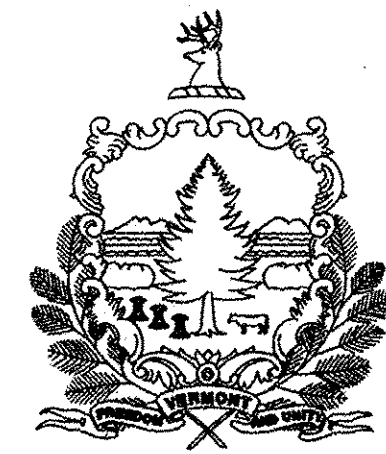
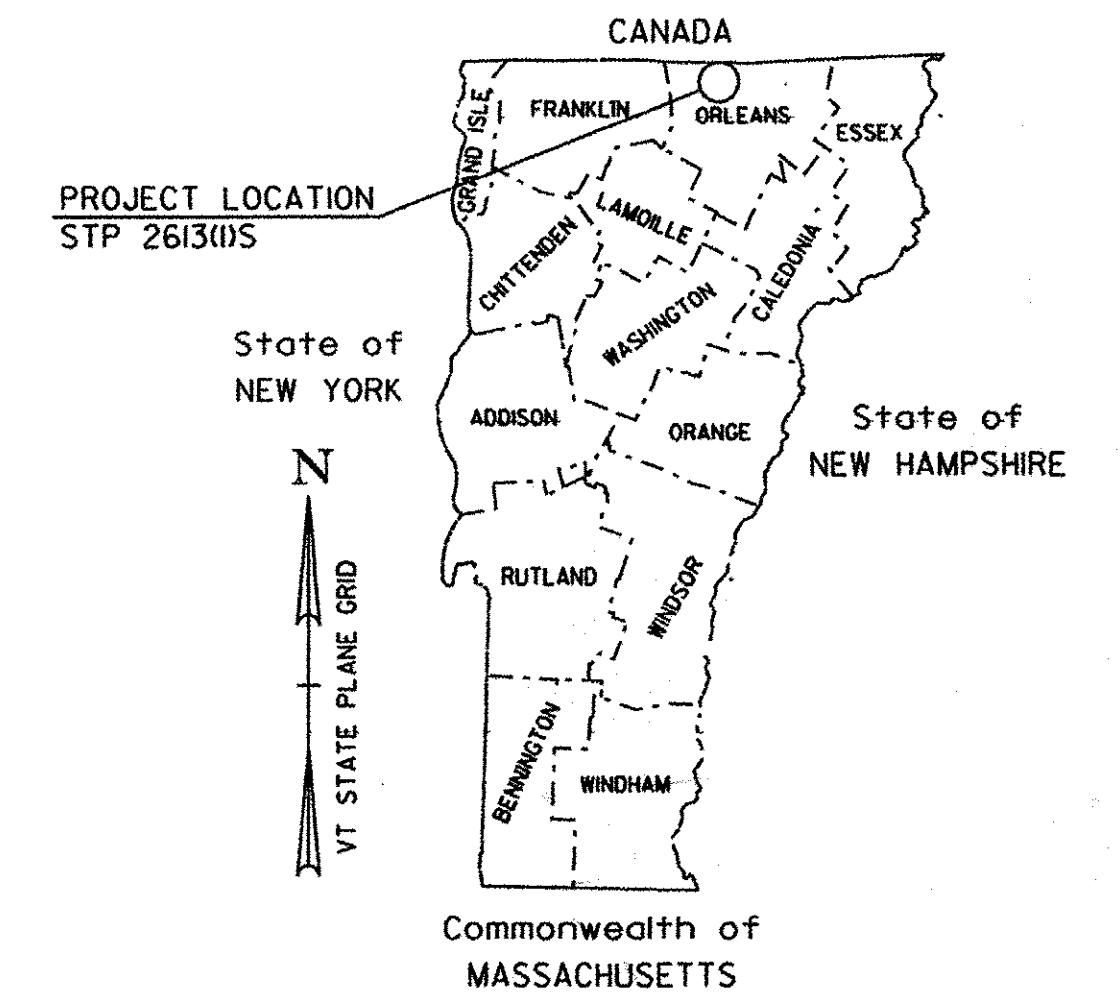


INDEX OF SHEETS  
SEE SHEET 2

# STATE OF VERMONT AGENCY OF TRANSPORTATION



## PROPOSED IMPROVEMENT TOWNS OF TROY AND NEWPORT COUNTY OF ORLEANS VT ROUTE 105



RECORD PLANS	
CONTRACTOR:	PIKE INDUSTRIES, INC. - BERLIN, VT
RESIDENT ENGINEER:	KEVIN MCCLURE
CONSTRUCTION BEGAN:	APRIL 19, 2010
CONSTRUCTION COMPLETE:	OCTOBER 11, 2010
RECORD PLANS BY:	KEVIN MCCLURE & C. PIERCE
I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.	
BY <u>Kevin McClure</u>	RESIDENT ENGINEER
DATE <u>9-1-11</u>	
NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.	

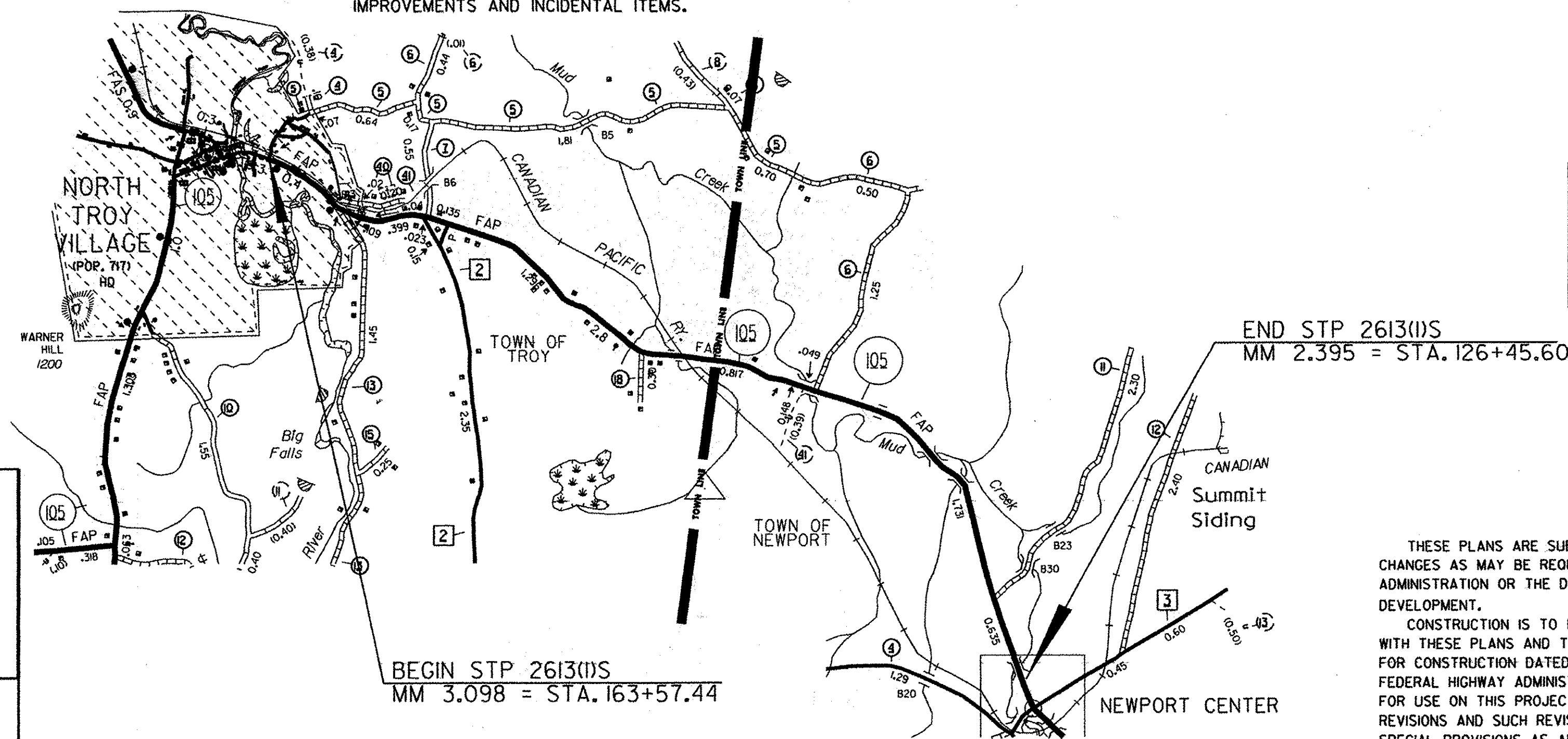
BEGINNING IN THE TOWN OF TROY NEAR THE INTERSECTION OF PINE ST. AND VT ROUTE 105 AT MILE MARKER 3,098 = STA. 163+57.44 AND EXTENDING EASTERLY ALONG VT ROUTE 105 FOR A DISTANCE OF 29,456.16 FT (5,579 MILES) TO AN ENDING POINT IN THE TOWN OF NEWPORT AT MILE MARKER 2,395 = STA. 126+45.60.

STATION TO STATION DATA	LENGTH (FEET)	LENGTH (MILES)
TOWN OF TROY STA. 163+57.44 TO STA. 301+80.00 (STOP) (MM 3.098 TO MM 5.716)	13,822.56	2.618
STA. 304+08.00 TO STA. 333+96.00 (RESUME) (MM 5.759 TO MM 6.325)	2,988.00	0.566
TOWN OF NEWPORT STA. 0+00.00 TO STA. 126+45.60 (MM 0.000 TO MM 2.395)	12,645.60	2.395
TOTAL ROADWAY LENGTH =	29,456.16	5.579
TOTAL PROJECT LENGTH =	29,456.16	5.579

TRAFFIC DATA

HIGHWAY SECTION	AADT		DHV		CUM. ESALS (2009-2019)	CUM. ESALS (2009-2029)
	2009	2019	2009	2019		
BEGIN OF PROJECT TO BEAR MOUNTAIN ROAD (TH #6) (MM 3.098 TROY - MM 0.197 NEWPORT)	1700	1800	190	200	481,000	1,122,000
BEAR MOUNTAIN ROAD (TH #6) TO END OF PROJECT (MM 0.197 NEWPORT - MM 2.395 NEWPORT)	2500	2700	280	300	511,000	1,299,000

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES RECLAIMING AND/OR COLD PLANING SEGMENTS OF THE EXISTING HIGHWAY AND OVERLAYING WITH A BINDER AND/OR LEVELING COURSE AND A WEARING COURSE, WITH PAVEMENT MARKINGS, GUARDRAIL IMPROVEMENTS, DRAINAGE IMPROVEMENTS AND INCIDENTAL ITEMS.



BITUMINOUS CONCRETE PAVEMENT SUPERPAVE MIXTURE DESIGN CRITERIA	
DESIGN LANE / DESIGN LIFE ESAL	1,299,000
DESIGN NUMBER OF GYRATIONS	65
PERFORMANCE GRADE ASPHALT BINDER	SEE SECTION 490 GENERAL SPECIAL PROVISIONS

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT.  
CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

CONVENTIONAL SYMBOLS	
COUNTY LINE	
TOWN LINE	
LIMITS OF ACCESS	
POINT OF ACCESS	
FENCE LINE	
STONE WALL	
TRAVELED WAY	
GUARD RAIL	
RAILROAD	
SURVEY LINE	
CULVERT	
POWER POLE	
TELEPHONE POLE	
TREES	
CONTROL OF ACCESS	
PROPERTY LINE	
R.O.W. TAKING LINE	
SLOPE RIGHTS	
TOP OF CUT	
TOE OF SLOPE	

**DuBois & King inc.**

engineering    planning    management    development

---

DATUM

VERTICAL N/A

HORIZONTAL N/A

DIRECTOR OF PROGRAM DEVELOPMENT	
APPROVED <u>[Signature]</u>	DATE <u>7-20-09</u>
PROJECT MANAGER : TED DOMEY	
PROJECT NAME : TROY-NEWPORT	
PROJECT NUMBER : STP 2613(1)	
SHEET 1 OF 31 SHEETS	

PLOTTED: 20-JUL-2009

INDEX OF SHEETS

1	TITLE SHEET
2	INDEX OF SHEETS
3-5	PROJECT TYPICAL SHEETS
6	BRIDGE DETAIL SHEET
7	BRIDGE LOCATION AND DETAIL SHEET
8	HANDWORK FOR DRIVES DETAIL SHEET
9-11	QUANTITY SHEETS 1-3
12	ITEM DETAIL SUMMARY SHEET
13	DITCH CLEANING DETAIL SHEET
14-23	PROJECT LAYOUT SHEETS
24-25	SUPERELEVATION BANKING DIAGRAM SHEETS
26-30	TRAFFIC SIGN SUMMARY SHEETS
31	CONSTRUCTION APPROACH SIGNING SHEET

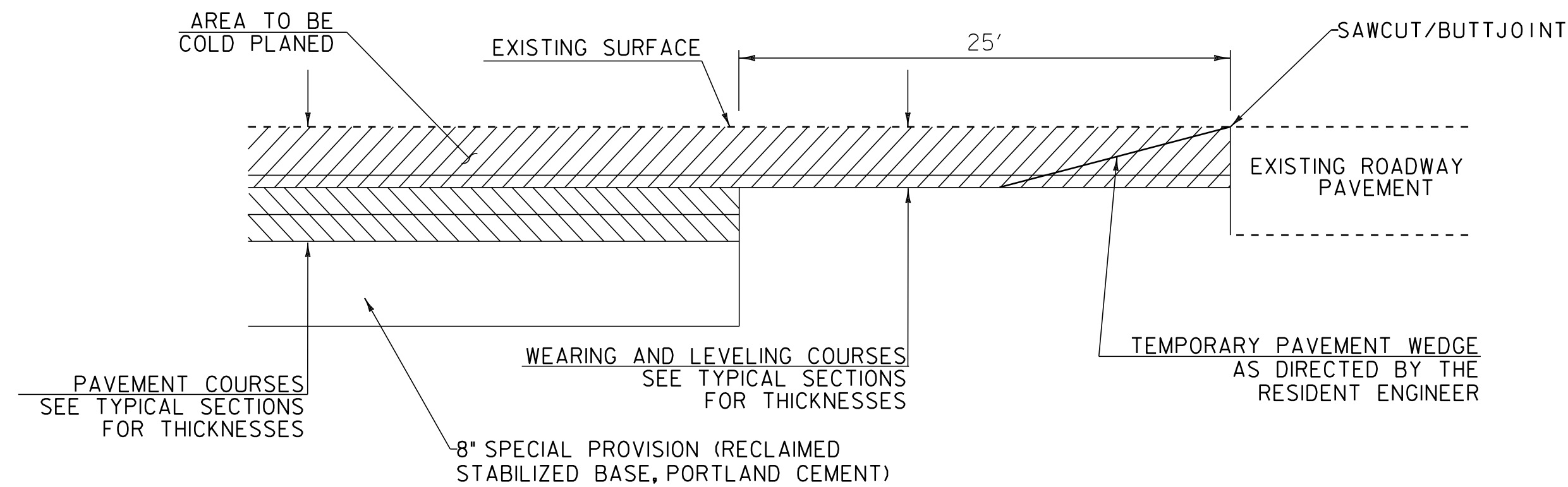
VAOT STANDARDS

B-1	BANKING TABLES	06/01/94
D-3	TREATED GUTTERS	06/01/94
D-8	REINFORCED CONCRETE DROP INLET WITH PRECAST COVER REINFORCED CONCRETE DROP INLET WITH GRATE (BOTTOM SECTION)	01/03/00
D-9	REINFORCED CONCRETE DROP INLET WITH VERTICAL CURB REINFORCED CONCRETE DROP INLET THROAT ADAPTER	06/01/94
D-15	PRECAST REINFORCED CONCRETE CATCH BASIN W/ CAST IRON GRATE PRECAST REINFORCED MAN HOLE W/ CAST IRON GRATE CAST IRON GRATE WITH FRAME, TYPE D CAST IRON GRATE WITH FRAME, TYPE E	06/01/94
E-100	CONSTRUCTION APPROACH SIGNS	01/02/04
E-100A	SIDE ROAD CONSTRUCTION APPROACH SIGNS	01/02/04
E-101	CONSTRUCTION SIGN DETAILS	05/30/03
E-102	CONSTRUCTION SIGN DETAILS	06/30/03
E-102A	CONSTRUCTION SIGN DETAILS	05/01/04
E-103	MAINLINE TRAFFIC CONTROL DIVIDED HIGHWAY ONE LANE CLOSED	03/01/04
E-106	TRAFFIC CONTROL - MISCELLANEOUS DETAILS	03/01/04
E-107	DELINEATION, BARRICADES AND DETOURS FOR CONSTRUCTION AREAS	06/30/03
E-107A	BREAKAWAY BARRICADE DETAILS	06/08/09
E-108A	CONSTRUCTION ZONE LONGITUDINAL DROP OFFS FOR PAVING	06/08/09
E-110	MAJOR MAINTENANCE OPERATION LANE CLOSURE	08/08/95
E-111	MINOR MAINTENANCE OPERATION	03/11/97
E-112	TYPICAL MOVING MAINTENANCE OPERATIONS	01/23/97
E-121	STANDARD SIGN PLACEMENT - CONVENTIONAL ROAD	08/08/95
E-136B	STATE ROUTE MARKER SIGN DETAILS	08/08/95
E-138	MILEMARKER DETAILS - STATE & TOWN HIGHWAYS	05/30/03
E-141	REGULATORY SIGN DETAILS	09/20/95
E-142	REGULATORY SIGN DETAILS	09/20/95
E-143	REGULATORY SIGN DETAILS	06/15/04
E-151	WARNING SIGN DETAILS	05/01/04
E-152	WARNING SIGN DETAILS	05/01/04
E-164	SQUARE STEEL SIGN POST	06/08/09
E-190	RAILROAD CROSSING SIGNS AND PAVEMENT MARKINGS	06/30/03
E-191	PAVEMENT MARKING DETAILS	02/01/99
E-193	PAVEMENT MARKING DETAILS	08/18/95
G-1	STEEL BEAM GUARDRAIL WITH STEEL POSTS	01/03/00
G-1D	STEEL BEAM GUARDRAIL APPROACH END TERMINAL STEEL BEAM GUARDRAIL TRAILING END TERMINAL ANCHOR FOR STEEL BEAM GUARDRAIL STEEL BEAM MEDIAN BARRIER	01/03/00
G-4	YIELDING MARKER POSTS	06/01/94
G-19	GENERIC PLANS FOR GUARDRAIL END TERMINALS	11/15/02
J-3	MAILBOX SUPPORT DETAIL	08/07/95
SB-R4B-82	GUARDRAIL APPROACH SECTION TYPE I & TYPE II	03/30/88
SB-R6-82	BRIDGE RAILING, HEAVY DUTY STEEL BEAM	01/06/95

INDEX  
OF  
SHEETS

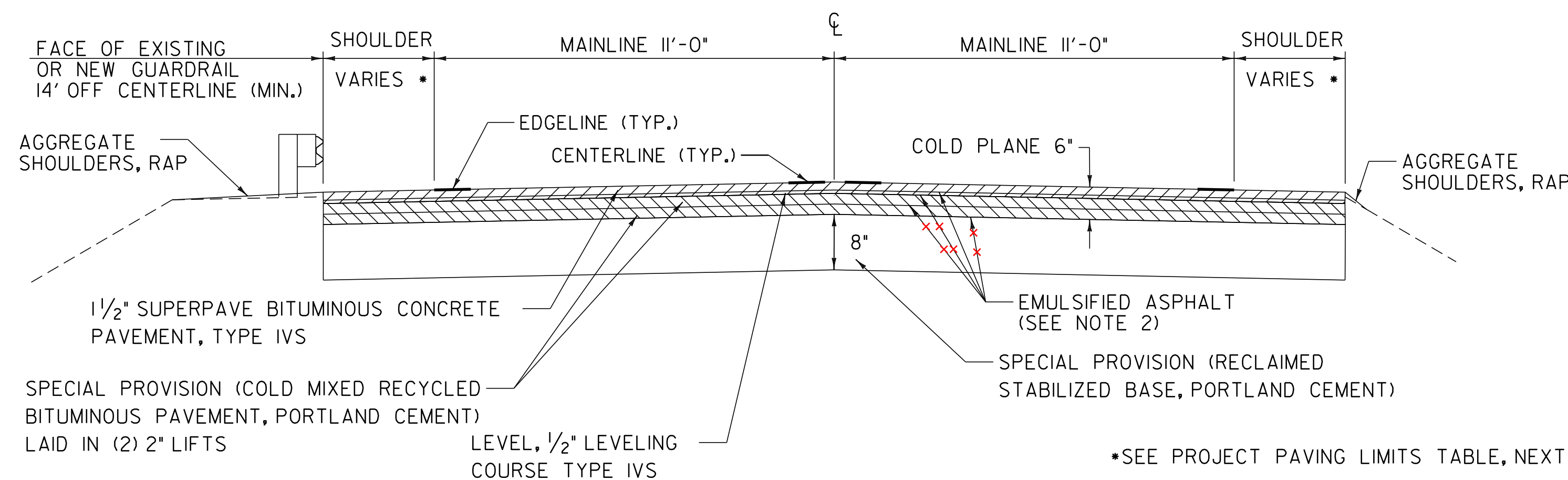
PROJECT NAME: TROY-NEWPORT  
PROJECT NUMBER: STP 2613(I)

FILE NAME: 06B056.dgn	PLOT DATE: 25-OCT-2011 12:16
PROJECT LEADER: CDL	DRAWN BY: SJL
DESIGNED BY: SJL	CHECKED BY: EPD
PLOT FILE: 06b056*02.i	SHEET 2 OF 31



**APPROACH AREA DETAIL (BEGIN AND END PROJECT)**

TROY  
 STA. 163+57.44  
 NEWPORT  
 STA. 126+45.60



**RECLAIMED STABILIZED TYPICAL SECTION**

TROY  
 STA. 188+00 TO 301+80  
 STA. 304+80 TO 333+96  
 NEWPORT  
 STA. 0+00 TO STA. 126+45.60

\*SEE PROJECT PAVING LIMITS TABLE, NEXT SHEET

**NOTES:**

1. THE WEARING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. THE LEVELING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. ALL ASPHALT CEMENT USED IN THE SUPERPAVE BITUMINOUS CONCRETE PAVEMENT SHALL BE AS SPECIFIED IN SECTION 490 OF THE GENERAL SPECIAL PROVISIONS.
2. EMULSIFIED ASPHALT, TYPE RS-1, SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES AND BETWEEN ALL COURSES OF PAVEMENT EXCLUDING RECLAIMED SURFACES AT THE RATE OF 0.025 GAL/SY OR AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT WILL BE UNDER ITEM 404.65, EMULSIFIED ASPHALT.
3. SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = +/- 1/4 INCH (TOTAL PAVEMENT THICKNESS EXCLUDING LEVELING).
4. COLD PLANING SHALL BE COMPLETED ACCORDING TO THE TYPICAL OR AS OTHERWISE NOTED ON THE PLANS. A FULL DEPTH BUTT JOINT SHALL BE CONSTRUCTED AT THE BEGINNING AND END OF THE PROJECT, ALL SIDE ROAD APPROACHES AS DENOTED ON THE PROJECT PLANS OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. SAWCUTTING WILL NOT BE PAID FOR SEPARATELY. IT SHALL BE CONSIDERED INCIDENTAL TO ITEM 900.675 SPECIAL PROVISION (COLD MIXED RECYCLED BITUMINOUS PAVEMENT, PORTLAND CEMENT). PAYMENT FOR COLD PLANING INCLUDED UNDER ITEM 900.675 SPECIAL PROVISION (COLD MIX RECYCLED BITUMINOUS PAVEMENT, PORTLAND CEMENT).
5. QUANTITIES FOR ITEM 604.412, "REHAB DI, CB OR MH CLASS I", ITEM 604.415, "REHAB DI, CB OR MH CLASS II", ITEM 604.418, "REHAB DI, CB OR MH CLASS III" AND ITEM 604.40, "CHANGING ELEVATION OF DROP INLETS, CATCH BASINS, OR MANHOLES HAVE BEEN INCLUDED TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER. ALL DI'S SHALL BE RAISED OR REHABILITATED SUCH THAT THE NEW GRATE ELEVATION IS LEVEL WITH THE SURROUNDING TERRAIN. DRAINAGE STRUCTURES CALLING FOR REHAB HAVE BEEN EVENLY ESTIMATED BETWEEN ITEMS 604.412, 604.415 AND 604.418 FOR ESTIMATING PURPOSES.
6. ESTIMATED QUANTITIES OF ITEM 608.25, "ALL PURPOSE EXCAVATOR RENTAL, TYPE I", 608.37, "TRUCK RENTAL", AND 608.40, "LOADER RENTAL, TYPE I" HAVE BEEN INCLUDED FOR THE PROVISION OF CONSTRUCTING GUARDRAIL END SECTIONS WITH EXCAVATED DITCHING MATERIAL. AN ESTIMATED QUANTITY OF 203.30, "EARTH BORROW" HAS BEEN INCLUDED FOR USE WITH THE GUARDRAIL END SECTIONS. 25 CY OF ITEM 203.30, "EARTH BORROW" HAS BEEN ESTIMATED FOR EACH NEW GUARDRAIL END SECTION. GUARDRAIL END SECTIONS SHALL BE CAPPED WITH AN ESTIMATED 3 INCH DEPTH OF ITEM 402.12, "AGGREGATE SHOULDERS" UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THE QUANTITIES INCLUDED REFLECT 5 TONS OF ITEM 402.12, "AGGREGATE SHOULDERS" FOR EACH NEW GUARDRAIL END SECTION. ITEM 653.20, "TEMPORARY EROSION MATTING" SHALL BE PLACED ON SLOPES GREATER THAN 1:6 CREATED BY THE GUARDRAIL END SECTIONS. THE QUANTITIES INCLUDED REFLECT 25 SY OF ITEM 653.20, "TEMPORARY EROSION MATTING" FOR EACH NEW GUARDRAIL END SECTION. PRIOR TO THE PLACEMENT OF TEMPORARY EROSION MATTING, THE AREA SHALL BE TOPSOILED AND SEEDED USING ITEM 651.35, "TOPSOIL" AND ITEM 651.15, "SEED".
7. YIELDING MARKER POSTS ARE TO BE USED TO REPLACE NON-YIELDING MARKER POSTS AT PIPE INLETS AND OUTLETS ONLY.
8. STEEL BEAM GUARDRAIL WITH STEEL POSTS SHALL BE USED ON THIS PROJECT.
9. ITEM 415.25 "EMULSIFIED ASPHALT, COLD MIX" SHALL BE USED AS A LIQUID BINDER IN THE PORTLAND CEMENT STABILIZED COLD MIX AS DIRECTED BY THE RESIDENT ENGINEER. AN ESTIMATED QUANTITY OF EMULSIFIED ASPHALT AT A RATE OF 2.0 GAL/SY HAS BEEN INCLUDED IN THE PLANS FOR THE 4" BASE COURSE.
10. THE PORTLAND CEMENT STABILIZED COLD MIX SHALL BE PRODUCED FROM THE COLD PLANE GRINDINGS GENERATED FROM THE PROJECT AND PAID FOR UNDER ITEM 900.675 SPECIAL PROVISION (COLD MIXED RECYCLED BITUMINOUS PAVEMENT, PORTLAND CEMENT). IF THERE IS NOT A SUFFICIENT AMOUNT OF COLD PLANE GRINDINGS AVAILABLE TO PRODUCE THE 4" LIFT OF PORTLAND CEMENT STABILIZED COLD MIX, THE CONTRACTOR SHALL OBTAIN THE GRINDINGS FROM ANOTHER SOURCE. THE COST ASSOCIATED WITH PROVIDING ADDITIONAL COLD PLANE GRINDINGS SHALL BE INCIDENTAL TO ITEM 900.675 SPECIAL PROVISION (COLD MIXED RECYCLED BITUMINOUS PAVEMENT, PORTLAND CEMENT). AN ESTIMATED QUANTITY OF ITEM 900.680 SPECIAL PROVISION (PORTLAND CEMENT FOR COLD MIXED RECYCLING) AT A QUANTITY OF 2% OF PORTLAND CEMENT FOR COLD MIXED RECYCLING HAS BEEN INCLUDED.
11. STABILIZING AGENT FOR THE RECLAIMED STABILIZED BASE WILL BE PORTLAND CEMENT. PAYMENT WILL BE MADE UNDER ITEM 900.680 "SPECIAL PROVISION (PORTLAND CEMENT FOR BASE STABILIZATION)".
12. THREE FEET OF BACKING IS REQUIRED BEHIND THE FACE OF GUARDRAIL WITH 6 FOOT POSTS. IF THIS CAN NOT BE OBTAINED THEN 8 FOOT POSTS SHALL BE USED. THESE ITEMS SHALL BE PAID FOR UNDER ITEM 621.20 "STEEL BEAM GUARDRAIL, GALVANIZED", ITEM 621.205 "STEEL BEAM GUARDRAIL, GALVANIZED W/8 FOOT POSTS".
13. IN AREAS WHERE A UTILITY POLE OR SIMILAR OBJECT IS LOCATED WITHIN 4 FEET BEHIND GUARDRAIL, A REDUCED POST SPACING SHALL BE USED. THE REDUCED SPACING IS DETAILED ON PROJECT TYPICAL SHEET 5.
14. ESTIMATED QUANTITY FOR ITEM 619.17 YIELDING MARKER POSTS HAS BEEN INCLUDED TO DELINEATE PIPE INLETS, PIPE OUTLETS AND DROP INLETS LOCATED OUTSIDE THE PAVEMENT SURFACES OR AS DIRECTED BY THE RESIDENT ENGINEER.
15. PRIOR TO RECLAIMING, ANY EXISTING SHOULDER MATERIAL DEEMED UNSUITBLE BY THE RESIDENT ENGINEER WILL BE EXCAVATED TO THE DEPTH OF RECLAIMING OR AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATED MATERIAL WILL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT AS DIRECTED BY THE RESIDENT ENGINEER. THIS WORK WILL BE PAID FOR USING THE APPROPRIATE RENTAL ITEMS. THE METHOD OF REMOVAL AND THE USE OF RENTAL ITEMS SHALL BE APPROVED BY THE RESIDENT ENGINEER. PRIOR TO ANY WORK BEING DONE, MATERIAL REMOVAL SHALL BE REPLACED WITH ITEM 301.40 SUBBASE, RAP. AN ADDITIONAL QUANTITY OF 301.40 SUBBASE, RAP HAS BEEN INCLUDED TO CORRECT SUPERELEVATION AND GRADATION DEFICIENCIES WITHIN THE RECLAIMED SECTION. AN ESTIMATED THICKNESS OF 2" HAS BEEN INCLUDED FOR THE ENTIRE RECLAIMED SURFACE AREA. IF THERE IS NOT SUFFICIENT AMOUNT OF COLD PLANE GRINDINGS AVAILABLE TO COMPLETE THESE REPAIRS, THE CONTRACTOR SHALL OBTAIN COLD PLANE GRINDINGS FROM ANOTHER SOURCE. THE COST ASSOCIATED WITH PROVIDING ADDITIONAL COLD PLANE GRINDINGS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 301.40 SUBBASE, RAP.
16. ALL EDGES OF PAVEMENT WITHIN THE COLD PLANE SECTION SHALL BE BACKED UP TO FULL HEIGHT WITH COLD PLANE GRINDINGS AS DIRECTED BY RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.13 AGGREGATE SHOULDERS, RAP. ADDITIONAL MATERIAL REQUIRED AFTER THE COLD PLANE GRINDINGS HAVE BEEN USED WILL BE PAID FOR UNDER ITEM 402.12 AGGREGATE SHOULDERS.
17. ITEM 609.10 DUST CONTROL WITH WATER IS AN ESTIMATED QUANTITY TO BE USED AS DUST CONTROL ALONG THE RECLAIMED SECTION. ITEM 609.15 DUST AND ICE CONTROL WITH CALCIUM CHLORIDE IS AN ESTIMATED QUANTITY TO BE USED AS DUST CONTROL FOR SIDEROADS.
18. A QUANTITY HAS BEEN ESTIMATED FOR ITEM 201.31 THINNING AND TRIMMING FOR SIGNS.
19. A QUANTITY HAS BEEN ESTIMATED FOR ITEM 617.10 RELOCATE MAILBOX, SINGLE SUPPORT.

