07-MAR-2008
DRAWN BY: LOCKE
CHECKED BY: PAVT MGMT
SHEET 4 OF 11

CRACK SEALING TO BE PERFORMED AT THE FOLLOWING LOCATIONS.
 LOCATION MAY BE ADJUSTED BY THE RESIDENT ENGINEER TO
 ACCOUNT FOR FIELD CONDITIONS.

ROAD	TOWN BEGINNING	MM BEGINNING	TOWN END	MM ENDING	LENGTH	LANE LENGTH	DISTRICT	ADT
189 (SB)	COLCHESTER	98.000	GEORGIA	106.900	8.900	17.800	8	10,700
191(NB)	GUILFORD	0.000	BRATTLEBORO	11.916	11.916	23.832	2	13,700
191(NB)	FAIRLEE	92.480	NEWBURY	105.030	12.550	25.100	4, 7	4,200
191(SB)	BRADFORD	98.400	NEWBURY	105.000	6.600	13.200	7	3,300
189 (NB)	COLCHESTER	91.875	COLCHESTER	98.005	6.130	12.260	5	16,700
189 (SB)	COLCHESTER	91.875	COLCHESTER	98.005	6.130	12.260	5	16,700
191(SB)	ROCKINGHAM	35.554	SPRINGFIELD	46.476	10.922	21.844	2	6,800
US 7	WALLINGFORD	0.000	WALLINGFORD	6.225	6.225	12.450	3	7,200
US 7	MIDDLEBURY	5.713	NEW HAVEN	5.987	7.754	15.508	5	10,600
US 7 (SB)	WALLINGFORD	6.520	RUTLAND TOWN	0.677	6.907	13.814	3	11,400
US 7 (NB)	WALLINGFORD	6.229	RUTLAND TOWN	1.177	7.698	15.396	3	22,800
VT 62	BERLIN	1.382	BARRE CITY	1.585	3.137	6.274	6	19,300
VT 62 (WB)	BERLIN	1.382	BERLIN	1.586	0.204	0.408	6	19,300
VT 62 (WB)	BERLIN	2.526	BARRE CITY	1.585	1.993	3.986	6	19,300
VT 103	LUDLOW	2.862	MT. HOLLY	0.000	10.603	21.206	3	8,500
VT 106	SPRINGFIELD	0.058	SPRINGFIELD	3.283	3.225	6.450	2	10,400
VT 109	CAMBRIDGE	0.000	BELVIDERE	2.910	10.952	21.904	8	1,400
VT 313	ARLINGTON	6.635	SUNDERLAND	0.717	1.990	3.980	1	3,200

TOTAL (MILES)

