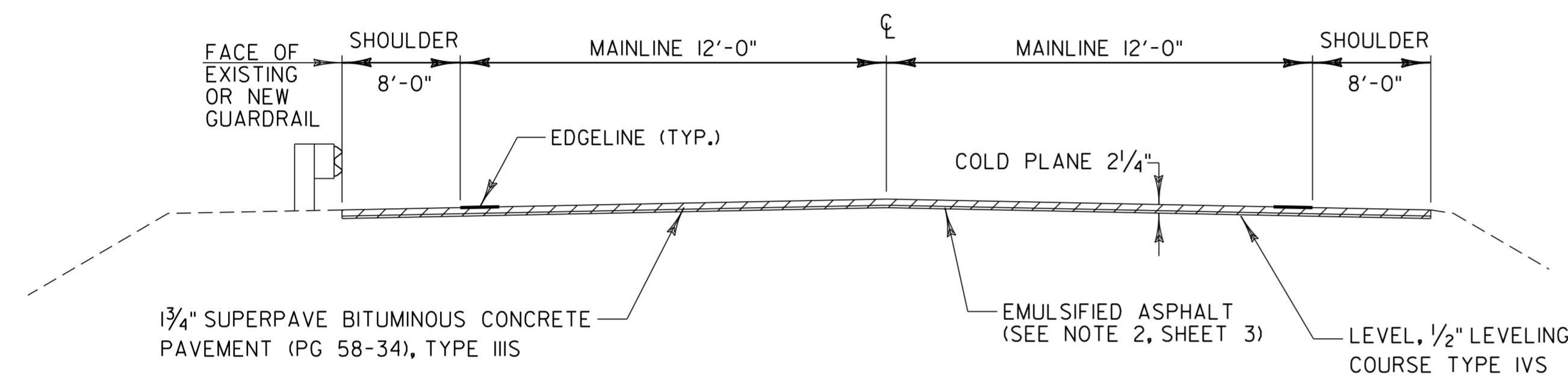
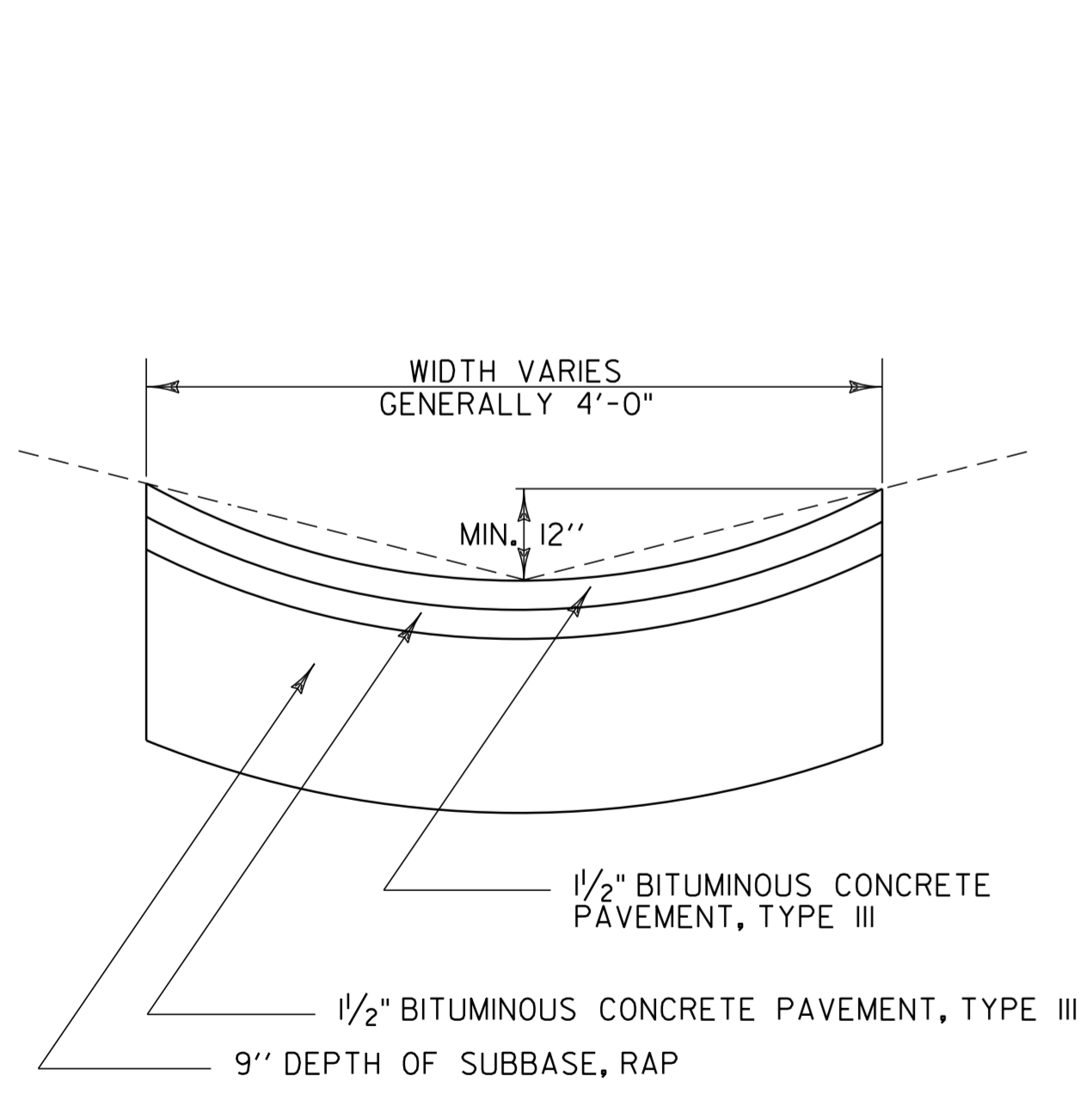


