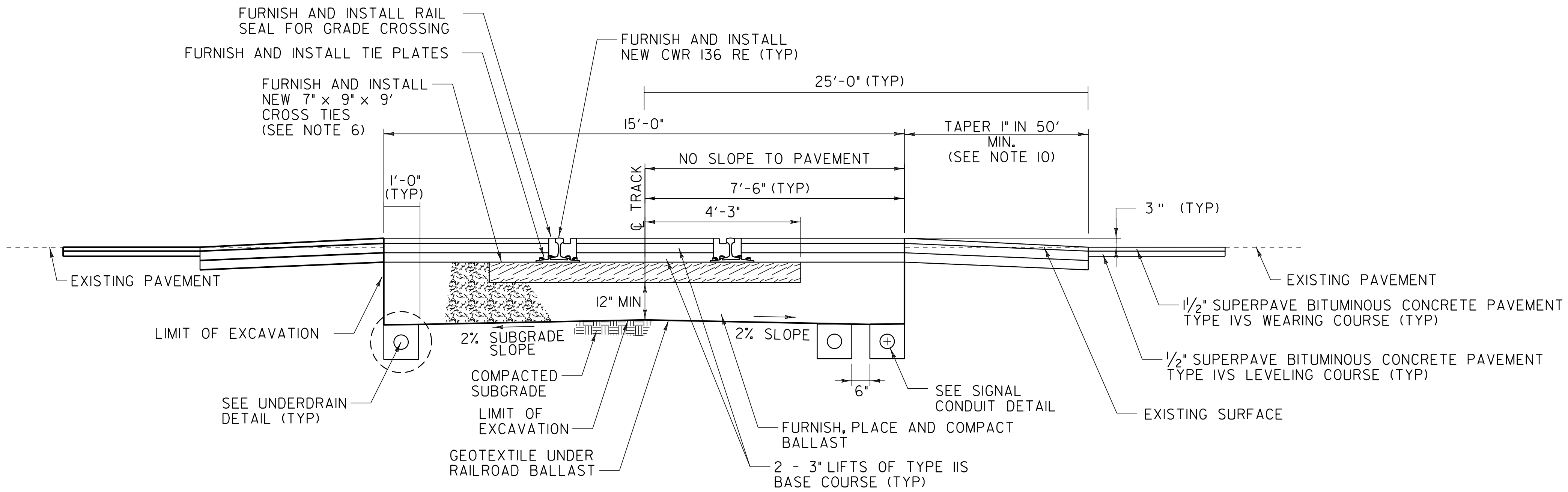


NOTES:

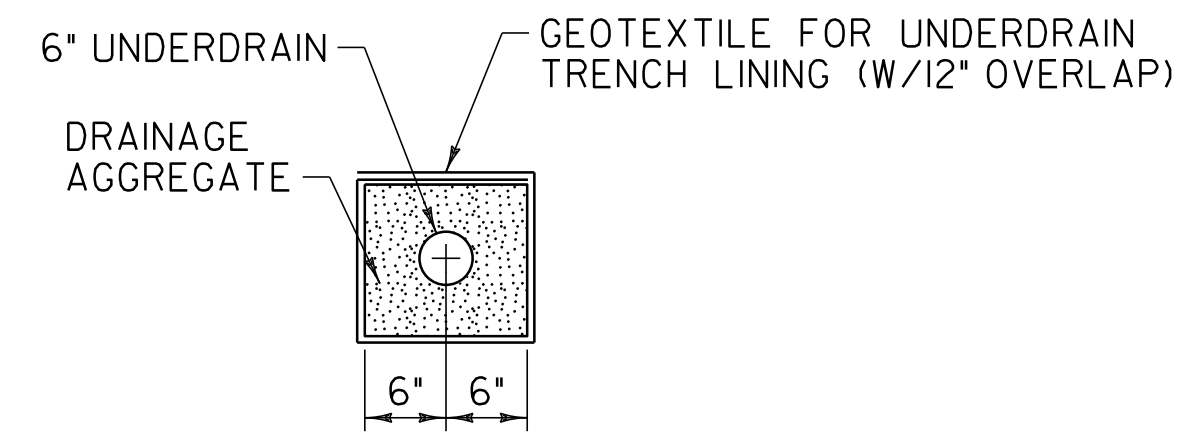
- ITEM 900.645 SPECIAL PROVISION (RECONSTRUCT RAIL-HIGHWAY CROSSING) (VT. 4A - AARDOT 248-953D) SHALL INCLUDE ALL ITEMS AND MATERIALS SHOWN IN THE DETAILS ON SHEET 77 & 78 TO CONSTRUCT THE PROPOSED ASPHALT GRADE CROSSING UNLESS OTHERWISE NOTED OR AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL INSTALL A GEOTEXTILE UNDER THE RAILROAD BALLAST WITHIN THE LIMITS OF THE CROSSING PER SECTION 649 OF THE 2006 VAOT STANDARD SPECIFICATIONS FOR CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL INSTALL THE ELASTOMERIC INTERFACE (ADVANTAGE GRADE) RAIL SEAL CONNECTED BY AN INTERCLAMP DEVICE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THESE DETAILS DO NOT ILLUSTRATE THE INTERCLAMP DEVICES. NO VEHICULAR TRAFFIC IS ALLOWED OVER THE RAIL SEAL UNTIL FULL DEPTH ASPHALT IS PLACED ON BOTH SIDES OF THE RAIL. ANY DAMAGE TO THE NEW ELASTOMERIC INTERFACE RESULTING FROM THE CONTRACTOR'S INSTALLATION METHODS SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE AS DIRECTED BY THE RESIDENT ENGINEER. THE COST OF FURNISHING AND INSTALLING THE ELASTOMERIC INTERFACE AND ATTACHMENT HARDWARE SHALL BE INCIDENTAL TO ITEM 900.645 SPECIAL PROVISION (RECONSTRUCT RAIL-HIGHWAY CROSSING) (VT. 4A - AARDOT 248-956).
- TEN DAYS PRIOR TO BEGINNING WORK ON THIS RAIL CROSSING THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH CLARENDON AND PITTSFORD RAILROAD. TRACK AND ROADWAY SURFACE WILL BE IN SERVICE AT THE END OF EACH WORK DAY (VEHICULAR AND RAIL TRAFFIC). IT IS ANTICIPATED THAT THE WORKING HOURS SHALL BE FROM 10:00 A.M. TO 7:00 P.M. THE CONTRACTOR IS RESPONSIBLE TO HAVE THE TRACK BACK IN SERVICE AT THE END OF EACH WORK DAY. THE COST FOR COORDINATION WITH CLARENDON AND PITTSFORD RAILROAD SHALL BE INCIDENTAL TO ITEM 900.645 SPECIAL PROVISION (TRAFFIC CONTROL, RAIL-HIGHWAY CROSSING) (AARDOT 248-953D).

THE CONTRACTOR SHALL COMPLETE ALL WORK ASSOCIATED WITH THIS CROSSING WITHIN FIVE CONTINUOUS DAYS. THE CONTRACTOR IS HEREBY NOTIFIED THAT LIQUIDATED DAMAGES IN ACCORDANCE WITH SUBSECTION 108.12 WILL BE ASSESSED FOR EVERY CALENDAR DAY FOLLOWING THE FIVE CONSECUTIVE WORKDAY PERIOD THAT THE SPECIFIED WORK IS NOT COMPLETED.