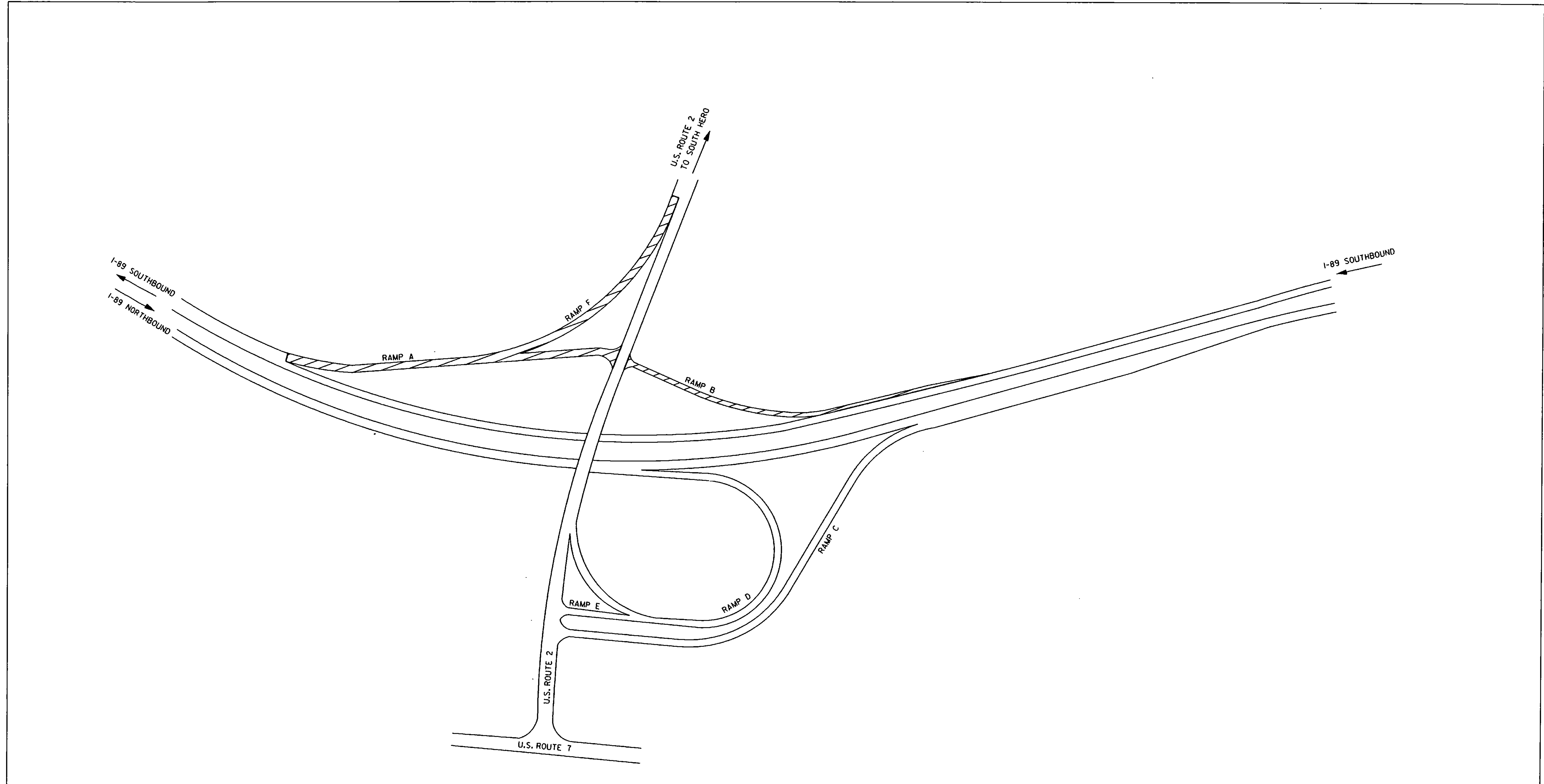
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**CRACK SEALING
LOCATION
DETAIL**

PROJECT NAME: STATEWIDE
 PROJECT NUMBER: STP CRAK(26)

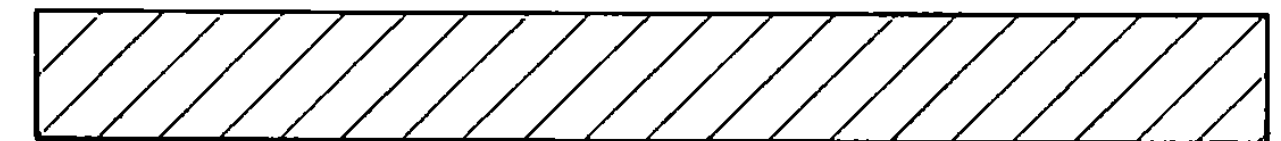
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 PROJECT LEADER: DOMEY
 DESIGNED BY: LOCKE
 p07k178det.i