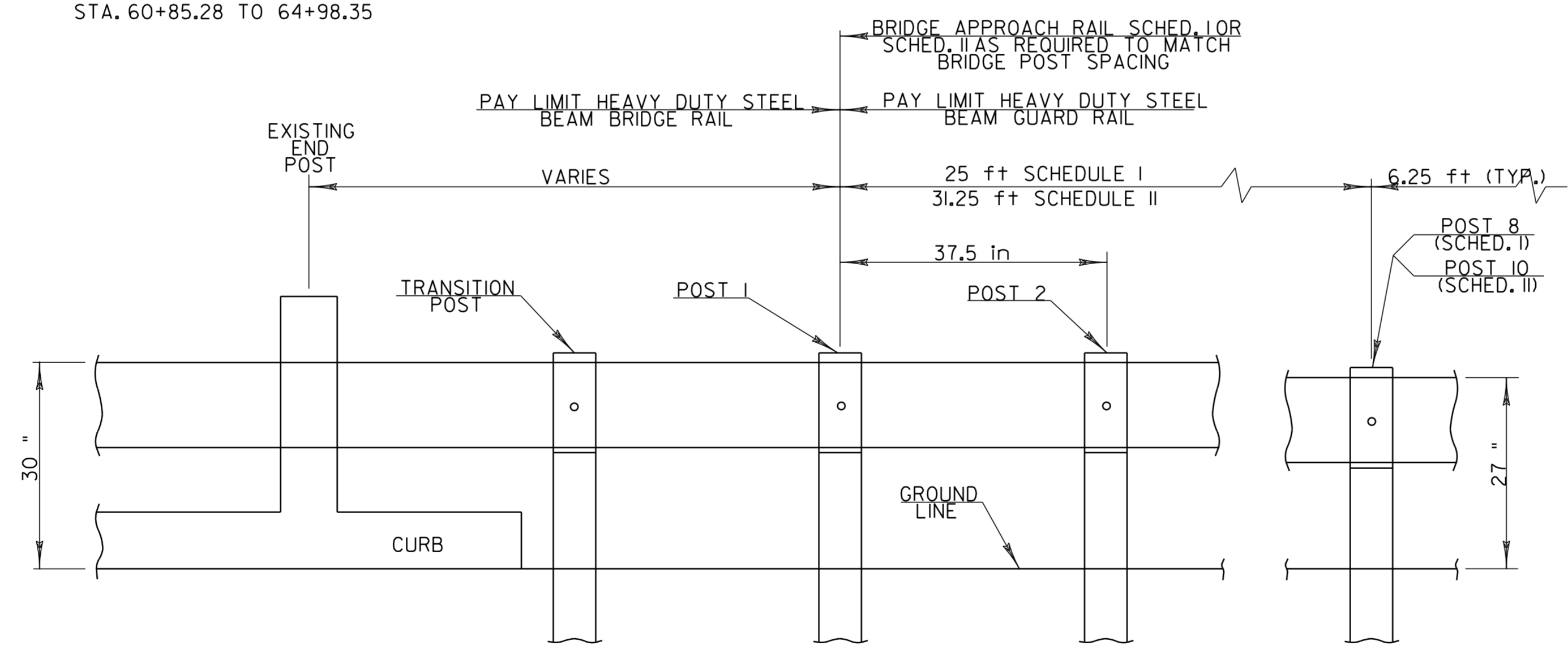
**CROSSETT HILL ROAD TYPICAL SECTION**  
DUXBURY  
STA. 218+18.00 AND 220+78.00



