



**STEEL BEAM GUARDRAIL, GALVANIZED**  
 STA. A 1+100.000, 1.8m LT. - STA. A 1+242.615, 1.8m LT.  
 STA. A 1+387.964, 1.8m LT. - STA. A 1+400.000, 1.8m LT.  
 STA. A 1+206.220, 6.6m RT. - STA. A 1+241.571, 6.6m RT.  
 STA. A 1+386.235, 6.6m RT. - STA. A 1+400.000, 6.6m RT.  
 STA. D 1+127.030, 1.8m RT. - STA. D 1+234.402, 1.8m RT.  
 STA. D 1+380.360, 1.8m RT. - STA. D 1+410.000, 1.8m RT.  
 STA. D 1+203.030, 6.6m LT. - STA. D 1+235.595, 6.6m LT.  
 STA. D 1+382.369, 6.6m LT. - STA. D 1+410.000, 6.6m LT.

**GRANITE SLOPE EDGING**  
 STA. A 1+100.000, 6.6m RT. - STA. A 1+235.000, 6.6m RT.  
 STA. A 1+100.000, 1.8m LT. - STA. A 1+242.000, 1.8m LT.  
 STA. D 1+100.000, 6.6m LT. - STA. D 1+236.000, 6.6m LT.  
 STA. D 1+100.000, 1.0m RT. - STA. D 1+127.030, 1.0m RT.

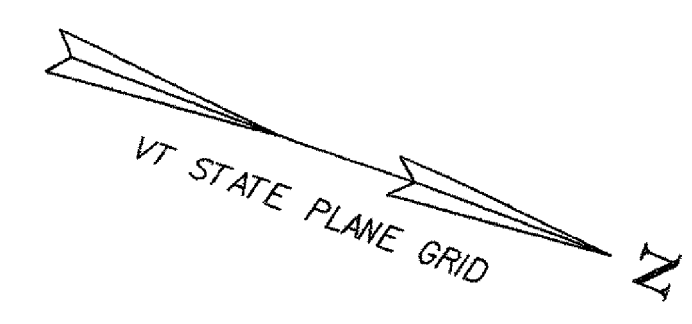
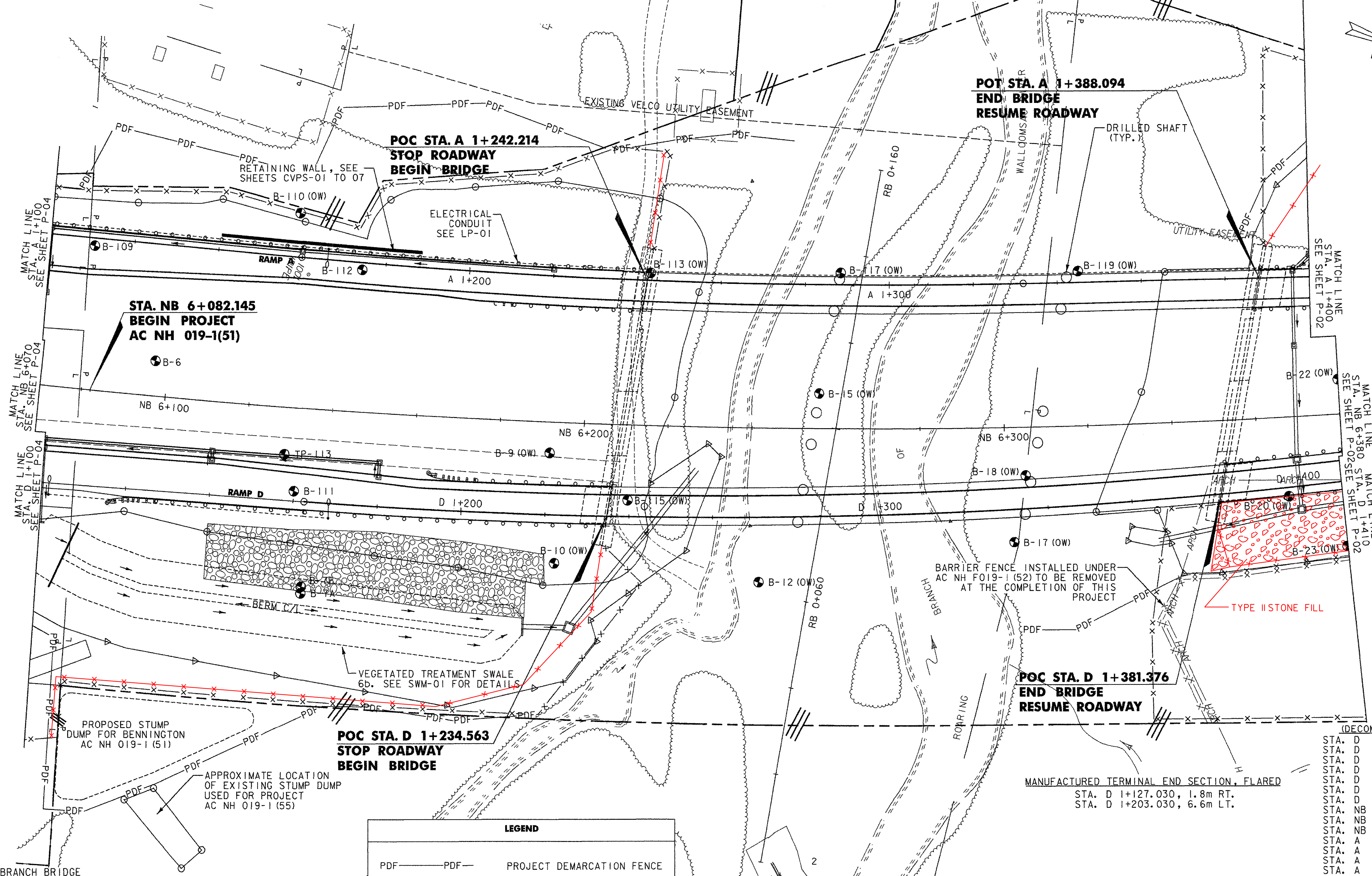
**WOVEN WIRE FENCE WITH STEEL POSTS**  
 STA. A 1+100.026, LT. - STA. A 1+247.646, LT.  
 STA. A 1+390.178, LT. - STA. A 1+400.000, LT.  
 STA. D 1+100.000, RT. - STA. D 1+243.152, RT.  
 STA. D 1+361.501, RT. - STA. D 1+410.000, RT.