PLOT DATE: 07-MAR-2008
 DRAWN BY: LOCKE
 CHECKED BY: PAVT MGMT
 SHEET 5 OF 11



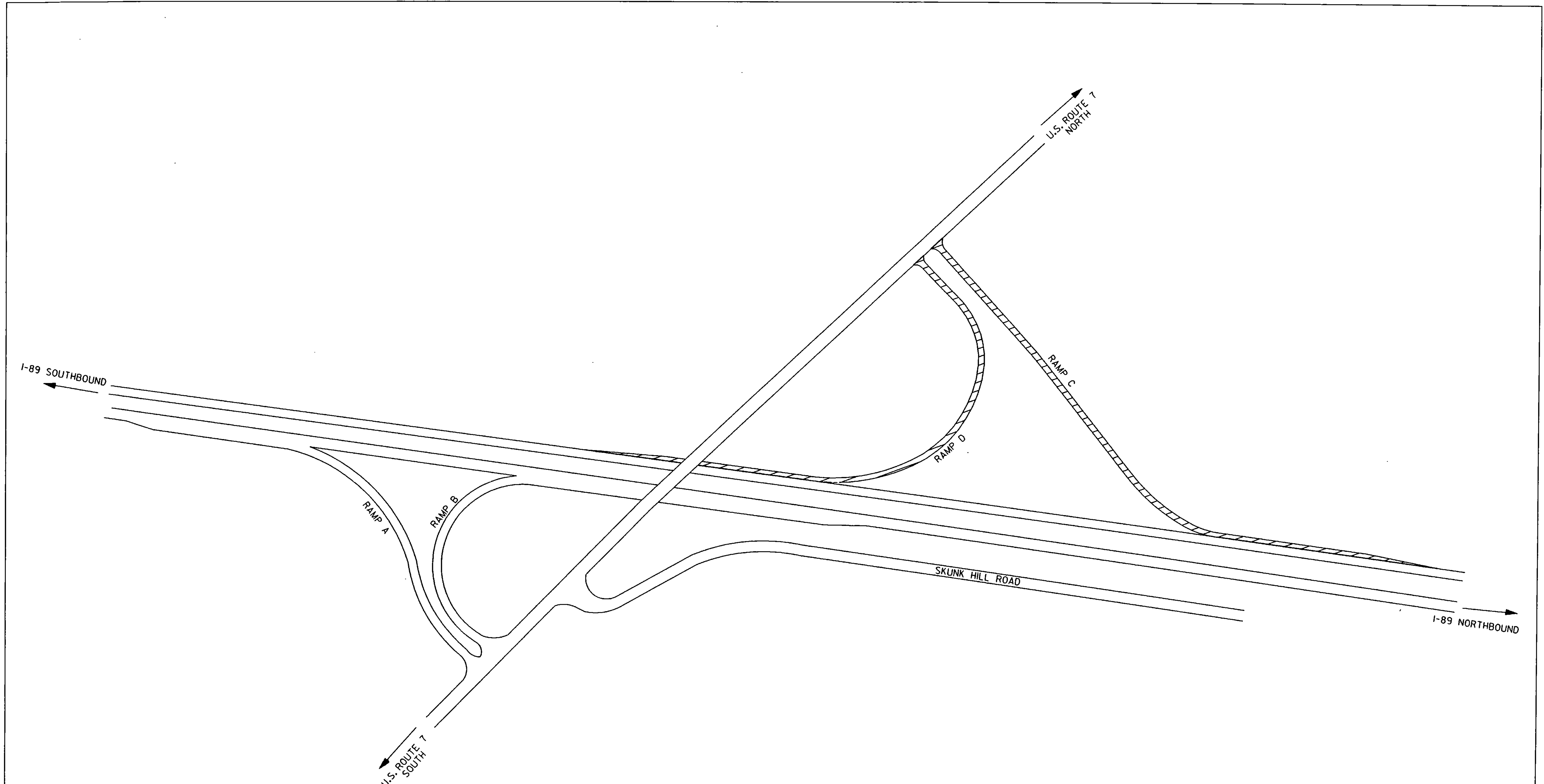
COLCHESTER INTERCHANGE # 17
I-89

RAMPS REQUIRING CRACK SEALING



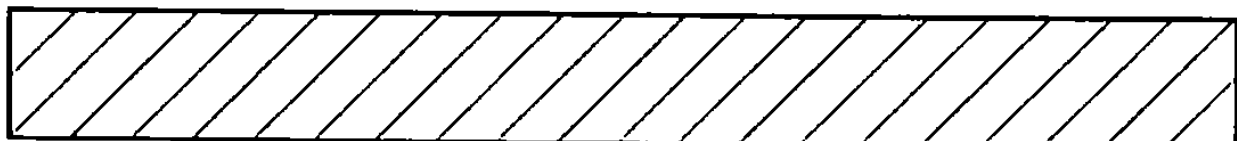
NOT TO SCALE

COLCHESTER INTERCHANGE DETAIL	PROJECT NAME: STATEWIDE	PLOT DATE: 07-MAR-2008
	PROJECT NUMBER: STP CRAK(26)	DRAWN BY: LOCKE
	FILE NAME: p07ki78.dgn	CHECKED BY: PAVT MGMT
	PROJECT LEADER: DOMEY DESIGNED BY: LOCKE p07ki78n+1.l	SHEET 6 OF 11