- THE CONTRACTOR SHALL INSTALL A 6" UNDERDRAIN ALONG BOTH SIDES OF THE PROPOSED ASPHALT RAILROAD GRADE CROSSING PER SECTION 605 OF THE 2006 VAOT STANDARD SPECIFICATIONS FOR CONSTRUCTION AND GRADE TO DRAIN TO THE EXISTING DITCHES AS DIRECTED BY THE RESIDENT ENGINEER.
- NEW 7"x9"x9' CROSS TIES SHALL BE INSTALLED WITHIN THE CROSSING AREA AS SHOWN ON THE PLANS. CROSS TIES LOCATED WITHIN THE APPROACH AREAS SHALL BE REPLACED WITH NEW 7"x9"x8'-6" TIES AS RECOMMENDED BY THE RAILROAD AND APPROVED BY THE RESIDENT ENGINEER.
- TIE PLATES SHALL BE MINIMUM 14" L x 7 3/4" W NEW OR FIT DOUBLE SHOULDERED TO FIT THE BASE OF THE RAIL ON WHICH THEY WILL BE USED. EACH TIE PLATE SHALL HAVE AT LEAST 6 HOLES PUNCHED FOR THE USE OF 5/8" TRACK SPIKES. FOUR OF THE HOLES SHALL BE SUCH THAT THEY ALLOW TWO RAIL HOLDING SPIKES ON EACH SIDE OF THE RAIL AND THERE SHALL ALSO BE AT LEAST TWO HOLES FOR PLATE HOLDING SPIKES.
- RAIL WEIGHTS SHALL BE DETERMINED BY THE RAILROAD AND APPROVED BY THE RESIDENT ENGINEER. IF THE CONTRACTOR IS UNABLE TO SUPPLY I36 RE RAIL, SUBSTITUTE RAIL (IF APPROVED) COST MUST INCLUDE APPROPRIATE COMPROMISE RAIL JOINT ASSEMBLY AND TRANSITION RAIL (IF REQUIRED).
- THE COST FOR PROVIDING TRAFFIC CONTROL DURING CONSTRUCTION OF THIS PAVED RAIL HIGHWAY CROSSING SHALL BE PAID UNDER ITEM 900.645 SPECIAL PROVISION (TRAFFIC CONTROL, RAIL-HIGHWAY CROSSING) (AARDOT 248-953D). ANY SIGNAL WORK REQUIRED BY THE RAILROAD WILL BE PAID FOR UNDER THIS ITEM 900.645 SPECIAL PROVISION (TRAFFIC CONTROL, RAIL-HIGHWAY CROSSING) (AARDOT 248-953D).
- THE COST OF PAVING THE TWO LIFTS OF TYPE IIS BASE COURSE AT THE RAIL APPROACHES AND BETWEEN THE RAILS SHALL NOT BE PAID SEPARATELY, BUT SHALL BE INCIDENTAL TO ITEM 900.645 SPECIAL PROVISION (RECONSTRUCT RAIL-HIGHWAY CROSSING) (VT. 4A - AARDOT 248-953D). THE COST OF PAVING THE TYPE IVS LEVELING AND WEARING COURSES AT THE RAIL APPROACHES AND THE TYPE IVS WEARING COURSE BETWEEN THE RAILS SHALL BE PAID AS PART OF THE STP HES 2614(I)S PAVING PROJECT UNDER ITEM 490.30 SUPERPAVE BITUMINOUS CONCRETE PAVEMENT.
- THE COST OF PROVIDING RAILROAD FLAGGERS SHALL BE PAID FOR UNDER ITEM 630.20 FLAGGERS, RAILROAD.
- EXISTING RAIL, PLATES AND OTM NOT REINSTALLED SHALL BECOME PROPERTY OF THE RAILROAD.
- THESE QUANTITIES ARE ESTIMATES ONLY. ANY DEVIATION FROM THESE QUANTITIES WILL NOT RESULT IN ADDITIONAL COMPENSATION UNDER CONTRACT ITEM 900.645 SPECIAL PROVISIONS (RECONSTRUCTION RAIL-HIGHWAY CROSSING) (VT. 4A-AARDOT 248-953D) UNLESS AUTHORIZED BY THE RESIDENT ENGINEER.
- UNCLASSIFIED EXCAVATION SHALL CONSIST OF THE EXCAVATION AND DISPOSAL FROM THE WORK SITE ALL MATERIALS OF ANY DESCRIPTION ENCOUNTERED IN THE COURSE OF CONSTRUCTION.

