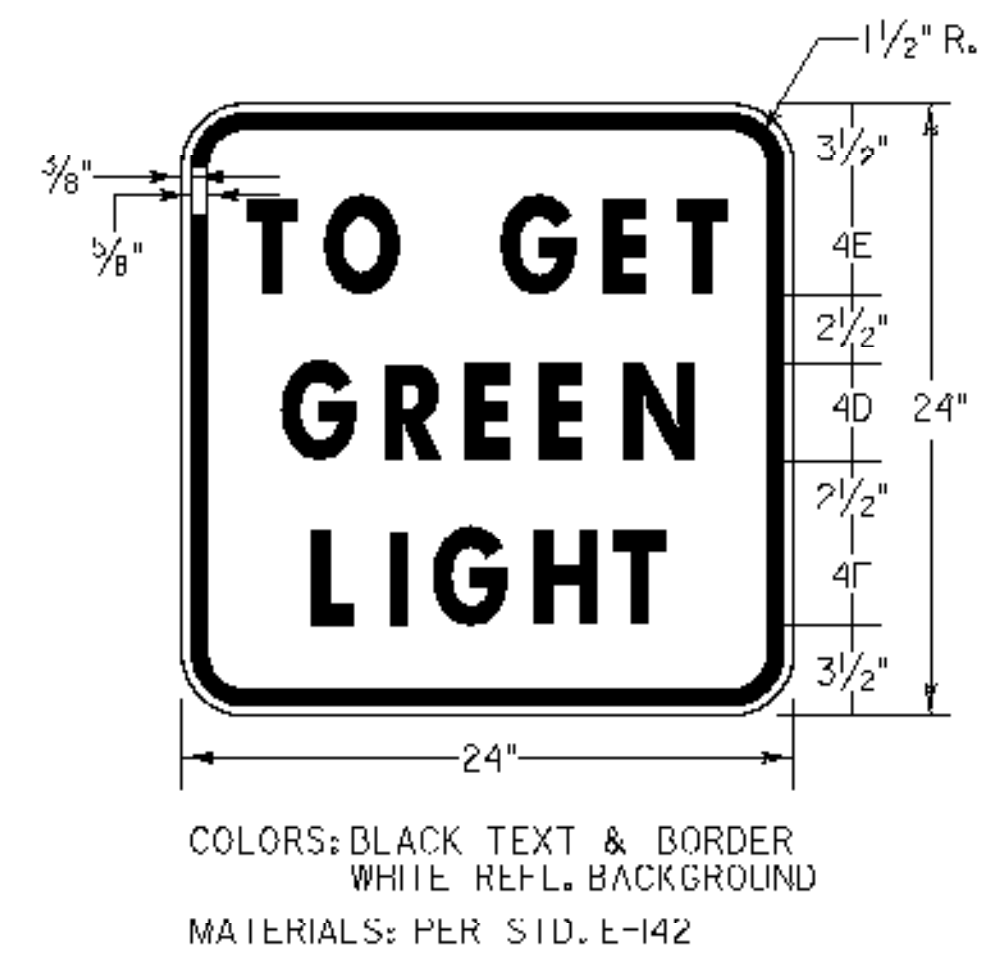


CONSTRUCTION APPROACH SIGNING
 NOT TO SCALE
 SEE STD E-100 FOR SIGN PLACEMENT

TRAFFIC CONTROL GENERAL NOTES

- DESIGN OF THE SIGNAL SUPPORT(S) AND ANY REQUIRED GUYING IS THE RESPONSIBILITY OF THE CONTRACTOR.
- SIGNAL TIMING/TIMING ADJUSTMENTS REQUESTED BY THE RESIDENT ENGINEER SHALL BE ACCOMPLISHED WITHIN A 48 HOUR PERIOD. PAYMENT SHALL BE INCIDENTAL TO ITEM NO. 678.40 - TEMPORARY TRAFFIC SIGNAL SYSTEM. THE ALL-RED CLEARANCE INTERVAL IS BASED ON AN ASSUMED SPEED OF 10-20 MPH. THE RESIDENT ENGINEER SHALL MAKE SEVERAL TRIAL RUNS TO DETERMINE THE PROPER ALL-RED CLEARANCE INTERVAL.
- SIGNAL FACES SHALL CONSIST OF 12" FNSFS, (RFD, YELLOW, AND GREEN)
- THE BOTTOM OF THE HOUSING OF A SIGNAL FACE SUSPENDED OVER A ROADWAY SHALL NOT BE LESS THAN 16 1/2 FEET NOR MORE THAN 19 FEET ABOVE THE GRADE AT THE CENTER OF THE ROADWAY. THE BOTTOM OF A SIGNAL FACE NOT MOUNTED OVER A ROADWAY, SHALL NOT BE LESS THAN 8 FEET NOR MORE THAN 15 FEET ABOVE THE GROUND. CAUTION SHOULD BE USED TO INSURE COMPLIANCE WITH THE HEIGHT REQUIREMENTS IN THE EVENT THE NEW APPROACH GRADES DIFFER SIGNIFICANTLY FROM THE OLD ROAD GRADE.
- SIGNAL FACES FOR ANY ONE APPROACH SHALL NOT BE LESS THAN 8 FEET APART MEASURED HORIZONTALLY BETWEEN CENTER OF FACES.