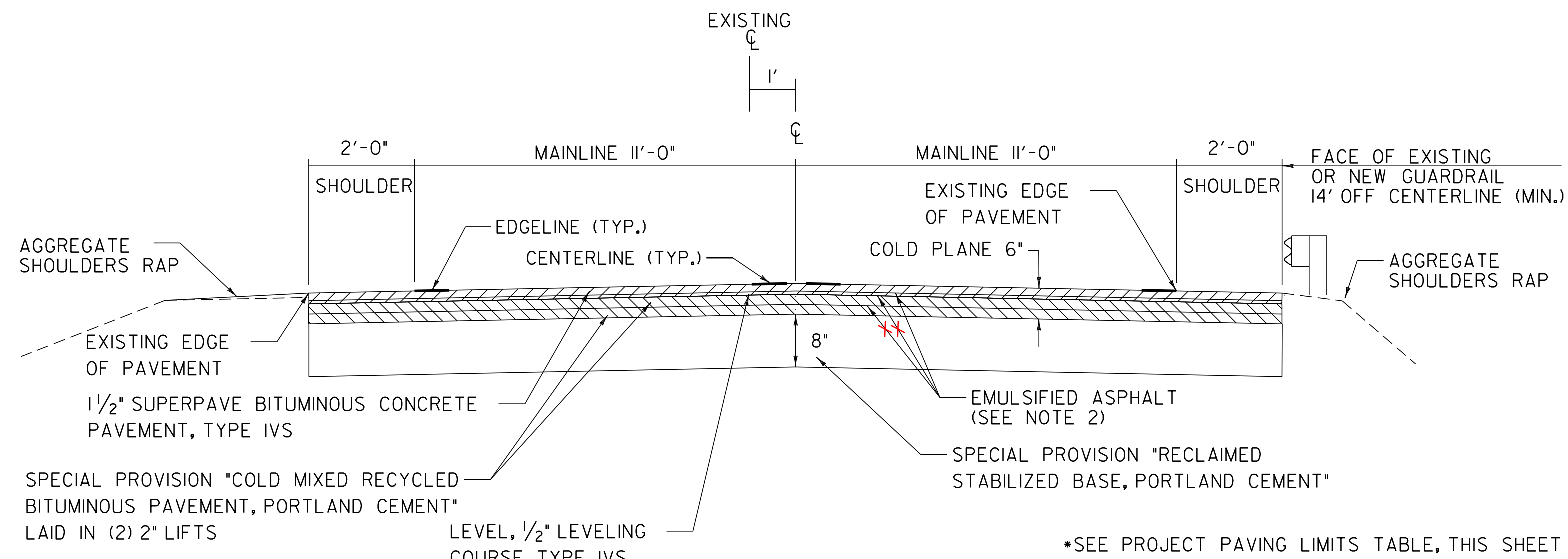
**PROJECT  
 TYPICAL  
 SHEET 1**

PROJECT NAME: TROY-NEWPORT  
 PROJECT NUMBER: STP 2613(1)

FILE NAME: 06B056.dgn  
 PROJECT LEADER: CDL  
 DESIGNED BY: SJL  
 PLOT FILE: 06b056\*03.1

PLOT DATE: 25-OCT-2011 12:17  
 DRAWN BY: SJL  
 CHECKED BY: EPD  
 SHEET 3 OF 31

**DETAILS ARE NOT TO SCALE**



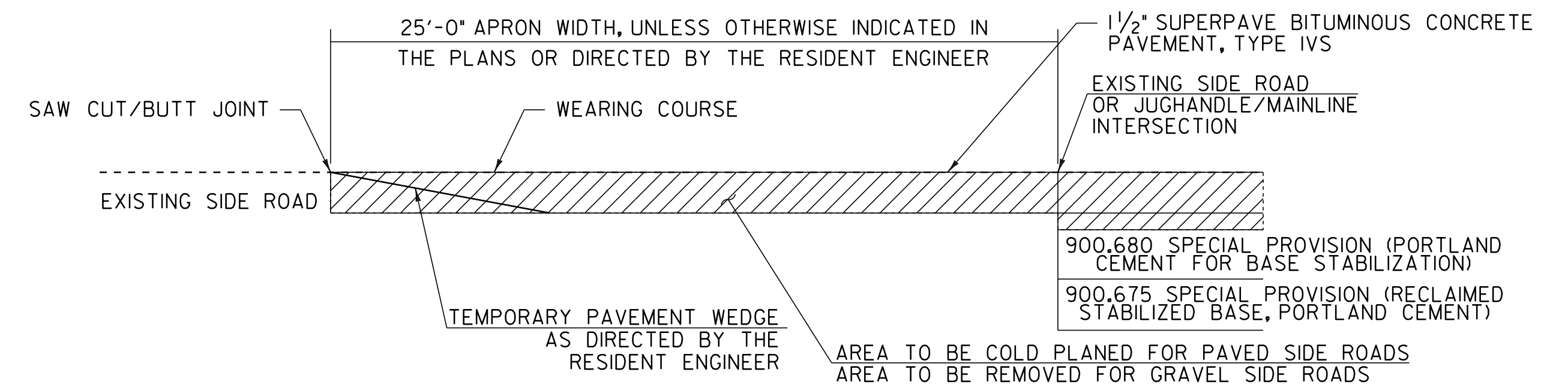
**RECLAIMED STABILIZED TYPICAL SECTION**

TROY  
STA. 163+57.44 TO 188+00.00

NOTE: WIDEN RIGHT SIDE ONLY. SHIFT CENTERLINE APPROXIMATELY 1' USING A 100:1 TAPER TO CREATE DESIRED TYPICAL

**PROJECT PAVING LIMITS**

TOWN AND ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING TON	NOTES
TROY	163+57.44	188+00.00	2' - 11' - 11' - 2'	1 1/2"	198.46	COLD PLANE 6", RECLAIM 8" WITH S.P. (RECLAIMED STABILIZED BASE, PORTLAND CEMENT), THEN PAVE WITH 4" S.P. (COLD MIXED RECYCLED BITUMINOUS PAVEMENT) THEN LEVEL AND PAVE WITH 1 1/2" TYPE IVS
	188+00.00	190+00.00	2 1/2' - 11' - 11' - 2 1/2'	1 1/2"	16.88	COLD PLANE 6", RECLAIM 8" WITH S.P. (RECLAIMED STABILIZED BASE, PORTLAND CEMENT), THEN PAVE WITH 4" S.P. (COLD MIXED RECYCLED BITUMINOUS PAVEMENT) THEN LEVEL AND PAVE WITH 1 1/2" TYPE IVS
	190+00.00	301+80.00	3' - 11' - 11' - 3'	1 1/2"	1014.48	COLD PLANE 6", RECLAIM 8" WITH S.P. (RECLAIMED STABILIZED BASE, PORTLAND CEMENT), THEN PAVE WITH 4" S.P. (COLD MIXED RECYCLED BITUMINOUS PAVEMENT) THEN LEVEL AND PAVE WITH 1 1/2" TYPE IVS
	304+08.00	307+00.00	3' - 11' - 11' - 3'	1 1/2"	26.49	COLD PLANE 6", RECLAIM 8" WITH S.P. (RECLAIMED STABILIZED BASE, PORTLAND CEMENT), THEN PAVE WITH 4" S.P. (COLD MIXED RECYCLED BITUMINOUS PAVEMENT) THEN LEVEL AND PAVE WITH 1 1/2" TYPE IVS
	307+00.00	308+50.00	TRANSITION FROM 3' TO 6' SHOULDERS	1 1/2"	14.53	COLD PLANE 6", RECLAIM 8" WITH S.P. (RECLAIMED STABILIZED BASE, PORTLAND CEMENT), THEN PAVE WITH 4" S.P. (COLD MIXED RECYCLED BITUMINOUS PAVEMENT) THEN LEVEL AND PAVE WITH 1 1/2" TYPE IVS
	308+50.00	333+96.00	6' - 11' - 11' - 6'	1 1/2"	270.51	COLD PLANE 6", RECLAIM 8" WITH S.P. (RECLAIMED STABILIZED BASE, PORTLAND CEMENT), THEN PAVE WITH 4" S.P. (COLD MIXED RECYCLED BITUMINOUS PAVEMENT) THEN LEVEL AND PAVE WITH 1 1/2" TYPE IVS
NEWPORT	0+00.00	126+45.60	6' - 11' - 11' - 6'	1 1/2"	1343.60	COLD PLANE 6", RECLAIM 8" WITH S.P. (RECLAIMED STABILIZED BASE, PORTLAND CEMENT), THEN PAVE WITH 4" S.P. (COLD MIXED RECYCLED BITUMINOUS PAVEMENT) THEN LEVEL AND PAVE WITH 1 1/2" TYPE IVS



**TRANSITION AREA FOR SIDE ROADS**

SEE LAYOUT SHEETS FOR LOCATIONS OF ALL SIDE ROADS

**SEEDING FORMULA**

RATE: DOUBLE IF HYDROSEEDING

% WT.	LBS./A.	NAME	PUR %	GERM %
38	32	CREeping RED FESCUE	98	90
29	24	SPARTAN HARD FESCUE	95	85
15	12	AZAY SHEEP'S FESCUE	95	87
15	12	ANNUAL RYE GRASS	95	90
3	--	INERTS	--	--
100.0	80 LB/A			

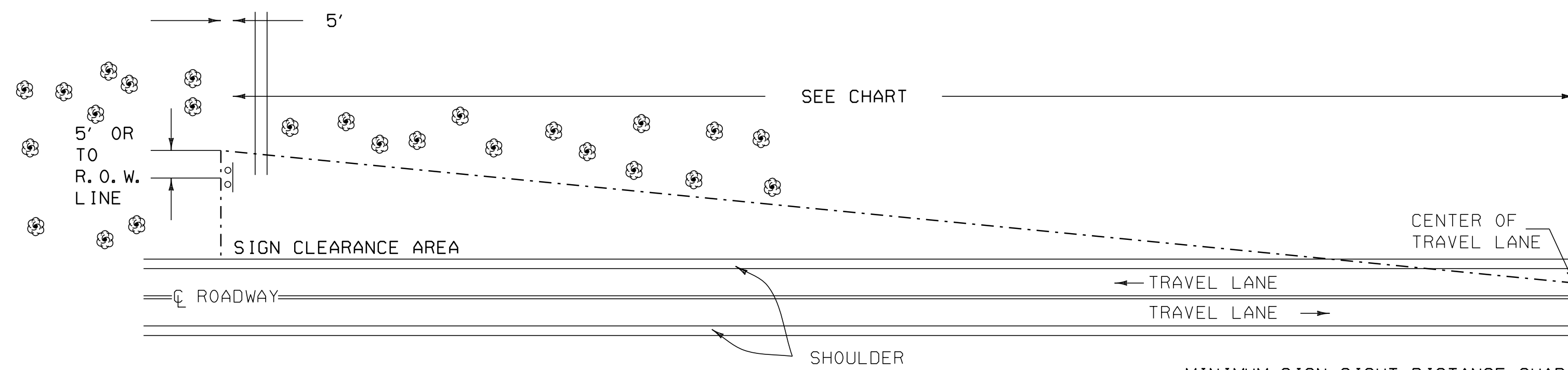
**GENERAL NOTES**

- SEED MIXTURE: SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS SEED.
- SEED: TO BE APPLIED PER SEEDING FORMULAS OR AS DIRECTED BY THE ENGINEER.
- FERTILIZER: FORMULA 10-20-10, TO BE USED WITH SEED, APPLIED AT THE RATE OF 500 LBS./ACRE. (HYDRO SEEDERS MAY USE 19-19-19 FORMULA).
- AGRICULTURAL LIMESTONE: TO BE APPLIED AT THE RATE OF 2 TONS/ACRE, OR AS DIRECTED BY THE ENGINEER.
- TOPSOIL: TO BE USED WITH SEED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.

**PROJECT TYPICAL SHEET 2**

DETAILS ARE NOT TO SCALE

PROJECT NAME: TROY-NEWPORT	PLOT DATE: 25-OCT-2011 12:17
PROJECT NUMBER: STP 2613(1)	DRAWN BY: SJL
FILE NAME: 06B056.dgn	CHECKED BY: EPD
PROJECT LEADER: CDL	SHEET 4 OF 31
DESIGNED BY: SJL	
PLOT FILE: 06b056*04.i	

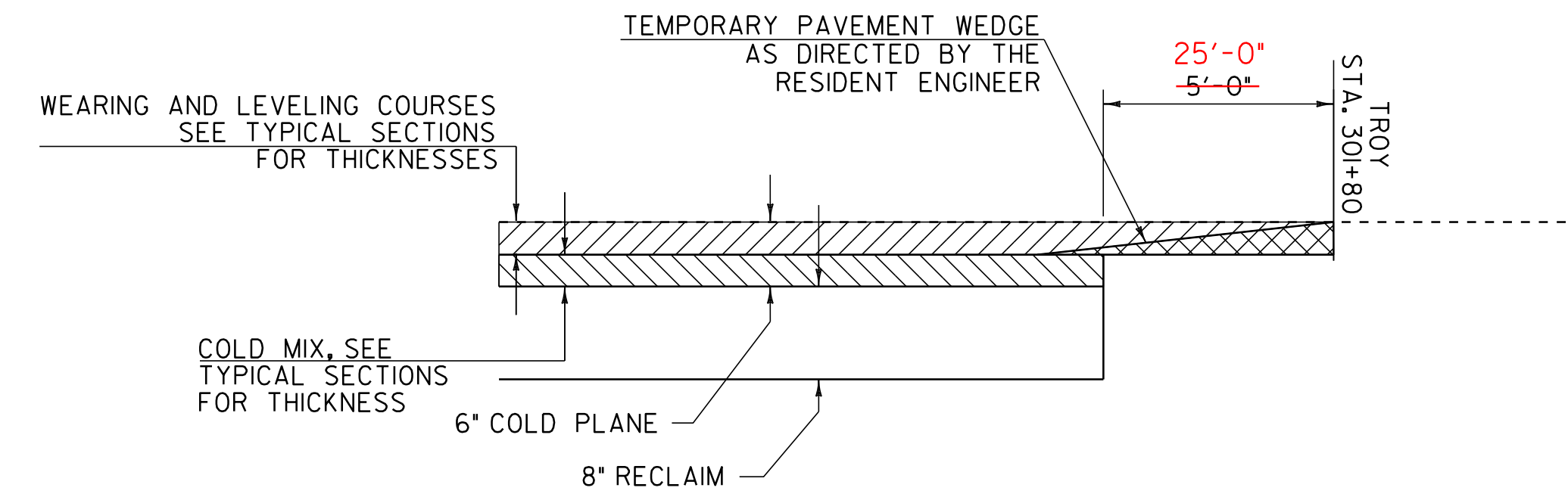


THE CONTRACTOR SHALL REMOVE ALL WOODY STEMMED GROWTH INCLUDING BRUSH, SAPLINGS, TREE LIMBS GROWING WITHIN OR PROJECTING INTO THE CLEARANCE AREA AND DOWN TO GROUND LEVEL OR AT LEAST 10 FT BELOW THE BOTTOM OF THE SIGN. PAYMENT WILL BE FOR THINNING AND TRIMMING FOR SIGNS, ITEM 201.31, AND PAID FOR PER EACH. NO CHEMICALS (POISONS OR DEFOLIANTS) ALLOWED.

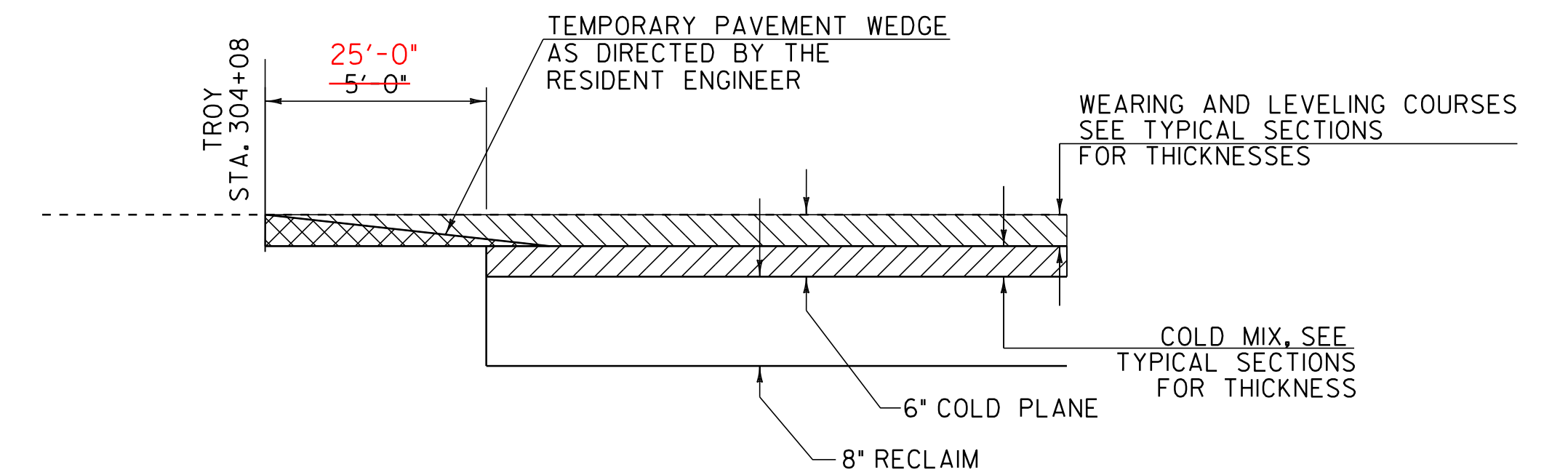
**CLEARING LIMITS FOR SIGNS ON CONVENTIONAL ROADS**

MINIMUM SIGN SIGHT DISTANCE CHART

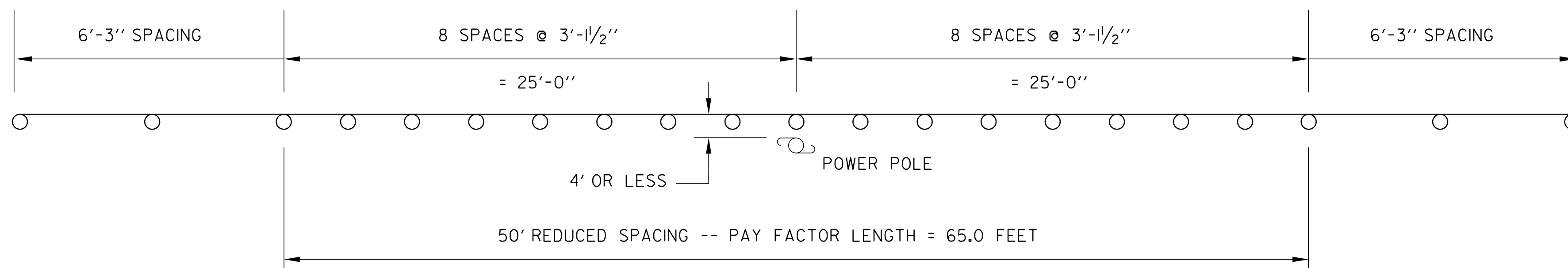
APPROACH SPEED (mph)	SIGHT DISTANCE (feet)
30 OR LESS	300
35	350
40	400
45	450
50	500
55	550



**TRANSITION FOR STOP STA. 301+80 (TROY)**



**TRANSITION FOR RESUME STA. 304+08 (TROY)**

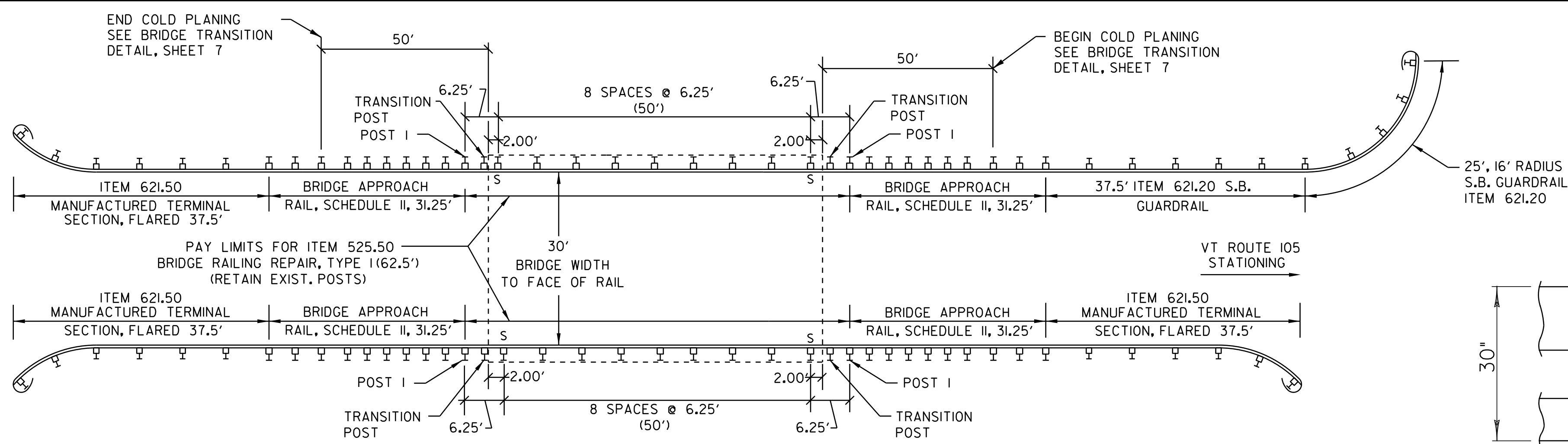


**REDUCED SPACING GUARDRAIL DETAIL**

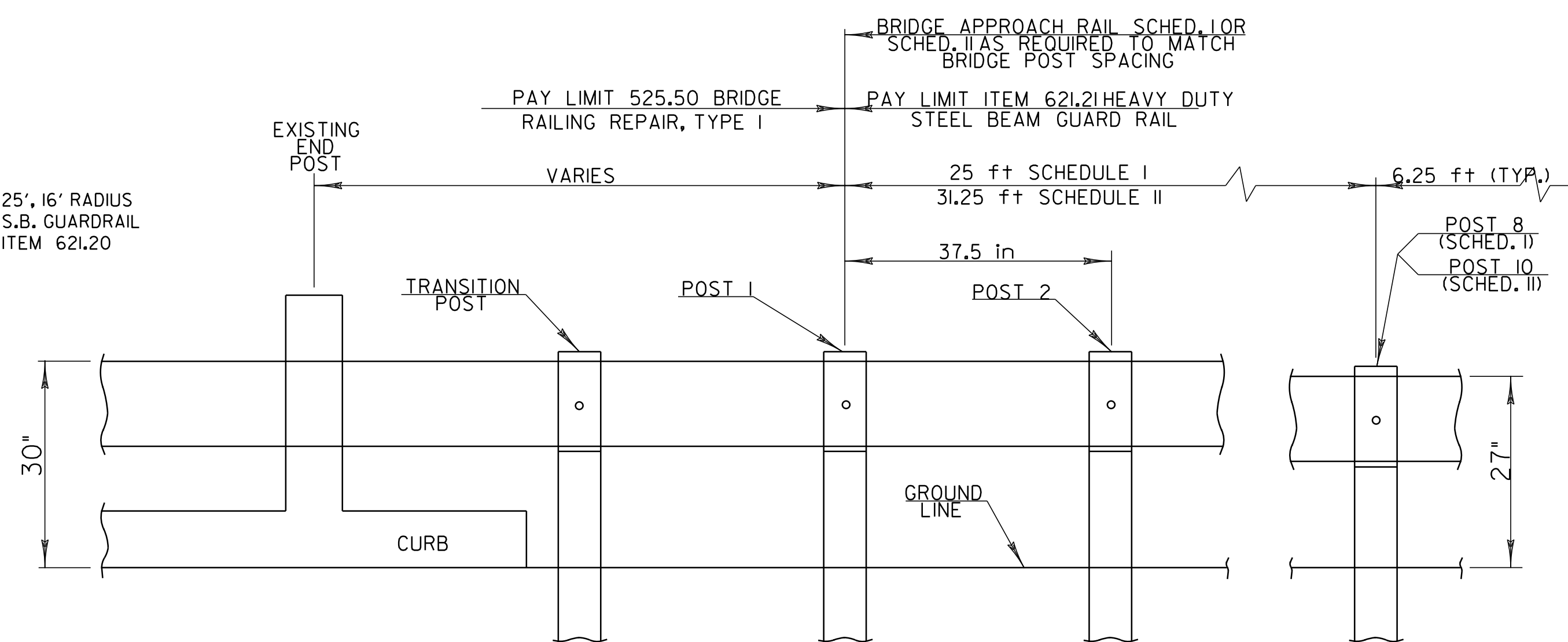
OBJECTS WITHIN 4' OF GUARDRAIL

DETAILS ARE NOT TO SCALE

<b>PROJECT TYPICAL SHEET 3</b>	PROJECT NAME: TROY-NEWPORT	PLOT DATE: 25-OCT-2011 12:17
	PROJECT NUMBER: STP 2613(I)	DRAWN BY: SJL
	FILE NAME: 06B056.dgn	CHECKED BY: EPD
	PROJECT LEADER: CDL	SHEET 5 OF 31
DESIGNED BY: SJL		
PLOT FILE: 06b056*05.1		



**NEWPORT BRIDGE #57**  
**FIELD STA. 9+09 - MM 0.172**



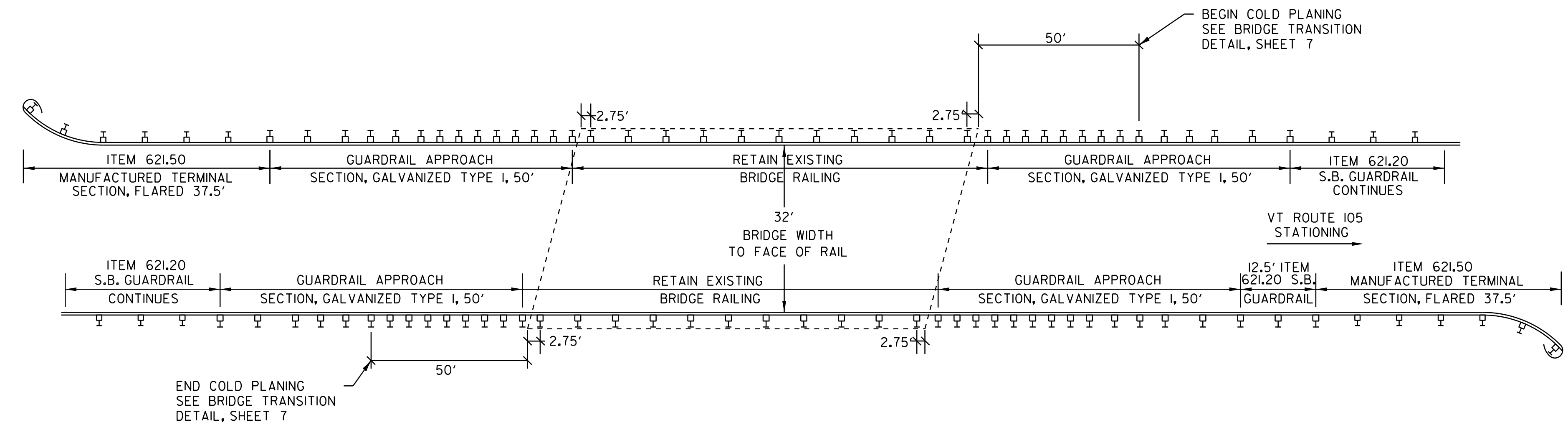
**BRIDGE APPROACH RAILING**

**NOTES:**

- BRIDGE RAIL SHALL BE ITEM 621.21 HEAVY DUTY STEEL BEAM RAIL.
- BRIDGE APPROACH RAIL HEIGHT SHALL BE TRANSITIONED TO NORMAL ROADWAY RAIL HEIGHT IN 25 FEET.
- APPROACH RAILING SHALL BE ITEM 621.21 HEAVY DUTY STEEL BEAM FOR LENGTHS OF 25 FEET OR 31.25 FEET DETERMINED BY THE SCHEDULE. SEE BRIDGE DETAILS.
- FOR BRIDGE RAILING, THE TRANSITION POST SHALL HAVE AN OFFSET BLOCK AND BE LOCATED AS CLOSE AS PRACTICAL TO THE MID-POINT BETWEEN THE BRIDGE END POST AND APPROACH RAIL POST I.
- SPICES SHALL LAP IN DIRECTION OF TRAFFIC FLOW.