**COLD PLANE AND OVERLAY SECTION**  
DUXBURY  
STA. 323+00.00 TO STA. 328+50.17  
MORETOWN  
STA. 60+85.28 TO 64+98.35

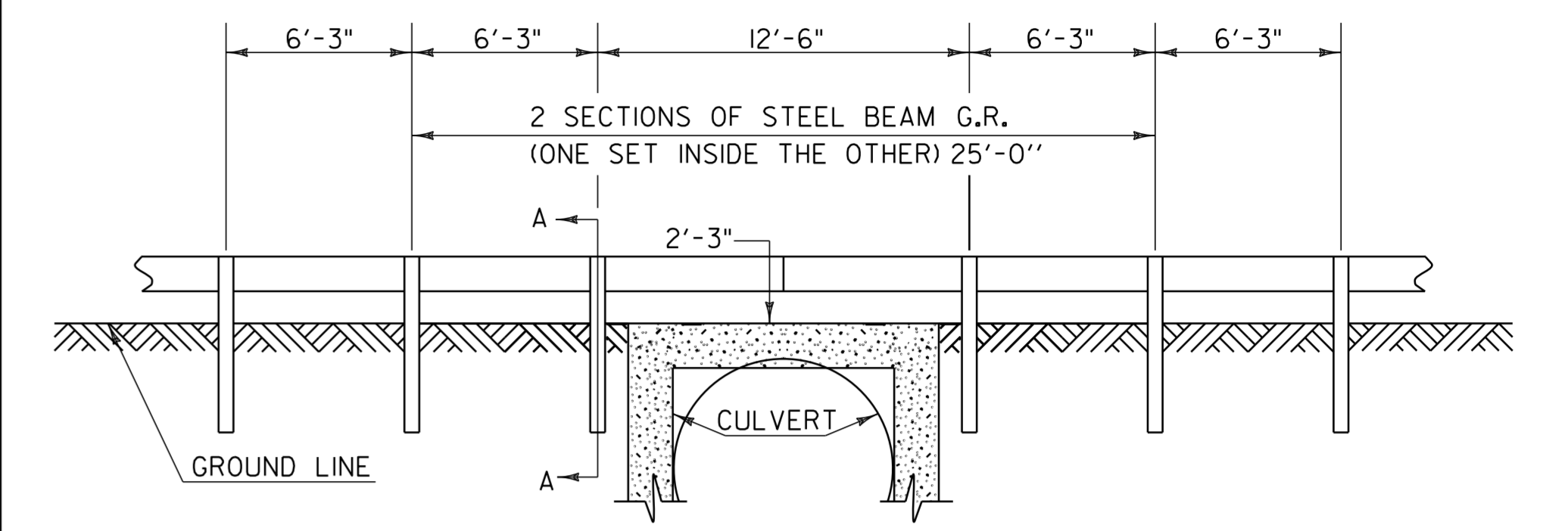
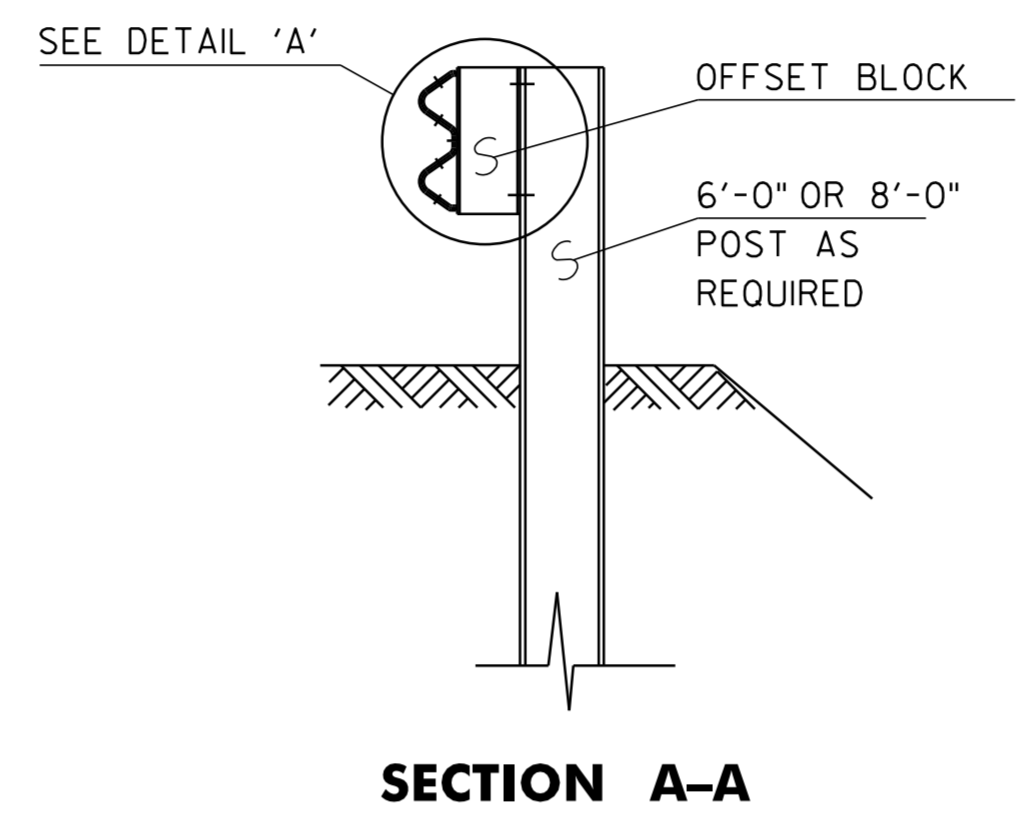
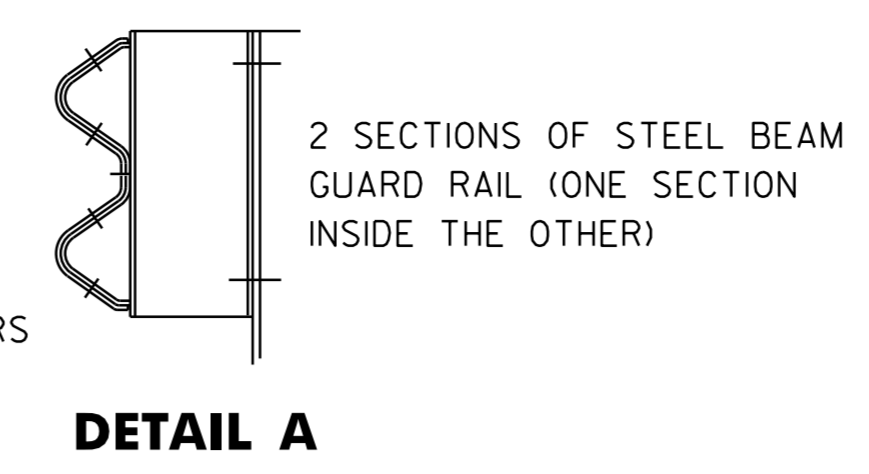