- SIGNAL HEADS MAY BE HUNG ON A SPAN WIRE OR ON A CANTILEVER MAST ARM. AT LEAST ONE SIGNAL HEAD SHALL BE UNMISTAKABLY IN LINE WITH THE CENTER OF APPROACHING TRAFFIC AT ALL TIMES. THE SECOND SIGNAL HEAD MAY BE POST MOUNTED, LOCATED AT A DISTANCE NO GREATER THAN 1 1/2 FEET FROM THE CENTER OF THE APPROACH LANE WHEN THE STOP BAR IS 40 FEET FROM THE SIGNAL HEAD. CONSULT M.U.T.C.D. FOR ADDITIONAL INFORMATION CONCERNING SIGNAL PLACEMENT.
- SIGNAL HEAD PLACEMENT IS CRITICAL. HEADS SHALL BE ADJUSTED TO REFLECT LANE LOCATION CHANGES.
- NOT USED.
- WHEN USED, TEMPORARY DETECTOR SHALL BE 4' X 40' FOR PRESENCE DETECTION AT THE STOP BAR WITH THE NEAR PORTION LOCATED 5 FEET BEYOND THE STOP BAR.
- INTERVAL TIMING SHOWN IN SECONDS.
- INTERCONNECT BETWEEN SIGNAL POLES BY WHATEVER MEANS POSSIBLE OR CONVENIENT TO PROVIDE FOR A SAFE INSTALLATION.
- PLACE TEMPORARY POLES BEHIND GUARDRAIL WHERE POSSIBLE.
- POLES SUPPORTING SPAN WIRES AND/OR MAST ARMS SHALL BE ADEQUATELY BRACED OR GUYED AND SHALL NOT BE PLACED SO AS TO CREATE A HAZARD TO THE TRAVELING PUBLIC.
- NOT USED.
- STOP BARS SHALL BE LOCATED A MINIMUM 40' AND A MAXIMUM OF 120' FROM THE NEAREST SIGNAL HEAD.
- PAYMENT FOR THE VEHICLE DETECTORS SHALL BE UNDER ITEM NO 678.42 TEMPORARY DETECTOR FOR EACH UNIT INSTALLED.