**BRIDGE APPROACH RAILING**

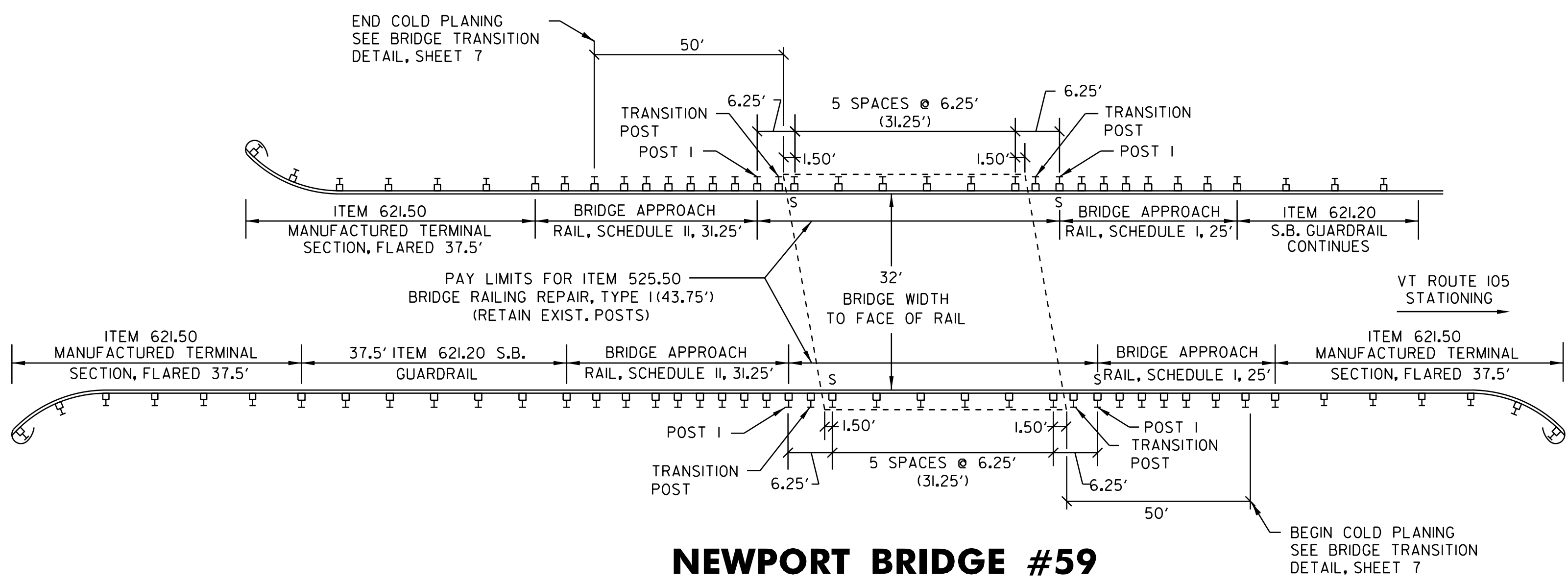
WHEN A RAIL PANEL SPLICE OCCURS AT POST NO. 1 USE SCHEDULE I FOR APPROACH RAILING WHEN A RAIL PANEL SPLICE OCCURS AT BRIDGE END POST USE SCHEDULE II FOR APPROACH RAILING.



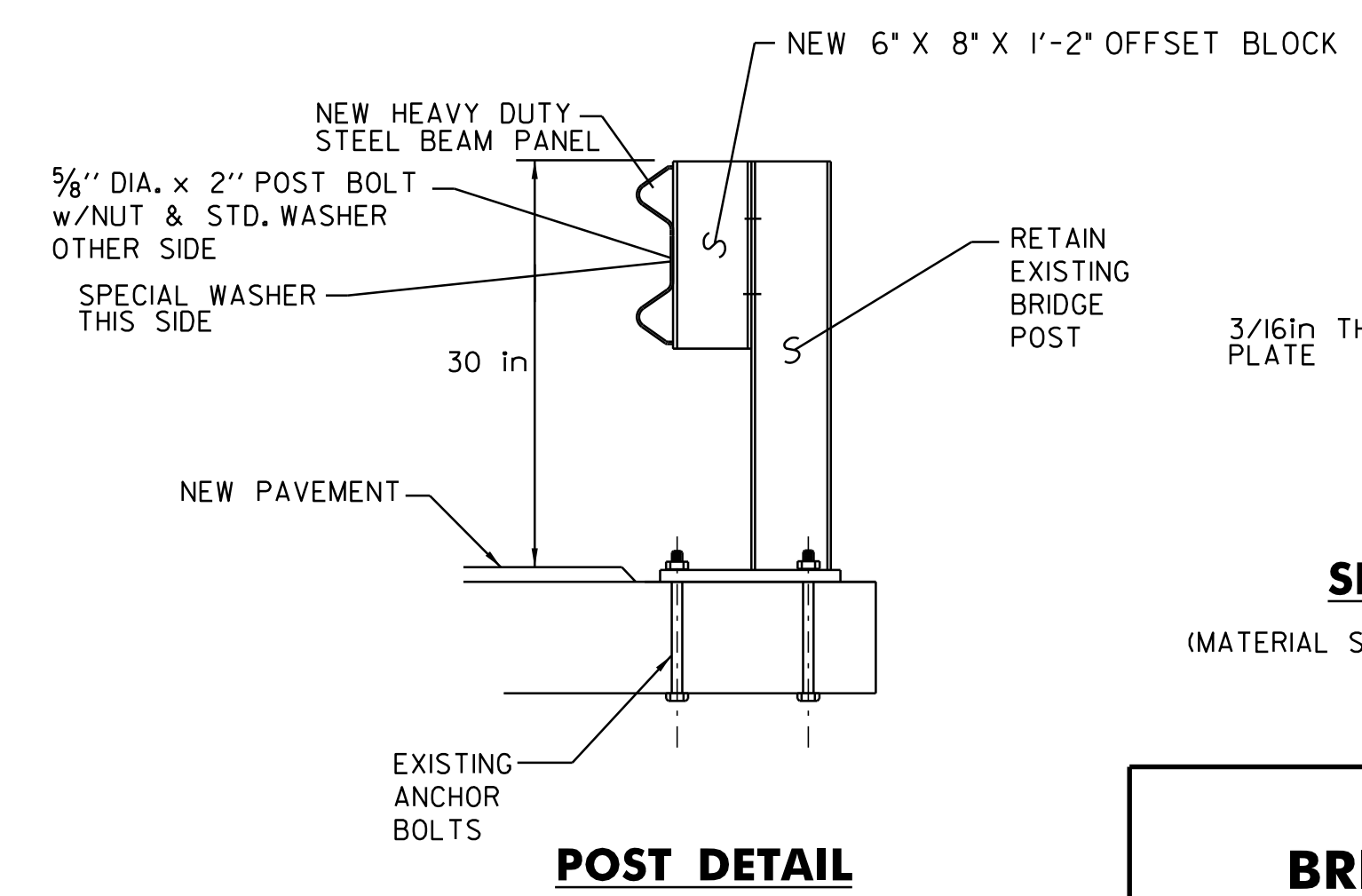
**NEWPORT BRIDGE #58**  
**FIELD STA. 53+94 - MM 1.022**

SCHEDULE I		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1 1/2"	1.4 x 12.5 ft
2	3'-1 1/2"	
3	3'-1 1/2"	
4	3'-1 1/2"	
5	4'-2"	1.2 x 12.5 ft
6	4'-2"	
7	4'-2"	
8	4'-2"	1.0 (TYP.)
9	6'-3" (TYP.)	

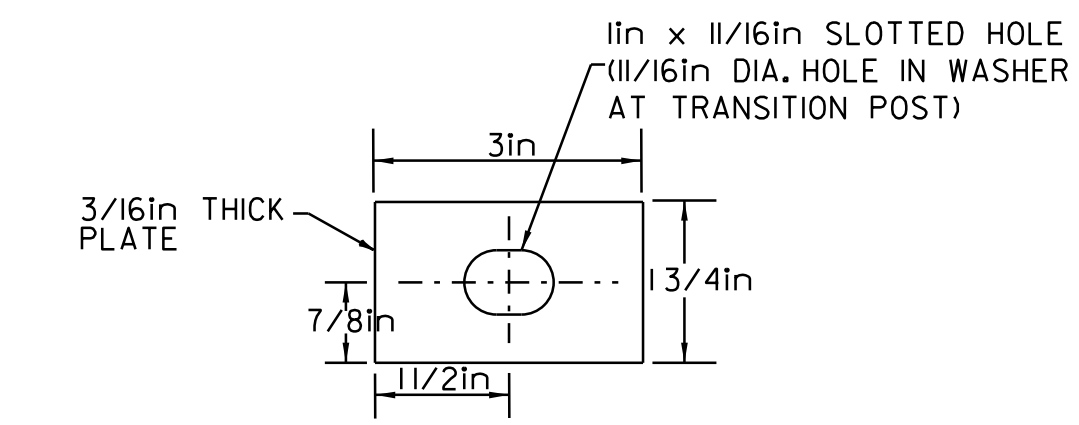
SCHEDULE II		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1 1/2"	1.4 x 18.75 ft
2	3'-1 1/2"	
3	3'-1 1/2"	
4	3'-1 1/2"	
5	3'-1 1/2"	
6	3'-1 1/2"	
7	4'-2"	1.2 x 12.5 ft
8	4'-2"	
9	4'-2"	
10	4'-2"	1.0 (TYP.)
11	6'-3" (TYP.)	



**NEWPORT BRIDGE #59**  
**FIELD STA. 64+16 - MM 1.215**



**POST DETAIL**

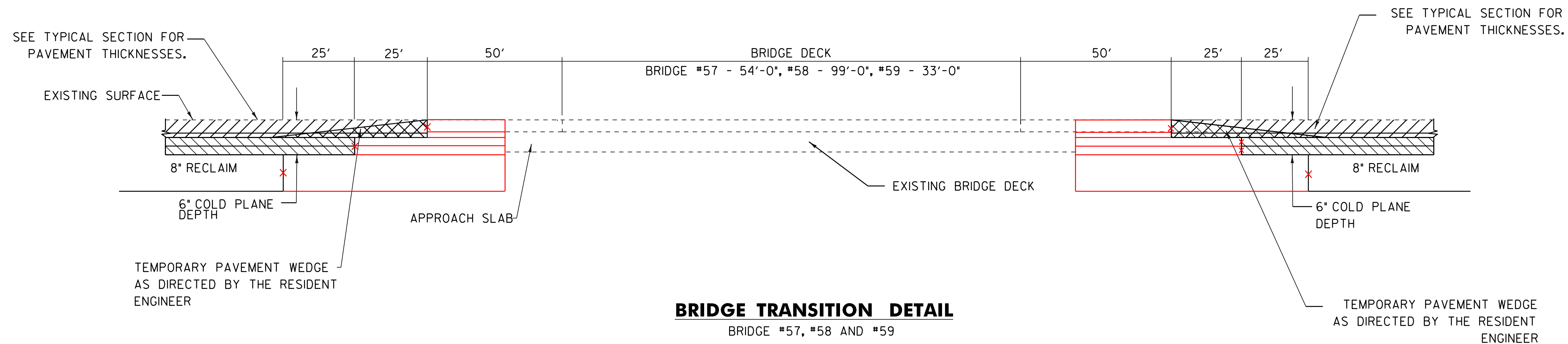


**SPECIAL WASHER DETAIL**

(MATERIAL SHALL MEET AASHTO M270/M270 GRADE 345W)

**DETAILS ARE NOT TO SCALE**

<b>BRIDGE DETAIL SHEET</b>	PROJECT NAME: TROY-NEWPORT	PLOT DATE: 25-OCT-2011 12:17
	PROJECT NUMBER: STP 2613(1)	DRAWN BY: RHB
	FILE NAME: 06B056.dgn	DESIGNED BY: RHB
	PLOT FILE: 06b056*06.1	CHECKED BY: EPD
		SHEET 6 OF 31



**NOTES:**

1. COLD PLANING AND PAVING LIMITS WILL BEGIN AND END ON EITHER SIDE OF THE BRIDGES AS SHOWN ON THE BRIDGE DETAIL. MEMBRANE WORK FOR THESE BRIDGES WILL BE PERFORMED UNDER THE STATEWIDE-NORTHEAST REGION BHF MEMB (19) CONTRACT.
2. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID THE ACCUMULATION OF DEBRIS IN THE DRAINAGE STRUCTURES LOCATED AT CURB LINE AND IN THE EXPANSION JOINTS. THE CONTRACTOR SHALL EXAMINE THESE BRIDGE FEATURES ON A REGULAR BASIS TO ENSURE THAT DEBRIS HAS NOT ACCUMULATED. ANY DEBRIS WHICH IS PRESENT SHALL BE REMOVED BY THE CONTRACTOR AT NO COST TO THE STATE.

<b>BRIDGE QUANTITY SHEET</b>					
STATION	POS.	BRIDGE NUMBER	OFFSET BLOCK	525.10 REMOVAL OF EXISTING RAILING	525.50 BRIDGE RAILING REPAIR, TYPE I
				LF	LF
<b>NEWPORT</b>					
8+78.75 - 9+41.25	LT	57	8'	62.5	62.5
8+78.75 - 9+41.25	RT	57	8'	62.5	62.5
63+88.75 - 64+32.50	LT	59	8'	43.75	43.75
63+98.75 - 64+42.50	RT	59	8'	43.75	43.75
SUBTOTAL				212.50	212.50
ROUNDING				-	-
TOTAL				212.50	212.50

•ESTIMATED QUANTITY

**DETAILS ARE NOT TO SCALE**

<b>BRIDGE LOCATION &amp; DETAIL SHEET</b>	PROJECT NAME: TROY-NEWPORT	
	PROJECT NUMBER: STP 2613(1)	
	FILE NAME: 06B056.dgn	PLOT DATE: 25-OCT-2011 12:17
	PROJECT LEADER: CDL	DRAWN BY: RHB
DESIGNED BY: RHB	CHECKED BY: EPD	
PLOT FILE: 06B056_7.i	SHEET 7 OF 31	









STATE OF VERMONT  
AGENCY OF TRANSPORTATION

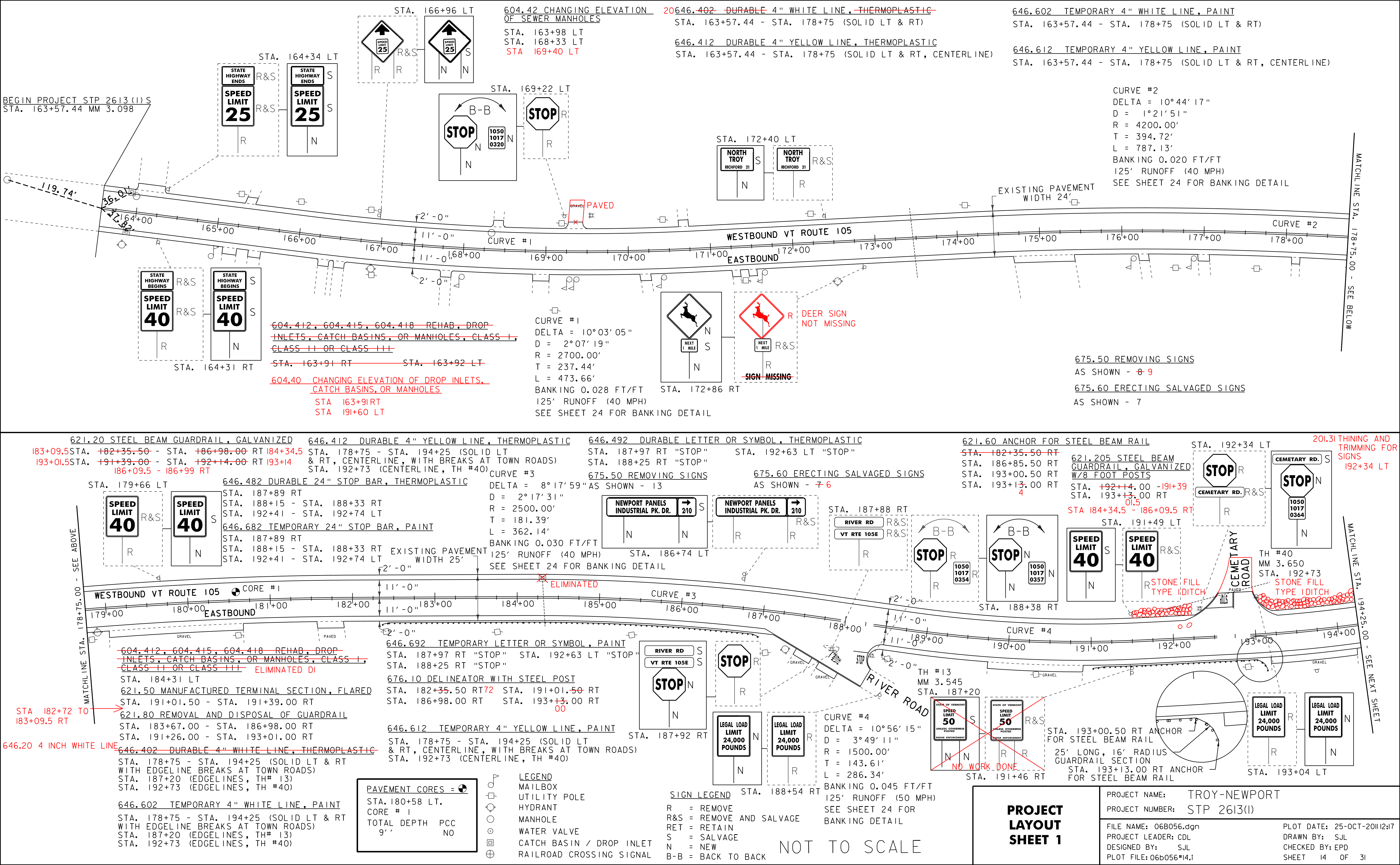
ITEM DETAIL SUMMARY SHEET

BEGIN STATION	END STATION	POS.	203.30 EARTH BORROW	621.215 WASTE DITCHING MATERIAL	402.12 AGG. SHOULD.	525.10 REMOVAL OF EXISTING RAILING	525.50 BRIDGE RAILING REPAIR, TYPE I	604.40 CHANGE ELEV. OF D. I., CB, OR MH	604.412 REHAB. D. I. CLASS I	604.415 REHAB. D. I. CLASS II	604.418 REHAB. D. I. CLASS III	604.42 CHANGE ELEV. OF SMH	619.17 YIELDING MARKER POSTS	621.20 STEEL BEAM G. R. GALV.	621.205 STEEL BEAM G. R. GALV. W/8FT POSTS	621.21 HD STEEL BEAM G. R. GALV.	621.50 MANUF. TERMINAL SECTION FLARED	621.60 ANCHOR FOR S. B. RAIL	621.70 G. R. APP. SECTION, GALV. TYPE I	621.80 REMOVE & DISP. OF G. R.	676.10 DEL IN. WITH STEEL POSTS	676.12 REMOVAL OF EXISTING DELINEATOR	REMARKS	
TROY			CY	<del>6Y</del>	TON	FT	FT	EACH	EACH	EACH	EACH	EACH	EACH	FT	FT	FT	EACH	EACH	EA	FT	EACH	EACH	STA. 163+91 RT & 191+60 LT	
163+57.44	333+96.00	LT&RT		1804				2+	+	+	+		42										ESTIMATED QUANTITY TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER	
163+98		LT										1												
168+33		LT										1												
169+40												1		214.5										
182+35.50	186+98.00	RT												466.5	175		1	2		331	2	2	INSTALL <del>12' 6" LONG 16' RADIUS WITH ANCHOR AT MTS AT</del> STA. 182+35.50 AND <del>STA. 186+85.50</del> ANCHOR AT 186+85.5	
191+01.50	193+13.00	RT	25		5									89.9	100		1	2		175	2	2	INSTALL FLARED MTS AT STA. 191+01.50 AND 25' LONG, 16' RAD. WITH ANCHORS AT STA. 193+00.50 AND STA. 193+13.00	
298+89.00	301+89.00	LT	50		10									237.5						275	2	2	INSTALL FLARED MTS AT STA. <del>289+89.00</del> AND 298+89 STA. <del>301+51.50</del>	
302+43.00	303+02.00	RT	25		5																			
303+57.00	304+57.00	RT	50		10																			
NEWPORT																								
0+00	126+45.60	LT&RT		1127									30										ESTIMATED QUANTITY TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER	
8+10.00	10+22.50	LT	25		5	62.5	62.5							52.0										INSTALL FLARED MTS AT STA. 8+10.00 AND 25' LONG, 16' RADIUS WITH ANCHORS AT STA. 10+10.00 AND STA. 10+22.50
8+10.00	10+10.00	RT	50		10	62.5	62.5							12.5		95	2			100	2	2	INSTALL FLARED MTS AT STA. 8+10.00 AND STA. 9+72.50	
26+08.00	33+58.00	RT	50		10									677				2					INSTALL FLARED MTS AT STA. 26+08.00 AND <del>STA. 33+20.50</del> 2 ANCHORS STA 32+92.5 33+20	
32+22.00	34+11.50	LT	25		5									164.5			1	2		200	2	2	INSTALL FLARED MTS AT STA. 32+22.00 AND 25' LONG, 16' RADIUS WITH ANCHORS AT STA. 33+99.00 AND STA. 34+11.50	
50+74.00	53+36.50	RT	25		5									175			1		1	137.5	1	1	INSTALL FLARED MTS AT STA. 50+74.00 ATTACH TO BR. #58 BRIDGE RAIL	
52+65.00	53+52.50	LT	25		5												1		1	87.5	1	1	INSTALL FLARED MTS AT STA. 52+65.00	
BRIDGE #58																								
54+46.00	55+46.00	RT	25		5									12.5			1		1	100	1	1	ATTACH TO BR. #58 BRIDGE RAIL INSTALL FLARED MTS AT STA. 55+08.50	
54+69.00	56+81.50	LT	25		5									125			1		1	100	1	1	ATTACH TO BR. #58 BRIDGE RAIL INSTALL FLARED MTS AT STA. 56+44.00	
62+92.50	65+05.00	RT	50	41.3	10	43.75	43.75							0-37.5	102	86.25	2	1		37.5	2	2	INSTALL FLARED MTS AT STA. <del>62+92.50</del> AND STA. 64+67.50 AND ANCHOR 62+80	
63+20.00	65+95.00	LT	50		10	43.75	43.75							100		86.25	2			37.5	2	2	INSTALL FLARED MTS AT STA. 63+20.00 AND STA. 65+57.50	
118+73.00	<del>122+98.00</del>	LT	50		10									500						425	2	2	INSTALL FLARED MTS AT STA. 118+73.00 AND <del>STA. 122+60.50</del>	
119+90.00	124+40.00	RT	50		10									387.5						450	2	2	INSTALL FLARED MTS AT STA. 119+90.00 AND STA. 124+02.50	
<del>123+21.00</del>	124+46.00	LT	50		10									50						100	2	2	INSTALL FLARED MTS AT STA. <del>123+21.00</del> AND STA. 124+08.50	
SUBTOTAL			650	2931	130	212.50	212.50	+	+	+	+	2	72	2,910	100	362.5	23	8	4	3,256	28	28		
ROUNDING			-	-	-	12.5	12.5	-	-	-	-	-	-	90	-	12.5	-	-	-	44	-	-		
TOTAL			650	2931	130	225	225	+2	+	+	+	2	3	72	78	3,000	100	375	4	3,300	28	28		
				41.3										2685	439.5					3231	27			