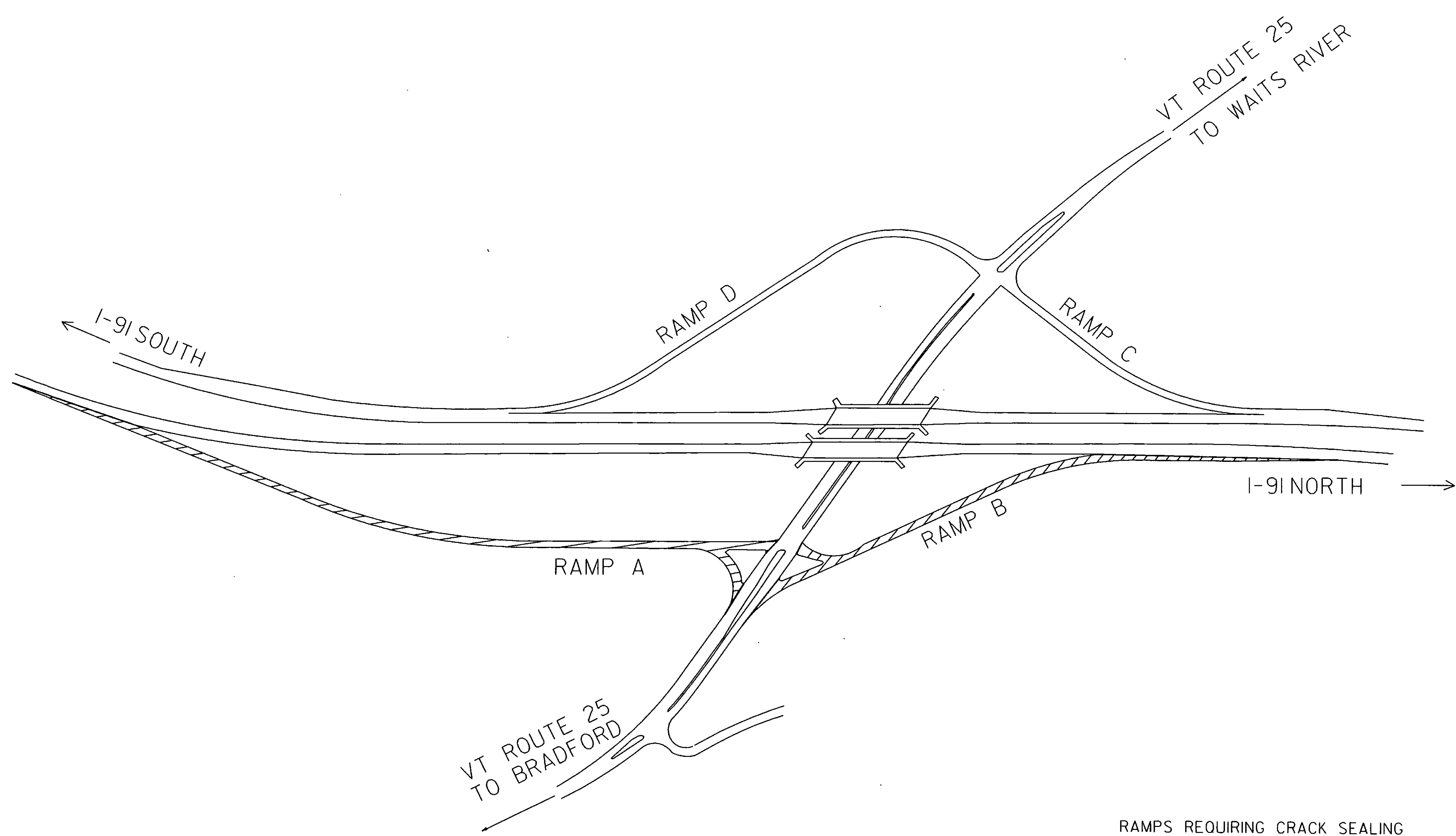


GEORGIA INTERCHANGE #18
I-89

NOT TO SCALE

RAMPS REQUIRING CRACK SEALING


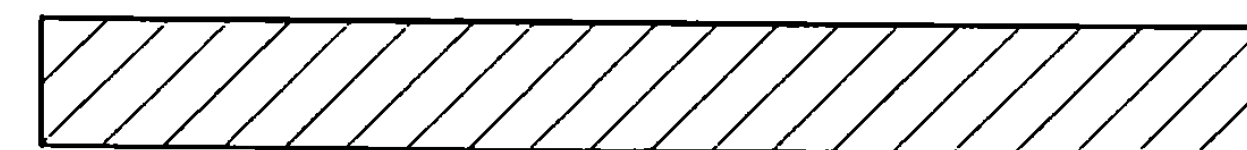
GEORGIA INTERCHANGE DETAIL	PROJECT NAME: STATEWIDE	PLOT DATE: 07-MAR-2008
	PROJECT NUMBER: STP CRAK(26)	DRAWN BY: LOCKE
	FILE NAME: p07k178.dgn	CHECKED BY: PAVT MGMT
	DESIGNED BY: LOCKE p07k178Int2.i	SHEET 7 OF 11