**BITUMINOUS CONCRETE GUTTER**  
DUXBURY  
STA. 208+00 LT TO STA. 210+00 LT

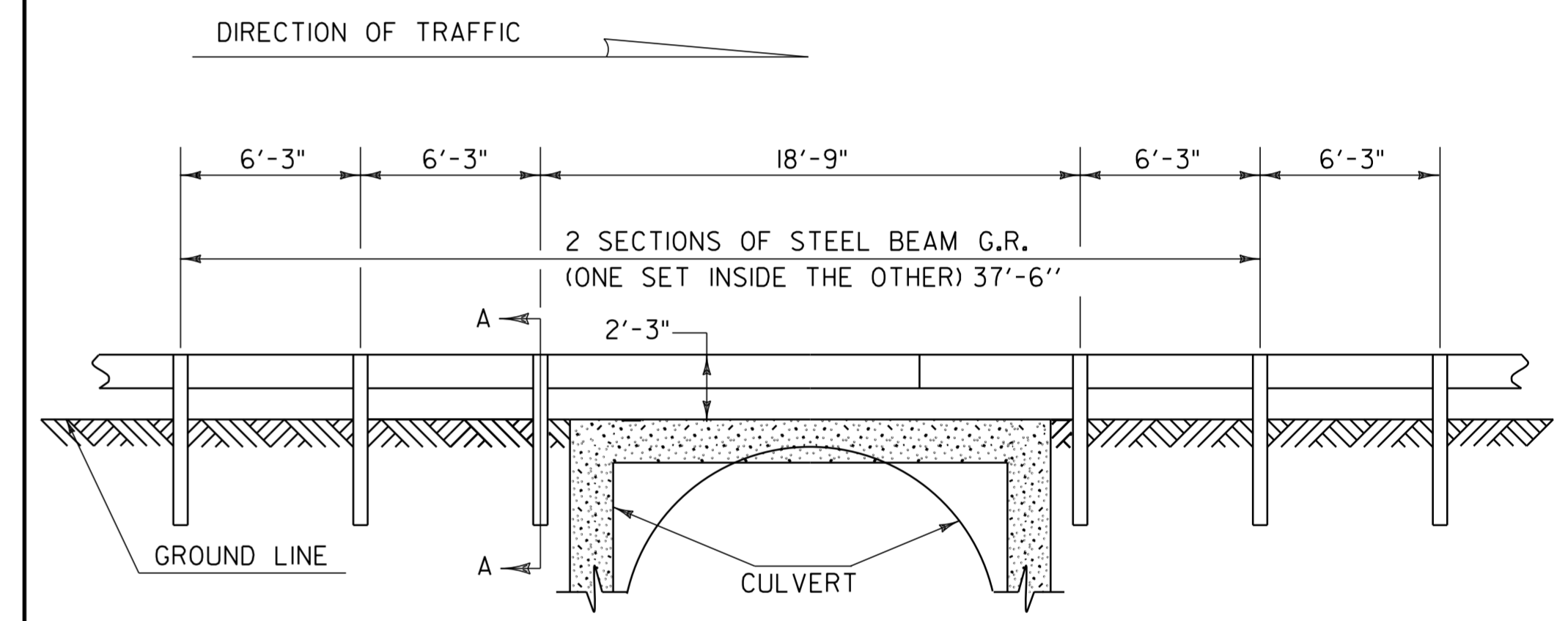


**BRIDGE APPROACH RAILING**

- NOTES**
- BRIDGE RAIL SHALL BE HEAVY DUTY STEEL BEAM RAIL.
  - BRIDGE APPROACH RAIL HEIGHT SHALL BE TRANSITIONED TO NORMAL ROADWAY RAIL HEIGHT IN 25 FEET.
  - APPROACH RAILING SHALL BE HEAVY DUTY STEEL BEAM FOR 25 FEET FROM THE END OF THE BRIDGE ENDS OF THE BRIDGE.
  - FOR BRIDGE RAILING, THE TRANSITION POST SHALL HAVE AN OFFSET BLOCK AND BE LOCATED AS CLOSE AS PRACTICAL TO THE MID-POINT BETWEEN THE BRIDGE END POST AND APPROACH RAIL POST 1.
  - SPLICES SHALL LAP IN DIRECTION OF TRAFFIC FLOW.



**DETAIL OF STEEL BEAM GUARD RAIL AT SMALL CULVERTS**  
DUXBURY  
STA. 84+95 AND STA. 137+16



**DETAIL OF STEEL BEAM GUARD RAIL AT LARGE CULVERTS**  
DUXBURY STA. 293+75

**BRIDGE APPROACH RAILING**

WHEN A RAIL PANEL SPLICE OCCURS AT POST NO. 1 USE SCHEDULE I FOR APPROACH RAILING WHEN A RAIL PANEL SPLICE OCCURS AT BRIDGE END POST USE SCHEDULE II FOR APPROACH RAILING.

SCHEDULE I		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1 1/2"	1.4 x 12.5 ft
2	3'-1 1/2"	
3	3'-1 1/2"	
4	3'-1 1/2"	
5	3'-1 1/2"	
6	4'-2"	1.2 x 12.5 ft
7	4'-2"	
8	4'-2"	
9	6'-3" (TYP.)	1.0 (TYP.)

SCHEDULE II		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1 1/2"	1.4 x 18.75 ft
2	3'-1 1/2"	
3	3'-1 1/2"	
4	3'-1 1/2"	
5	3'-1 1/2"	
6	4'-2"	1.2 x 12.5 ft
7	4'-2"	
8	4'-2"	
9	4'-2"	1.0 (TYP.)
10	4'-2"	
11	6'-3" (TYP.)	1.0 (TYP.)

**PROJECT TYPICAL SHEET 2**

PROJECT NAME: MORETOWN-DUXBURY-MORETOWN  
PROJECT NUMBER: STP 2507 (I)S

FILE NAME: p05b050.DGN  
PROJECT LEADER: EPD  
DESIGNED BY: CDL  
PLOT FILE: p05b050+p2.i

PLOT DATE: 31-OCT-2012 16:08  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET 4 OF 33