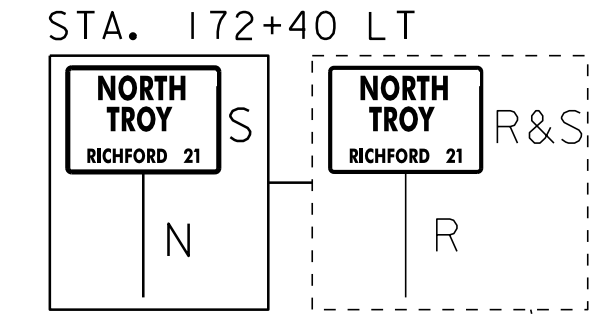
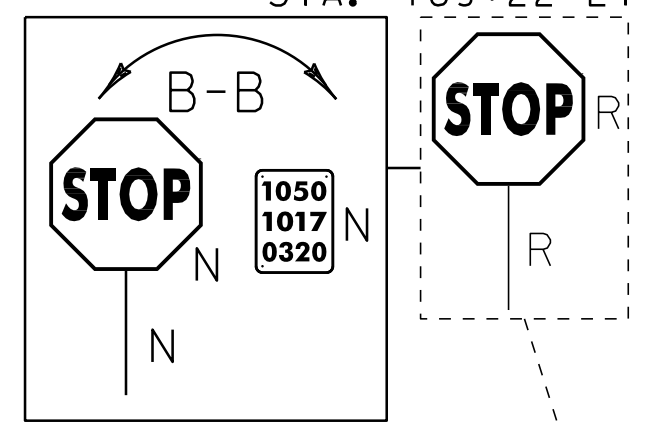
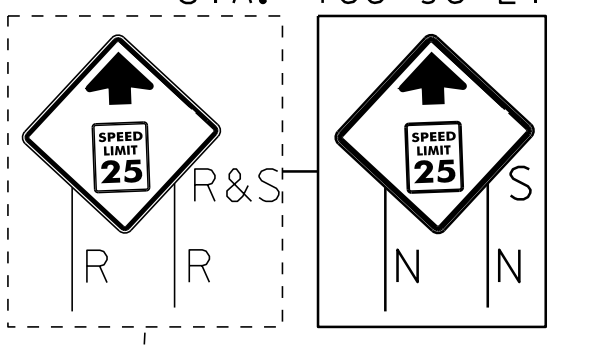
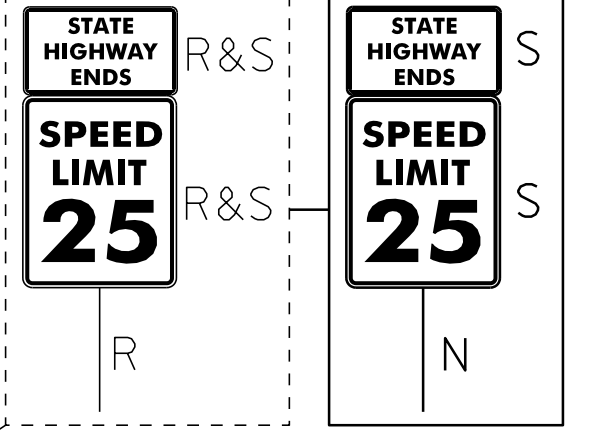
ITEM  
DETAIL  
SUMMARY  
SHEET

PROJECT NAME: TROY-NEWPORT  
PROJECT NUMBER: STP 2613(I)  
FILE NAME: 06B056.dgn  
PROJECT LEADER: CDL  
DESIGNED BY: SJL  
PLOT FILE: 06b056\*12.i  
PLOT DATE: 25-OCT-2011 12:17  
DRAWN BY: SJL  
CHECKED BY: EPD  
SHEET 12 OF 31





BEGIN PROJECT STP 2613(1) S  
STA. 163+57.44 MM 3.098



646.602 TEMPORARY 4" WHITE LINE, PAINT  
STA. 163+57.44 - STA. 178+75 (SOLID LT & RT)

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
STA. 163+57.44 - STA. 178+75 (SOLID LT & RT, CENTERLINE)

CURVE #2  
DELTA = 10°44'17"  
D = 1°21'51"  
R = 4200.00'  
T = 394.72'  
L = 787.13'  
BANKING 0.020 FT/FT  
125' RUNOFF (40 MPH)  
SEE SHEET 24 FOR BANKING DETAIL

~~604.412, 604.415, 604.418 REHAB, DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, CLASS II OR CLASS III~~  
STA. 163+91 RT STA. 163+92 LT

~~604.40 CHANGING ELEVATION OF DROP INLETS, CATCH BASINS, OR MANHOLES~~  
STA 163+91 RT  
STA 191+60 LT

CURVE #1  
DELTA = 10°03'05"  
D = 2°07'19"  
R = 2700.00'  
T = 237.44'  
L = 473.66'  
BANKING 0.028 FT/FT  
125' RUNOFF (40 MPH)  
SEE SHEET 24 FOR BANKING DETAIL



675.50 REMOVING SIGNS  
AS SHOWN - 8-9

675.60 ERECTING SALVAGED SIGNS  
AS SHOWN - 7

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
183+09.5 STA. 182+35.50 - STA. 186+98.00 RT 184+34.5  
193+01.5 STA. 191+39.00 - STA. 192+14.00 RT 193+14  
186+09.5 - 186+99 RT

646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC  
STA. 178+75 - STA. 194+25 (SOLID LT & RT, CENTERLINE, WITH BREAKS AT TOWN ROADS)  
STA. 192+73 (CENTERLINE, TH #40)

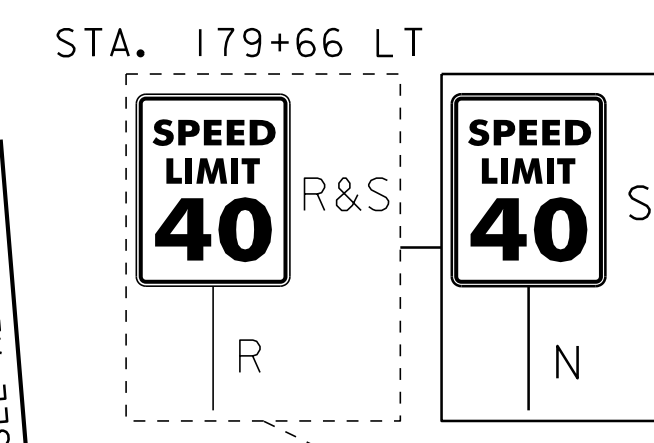
646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
STA. 187+97 RT "STOP"  
STA. 188+25 RT "STOP"

675.50 REMOVING SIGNS AS SHOWN - 13

675.60 ERECTING SALVAGED SIGNS AS SHOWN - 7-6

621.60 ANCHOR FOR STEEL BEAM RAIL  
STA. 182+35.50 RT  
STA. 186+85.50 RT  
STA. 193+00.50 RT  
STA. 193+13.00 RT

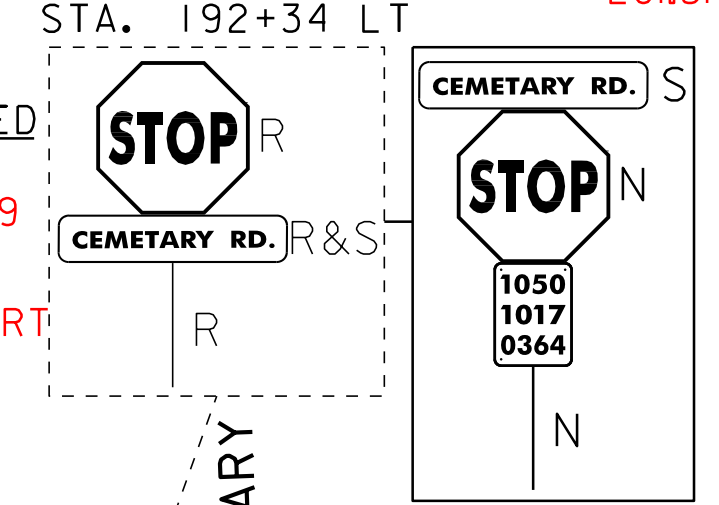
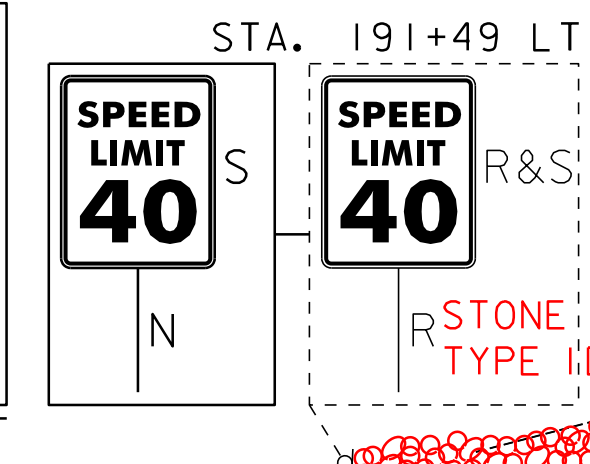
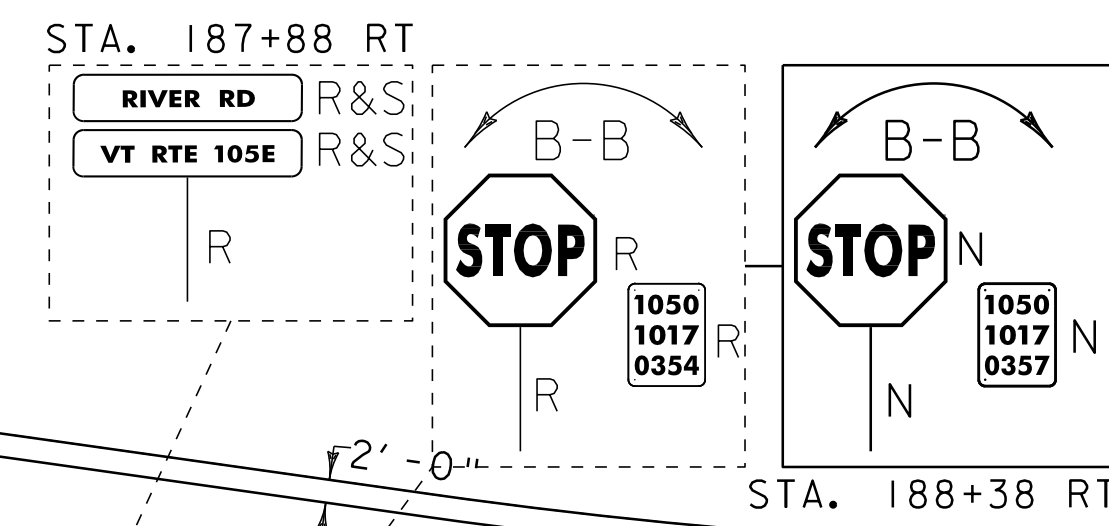
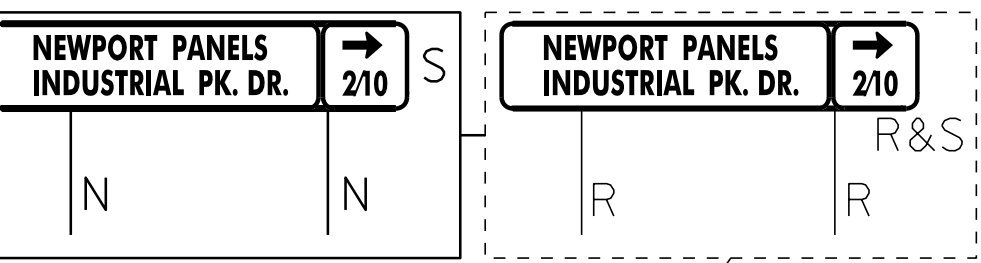
201.31 THINNING AND TRIMMING FOR SIGNS  
192+34 LT



646.482 DURABLE 24" STOP BAR, THERMOPLASTIC  
STA. 187+89 RT  
STA. 188+15 - STA. 188+33 RT  
STA. 192+41 - STA. 192+74 LT

646.682 TEMPORARY 24" STOP BAR, PAINT  
STA. 187+89 RT  
STA. 188+15 - STA. 188+33 RT  
STA. 192+41 - STA. 192+74 LT

CURVE #3  
DELTA = 8°17'59" AS SHOWN - 13  
D = 2°17'31"  
R = 2500.00'  
T = 181.39'  
L = 362.14'  
BANKING 0.030 FT/FT  
125' RUNOFF (40 MPH)  
SEE SHEET 24 FOR BANKING DETAIL



646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
STA. 187+97 RT "STOP" STA. 192+63 LT "STOP"  
STA. 188+25 RT "STOP"

676.10 DELINEATOR WITH STEEL POST  
STA. 182+35.50 RT 72 STA. 191+01.50 RT  
STA. 186+98.00 RT STA. 193+13.00 RT 00

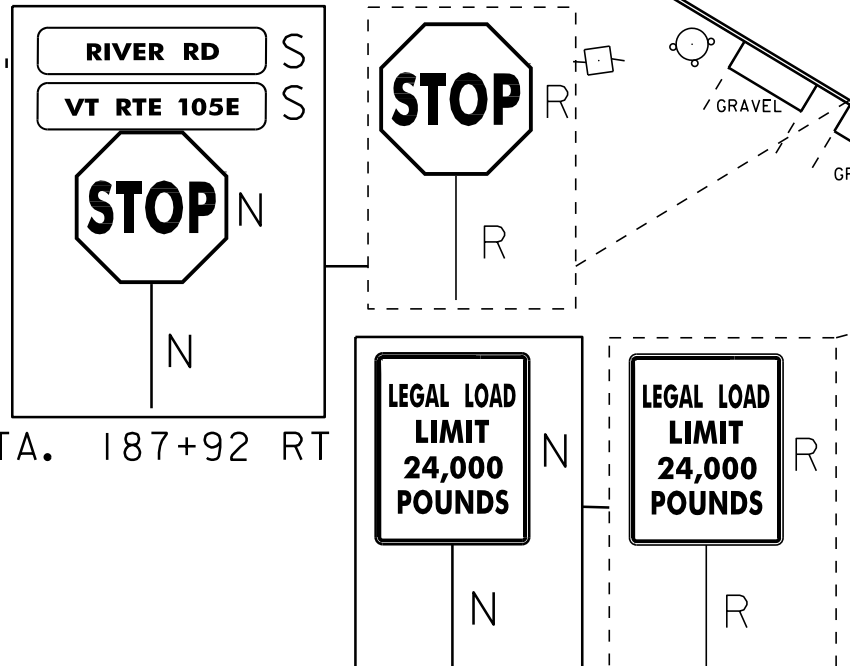
646.612 TEMPORARY 4" YELLOW LINE, PAINT  
STA. 178+75 - STA. 194+25 (SOLID LT & RT, CENTERLINE, WITH BREAKS AT TOWN ROADS)  
STA. 192+73 (CENTERLINE, TH #40)

646.602 TEMPORARY 4" WHITE LINE, PAINT  
STA. 178+75 - STA. 194+25 (SOLID LT & RT WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 187+20 (EDGELINES, TH# 13)  
STA. 192+73 (EDGELINES, TH #40)

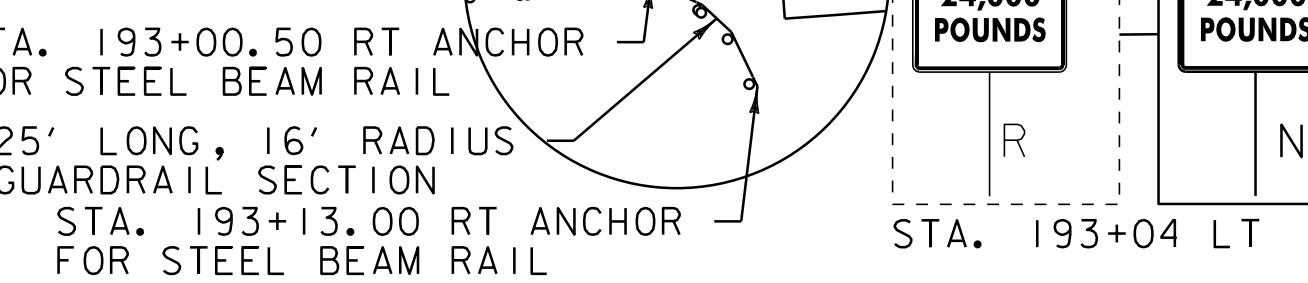
621.50 MANUFACTURED TERMINAL SECTION, FLARED  
STA. 184+31 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
STA. 183+67.00 - STA. 186+98.00 RT  
STA. 191+26.00 - STA. 193+01.00 RT

646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC  
STA. 178+75 - STA. 194+25 (SOLID LT & RT WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 187+20 (EDGELINES, TH# 13)  
STA. 192+73 (EDGELINES, TH #40)



CURVE #4  
DELTA = 10°56'15"  
D = 3°49'11"  
R = 1500.00'  
T = 143.61'  
L = 286.34'  
BANKING 0.045 FT/FT  
125' RUNOFF (50 MPH)  
SEE SHEET 24 FOR BANKING DETAIL



PAVEMENT CORES =

STA. 180+58 LT.  
CORE # 1  
TOTAL DEPTH PCC  
9" NO

LEGEND  
MAILBOX  
UTILITY POLE  
HYDRANT  
MANHOLE  
WATER VALVE  
CATCH BASIN / DROP INLET  
RAILROAD CROSSING SIGNAL

SIGN LEGEND  
R = REMOVE  
R&S = REMOVE AND SALVAGE  
RET = RETAIN  
S = SALVAGE  
N = NEW  
B-B = BACK TO BACK

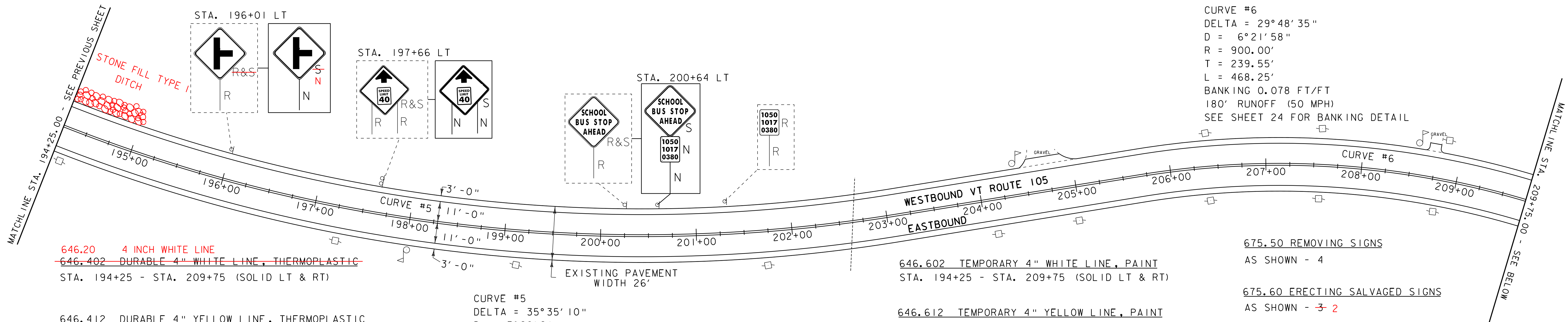
NOT TO SCALE

PROJECT LAYOUT SHEET 1

PROJECT NAME: TROY-NEWPORT  
PROJECT NUMBER: STP 2613(1)

FILE NAME: 06B056.dgn  
PROJECT LEADER: CDL  
DESIGNED BY: SJL  
PLOT FILE: 06b056\*14.i

PLOT DATE: 25-OCT-2011 12:17  
DRAWN BY: SJD  
CHECKED BY: EPL  
SHEET 14 OF 31



CURVE #6  
 DELTA = 29°48'35"  
 D = 6°21'58"  
 R = 900.00'  
 T = 239.55'  
 L = 468.25'  
 BANKING 0.078 FT/FT  
 180' RUNOFF (50 MPH)  
 SEE SHEET 24 FOR BANKING DETAIL

646.20 4 INCH WHITE LINE  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 STA. 194+25 - STA. 209+75 (SOLID LT & RT)

646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC  
 STA. 194+25 - STA. 209+75  
 (SOLID LT & RT, CENTERLINE)

CURVE #5  
 DELTA = 35°35'10"  
 D = 3°28'21"  
 R = 1650.00'  
 T = 529.54'  
 L = 1024.81'  
 BANKING 0.058 FT/FT  
 150' RUNOFF (50 MPH)  
 SEE SHEET 24 FOR BANKING DETAIL

646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 194+25 - STA. 209+75 (SOLID LT & RT)

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 194+25 - STA. 209+75  
 (SOLID LT & RT, CENTERLINE)

675.50 REMOVING SIGNS  
 AS SHOWN - 4

675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 3-2

CURVE #7  
 DELTA = 4°46'59"  
 D = 1°25'57"  
 R = 4000.00'  
 T = 167.06'  
 L = 333.92'  
 BANKING 0.029 FT/FT  
 150' RUNOFF (50 MPH)  
 SEE SHEET 24 FOR BANKING DETAIL

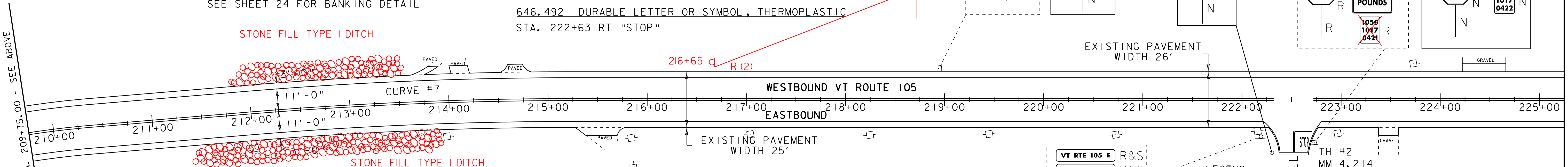
646.20 4 INCH WHITE LINE  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~

STA. 209+75 - STA. 225+25 (SOLID LT & RT,  
 WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 222+52 (EDGELINES, TH #2)

646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC  
 STA. 209+75 - STA. 225+25 (SOLID LT & RT, CENTERLINE,  
 WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 222+52 (CENTERLINE, TH #2)

646.482 DURABLE 24" STOP BAR, THERMOPLASTIC  
 STA. 222+53 - STA. 222+70 RT

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 222+63 RT "STOP"



646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 209+75 - STA. 225+25 (SOLID LT & RT  
 WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 222+52 (EDGELINES, TH #2)

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 209+75 - STA. 225+25 (SOLID LT & RT, CENTERLINE,  
 WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 222+52 (CENTERLINE, TH #2)

646.682 TEMPORARY 24" STOP BAR, PAINT  
 STA. 222+53 - STA. 222+70 RT

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 222+63 RT "STOP"

675.50 REMOVING SIGNS  
 AS SHOWN - 7-8

675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 4

STA. 222+30 RT  
 LEGAL LOAD  
 LIMIT  
 24,000  
 POUNDS

STA. 222+73 RT  
 STOP  
 LEGAL LOAD  
 LIMIT  
 24,000  
 POUNDS

B-B  
 VT RTE 105 E S  
 EAST HILL S  
 STOP  
 1050 1017 0422

VT RTE 105 E R&S  
 EAST HILL R&S  
 STA. 222+19 RT

LEGEND  
 MAILBOX  
 UTILITY POLE  
 HYDRANT  
 MANHOLE  
 WATER VALVE  
 CATCH BASIN / DROP INLET  
 RAILROAD CROSSING SIGNAL

SIGN LEGEND  
 R = REMOVE  
 R&S = REMOVE AND SALVAGE  
 RET = RETAIN  
 S = SALVAGE  
 N = NEW  
 B-B = BACK TO BACK

**PROJECT LAYOUT SHEET 2**

PROJECT NAME: TROY-NEWPORT  
 PROJECT NUMBER: STP 2613(I)

FILE NAME: 06B056.dgn  
 PROJECT LEADER: CDL  
 DESIGNED BY: SJL  
 PLOT FILE: 06b056#15.i  
 PLOT DATE: 25-OCT-2011 12:17  
 DRAWN BY: SJL  
 CHECKED BY: EPD  
 SHEET 15 OF 31

NOT TO SCALE

646.20 4 INCH WHITE LINE

~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
STA. 225+25 - STA. 240+50 (SOLID LT & RT)

646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC

STA. 225+25 - STA. 229+68 (SOLID LT)  
STA. 225+25 - STA. 240+50 (SOLID RT)  
STA. 229+68 - STA. 240+24 (DASHED LT)  
STA. 240+24 - STA. 240+50 (SOLID LT)

646.602 TEMPORARY 4" WHITE LINE, PAINT  
STA. 225+25 - STA. 240+50 (SOLID LT & RT)

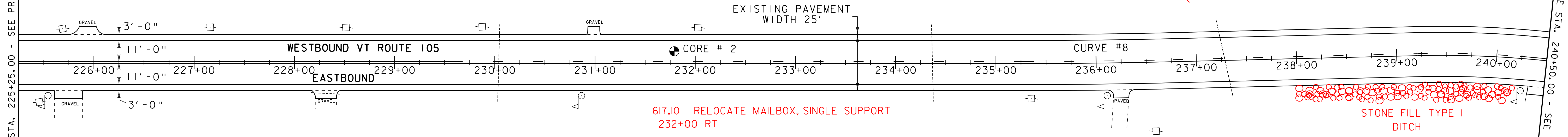
646.612 TEMPORARY 4" YELLOW LINE, PAINT

STA. 225+25 - STA. 229+68 (SOLID LT)  
STA. 225+25 - STA. 240+50 (SOLID RT)  
STA. 229+68 - STA. 240+24 (DASHED LT)  
STA. 240+24 - STA. 240+50 (SOLID LT)

~~CURVE #8  
DELTA = 1°25'09"  
D = 0°57'18"  
R = 6000.00'  
T = 74.31'  
L = 148.62'  
BANKING 0.021 FT/FT  
150' RUNOFF (50 MPH)  
SEE SHEET 24 FOR BANKING DETAIL~~  
ELIMINATED  
SEE W.O.#2

MATCHLINE STA. 225+25.00 - SEE PREVIOUS SHEET

MATCHLINE STA. 240+50.00 - SEE BELOW



617.10 RELOCATE MAILBOX, SINGLE SUPPORT  
232+00 RT

STONE FILL TYPE I  
DITCH

PAVEMENT CORES = ●  
STA. 231+79 LT.  
CORE # 2  
TOTAL DEPTH PCC  
11' NO

646.20 4 INCH WHITE LINE

~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
STA. 240+50 - STA. 256+25 (SOLID LT & RT)

646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC  
STA. 240+50 - STA. 256+25 (SOLID LT & RT, CENTERLINE)

646.602 TEMPORARY 4" WHITE LINE, PAINT  
STA. 240+50 - STA. 256+25 (SOLID LT & RT)

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
STA. 240+50 - STA. 256+25 (SOLID LT & RT, CENTERLINE)

675.50 REMOVING SIGNS  
AS SHOWN - 2

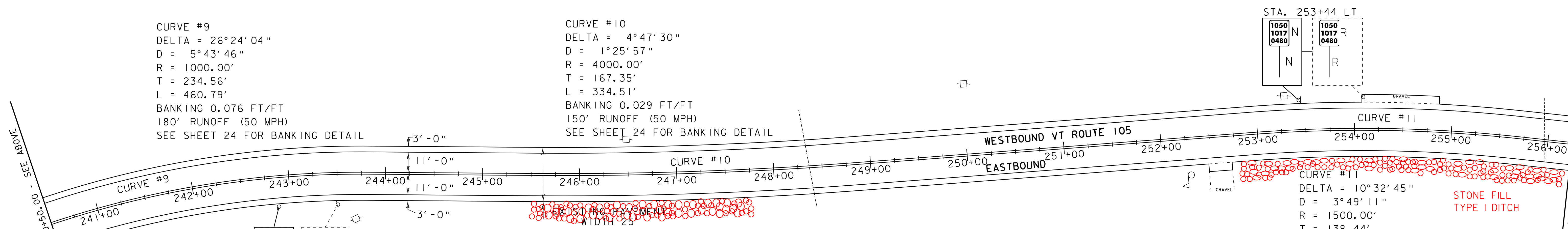
CURVE #9  
DELTA = 26°24'04"  
D = 5°43'46"  
R = 1000.00'  
T = 234.56'  
L = 460.79'  
BANKING 0.076 FT/FT  
180' RUNOFF (50 MPH)  
SEE SHEET 24 FOR BANKING DETAIL

CURVE #10  
DELTA = 4°47'30"  
D = 1°25'57"  
R = 4000.00'  
T = 167.35'  
L = 334.51'  
BANKING 0.029 FT/FT  
150' RUNOFF (50 MPH)  
SEE SHEET 24 FOR BANKING DETAIL

STA. 253+44 LT  
CURVE #11  
DELTA = 10°32'45"  
D = 3°49'11"  
R = 1500.00'  
T = 138.44'  
L = 276.09'  
BANKING 0.062 FT/FT  
150' RUNOFF (50 MPH)  
SEE SHEET 24 FOR BANKING DETAIL