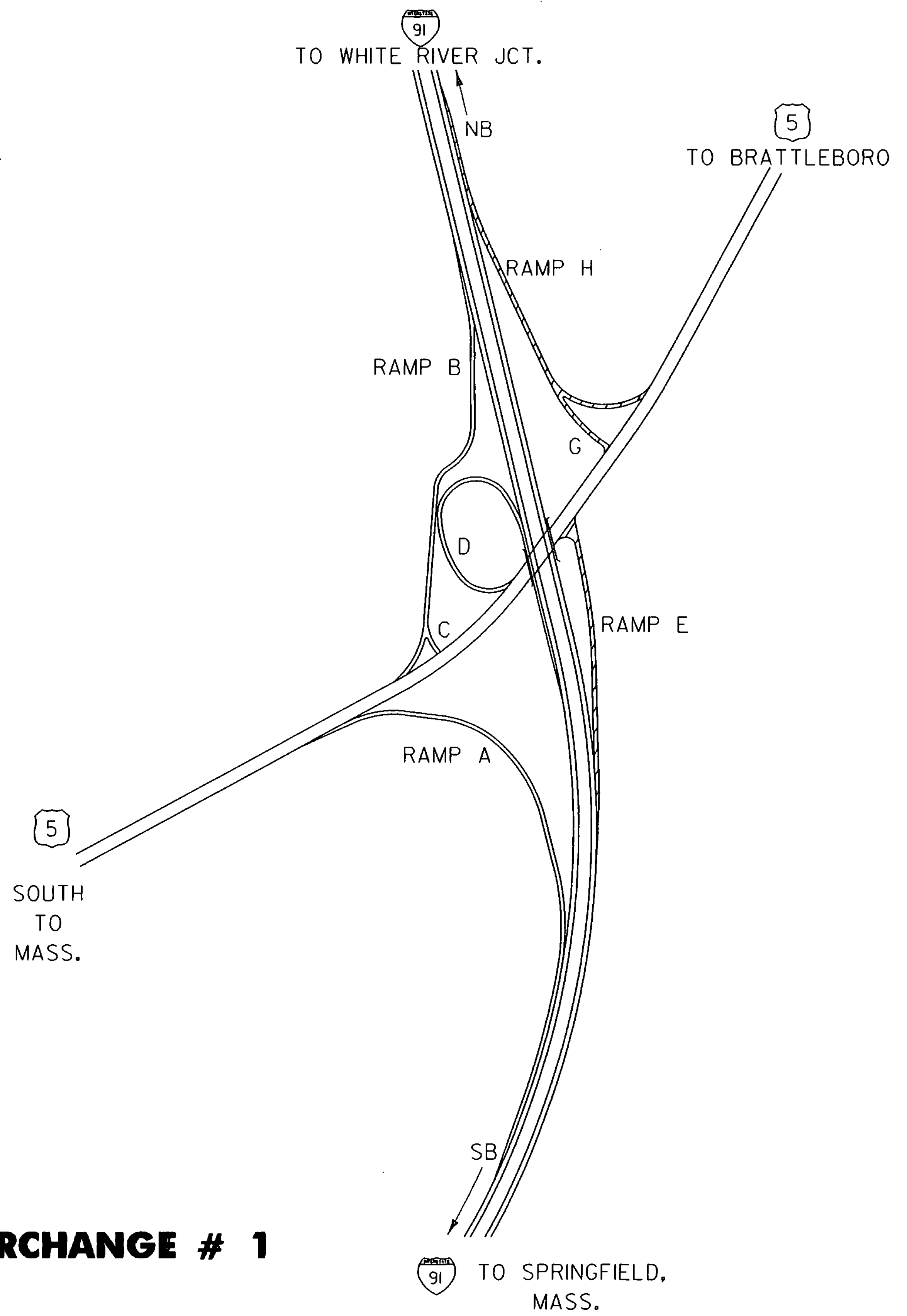
BRADFORD INTERCHANGE # 16
I-91

NOT TO SCALE

RAMPS REQUIRING CRACK SEALING

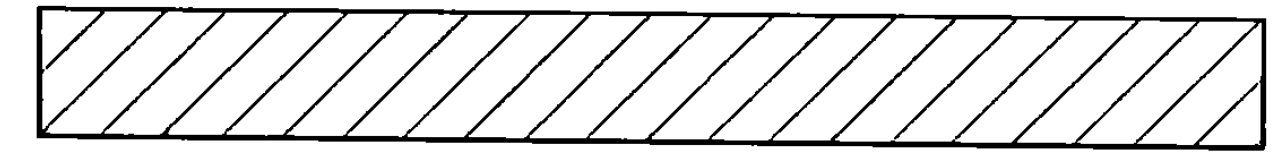


BRADFORD INTERCHANGE DETAIL	PROJECT NAME: STATEWIDE	PLOT DATE: 07-MAR-2008
	PROJECT NUMBER: STP CRAK(26)	DRAWN BY: LOCKE
