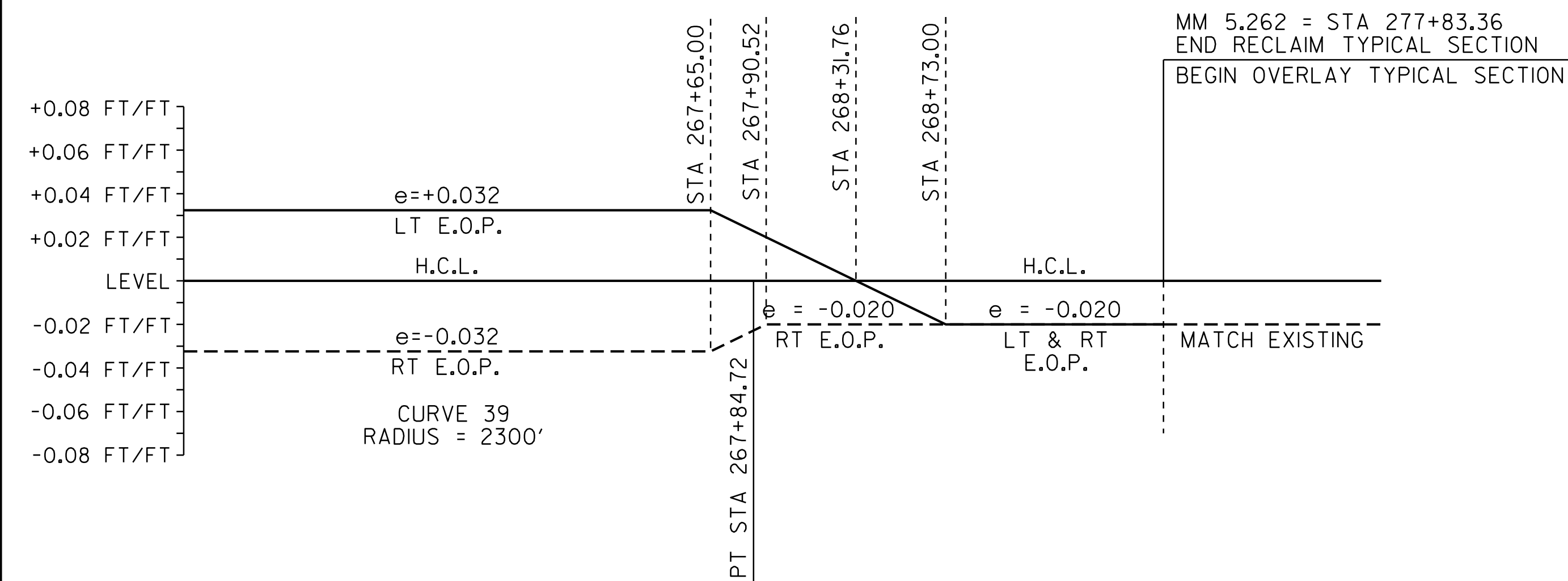


(SEE BELOW)

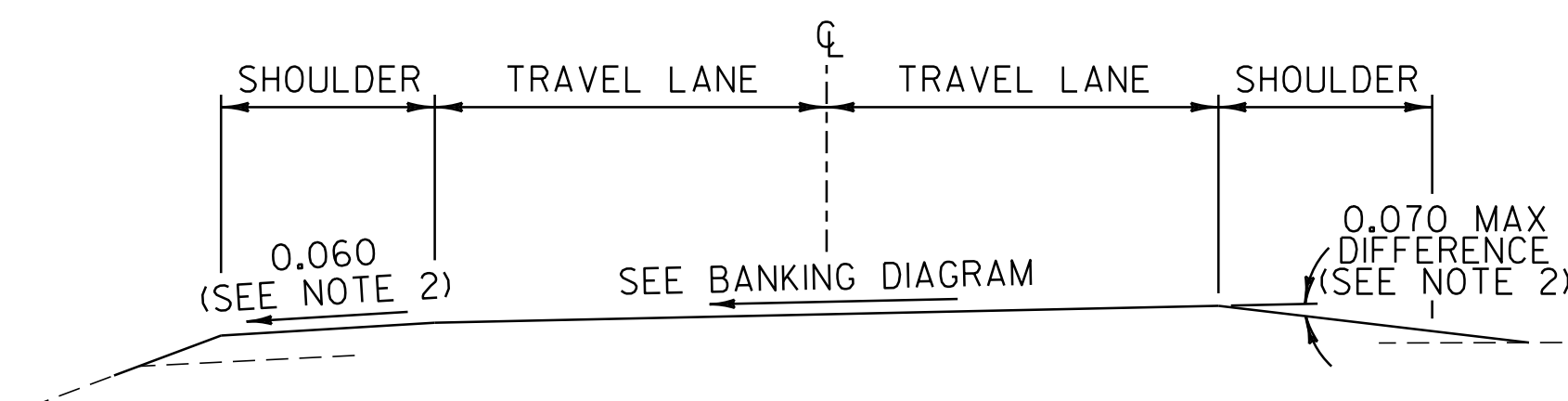


MM 5.262 = STA 277+83.36  
 END RECLAIM TYPICAL SECTION  
 BEGIN OVERLAY TYPICAL SECTION

MATCH EXISTING

**SUPERELEVATION BANKING NOTES:**

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE MAXIMUM ROLL-OVER BETWEEN LANE AND SHOULDER CROSS SLOPES ON THE OUTSIDE (HIGH SIDE) OF A SUPERELEVATED CURVE SHALL BE SEVEN PERCENT. THE SHOULDER CROSS SLOPE ON THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL BE A MINIMUM OF SIX PERCENT OR MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE WHEN THE CROSS SLOPE EXCEEDS SIX PERCENT.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



**NOT TO SCALE**  
**SUPERELEVATION BANKING DIAGRAMS SHEET #9**

PROJECT NAME: WARREN GORE - NORTON  
 PROJECT NUMBER: STP 2725(1)S

FILE NAME: p07c218.dgn  
 PROJECT LEADER: D.E.G.  
 DESIGNED BY: M.J.L.  
 IPARM FILE: p07c218sbd9.i

PLOT DATE: 31-OCT-2011 11:54  
 DRAWN BY: C.A.K.  
 CHECKED BY: D.W.E.  
 SHEET 93 OF 107