MATCHLINE STA. 240+50.00 - SEE ABOVE

MATCHLINE STA. 256+25.00 - SEE NEXT SHEET



1050 1017 0460 N  
1050 1017 0460 R  
STA. 242+88 RT

- LEGEND
- MAILBOX
  - UTILITY POLE
  - HYDRANT
  - MANHOLE
  - WATER VALVE
  - CATCH BASIN / DROP INLET
  - ⊕ RAILROAD CROSSING SIGNAL

- SIGN LEGEND
- R = REMOVE
  - R&S = REMOVE AND SALVAGE
  - RET = RETAIN
  - S = SALVAGE
  - N = NEW
  - B-B = BACK TO BACK

NOT TO SCALE

**PROJECT LAYOUT SHEET 3**

PROJECT NAME: TROY-NEWPORT	PLOT DATE: 25-OCT-2011 12:17
PROJECT NUMBER: STP 2613(I)	DRAWN BY: SJL
FILE NAME: 06B056.dgn	CHECKED BY: EPD
DESIGNED BY: SJL	SHEET 16 OF 31
PLOT FILE: 06b056*16.i	

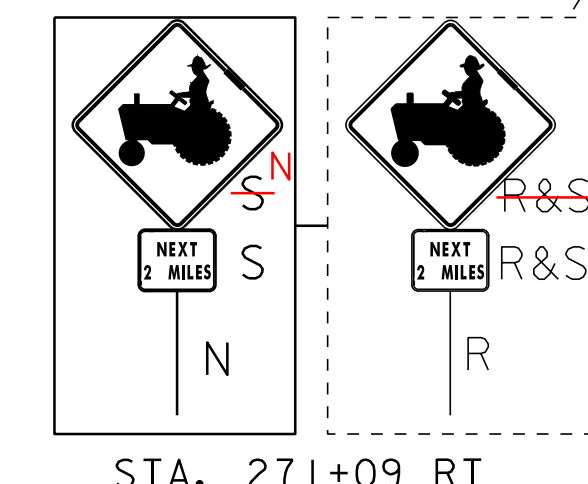
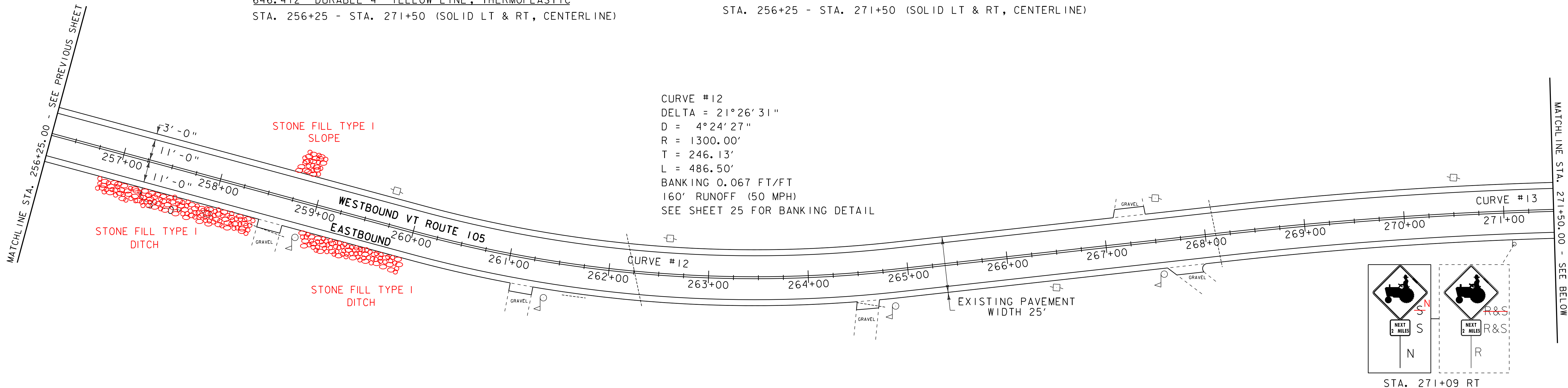
646.20 4 INCH WHITE LINE  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 STA. 256+25 - STA. 271+50 (SOLID LT & RT)

646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 256+25 - STA. 271+50 (SOLID LT & RT)

675.50 REMOVING SIGNS  
 AS SHOWN - 2  
 675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 2-1

646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC  
 STA. 256+25 - STA. 271+50 (SOLID LT & RT, CENTERLINE)

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 256+25 - STA. 271+50 (SOLID LT & RT, CENTERLINE)



CURVE #12  
 DELTA = 21°26'31"  
 D = 4°24'27"  
 R = 1300.00'  
 T = 246.13'  
 L = 486.50'  
 BANKING 0.067 FT/FT  
 160' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL

CURVE #13  
 DELTA = 7°55'57"  
 D = 1°25'57"  
 R = 4000.00'  
 T = 277.34'  
 L = 553.80'  
 BANKING 0.029 FT/FT  
 150' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL

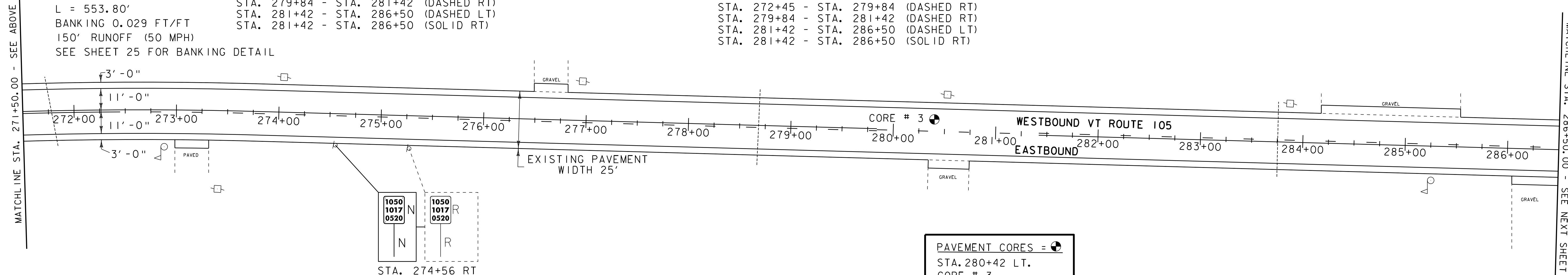
646.20 4 INCH WHITE LINE  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 STA. 271+50 - STA. 286+50 (SOLID LT & RT)

646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 271+50 - STA. 286+50 (SOLID LT & RT)

675.50 REMOVING SIGNS  
 AS SHOWN - 1

646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC  
 STA. 271+50 - STA. 272+45 (SOLID RT)  
 STA. 271+50 - STA. 279+84 (SOLID LT)  
 STA. 272+45 - STA. 279+84 (DASHED RT)  
 STA. 279+84 - STA. 281+42 (DASHED RT)  
 STA. 281+42 - STA. 286+50 (DASHED LT)  
 STA. 281+42 - STA. 286+50 (SOLID RT)

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 271+50 - STA. 272+45 (SOLID RT)  
 STA. 271+50 - STA. 279+84 (SOLID LT)  
 STA. 272+45 - STA. 279+84 (DASHED RT)  
 STA. 279+84 - STA. 281+42 (DASHED RT)  
 STA. 281+42 - STA. 286+50 (DASHED LT)  
 STA. 281+42 - STA. 286+50 (SOLID RT)



PAVEMENT CORES = ●  
 STA. 280+42 LT.  
 CORE # 3  
 TOTAL DEPTH PCC  
 9' NO

- LEGEND**
- MAILBOX
  - UTILITY POLE
  - HYDRANT
  - MANHOLE
  - WATER VALVE
  - CATCH BASIN / DROP INLET
  - ⊕ RAILROAD CROSSING SIGNAL

- SIGN LEGEND**
- R = REMOVE
  - R&S = REMOVE AND SALVAGE
  - RET = RETAIN
  - S = SALVAGE
  - N = NEW
  - B-B = BACK TO BACK

NOT TO SCALE

**PROJECT LAYOUT SHEET 4**

PROJECT NAME: TROY-NEWPORT	FILE NAME: 06B056.dgn	PLOT DATE: 25-OCT-2011 12:17
PROJECT NUMBER: STP 2613(I)	PROJECT LEADER: CDL	DRAWN BY: SJL
	DESIGNED BY: SJL	CHECKED BY: EPD
	PLOT FILE: 06b056#17.i	SHEET 17 OF 31

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 299+26.50 - STA. 301+50.00 LT  
 621.50 MANUFACTURED TERMINAL SECTION, FLARED  
 STA. 298+89.00 - STA. 299+26.50 LT  
 621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 298+89.00 - STA. 301+50.00 LT  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~ 646.20 4 INCH WHITE LINE  
 STA. 286+50 - STA. 301+50 (SOLID LT & RT WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 290+64 (EDGELINES, TH #18)  
 646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC  
 STA. 286+50 - STA. 289+34 (DASHED LT)  
 STA. 286+50 - STA. 301+50 (SOLID RT WITH BREAKS AT TOWN ROADS)  
 STA. 289+34 - STA. 301+50 (SOLID LT WITH BREAKS AT TOWN ROADS)

646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 286+50 - STA. 301+50 (SOLID LT & RT WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 290+64 (EDGELINES, TH #18)

675.50 REMOVING SIGNS  
 AS SHOWN - 7  
 675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 2  
 676.10 DELINEATOR WITH STEEL POST  
 STA. 298+89.00 LT

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 286+50 - STA. 289+34 (DASHED LT)  
 STA. 286+50 - STA. 301+50 (SOLID RT WITH BREAKS AT TOWN ROADS)  
 STA. 289+34 - STA. 301+50 (SOLID LT WITH BREAKS AT TOWN ROADS)

201.31 THINNING AND TRIMMING FOR SIGNS  
 297+25 RT

646.512 DURABLE RAILROAD CROSSING SYMBOL, THERMOPLASTIC  
 STA. 297+25 RT

646.712 TEMPORARY RAILROAD CROSSING SYMBOL, PAINT  
 STA. 297+25 RT

CURVE #14  
 DELTA = 31°59'59"  
 D = 6°01'52"  
 R = 950.00'  
 T = 272.41'  
 L = 530.58'  
 BANKING 0.077 FT/FT  
 180' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL

CURVE #15  
 DELTA = 1°24'43"  
 D = 0°57'18"  
 R = 6000.00'  
 T = 73.93'  
 L = 147.85'  
 BANKING 0.020 FT/FT  
 150' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL

PAVEMENT CORES =   
 STA. 305+18 LT.  
 CORE # 4  
 TOTAL DEPTH PCC NO

646.20 4 INCH WHITE LINE  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 STA. 301+50 - STA. 315+50 (SOLID LT & RT)  
 646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC  
 STA. 301+50 - STA. 315+50 (SOLID LT & RT, CENTERLINE)  
 646.21 4 INCH YELLOW LINE  
 301+65 ~ 315+50  
 646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 301+50 - STA. 315+50 (SOLID LT & RT)

646.512 DURABLE RAILROAD CROSSING SYMBOL, THERMOPLASTIC  
 STA. 311+57 LT  
 646.712 TEMPORARY RAILROAD CROSSING SYMBOL, PAINT  
 STA. 311+57 LT

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 301+50 - STA. 315+50 (SOLID LT & RT, CENTERLINE)  
 675.50 REMOVING SIGNS  
 AS SHOWN - 5-3  
 675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - +

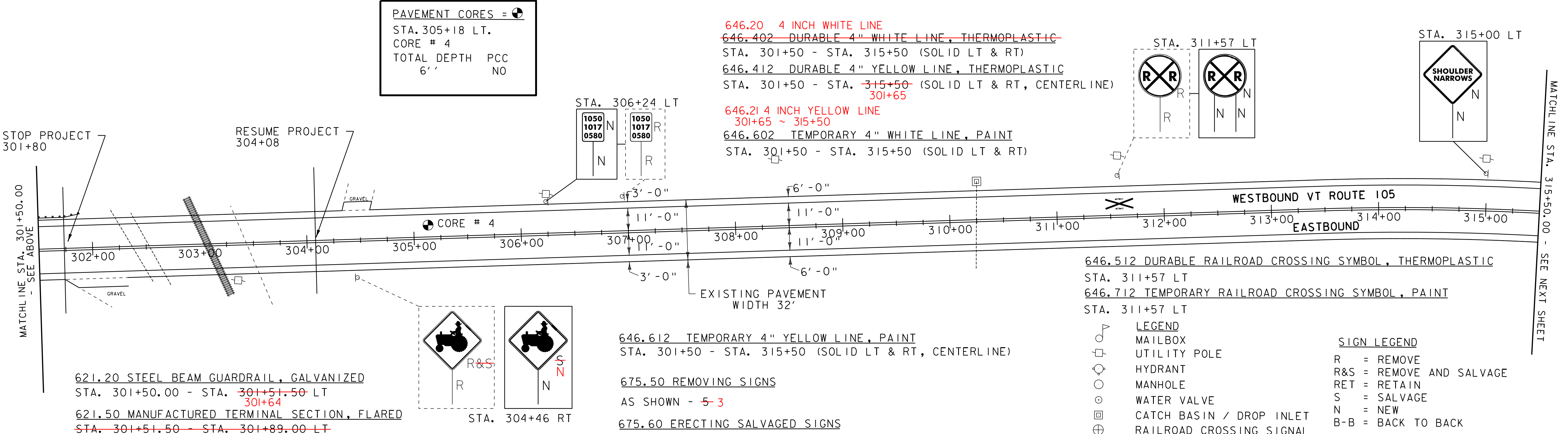
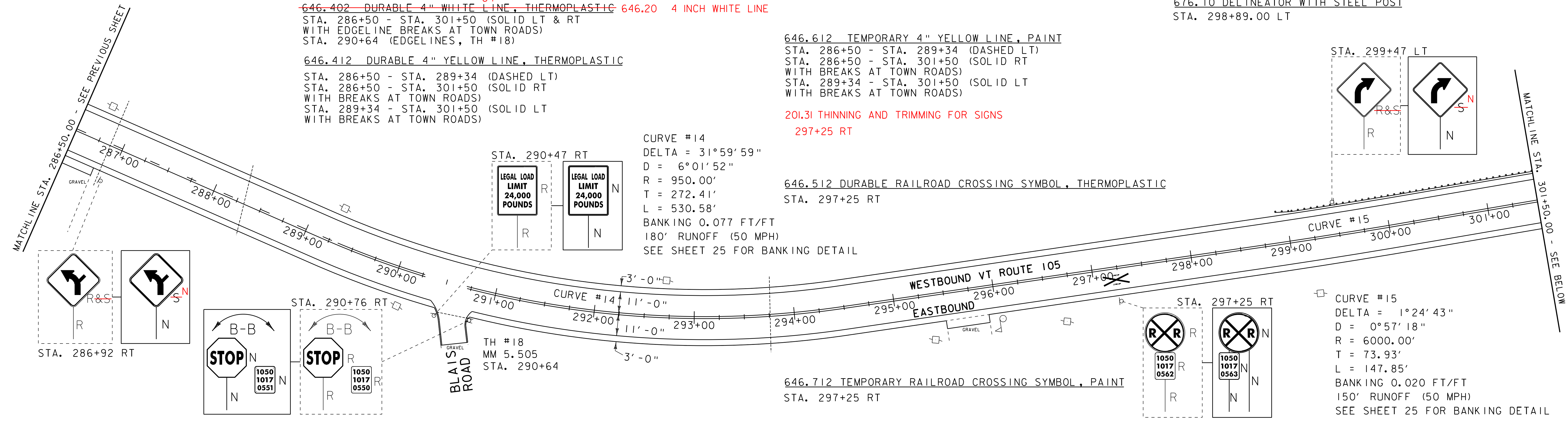
676.10 DELINEATOR WITH STEEL POST  
 STA. 301+89.00 LT

- |  |                          |  |                          |
|--|--------------------------|--|--------------------------|
|  | LEGEND                   |  | SIGN LEGEND              |
|  | MAILBOX                  |  | R = REMOVE               |
|  | UTILITY POLE             |  | R&S = REMOVE AND SALVAGE |
|  | HYDRANT                  |  | RET = RETAIN             |
|  | MANHOLE                  |  | S = SALVAGE              |
|  | WATER VALVE              |  | N = NEW                  |
|  | CATCH BASIN / DROP INLET |  | B-B = BACK TO BACK       |
|  | RAILROAD CROSSING SIGNAL |  |                          |

**PROJECT LAYOUT SHEET 5**

PROJECT NAME: TROY-NEWPORT  
 PROJECT NUMBER: STP 2613(I)  
 FILE NAME: 06B056.dgn  
 PROJECT LEADER: CDL  
 DESIGNED BY: SJL  
 PLOT FILE: 06b056#18.i  
 PLOT DATE: 25-OCT-2011 12:18  
 DRAWN BY: SJL  
 CHECKED BY: EPD  
 SHEET 18 OF 31

NOT TO SCALE



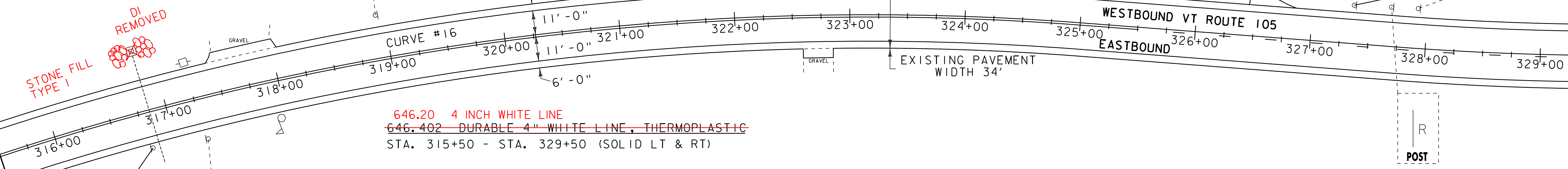
621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 301+50.00 - STA. 301+51.50 LT  
 621.50 MANUFACTURED TERMINAL SECTION, FLARED  
 STA. 301+51.50 - STA. 301+89.00 LT  
 STA. 303+57.00 - STA. 303+94.50 RT  
 STA. 304+19.50 - STA. 304+57.00 RT  
 621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 301+50.00 - STA. 301+89.00 LT  
 STA. 302+43.00 - STA. 303+02.00 RT

REMOVAL OF EXISTING POST SHALL BE PAID AS ITEM 675.50 "REMOVING SIGNS"

CURVE #16  
 DELTA = 27°32'18"  
 D = 2°23'14"  
 R = 2400.00'  
 T = 588.13'  
 L = 1153.53'  
 BANKING 0.045 FT/FT  
 150' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL

MATCHLINE STA. 315+50.00 - SEE PREVIOUS SHEET

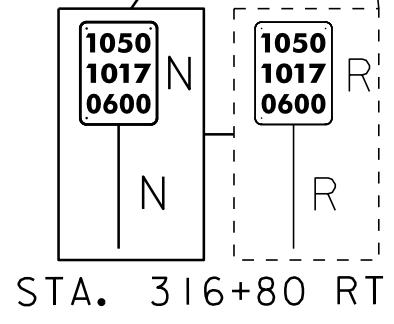
MATCHLINE STA. 329+50.00 - SEE BELOW



- 646.20 4 INCH WHITE LINE
- ~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~
- STA. 315+50 - STA. 329+50 (SOLID LT & RT)
- 646.21 4 INCH YELLOW LINE
- ~~646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC~~
- STA. 315+50 - STA. 329+50 (SOLID LT)
- STA. 315+50 - STA. 325+51 (SOLID RT)
- STA. 325+51 - STA. 329+50 (DASHED RT)
- 646.602 TEMPORARY 4" WHITE LINE, PAINT
- STA. 315+50 - STA. 329+50 (SOLID LT & RT)
- 646.612 TEMPORARY 4" YELLOW LINE, PAINT
- STA. 315+50 - STA. 329+50 (SOLID LT)
- STA. 315+50 - STA. 325+51 (SOLID RT)
- STA. 325+51 - STA. 329+50 (DASHED RT)

675.50 REMOVING SIGNS  
 AS SHOWN - 4

REMOVAL OF EXISTING POST SHALL BE PAID AS ITEM 675.50 "REMOVING SIGNS"



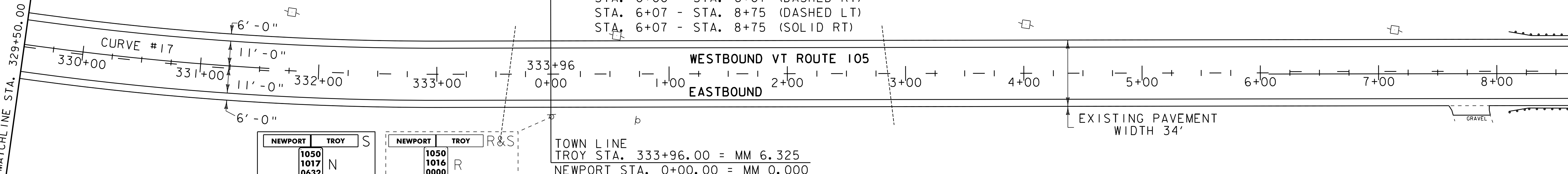
- 621.50 MANUFACTURED TERMINAL SECTION, FLARED
- STA. 8+10.00 - STA. 8+47.50 LT
- STA. 8+10.00 - STA. 8+47.50 RT
- 621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
- STA. 8+35.00 - STA. 8+75.00 LT 78.75
- STA. 8+35.00 - STA. 8+75.00 RT 78.75
- 621.21 HD STEEL BEAM GUARDRAIL, GALVANIZED
- STA. 8+47.50 - STA. 8+75.00 LT 78.75
- STA. 8+47.50 - STA. 8+75.00 RT 78.75
- ~~646.20 4 INCH WHITE LINE~~
- ~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~
- STA. 329+50 - STA. 333+96 (SOLID LT & RT)
- STA. 0+00 - STA. 8+75 (SOLID LT & RT)

- ~~646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC~~
- STA. 329+50 - STA. 331+58 (SOLID LT)
- STA. 329+50 - STA. 331+58 (DASHED RT)
- STA. 331+58 - STA. 333+96 (DASHED RT)
- STA. 0+00 - STA. 6+07 (DASHED RT)
- STA. 6+07 - STA. 8+75 (DASHED LT)
- STA. 6+07 - STA. 8+75 (SOLID RT)
- 646.602 TEMPORARY 4" WHITE LINE, PAINT
- STA. 329+50 - STA. 333+96 (SOLID LT & RT)
- STA. 0+00 - STA. 8+75 (SOLID LT & RT)
- 646.612 TEMPORARY 4" YELLOW LINE, PAINT
- STA. 329+50 - STA. 331+58 (SOLID LT)
- STA. 329+50 - STA. 331+58 (DASHED RT)
- STA. 331+58 - STA. 333+96 (DASHED RT)
- STA. 0+00 - STA. 6+07 (DASHED RT)
- STA. 6+07 - STA. 8+75 (DASHED LT)
- STA. 6+07 - STA. 8+75 (SOLID RT)

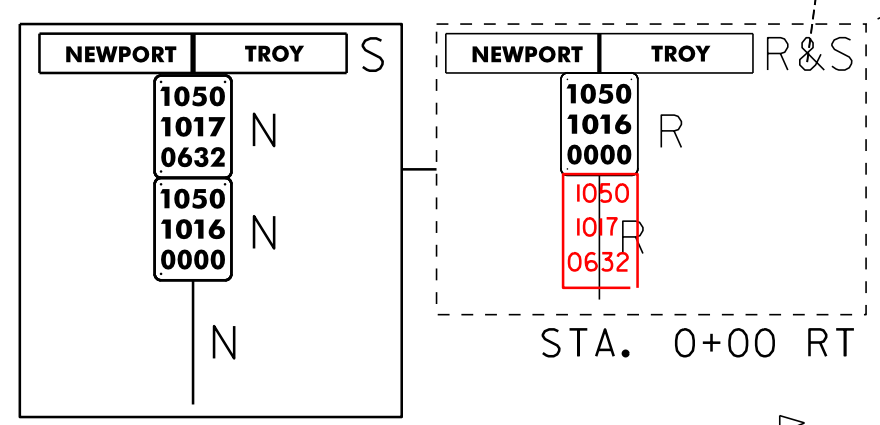
- 675.50 REMOVING SIGNS
- AS SHOWN - 2-3
- 675.60 ERECTING SALVAGED SIGNS
- AS SHOWN - 1
- 676.10 DELINEATOR WITH STEEL POST
- STA. 8+10.00 LT
- STA. 8+10.00 RT

MATCHLINE STA. 329+50.00 - SEE ABOVE

MATCHLINE STA. 8+75.00 - SEE NEXT SHEET



CURVE #17  
 DELTA = 12°02'50"  
 D = 2°17'31"  
 R = 2500.00'  
 T = 263.80'  
 L = 525.66'  
 BANKING 0.043 FT/FT  
 150' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL



- LEGEND
- MAILBOX
  - UTILITY POLE
  - HYDRANT
  - MANHOLE
  - WATER VALVE
  - CATCH BASIN / DROP INLET
  - ⊕ RAILROAD CROSSING SIGNAL

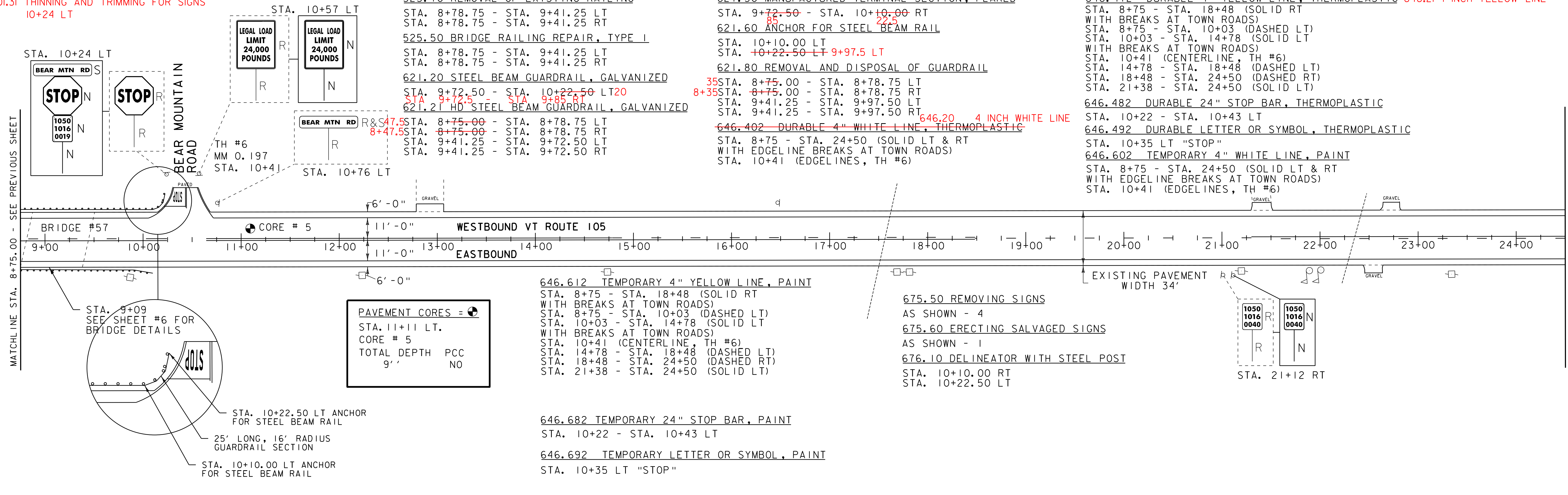
- SIGN LEGEND
- R = REMOVE
  - R&S = REMOVE AND SALVAGE
  - RET = RETAIN
  - S = SALVAGE
  - N = NEW
  - B-B = BACK TO BACK

NOT TO SCALE

**PROJECT LAYOUT SHEET 6**

PROJECT NAME: TROY-NEWPORT	PLOT DATE: 25-OCT-2011 12:18
PROJECT NUMBER: STP 2613(I)	DRAWN BY: SJL
FILE NAME: 06B056.dgn	CHECKED BY: EPD
DESIGNED BY: SJL	SHEET 19 OF 31
PLOT FILE: 06b056*19.i	