- SIGNS AND POSTS AS SHOWN ON THIS SHEET AND NOTED BELOW ARE INCIDENTAL TO ITEM 678.40 - TEMPORARY TRAFFIC SIGNAL SYSTEM. THE TEMPORARY STOP BARS SHALL BE PAID FOR UNDER ITEM NO. 646.680 - TEMPORARY 24" STOP BAR.
- SEE STANDARD E-140 FOR "STOP HERE ON RED" SIGN DETAIL AND E-101 FOR "SIGNAL AHEAD" SYMBOL SIGN. SEE STANDARD E-121 FOR SIGN PLACEMENT. SEE STANDARD E-171A AND E-172 FOR ADDITIONAL INFORMATION ON SIGNALS AND DETECTORS.
- A "SIGNAL AHEAD" SIGN SHALL BE PLACED AT LEAST 500' FROM THE SIGNAL.
- ALL ELECTRICAL WORK SHALL MEET THE REQUIREMENTS OF THE NATIONAL ELECTRICAL CODE AND STATE INSPECTOR.
- TRAFFIC CONTROL WARNING SIGNS SHALL BE PROVIDED ON EACH APPROACH PER STANDARD E-107. ADDITIONAL PROJECT CONSTRUCTION SIGNS SHALL BE INSTALLED AS REQUIRED BY THE RESIDENT ENGINEER PER STANDARD E-100, E-101, E-102 & E-102A. PAYMENT FOR THESE SIGNS, THE REFLECTORIZED PLASTIC DRUMS, ETC., SHALL BE PAID UNDER ITEM NO. 641.0 - TRAFFIC CONTROL. ALL DIAMOND-SHAPED WARNING SIGNS SHALL BE 48" X 48" FABRICATED ON ASTM TYPE IX RETROREFLECTIVE ORANGE SHEETING AND INSTALLED ON 2 POSTS EACH. "SPEED LIMIT 40" AND "FINE DOUBLED FOR SPEEDING IN WORK ZONE" SIGNS SHALL BE 36" X 48" AND FABRICATED ON MINIMUM ASTM TYPE III SHEETING AND INSTALLED ON 2 POSTS EACH.
- THE "TO GET GREEN LIGHT" SIGN IS TO BE USED ONLY ON APPROACHES WITH TEMPORARY DETECTORS.
- PAYMENT FOR INSTALLING AND REMOVING THE TEMPORARY BARRIER SHALL BE INCIDENTAL TO ITEM NO. 621.90 TEMPORARY TRAFFIC BARRIER.
- WHEN TEMPORARY BARRIER IS USED, BARRIER ENDS FACING ONCOMING TRAFFIC SHALL BE TAPERED BEYOND THE CLEAR ZONE, OR PROTECTED WITH AN APPROVED END TREATMENT DESIGNED FOR THE 85TH PERCENTILE SPEED OR THE POSTED SPEED LIMIT OF THE ROADWAY.
- PAYMENT FOR INSTALLING AND REMOVING ENERGY ABSORPTION ATTENUATORS SHALL BE INCIDENTAL TO ITEM NO. 621.56 - ENERGY ABSORPTION ATTENUATOR. CONTRACTOR SHALL REFER TO VTRANS' APPROVED PRODUCTS LISTING FOR OPTIONS OF ENERGY ABSORPTION ATTENUATORS TO BE USED ON THE PROJECT.



PHASING DIAGRAM

PHASE				
MINIMUM				
EXTENSION				
MAXIMUM				
HEAD				
HEAD				

SPECIAL REQUIREMENTS

APPROACH	TEMPORARY VEHICLE DETECTOR	FLASHING BEACON ON ADVANCED WARNING SIGN

ENTER CHECK MARK IN APPROPRIATE BOX WHEN REQUIRED ON THIS PROJECT

PHASING SHALL BE DETERMINED BY ENGINEER AT THE TIME OF INSTALLATION OF TEMPORARY SIGNAL SYSTEM.

PROJECT NAME: WOLCOTT
 PROJECT NUMBER: STP 030-2(22)
 FILE NAME: d07b096bdr.dgn PLOT DATE: 11-SEP-2007
 PROJECT LEADER: JLS DRAWN BY: MBL
 DESIGNED BY: DLG/MBL CHECKED BY: SDM
 TRAFFIC CONTROL DETAILS AND NOTES SHEET 14 OF 22