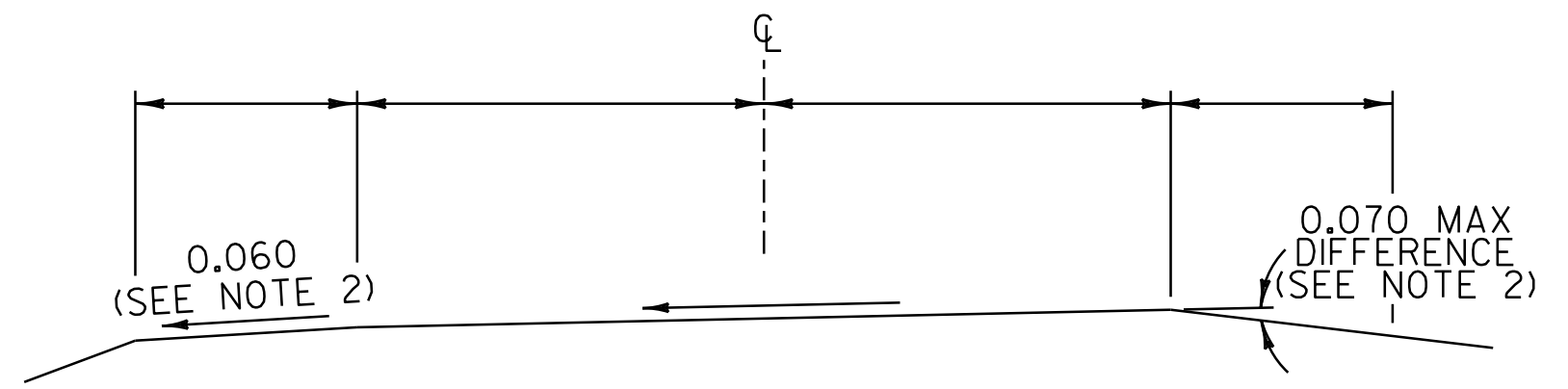


SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE MAXIMUM ROLL-OVER BETWEEN LANE AND SHOULDER CROSS SLOPES ON THE OUTSIDE (HIGH SIDE) OF A SUPERELEVATED CURVE SHALL BE SEVEN PERCENT. THE SHOULDER CROSS SLOPE ON THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL BE A MINIMUM OF SIX PERCENT OR MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE WHEN THE CROSS SLOPE EXCEEDS SIX PERCENT.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



NOT TO SCALE
SUPERELEVATION BANKING DIAGRAMS SHEET #15

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		DESIGNED BY:	M.J.L.	CHECKED BY:	D.W.E.
		IPARM FILE:	p07c216sbd15.i	SHEET	56 OF 107