201.31 THINNING AND TRIMMING FOR SIGNS  
10+24 LT



MATCHLINE STA. 8+75.00 - SEE PREVIOUS SHEET

MATCHLINE STA. 24+50.00 - SEE BELOW

621.50 MANUFACTURED TERMINAL SECTION, FLARED  
 STA. 26+08.00 - STA. 26+45.50 RT  
 STA. 32+22.00 - STA. 32+59.50 LT  
~~STA. 33+20.50 - STA. 33+58.00 RT~~

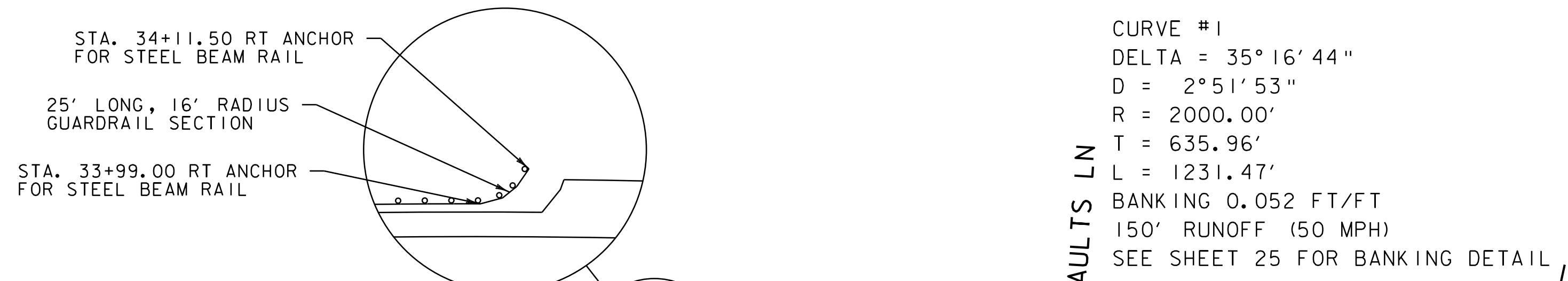
621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 27+83.00 - STA. 33+58.00 RT  
 STA. 33+22.00 - STA. 34+11.50 LT

~~646.20 4 INCH WHITE LINE~~  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 STA. 24+50 - STA. 38+25 (SOLID LT & RT)

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 26+45.50 - STA. 33+20.50 RT  
 STA. 32+59.50 - STA. 34+11.50 LT

621.60 ANCHOR FOR STEEL BEAM RAIL  
 STA. 33+99.00 LT STA 32+92.5 RT  
 STA. 34+11.50 LT STA 33+05 RT

676.10 DELINEATOR WITH STEEL POST  
 STA. 26+08.00 RT  
 STA. 32+22.00 LT  
~~STA. 33+58.00 RT~~  
 STA. 34+11.50 LT  
 32+92 RT  
 34+87 LT



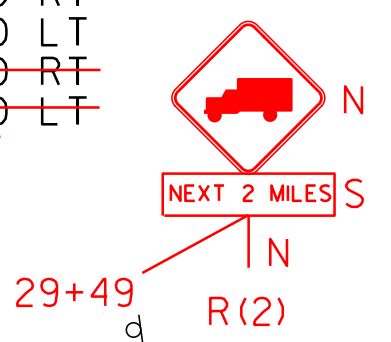
MATCHLINE STA. 24+50.00 - SEE ABOVE

MATCHLINE STA. 38+25.00 - SEE NEXT SHEET

~~646.21 4 INCH YELLOW LINE~~  
~~646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC~~  
 STA. 24+50 - STA. 27+72 (SOLID LT)  
 STA. 24+50 - STA. 27+72 (DASHED RT)  
 STA. 27+72 - STA. 35+64 (DASHED LT)  
 STA. 27+72 - STA. 38+25 (SOLID RT)  
 STA. 35+64 - STA. 38+25 (SOLID LT)

646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 24+50 - STA. 38+25 (SOLID LT & RT)

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 24+50 - STA. 27+72 (SOLID LT)  
 STA. 24+50 - STA. 27+72 (DASHED RT)  
 STA. 27+72 - STA. 35+64 (DASHED LT)  
 STA. 27+72 - STA. 38+25 (SOLID RT)  
 STA. 35+64 - STA. 38+25 (SOLID LT)



- LEGEND**
- MAILBOX
  - UTILITY POLE
  - HYDRANT
  - MANHOLE
  - WATER VALVE
  - ⊕ CATCH BASIN / DROP INLET
  - ⊕ RAILROAD CROSSING SIGNAL
- SIGN LEGEND**
- R = REMOVE
  - R&S = REMOVE AND SALVAGE
  - RET = RETAIN
  - S = SALVAGE
  - N = NEW
  - B-B = BACK TO BACK

**PROJECT LAYOUT SHEET 7**

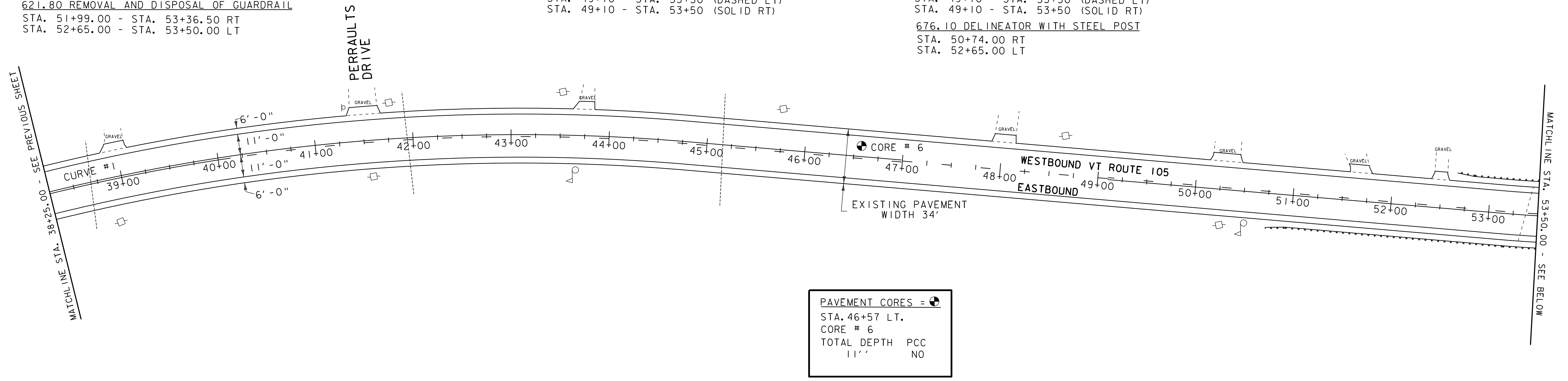
PROJECT NAME: TROY-NEWPORT	PLOT DATE: 25-OCT-2011 12:18
PROJECT NUMBER: STP 2613(I)	DRAWN BY: SJL
FILE NAME: 06B056.dgn	DESIGNED BY: SJL
PROJECT LEADER: CDL	CHECKED BY: EPD
PLOT FILE: 06b056*20.i	SHEET 20 OF 31

NOT TO SCALE

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 51+11.50 - STA. 52+86.50 RT  
 621.50 MANUFACTURED TERMINAL SECTION, FLARED  
 STA. 50+74.00 - STA. 51+11.50 RT  
 STA. 52+65.00 - STA. 53+02.50 LT  
 621.70 GUARDRAIL APPROACH SECTION, GALVANIZED TYPE I  
 STA. 52+86.50 - STA. 53+36.50 RT  
 STA. 53+02.50 - STA. 53+50.00 LT  
 621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 51+99.00 - STA. 53+36.50 RT  
 STA. 52+65.00 - STA. 53+50.00 LT

646.20 4 INCH WHITE LINE  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 STA. 38+25 - STA. 53+50 (SOLID LT & RT)  
 646.21 4 INCH YELLOW LINE  
~~646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC~~  
 STA. 38+25 - STA. 40+13 (SOLID RT)  
 STA. 38+25 - STA. 46+99 (SOLID LT)  
 STA. 40+13 - STA. 46+99 (DASHED RT)  
 STA. 46+99 - STA. 49+10 (DASHED RT)  
 STA. 49+10 - STA. 53+50 (DASHED LT)  
 STA. 49+10 - STA. 53+50 (SOLID RT)

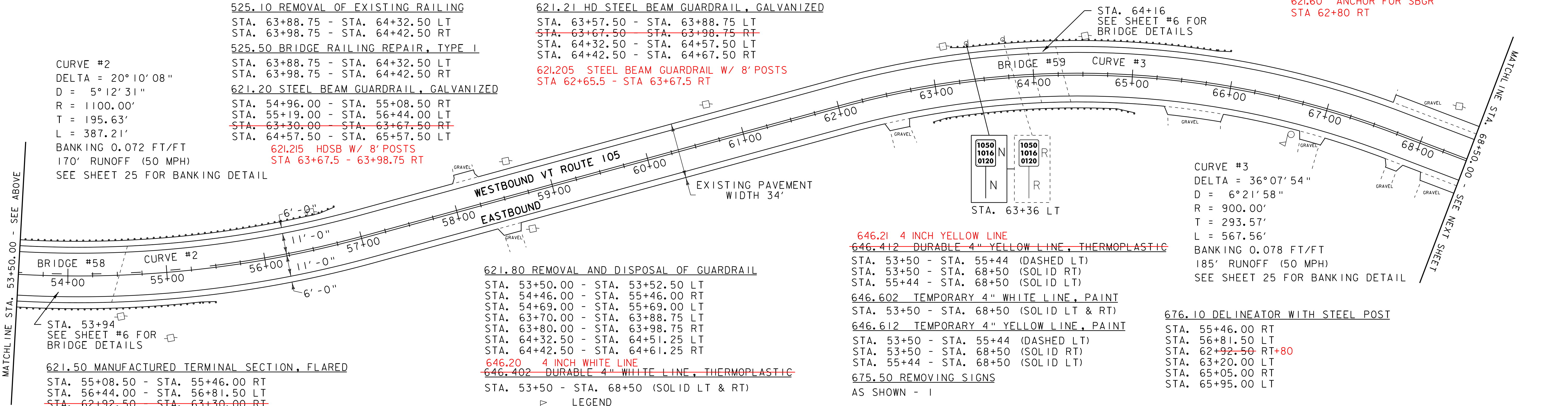
646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 38+25 - STA. 53+50 (SOLID LT & RT)  
 646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 38+25 - STA. 40+13 (SOLID RT)  
 STA. 38+25 - STA. 46+99 (SOLID LT)  
 STA. 40+13 - STA. 46+99 (DASHED RT)  
 STA. 46+99 - STA. 49+10 (DASHED RT)  
 STA. 49+10 - STA. 53+50 (DASHED LT)  
 STA. 49+10 - STA. 53+50 (SOLID RT)  
 676.10 DELINEATOR WITH STEEL POST  
 STA. 50+74.00 RT  
 STA. 52+65.00 LT



PAVEMENT CORES = ●  
 STA. 46+57 LT.  
 CORE # 6  
 TOTAL DEPTH PCC  
 11' NO

525.10 REMOVAL OF EXISTING RAILING  
 STA. 63+88.75 - STA. 64+32.50 LT  
 STA. 63+98.75 - STA. 64+42.50 RT  
 525.50 BRIDGE RAILING REPAIR, TYPE I  
 STA. 63+88.75 - STA. 64+32.50 LT  
 STA. 63+98.75 - STA. 64+42.50 RT  
 621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 54+96.00 - STA. 55+08.50 RT  
 STA. 55+19.00 - STA. 56+44.00 LT  
~~STA. 63+30.00 - STA. 63+67.50 RT~~  
 STA. 64+57.50 - STA. 65+57.50 LT  
 621.215 HDSB W/ 8' POSTS  
 STA 63+67.5 - 63+98.75 RT  
 CURVE #2  
 DELTA = 20° 10' 08"  
 D = 5° 12' 31"  
 R = 1100.00'  
 T = 195.63'  
 L = 387.21'  
 BANKING 0.072 FT/FT  
 170' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL

621.21 HD STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 63+57.50 - STA. 63+88.75 LT  
~~STA. 63+67.50 - STA. 63+98.75 RT~~  
 STA. 64+32.50 - STA. 64+57.50 LT  
 STA. 64+42.50 - STA. 64+67.50 RT  
 621.205 STEEL BEAM GUARDRAIL W/ 8' POSTS  
 STA 62+65.5 - STA 63+67.5 RT



MATCHLINE STA. 53+50.00 - SEE ABOVE

MATCHLINE STA. 68+50.00 - SEE NEXT SHEET

621.50 MANUFACTURED TERMINAL SECTION, FLARED  
 STA. 55+08.50 - STA. 55+46.00 RT  
 STA. 56+44.00 - STA. 56+81.50 LT  
~~STA. 62+92.50 - STA. 63+30.00 RT~~  
 STA. 63+20.00 - STA. 63+57.50 LT  
 STA. 64+67.50 - STA. 65+05.00 RT  
 STA. 65+57.50 - STA. 65+95.00 LT  
 621.70 GUARDRAIL APPROACH SECTION, GALVANIZED TYPE I  
 STA. 53+50.00 - STA. 53+52.50 LT  
 STA. 54+46.00 - STA. 54+96.00 RT  
 STA. 54+69.00 - STA. 55+19.00 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 53+50.00 - STA. 53+52.50 LT  
 STA. 54+46.00 - STA. 55+46.00 RT  
 STA. 54+69.00 - STA. 55+69.00 LT  
 STA. 63+70.00 - STA. 63+88.75 LT  
 STA. 63+80.00 - STA. 63+98.75 RT  
 STA. 64+32.50 - STA. 64+51.25 LT  
 STA. 64+42.50 - STA. 64+61.25 RT  
 646.20 4 INCH WHITE LINE  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 STA. 53+50 - STA. 68+50 (SOLID LT & RT)

646.21 4 INCH YELLOW LINE  
~~646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC~~  
 STA. 53+50 - STA. 55+44 (DASHED LT)  
 STA. 53+50 - STA. 68+50 (SOLID RT)  
 STA. 55+44 - STA. 68+50 (SOLID LT)  
 646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 53+50 - STA. 68+50 (SOLID LT & RT)  
 646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 53+50 - STA. 55+44 (DASHED LT)  
 STA. 53+50 - STA. 68+50 (SOLID RT)  
 STA. 55+44 - STA. 68+50 (SOLID LT)  
 675.50 REMOVING SIGNS  
 AS SHOWN - I

621.60 ANCHOR FOR SBGR  
 STA 62+80 RT

CURVE #3  
 DELTA = 36° 07' 54"  
 D = 6° 21' 58"  
 R = 900.00'  
 T = 293.57'  
 L = 567.56'  
 BANKING 0.078 FT/FT  
 185' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL

676.10 DELINEATOR WITH STEEL POST  
 STA. 55+46.00 RT  
 STA. 56+81.50 LT  
 STA. 62+92.50 RT+80  
 STA. 63+20.00 LT  
 STA. 65+05.00 RT  
 STA. 65+95.00 LT

- LEGEND
- MAILBOX
  - UTILITY POLE
  - HYDRANT
  - MANHOLE
  - WATER VALVE
  - CATCH BASIN / DROP INLET
  - ⊕ RAILROAD CROSSING SIGNAL
- SIGN LEGEND
- R = REMOVE
  - R&S = REMOVE AND SALVAGE
  - RET = RETAIN
  - S = SALVAGE
  - N = NEW
  - B-B = BACK TO BACK

NOT TO SCALE

PROJECT LAYOUT SHEET 8

PROJECT NAME: TROY-NEWPORT	FILE NAME: 06B056.dgn	PLOT DATE: 25-OCT-2011 12:18
PROJECT NUMBER: STP 2613(I)	PROJECT LEADER: CDL	DRAWN BY: SJL
	DESIGNED BY: SJL	CHECKED BY: EPD
	PLOT FILE: 06b056*21.i	SHEET 21 OF 31

~~646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC~~  
 646.21 4 INCH YELLOW LINE  
 STA. 68+50 - STA. 78+14 (SOLID RT)  
 STA. 68+50 - STA. 82+50 (SOLID LT)  
 STA. 78+14 - STA. 82+50 (DASHED RT)

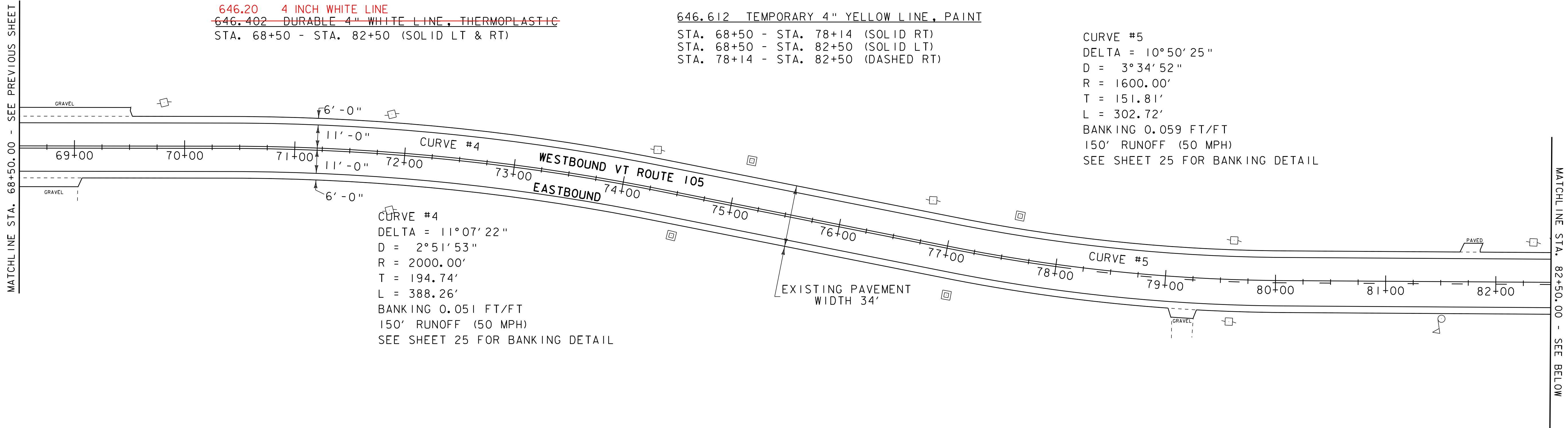
646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 68+50 - STA. 82+50 (SOLID LT & RT)

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 68+50 - STA. 78+14 (SOLID RT)  
 STA. 68+50 - STA. 82+50 (SOLID LT)  
 STA. 78+14 - STA. 82+50 (DASHED RT)

CURVE #5  
 DELTA = 10°50'25"  
 D = 3°34'52"  
 R = 1600.00'  
 T = 151.81'  
 L = 302.72'  
 BANKING 0.059 FT/FT  
 150' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL

~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 646.20 4 INCH WHITE LINE  
 STA. 68+50 - STA. 82+50 (SOLID LT & RT)

CURVE #4  
 DELTA = 11°07'22"  
 D = 2°51'53"  
 R = 2000.00'  
 T = 194.74'  
 L = 388.26'  
 BANKING 0.051 FT/FT  
 150' RUNOFF (50 MPH)  
 SEE SHEET 25 FOR BANKING DETAIL

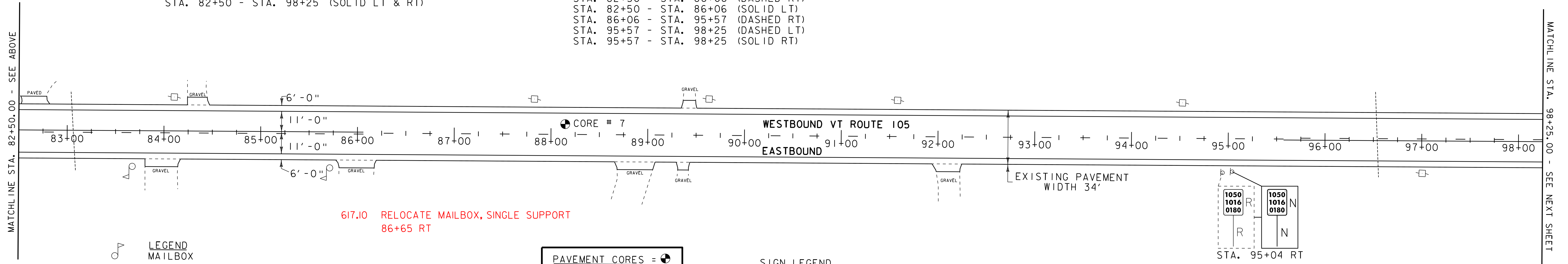


~~646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC~~  
 646.21 4 INCH YELLOW LINE  
 STA. 82+50 - STA. 86+06 (DASHED RT)  
 STA. 82+50 - STA. 86+06 (SOLID LT)  
 STA. 86+06 - STA. 95+57 (DASHED RT)  
 STA. 95+57 - STA. 98+25 (DASHED LT)  
 STA. 95+57 - STA. 98+25 (SOLID RT)  
 646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 82+50 - STA. 98+25 (SOLID LT & RT)

675.50 REMOVING SIGNS  
 AS SHOWN - 1

~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 646.20 4 INCH WHITE LINE  
 STA. 82+50 - STA. 98+25 (SOLID LT & RT)

646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 82+50 - STA. 86+06 (DASHED RT)  
 STA. 82+50 - STA. 86+06 (SOLID LT)  
 STA. 86+06 - STA. 95+57 (DASHED RT)  
 STA. 95+57 - STA. 98+25 (DASHED LT)  
 STA. 95+57 - STA. 98+25 (SOLID RT)

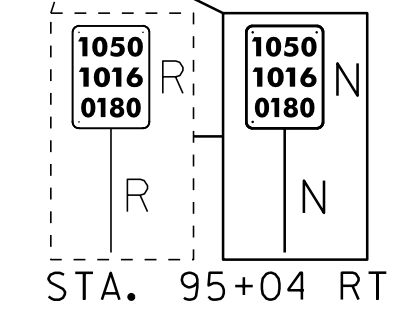


617.10 RELOCATE MAILBOX, SINGLE SUPPORT  
 86+65 RT

- LEGEND
- MAILBOX
  - UTILITY POLE
  - ⊙ HYDRANT
  - MANHOLE
  - WATER VALVE
  - ⊞ CATCH BASIN / DROP INLET
  - ⊕ RAILROAD CROSSING SIGNAL

PAVEMENT CORES = ⊕  
 STA. 88+22 LT.  
 CORE # 7  
 TOTAL DEPTH PCC  
 9' NO

- SIGN LEGEND
- R = REMOVE
  - R&S = REMOVE AND SALVAGE
  - RET = RETAIN
  - S = SALVAGE
  - N = NEW
  - B-B = BACK TO BACK



**PROJECT LAYOUT SHEET 9**

NOT TO SCALE

PROJECT NAME: TROY-NEWPORT  
 PROJECT NUMBER: STP 2613(I)

FILE NAME: 06B056.dgn  
 PROJECT LEADER: CDL  
 DESIGNED BY: SJL  
 PLOT FILE: 06b056\*22.1

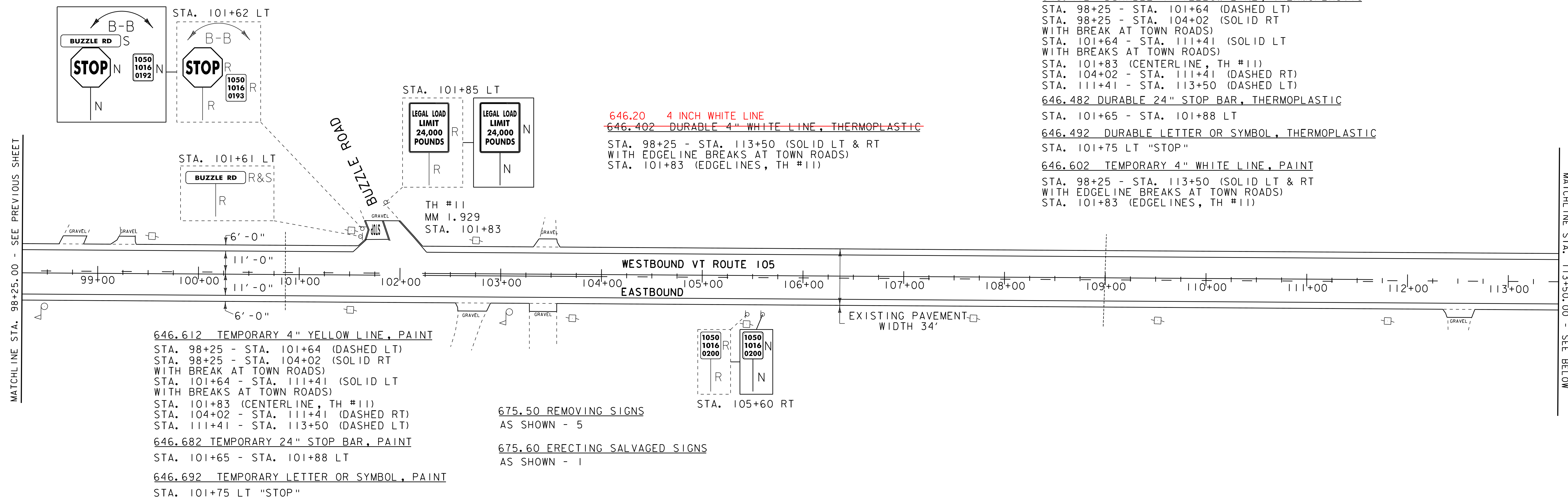
PLOT DATE: 25-OCT-2011 12:18  
 DRAWN BY: SJL  
 CHECKED BY: EPD  
 SHEET 22 OF 31

646.21 4 INCH YELLOW LINE  
~~646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC~~  
 STA. 98+25 - STA. 101+64 (DASHED LT)  
 STA. 98+25 - STA. 104+02 (SOLID RT WITH BREAK AT TOWN ROADS)  
 STA. 101+64 - STA. 111+41 (SOLID LT WITH BREAKS AT TOWN ROADS)  
 STA. 101+83 (CENTERLINE, TH #11)  
 STA. 104+02 - STA. 111+41 (DASHED RT)  
 STA. 111+41 - STA. 113+50 (DASHED LT)  
 646.482 DURABLE 24" STOP BAR, THERMOPLASTIC  
 STA. 101+65 - STA. 101+88 LT  
 646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 101+75 LT "STOP"  
 646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 98+25 - STA. 113+50 (SOLID LT & RT WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 101+83 (EDGELINES, TH #11)

646.20 4 INCH WHITE LINE  
~~646.402 DURABLE 4" WHITE LINE, THERMOPLASTIC~~  
 STA. 98+25 - STA. 113+50 (SOLID LT & RT WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 101+83 (EDGELINES, TH #11)

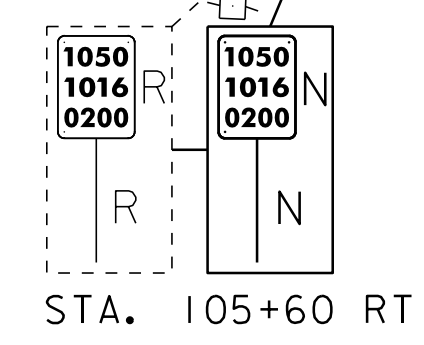
MATCHLINE STA. 98+25.00 - SEE PREVIOUS SHEET

MATCHLINE STA. 113+50.00 - SEE BELOW



646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 98+25 - STA. 101+64 (DASHED LT)  
 STA. 98+25 - STA. 104+02 (SOLID RT WITH BREAK AT TOWN ROADS)  
 STA. 101+64 - STA. 111+41 (SOLID LT WITH BREAKS AT TOWN ROADS)  
 STA. 101+83 (CENTERLINE, TH #11)  
 STA. 104+02 - STA. 111+41 (DASHED RT)  
 STA. 111+41 - STA. 113+50 (DASHED LT)  
 646.682 TEMPORARY 24" STOP BAR, PAINT  
 STA. 101+65 - STA. 101+88 LT  
 646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 101+75 LT "STOP"