**REMOVAL OF EXISTING FENCE**  
 STA. D 1+374.4, RT. - STA. D 1+410.0, RT. (EXIST BARRIER FENCE)

**PROJECT DEMARCATION FENCE**  
 STA. A 1+100.000, 18.4m LT. - STA. A 1+297.520, 68.1m LT.  
 STA. A 1+384.112, 10.7m LT. - STA. A 1+400.000, 30.1m LT.  
 STA. D 1+100.000, 32.7m RT. - STA. D 1+221.487, 45.0m RT.  
 STA. D 1+332.340, 27.4m RT. - STA. D 1+370.353, 17.8m RT.

**ANCHOR FOR STEEL BEAM RAIL**  
 STA. A 1+208.220, 6.6m RT.

**BITUMINOUS CONCRETE CURB, TYPE A**  
 STA. A 1+388.000, 1.8m LT. - STA. A 1+400.000, 1.8m LT.  
 STA. A 1+397.000, 6.6m RT. - STA. A 1+400.000, 6.6m RT.  
 STA. D 1+127.030, 1.8m RT. - STA. D 1+234.000, 1.8m RT.  
 STA. D 1+380.000, 1.8m RT. - STA. D 1+410.000, 1.8m RT.  
 STA. D 1+382.000, 6.6m LT. - STA. D 1+410.000, 6.6m LT.