	FILE NAME: p07k178.dgn	CHECKED BY: PAVT MGMT
	DESIGNED BY: LOCKE	SHEET 8 OF 11
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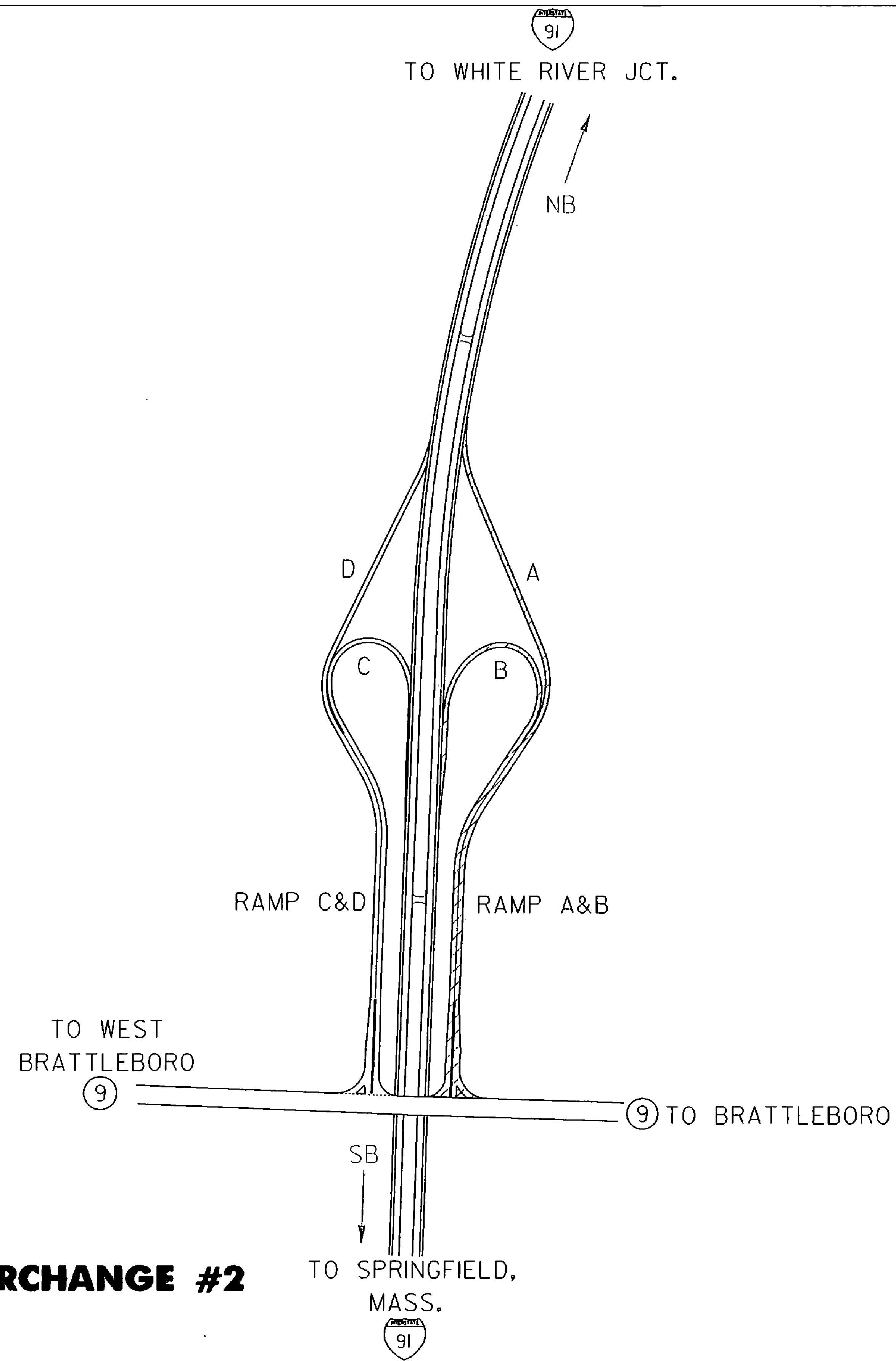
BRATTLEBORO INTERCHANGE # 1
I-91

RAMPS REQUIRING CRACK SEALING



NOT TO SCALE

BRATTLEBORO INTERCHANGE # 1 DETAIL	PROJECT NAME: STATEWIDE	PLOT DATE: 07-MAR-2008
	PROJECT NUMBER: STP CRAK(26)	DRAWN BY: LOCKE
	FILE NAME: p07ki78.dgn	CHECKED BY: PAVT MGMT
	PROJECT LEADER: DOMEY DESIGNED BY: LOCKE p07ki78in+4.l	SHEET 9 OF 11