675.50 REMOVING SIGNS  
 AS SHOWN - 5  
 675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 1



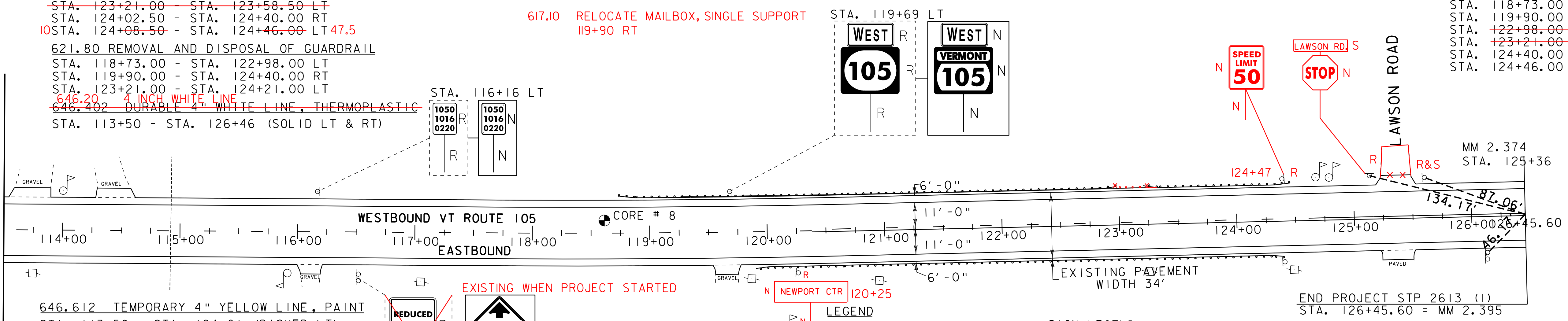
621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 119+10.50 - STA. 122+60.50 LT  
 STA. 120+27.50 - STA. 124+02.50 RT  
 STA. 123+58.50 - STA. 124+08.50 LT  
 621.50 MANUFACTURED TERMINAL SECTION, FLARED  
 STA. 118+73.00 - STA. 119+10.50 LT  
 STA. 119+90.00 - STA. 120+27.50 RT  
 STA. 122+60.50 - STA. 122+98.00 LT  
 STA. 123+21.00 - STA. 123+58.50 LT  
 STA. 124+02.50 - STA. 124+40.00 RT  
 STA. 124+08.50 - STA. 124+46.00 LT  
 621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 118+73.00 - STA. 122+98.00 LT  
 STA. 119+90.00 - STA. 124+40.00 RT  
 STA. 123+21.00 - STA. 124+21.00 LT

646.21 4 INCH YELLOW LINE  
~~646.412 DURABLE 4" YELLOW LINE, THERMOPLASTIC~~  
 STA. 113+50 - STA. 124+61 (DASHED LT)  
 STA. 120+38 - STA. 126+45.60 (SOLID RT)  
 STA. 124+61 - STA. 126+45.60 (SOLID LT)  
 646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 126+10 RT "SCHOOL"  
 617.10 RELOCATE MAILBOX, SINGLE SUPPORT  
 119+90 RT

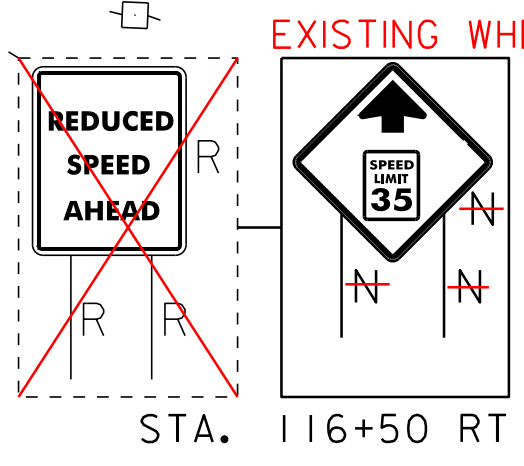
646.602 TEMPORARY 4" WHITE LINE, PAINT  
 STA. 113+50 - STA. 126+45.60 (SOLID LT & RT)

675.50 REMOVING SIGNS  
 AS SHOWN - 4  
 675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 1  
 676.10 DELINEATOR WITH STEEL POST  
 STA. 118+73.00 LT  
 STA. 119+90.00 RT  
 STA. 122+98.00 LT  
 STA. 123+21.00 LT  
 STA. 124+40.00 RT  
 STA. 124+46.00 LT

MATCHLINE STA. 113+50.00 - SEE ABOVE



646.612 TEMPORARY 4" YELLOW LINE, PAINT  
 STA. 113+50 - STA. 124+61 (DASHED LT)  
 STA. 120+38 - STA. 126+45.60 (SOLID RT)  
 STA. 124+61 - STA. 126+45.60 (SOLID LT)



PAVEMENT CORES =   
 STA. 118+69 LT.  
 CORE # 8  
 TOTAL DEPTH PCC  
 6' NO

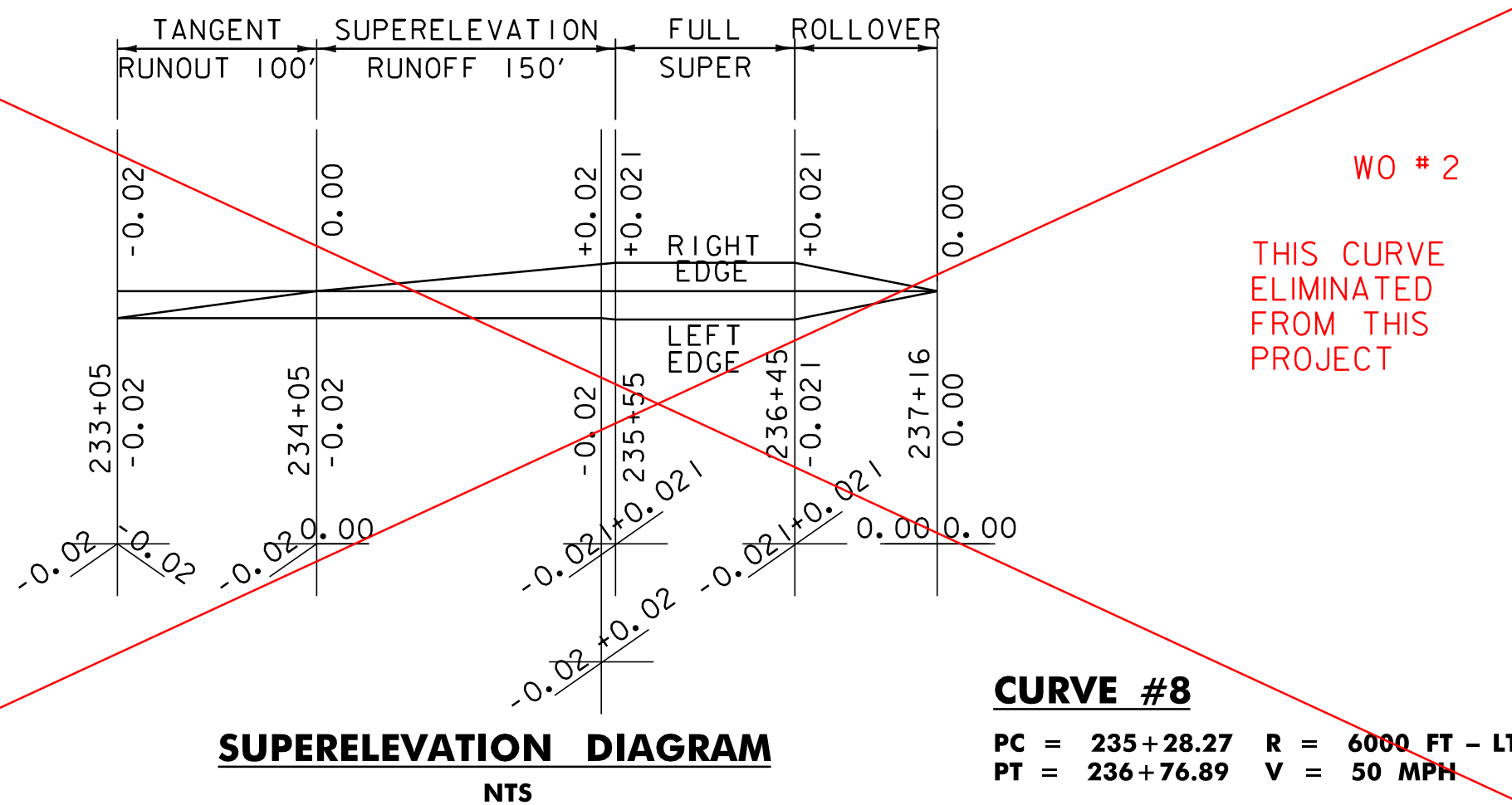
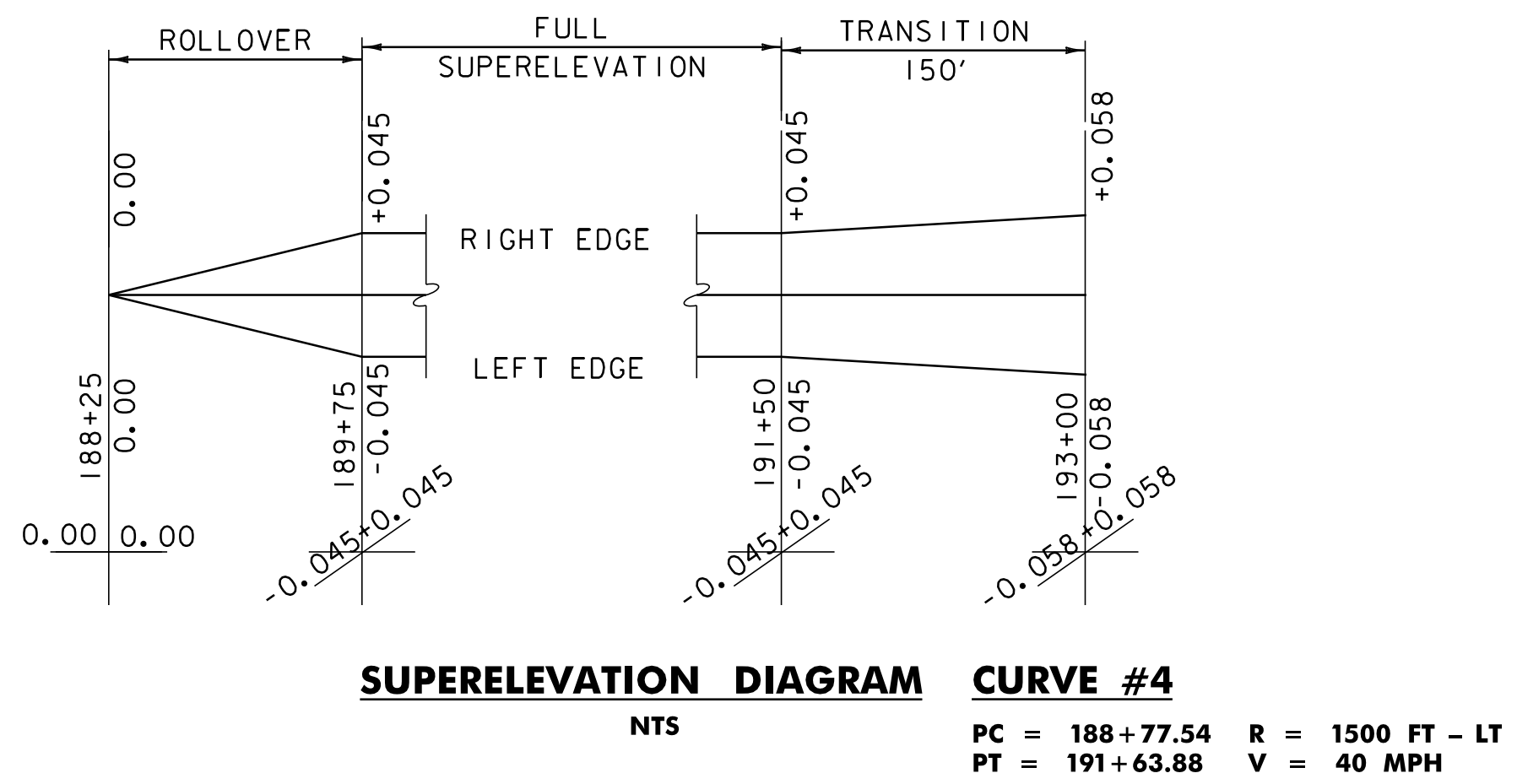
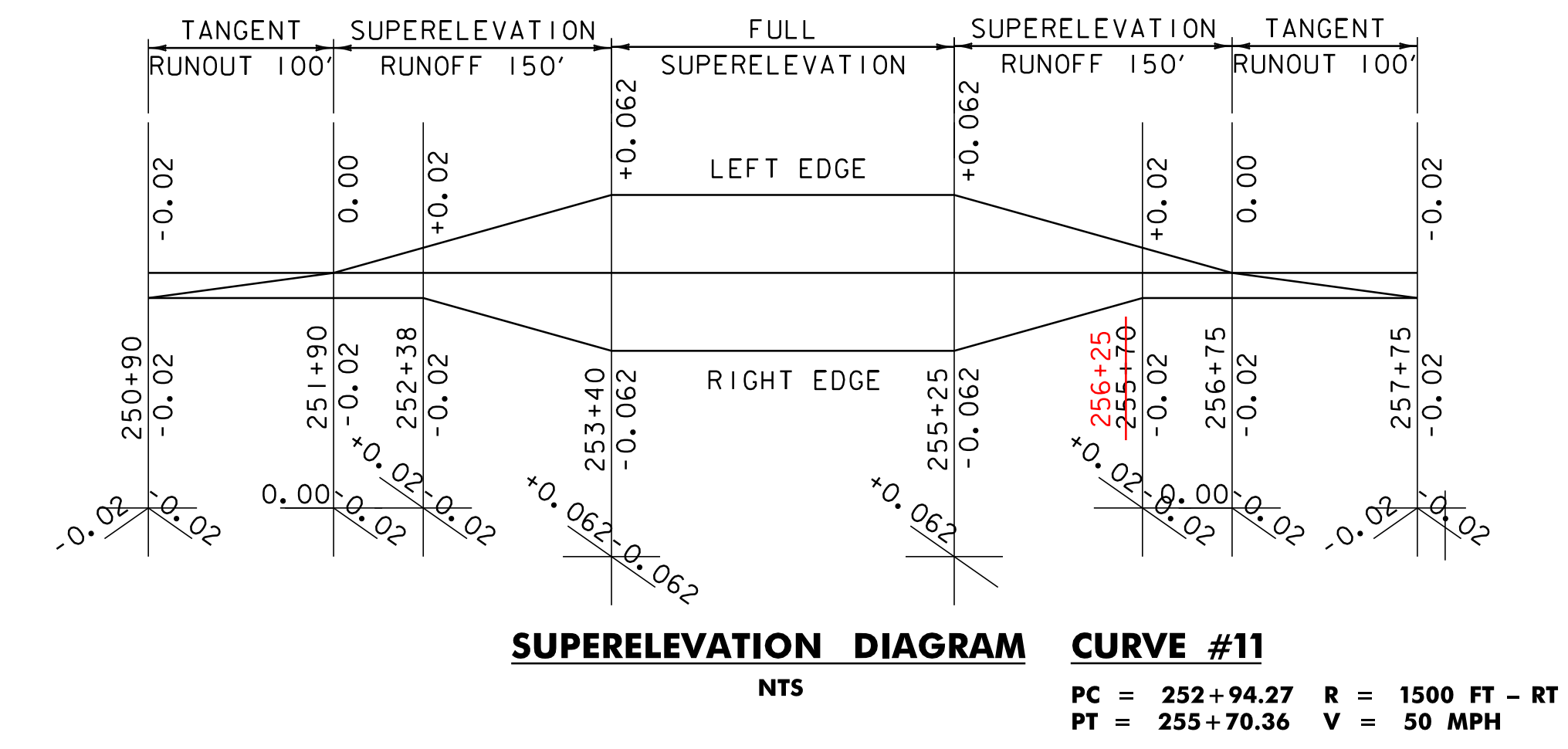
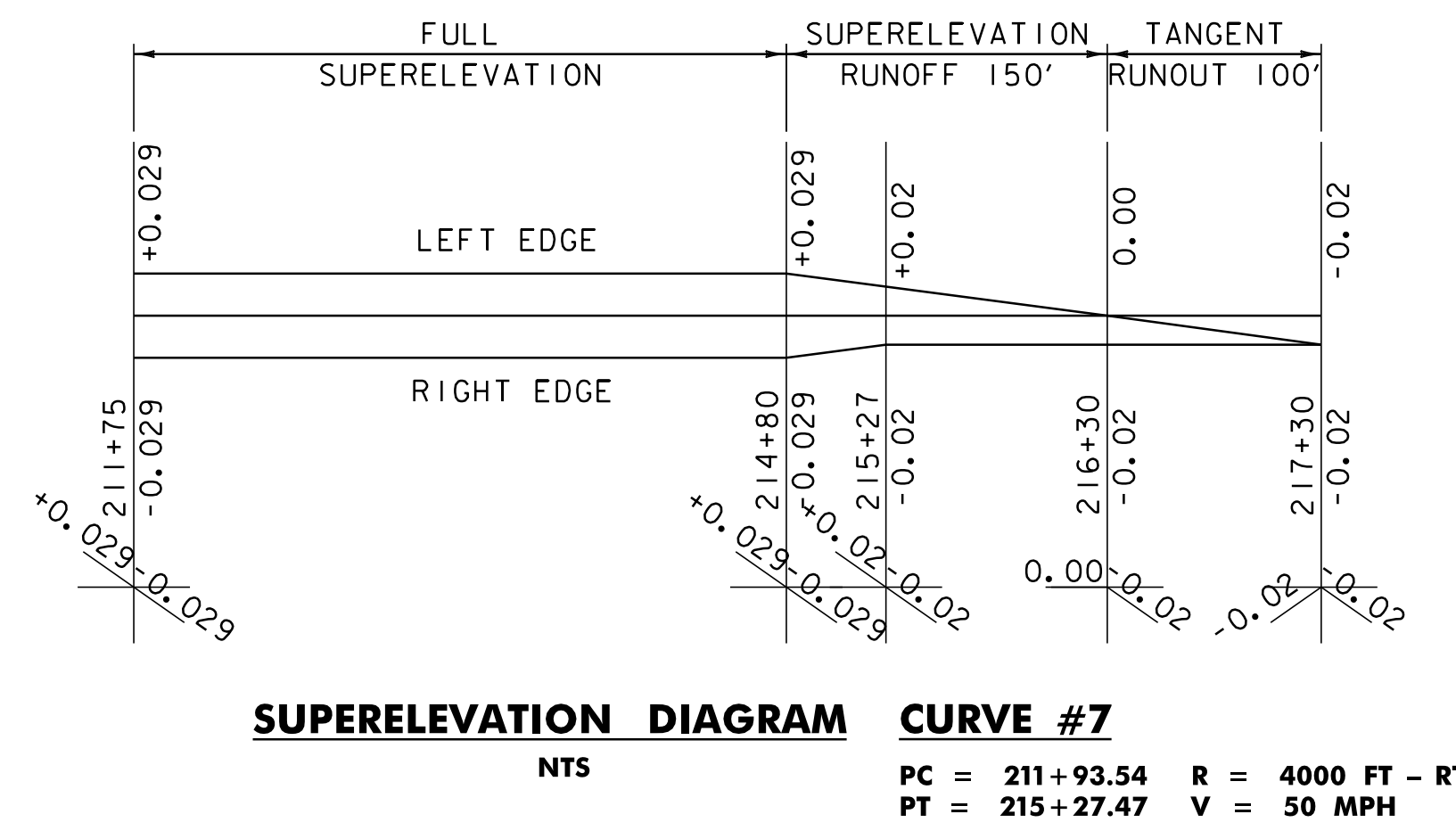
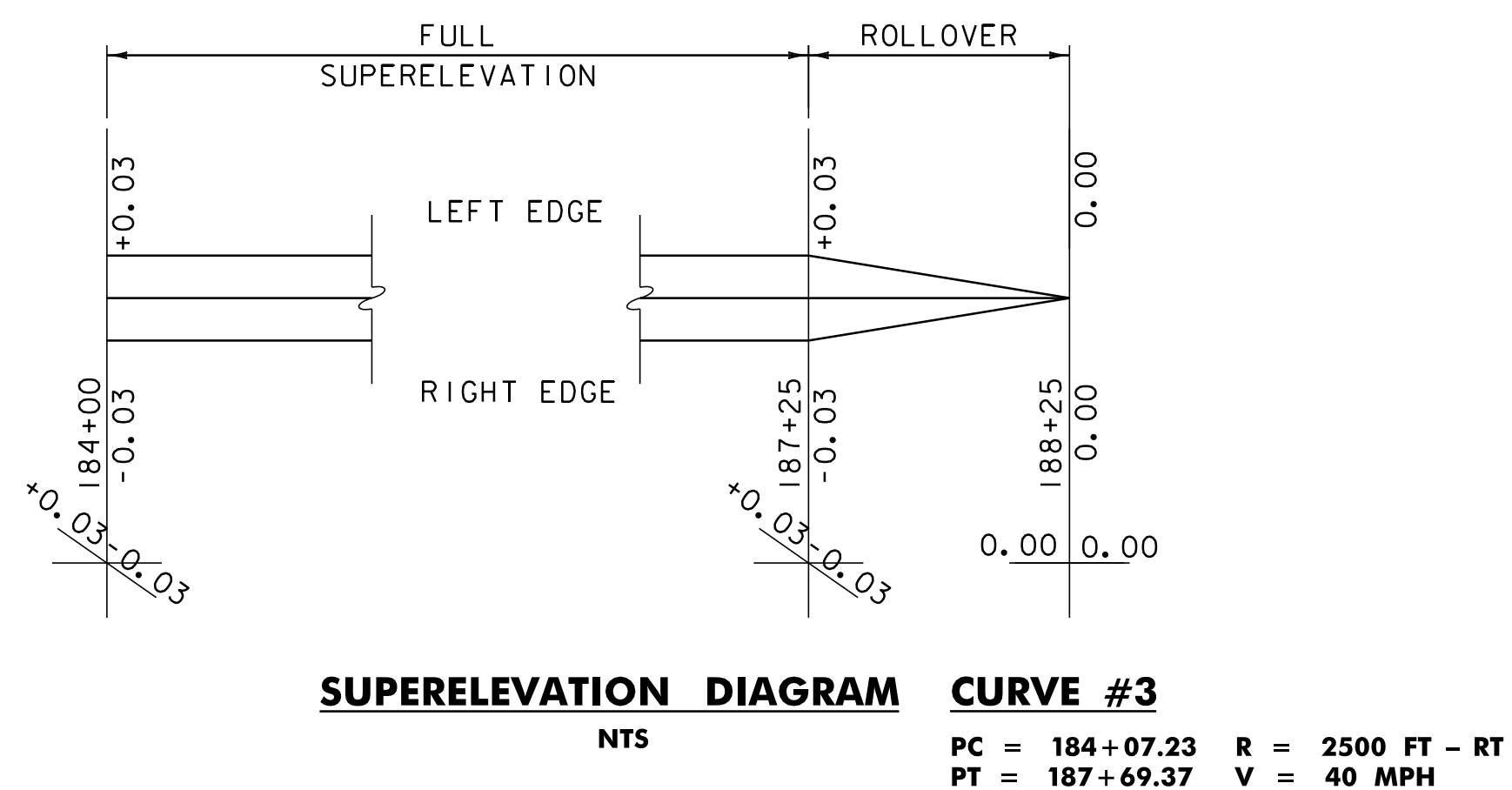
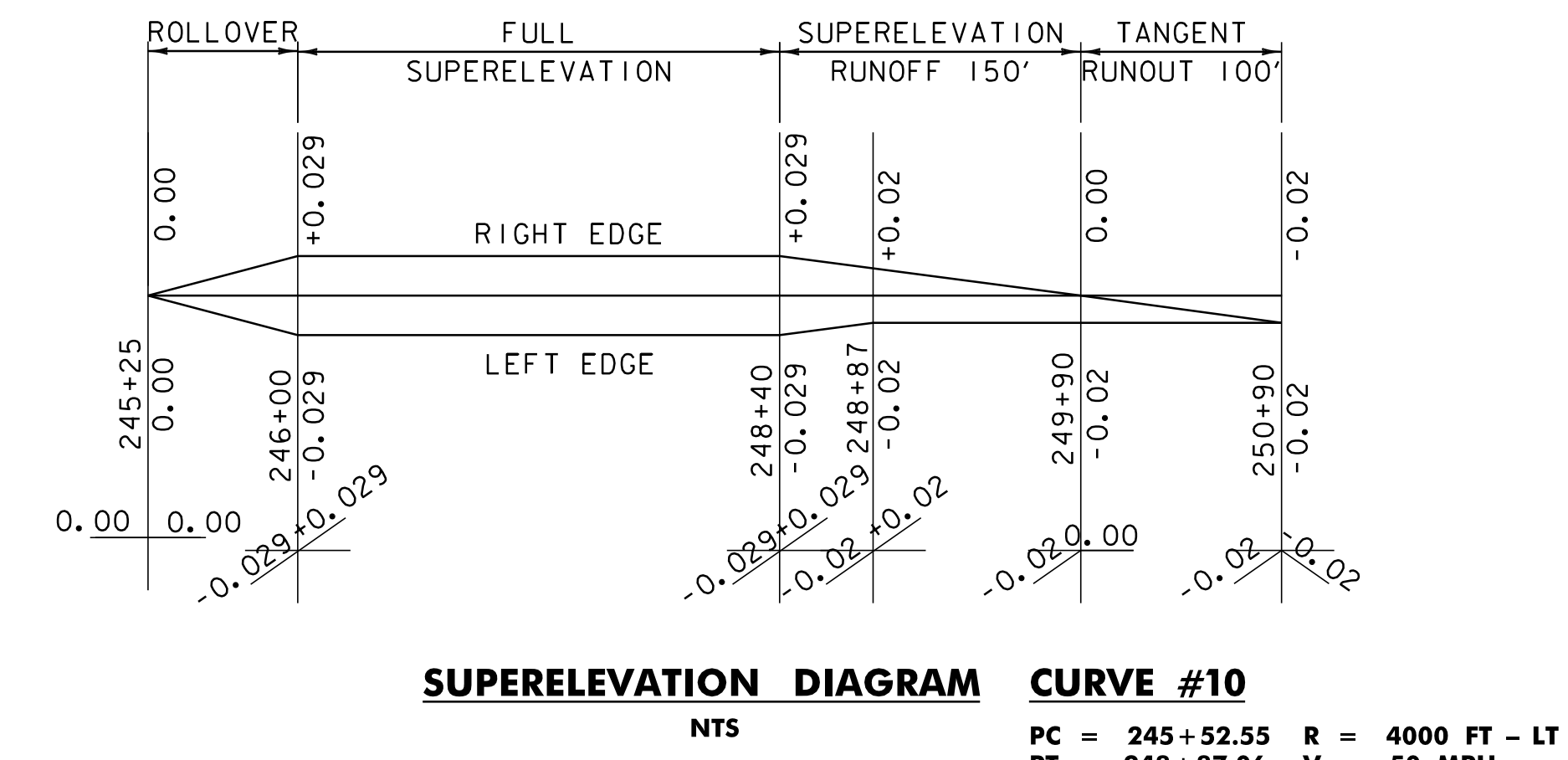
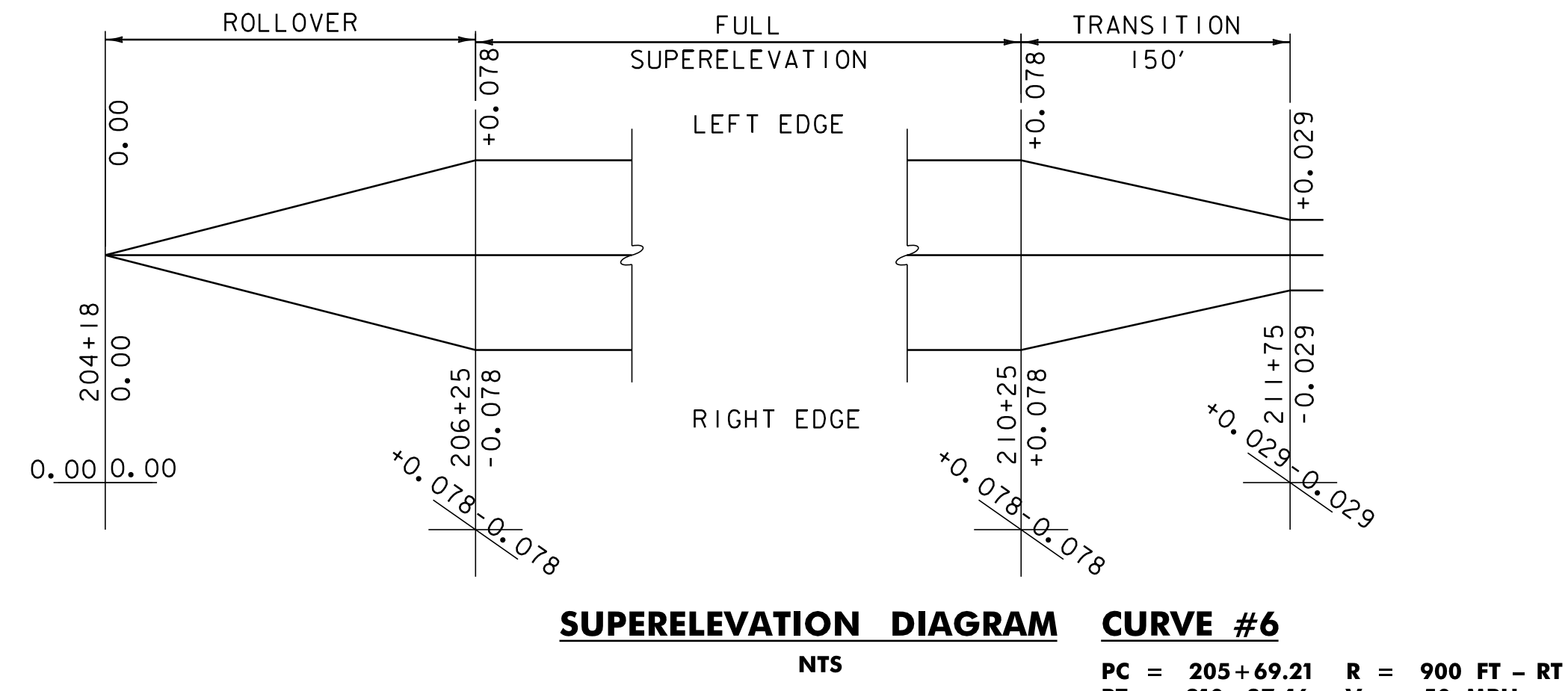
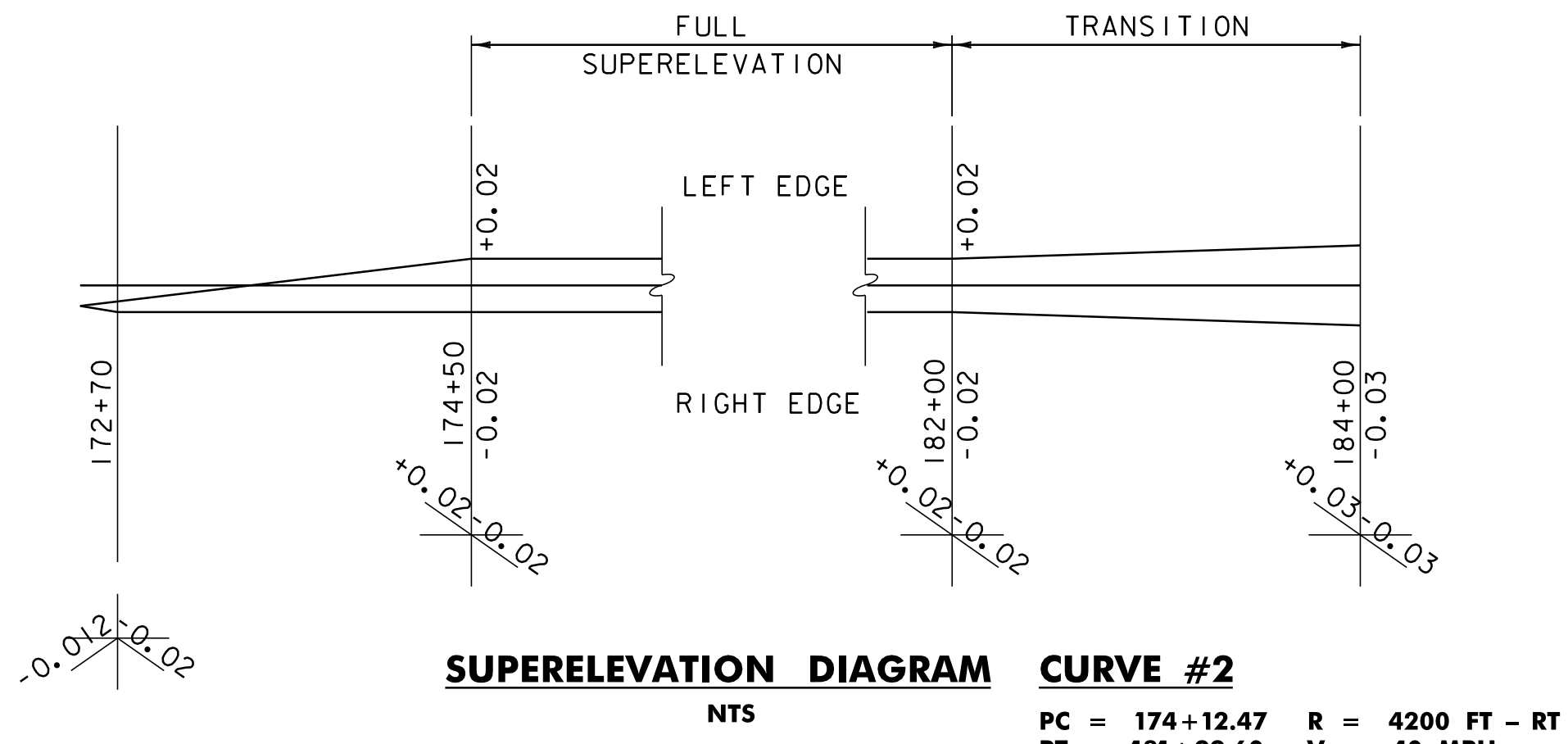
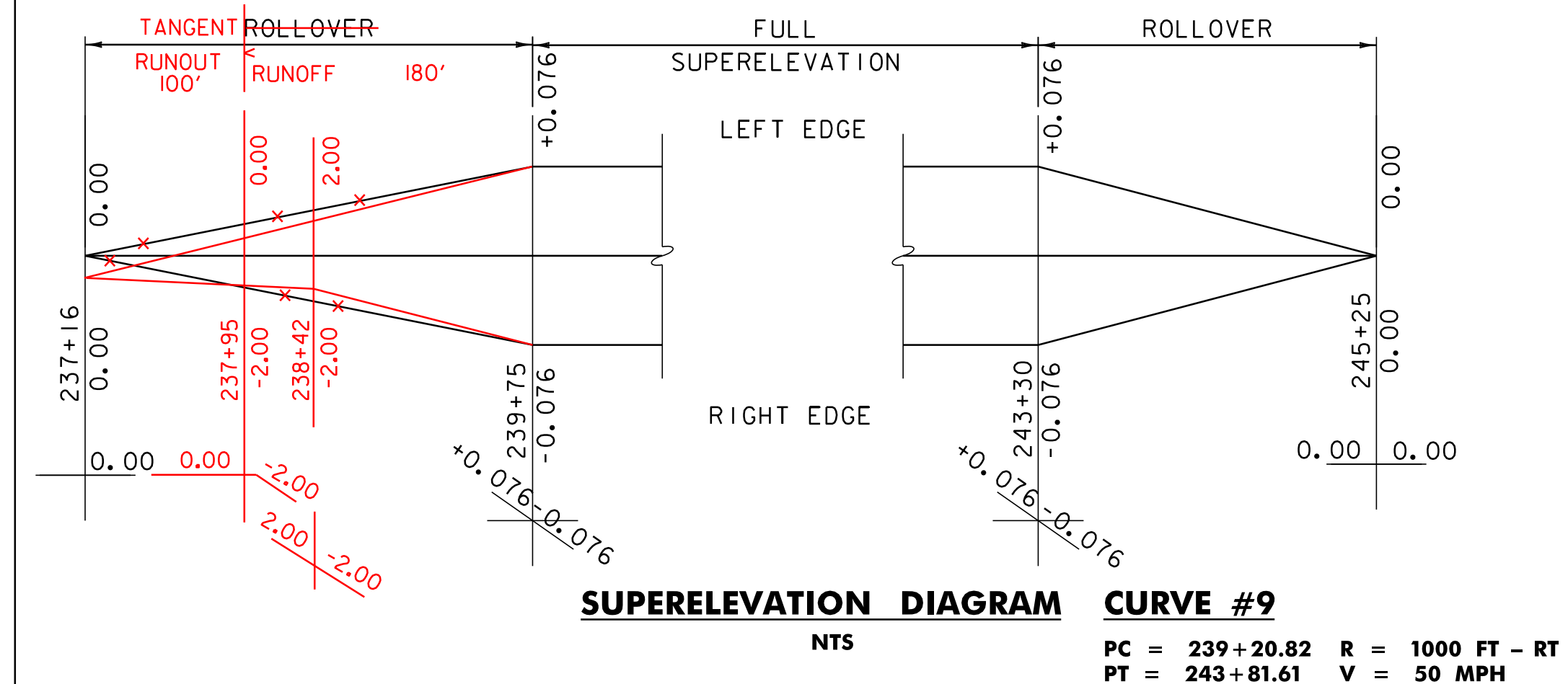
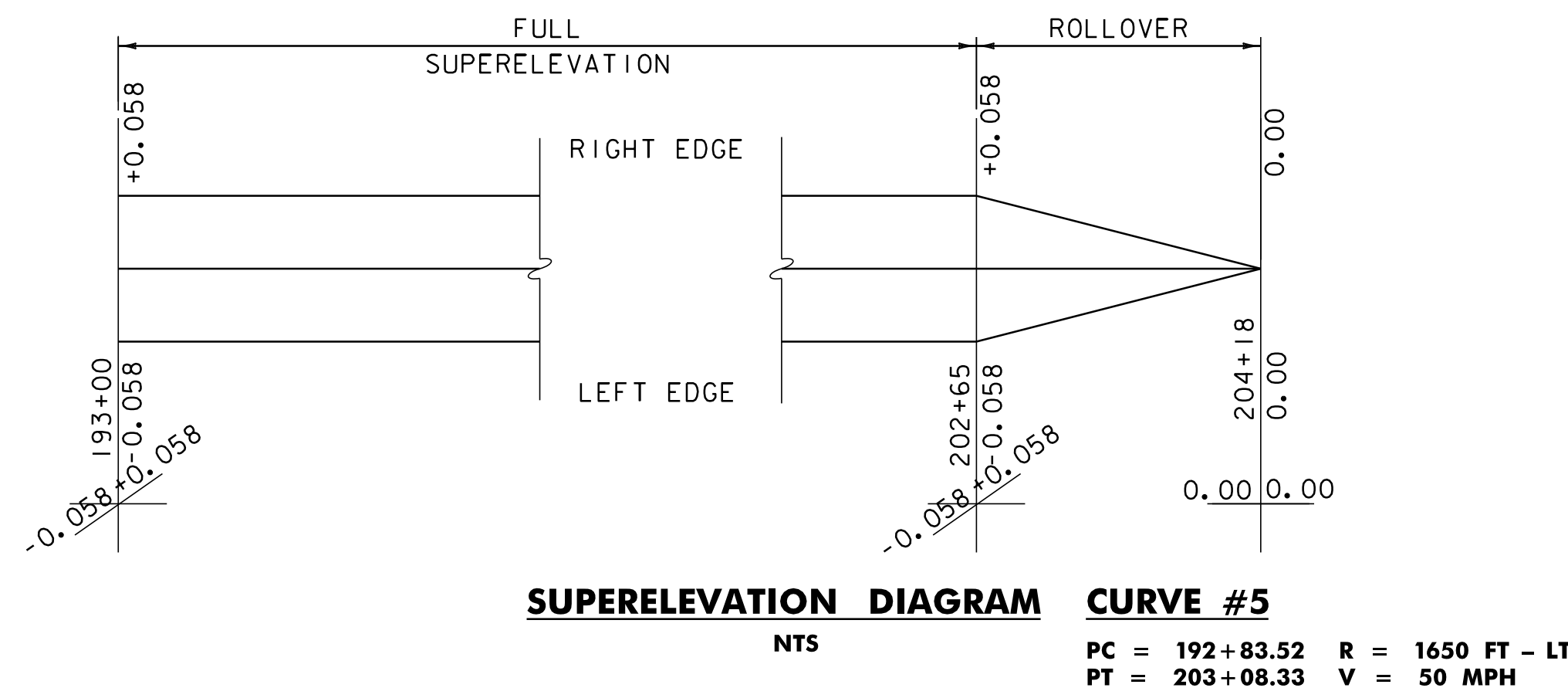
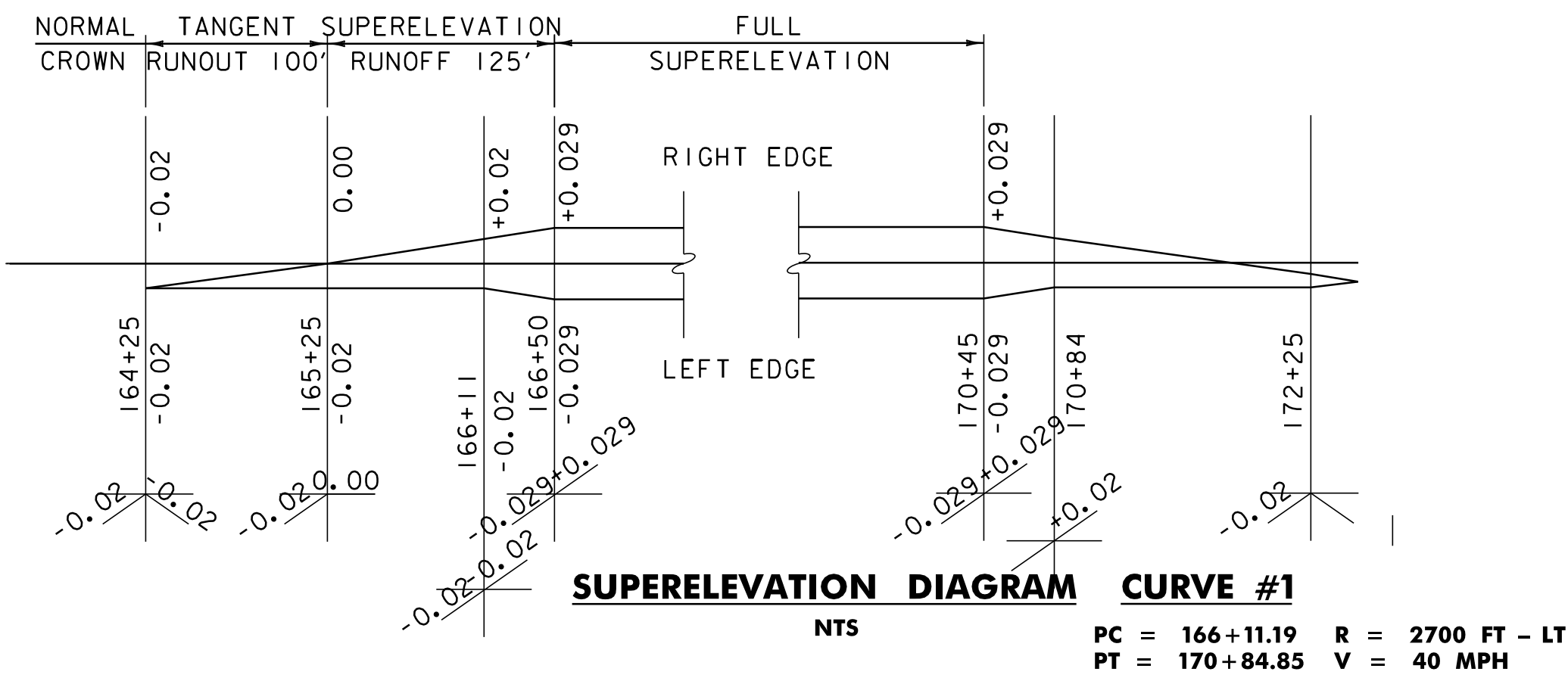
LEGEND  
  