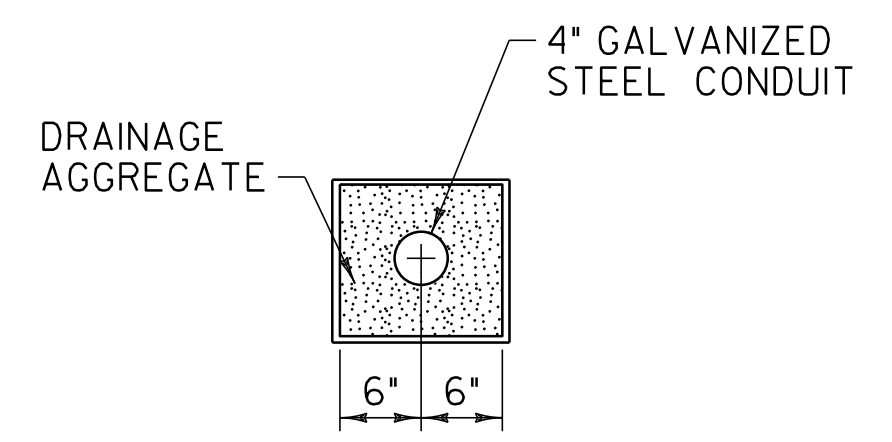


**TYPICAL SECTION - ASPHALT GRADE CROSSING**



**UNDERDRAIN WITH GEOTEXTILE**

SEE NOTE 5



**SIGNAL CONDUIT DETAIL**

900.645 SPECIAL PROVISION (RECONSTRUCT RAIL - HIGHWAY CROSSING) (VT. 4A - AARDOT 248-953D)			
LIST OF QUANTITIES			
DESCRIPTION	UNIT	QTY*	
REMOVAL OF EXISTING TRACK AND GRADE CROSSING SURFACE	TF	295	
FURNISH AND INSTALL TRACK (I36 RE CWR OR APPROVED EQUAL) WITH 7"x9"x8'-6" NEW TIES (CROSSING APPROACH)	TF	170	
FURNISH AND INSTALL TRACK (I36 RE CWR OR APPROVED EQUAL) WITH 7"x9"x9' NEW TIES (CROSSING AREA)	TF	125	
FURNISH AND INSTALL ELASTOMERIC INTERFACE FOR GRADE CROSSING	TF	80	
FURNISH, PLACE AND COMPACT BALLAST (BALLAST SIZE 1 1/2" AREMA 4A)	TON	300	
GEOTEXTILE BEDDING	SY	180	
6 INCH UNDERDRAIN	LF	200	
UNCLASSIFIED EXCAVATION	CY	150	
RAISE, ALIGN AND SURFACE TRACK (TO FINAL GRADE)	TF	300	
BASE COURSE TYPE IIS	TON	150	
FURNISH AND INSTALL 4" GALVANIZED STEEL CONDUIT 4 NPS	LF	140	

\*SEE NOTE 13

NOT TO SCALE

<b>AARDOT 248-953D GRADE CROSSING DETAIL SHEET</b>	PROJECT NAME: FAIR HAVEN - CASTLETON
	PROJECT NUMBER: STP HES 2614(I)S
	FILE NAME: p06c058.dgn
	PLOT DATE: 22-FEB-2012 12:52
PROJECT LEADER: D.E.G.	DRAWN BY: C.A.K.
DESIGNED BY: M.J.M.	CHECKED BY: D.E.G.
IPARM FILE: p06c058rr3.i	SHEET 78 OF 157