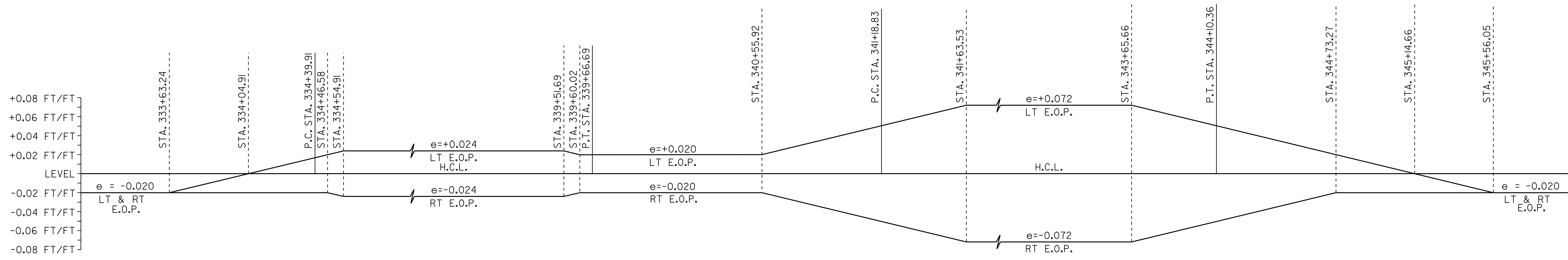


**CURVE 1 & CURVE 2 BANKING DIAGRAMS**

CURVE 1 RADIUS = 1000'  
 CURVE 2 RADIUS = 1000'



**CURVE 3 & CURVE 4 BANKING DIAGRAMS**

CURVE 3 RADIUS = 3500'  
 CURVE 4 RADIUS = 700'

**SUPERELEVATION BANKING NOTES:**

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE EXISTING ROADWAY.
2. THE MAXIMUM ROLL-OVER BETWEEN LANE AND SHOULDER CROSS SLOPES ON THE OUTSIDE (HIGH SIDE) OF A SUPERELEVATED CURVE SHALL BE SEVEN PERCENT. SHOULDER CROSS SLOPE ON THE INSIDE (LOW SIDE) OF A SUPERELEVATED CURVE SHALL BE A MINIMUM OF SIX PERCENT AND MATCH THE ADJACENT LANE CROSS SLOPE WHEN THE LANE CROSS SLOPE EXCEEDS SIX PERCENT.
3. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED AND A MAXIMUM SUPERELEVATION RATE OF 0.08.

**NOT TO SCALE**  
**SUPERELEVATION BANKING DIAGRAMS SHEET #1**

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DESIGNED BY: D.W.E.	SHEET 39 OF 80
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