 MAILBOX  
 UTILITY POLE  
 HYDRANT  
 MANHOLE  
 WATER VALVE  
 CATCH BASIN / DROP INLET  
 RAILROAD CROSSING SIGNAL

SIGN LEGEND  
 R = REMOVE  
 R&S = REMOVE AND SALVAGE  
 RET = RETAIN  
 S = SALVAGE  
 N = NEW  
 B-B = BACK TO BACK

**PROJECT LAYOUT SHEET 10**

PROJECT NAME: TROY-NEWPORT  
 PROJECT NUMBER: STP 2613(I)  
 FILE NAME: 06B056.dgn  
 PROJECT LEADER: CDL  
 DESIGNED BY: SJL  
 PLOT FILE: 06b056\*23.1  
 PLOT DATE: 25-OCT-2011 12:18  
 DRAWN BY: SJL  
 CHECKED BY: EPD  
 SHEET 23 OF 31

NOT TO SCALE



WO # 2

THIS CURVE  
 ELIMINATED  
 FROM THIS  
 PROJECT

<b>SUPERELEVATION BANKING DIAGRAM SHEET #1</b>	PROJECT NAME: TROY-NEWPORT	PLOT DATE: 25-OCT-2011 12:18
	PROJECT NUMBER: STP 2613 (I)	DRAWN BY: SJL
	FILE NAME: 06B056.DGN	CHECKED BY: CDL
	DESIGNED BY: SJL	SHEET 24 OF 31
	PLOT FILE: 06b056*24.i	







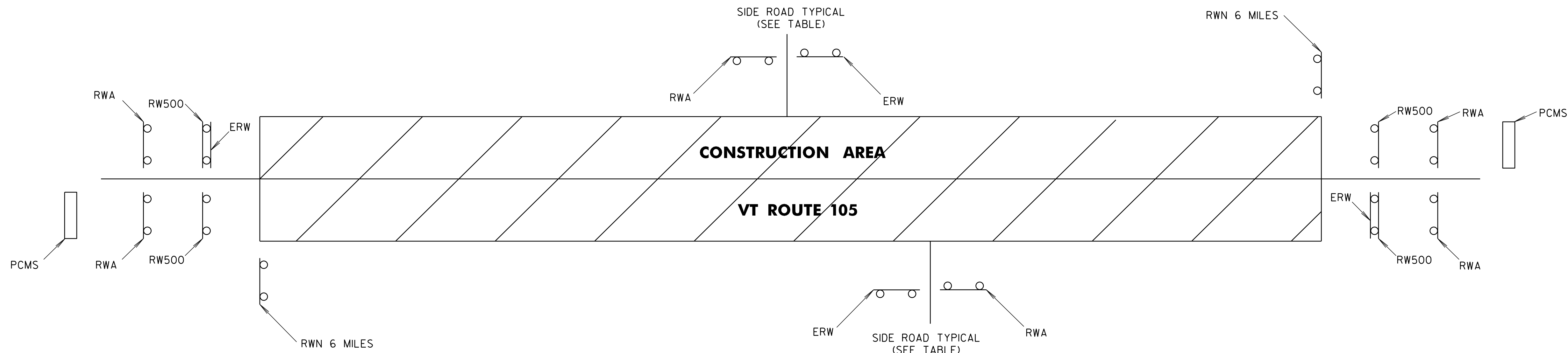






**CONSTRUCTION APPROACH SIGNING**

NOT TO SCALE  
SEE STD E-100, E-100A AND E-103 FOR SIGN PLACEMENT



**NOTES:**

- THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION. THE COST OF PREPARING THIS PLAN (AND MAKING CHANGES IF NECESSARY) SHALL NOT BE PAID SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 641.10, "TRAFFIC CONTROL".
- THE CONTRACTOR SHALL INCLUDE A CONSTRUCTION SIGN APPROACH PACKAGE FOR EXPECTED LANE CLOSURES AND WORK ZONE SPEED REDUCTIONS IN COMPLIANCE WITH THE TRAFFIC CONTROL NOTES AND PART 6 OF THE MUTCD. PAYMENT FOR PROVIDING THIS PACKAGE WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 641.10, "TRAFFIC CONTROL".
- THE BID PRICE FOR "TRAFFIC CONTROL", ITEM 641.10, SHALL INCLUDE ALL OF THE FOLLOWING, AS NEEDED: APPROACH AND ON-PROJECT CONSTRUCTION SIGNING, PORTABLE ARROW BOARDS, BARRIERS, BARRELS, CONES, BARRICADES, TEMPORARY REGULATORY AND WARNING SIGNS, AND POSTS AS DETAILED IN VTRANS STANDARDS. ALL ADJUSTING, RELOCATING, AND REMOVING OF THESE DEVICES AS DIRECTED BY THE RESIDENT ENGINEER SHALL ALSO BE INCLUDED. THE FOLLOWING ITEMS WILL BE PAID FOR SEPARATELY:  
646.602, 646.612, 646.682, 646.692, AND 646.712 - TEMPORARY PAVEMENT MARKINGS  
630.10 AND 630.15 - UNIFORMED TRAFFIC OFFICERS AND FLAGGERS
- PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) WILL BE PROVIDED FOR USE ALONG THIS PROJECT. THE PLACEMENT OF THESE UNITS AS WELL AS THE MESSAGE WILL BE APPROVED BY THE RESIDENT ENGINEER. THESE SIGNS WILL BE PAID FOR UNDER ITEM 641.15, "PORTABLE CHANGEABLE MESSAGE SIGN".  
  
PCMS SHOULD NOT REPLACE ANY OF THE SIGNING DETAILED IN THE MUTCD AND SHOULD NOT BE USED IF STANDARD TRAFFIC CONTROL DEVICES ADEQUATELY PROVIDE THE INFORMATION THE MOTORISTS NEED TO TRAVEL SAFELY.  
  
THE PCMS SHALL CONSIST OF EITHER ONE OR TWO PHASES. TYPICALLY, A PHASE SHALL CONSIST OF UP TO THREE LINES OF EIGHT CHARACTERS PER LINE. THE PCMS SHOULD BE USED AS A SUPPLEMENT AND NOT AS A SUBSTITUTE FOR CONVENTIONAL SIGNS AND PAVEMENT MARKINGS.  
  
THE PCMS SHOULD COMMUNICATE WHAT INFORMATION MOTORISTS NEED TO KNOW. UNNECESSARY INFORMATION SHOULD BE AVOIDED. MESSAGES SHOULD BE UPDATED PERIODICALLY TO DESCRIBE THE WORK ACTIVITY OCCURRING SO THAT THE PCMS CONTINUES TO COMMAND THE ATTENTION OF MOTORISTS.
- THE MUTCD 2003 SHALL BE THE STANDARD FOR ALL TRAFFIC CONTROL DEVICES. EXISTING SIGNS, SIGNALS AND MARKINGS SHALL BE VALID UNTIL SUCH TIME AS THEY ARE REPLACED OR RECONSTRUCTED. WHEN NEW TRAFFIC CONTROL DEVICES ARE ERECTED OR PLACED OR EXISTING TRAFFIC CONTROL DEVICES ARE REPLACED OR REPAIRED THE EQUIPMENT, DESIGN, METHOD OF INSTALLATION, PLACEMENT OR REPAIR SHALL CONFORM WITH SUCH STANDARDS.
- NO CONSTRUCTION SIGNS SHALL BE INSTALLED AS TO INTERFERE OR OBSTRUCT THE VIEW OF EXISTING TRAFFIC CONTROL DEVICES, STOPPING SIGHT DISTANCE, AND CORNER SIGHT DISTANCE FROM DRIVES AND TOWN HIGHWAYS.
- SEE THE TRAFFIC CONTROL NOTES FOR ADDITIONAL INFORMATION REGARDING IMPLEMENTATION OF THE TEMPORARY TRAFFIC CONTROL DEVICES.

**INTRODUCTION**

THE FOLLOWING TRAFFIC CONTROL INFORMATION IS INTENDED TO BE A GENERAL OUTLINE FOR HOW THE WORK SHOULD PROCEED. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE SPECIFIC DETAILS TO ADDRESS SPECIFIC SITUATIONS. THIS RESPONSIBILITY INCLUDES PROVIDING A PLAN DETAILING THE USE AND PLACEMENT OF SIGNS, CHANNELING DEVICES, ARROW PANELS, FLAGGERS AND UNIFORMED TRAFFIC OFFICERS (UTO'S) DURING LANE CLOSURES. IF THE CONTRACTOR DOES NOT WISH TO FOLLOW THIS OUTLINE, THE CONTRACTOR MAY SUBMIT AN ALTERNATE PROPOSAL TO THE PAVEMENT MANAGEMENT SECTION VIA THE ENGINEER. THE CONTRACTOR MUST ALLOW AT LEAST 4 WEEKS FOR REVIEW AND APPROVAL OF THE COMPREHENSIVE PLAN AND 2 WEEKS FOR REVIEW AND APPROVAL OF MINOR CHANGES/DETAILS. ALL TRAFFIC CONTROL DETAILS MUST BE DESIGNED AND IMPLEMENTED IN ACCORDANCE WITH THE MUTCD AND VTRANS STANDARDS E-100, E-100A, E-101, E-102, E-102A, E-106, E-107, E-107A, E-108A, E-103, E-110, E-111 AND E-112.

**TRAFFIC CONTROL NOTES**

ALL WORK SHALL BE DONE BETWEEN THE HOURS OF 7:00 AM AND 7:00 PM.  
  
THE CONTRACTOR SHALL BE OUT OF THE ROAD BY 3:00 PM EACH AND EVERY FRIDAY UNLESS GIVEN PERMISSION BY THE RESIDENT ENGINEER.  
  
A MINIMUM LANE WIDTH OF 10 FT. SHALL BE MAINTAINED.  
  
WHEN COLD PLANED BITUMINOUS PAVEMENT IS OPEN TO TRAFFIC, A "MOTORCYCLES USE CAUTION" SIGN, AS PER VTRANS STANDARD E-102A, SHALL BE PROVIDED.  
  
IT IS INTENDED TO HAVE NO LONGITUDINAL DROP-OFFS DURING THE OVERNIGHT HOURS. THEREFORE, THE FULL ROADWAY WIDTH SHOULD BE COLD PLANED OR PAVED DURING THE DAILY WORK PERIOD. WHEN NECESSARY, DROP-OFF PROTECTION IN THESE AREAS SHALL CONFORM TO VTRANS STANDARD E-108A.  
  
MAINTAIN ACCESS TO ALL PROPERTIES AT ALL TIMES FOR EMERGENCY VEHICLES. MAINTAIN ACCESS TO ALL COMMERCIAL AND MUNICIPAL PROPERTIES DURING BUSINESS HOURS. ACCESS TO RESIDENTIAL PROPERTIES MAY BE RESTRICTED FOR A SHORT DURATION (A FEW HOURS) WITH PERMISSION OF THE OWNER. COORDINATE MAJOR WORK ON COMMERCIAL OR MUNICIPAL ACCESSES WITH THE OWNER AT LEAST ONE WEEK PRIOR TO STARTING THE WORK. ALL ACCESSES SHALL ALSO BE KEPT FREE OF WORK AND TRAFFIC CONTROLLED BY UNIFORMED TRAFFIC OFFICERS WHEN REQUIRED.

DRUMS SHOULD BE USED TO CHANNELIZE OR DELINEATE TRAFFIC FLOW WHILE REFLECTORIZED CONES ARE BETTER SUITED TO DELINEATE DRIVES WITHIN THE WORKZONE.

SIDE ROAD NAME	ROAD WORK AHEAD	END ROAD WORK	ROAD WORK 500 FT	ROAD WORK NEXT 6 MILES	PCMS
<b>TROY</b>					
BEGIN PROJECT (VT 105)	2	1	2	1	1
RIVER RD	1	1			
CEMETARY RD	1	1			
EAST HILL	1	1			
BLAIS RD	1	1			
<b>NEWPORT</b>					
BEAR MTN RD	1	1			
BUZZLE RD	1	1			
LAWSON RD	1	1			
END PROJECT (VT 105)	2	1	2	1	1
TOTALS	11	9	4	2	2

**LEGEND**

- RWA = ROAD WORK AHEAD
- RW500 = ROAD WORK 500 FT
- ERW = END ROAD WORK
- RWN = ROAD WORK NEXT 6 MILES

<b>CONSTRUCTION APPROACH SIGNING SHEET</b>	PROJECT NAME: TROY-NEWPORT
	PROJECT NUMBER: STP 2613(1)
	FILE NAME: 06B056.dgn
	PLOT FILE: 06b056*31.i
	PLOT DATE: 25-OCT-2011 12:19
	DRAWN BY: SJL
	CHECKED BY: EPD
	SHEET 31 OF 31