MATCH LINE  
STA. A 1+100.000  
SEE SHEET P-04

MATCH LINE  
STA. NB 6+070.000  
SEE SHEET P-04

MATCH LINE  
STA. D 1+100.000  
SEE SHEET P-04

MATCH LINE  
STA. A 1+400.000  
SEE SHEET P-02

MATCH LINE  
STA. NB 6+380.000  
SEE SHEET P-02

MATCH LINE  
STA. D 1+410.000  
SEE SHEET P-02

**SPECIAL PROVISION (DECOMMISSION OBSERVATION WELL)**

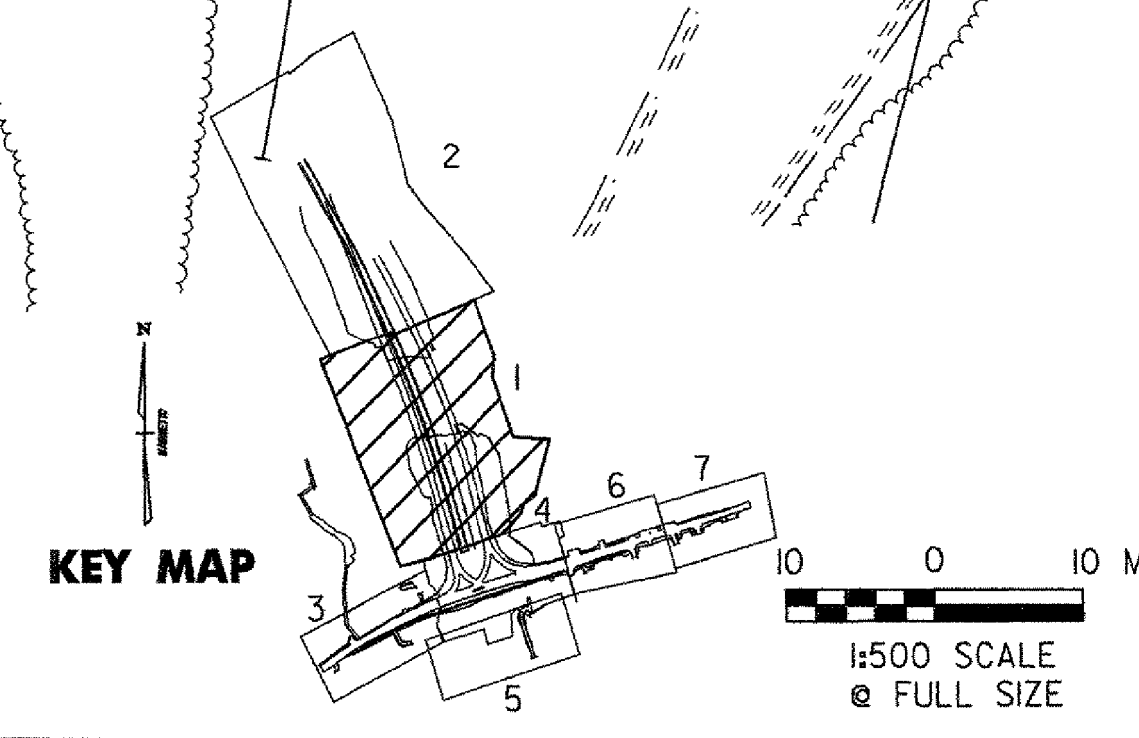
STA. D 1+409.833, 14.7m RT.	(B-23)
STA. D 1+222.503, 11.0m RT.	(B-10)
STA. D 1+239.654, 3.6m LT.	(B-115)
STA. D 1+270.844, 14.8m RT.	(B-12)
STA. D 1+331.536, 7.4m RT.	(B-17)
STA. D 1+335.063, 7.5m LT.	(B-18)
STA. D 1+397.330, 2.0m RT.	(B-20)
STA. NB 6+380.000, 8.0m LT.	(B-22)
STA. NB 6+256.000, 8.0m LT.	(B-15)
STA. NB 6+310.000, 5.0m LT.	(B-19)
STA. A 1+158.687, 10.0m LT.	(B-110)
STA. A 1+288.444, 2.7m LT.	(B-117)
STA. A 1+243.139, 1.7m LT.	(B-113)
STA. A 1+344.946, 2.5m LT.	(B-119)

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- NOTES:**
- REFERENCE ROARING BRANCH BRIDGE PLANS FOR DETAILS OF CONSTRUCTION BEYOND ROADWAY LIMITS.
  - TREE CLEARING WITHIN 30m OF THE ROARING BRANCH SHALL BE KEPT TO A MINIMUM.
  - RAMP D STA. D 1+380 - D 1+420 RT. NO SLOPE ROUNDING WILL BE USED SEE SECTIONS.
  - IN LOCATIONS WITH BITUMINOUS CONCRETE CURB AND ADDITIONAL PAVEMENT WIDTH OF 225 IS REQUIRED SEE TYPICAL SECTION SHEET TYP-02

**LEGEND**

PDF — PDF —	PROJECT DEMARCATION FENCE
X — X — X —	BARRIER FENCE
x — x — x —	CHAIN-LINK FENCE
x — x — x —	RIGHT OF WAY FENCE
WETLAND BOUNDARY	WETLAND BOUNDARY
ARCH — ARCH	ARCHAEOLOGICAL SITE
WATER BOUNDARY	WATER BOUNDARY
P —	PROPERTY LINE
L —	PROPERTY LINE



**VERMONT AGENCY OF TRANSPORTATION**

PROJECT NAME: BENNINGTON  
 PROJECT NUMBER: AC NH 019-1(51)

FILE NAME: ...z307\cgen\_plans\_500.dwg  
 DESIGN SUPERVISOR: GREG EDWARDS  
 DESIGNED BY: MARC FOISY  
 GENERAL PLAN P-01

PLOT DATE: 2/2/2009  
 DRAWN BY: STANTEC  
 CHECKED BY: GARY SANTY  
 SHEET 66 OF 367