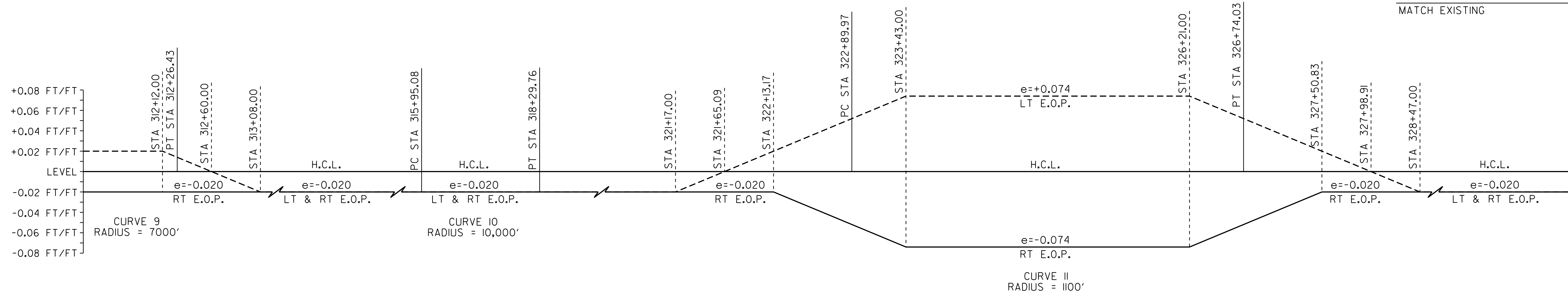
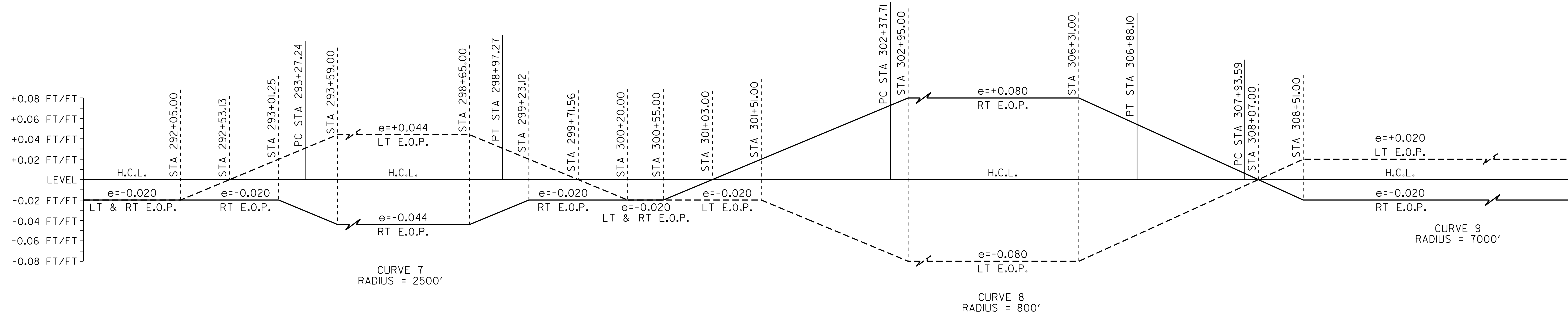


+0.08 FT/FT  
+0.06 FT/FT  
+0.04 FT/FT  
+0.02 FT/FT  
LEVEL  
-0.02 FT/FT  
-0.04 FT/FT  
-0.06 FT/FT  
-0.08 FT/FT

+0.08 FT/FT  
+0.06 FT/FT  
+0.04 FT/FT  
+0.02 FT/FT  
LEVEL  
-0.02 FT/FT  
-0.04 FT/FT  
-0.06 FT/FT  
-0.08 FT/FT

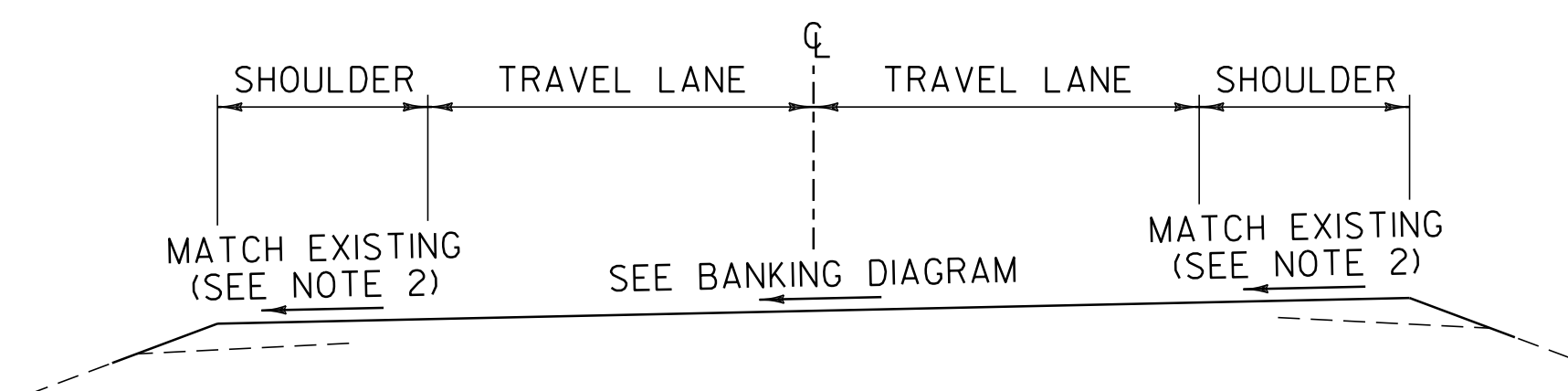


STP 2629(1)  
CORNWALL  
END RECLAIM TYPICAL SECTION  
STA 29+04.00  
MATCH EXISTING

(SEE BELOW)

**SUPERELEVATION BANKING NOTES:**

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE SHOULDER CROSS SLOPE ON BOTH THE OUTSIDE (HIGH SIDE) AND THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL MATCH THE ADJACENT TRAVEL LANE CROSS SLOPE.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.



<b>NOT TO SCALE</b> <b>SUPERELEVATION BANKING DIAGRAMS SHEET #2</b>	PROJECT NAME: WHITING - MIDDLEBURY	
	PROJECT NUMBER: STP 2629(1)	
	FILE NAME: p06c226.dgn	PLOT DATE: 30-MAR-2012 14:14
	PROJECT LEADER: D.E.G. DESIGNED BY: M.J.L. DWG. NO. p06c226sbd2.1	DRAWN BY: C.A.K. CHECKED BY: D.E.G. SHEET 45 OF 133