BRATTLEBORO INTERCHANGE #2
I-91

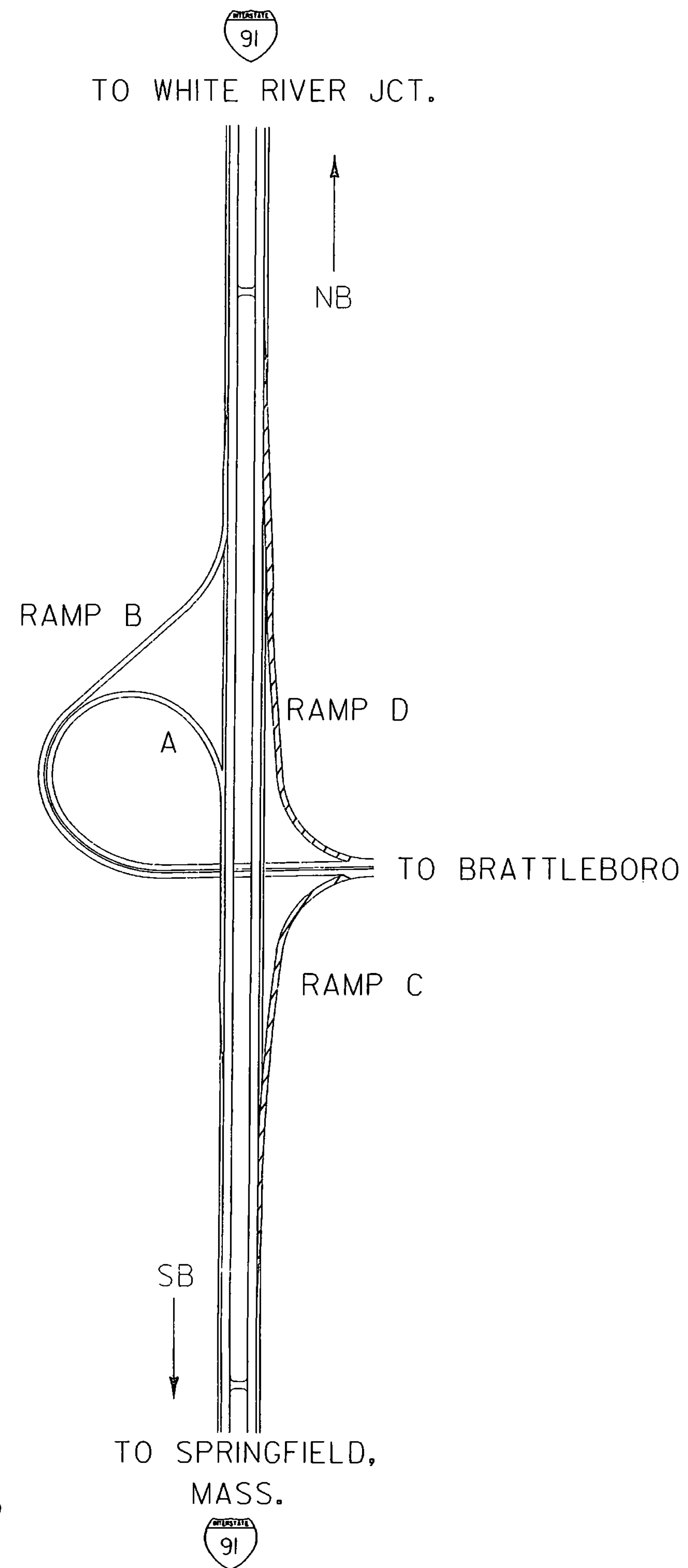
NOT TO SCALE

RAMPS REQUIRING CRACK SEALING



BRATTLEBORO INTERCHANGE #2
DETAIL

PROJECT NAME:	STATEWIDE	PLOT DATE:	07-MAR-2008
PROJECT NUMBER:	STP CRAK(26)	DRAWN BY:	LOCKE
FILE NAME:	p07k178.dgn	CHECKED BY:	PAVT MGMT
PROJECT LEADER:	DOMEY	SHEET 10	OF 11
DESIGNED BY:	LOCKE		
	p07k178in15.l		



BRATTLEBORO INTERCHANGE #3
I-91

NOT TO SCALE

RAMPS REQUIRING CRACK SEALING

BRATTLEBORO INTERCHANGE # 3 DETAIL	PROJECT NAME: STATEWIDE	PLOT DATE: 07-MAR-2008
	PROJECT NUMBER: STP CRAK(26)	DRAWN BY: LOCKE
	FILE NAME: p07ki78.dgn	CHECKED BY: PAVT MGMT
	DESIGNED BY: LOCKE	SHEET II OF II
	p07ki78Int6.I	