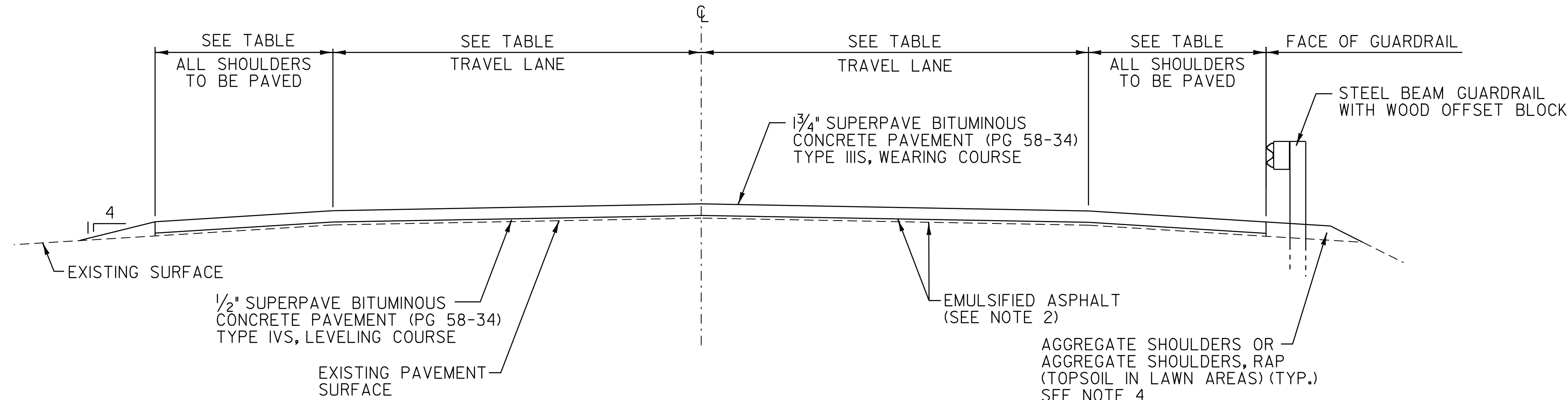


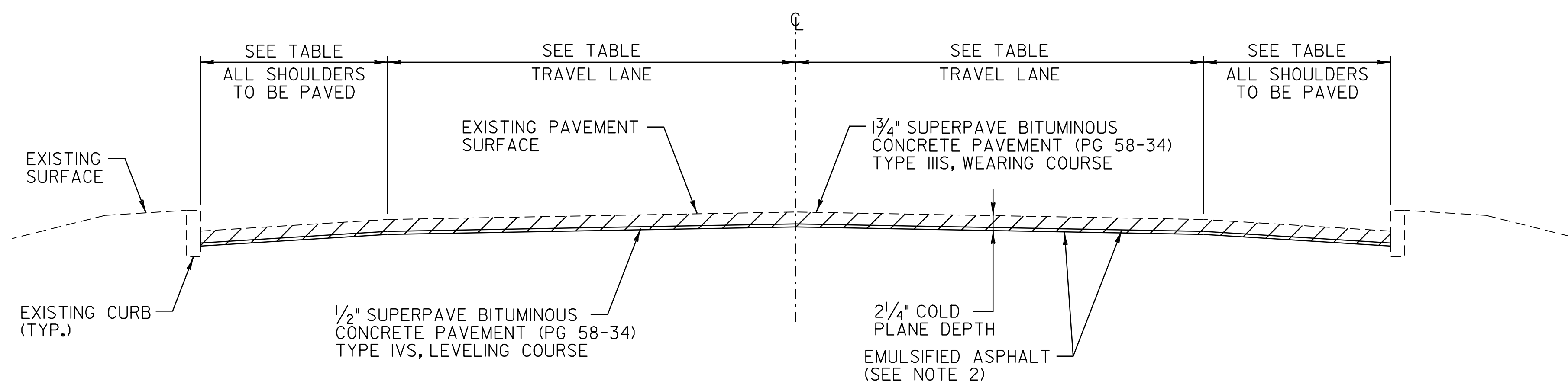
**NOTES**

1. THE WEARING COURSE SHALL BE TYPE IIIS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. THE LEVELING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. ASPHALT CEMENT USED IN THE SUPERPAVE BITUMINOUS CONCRETE PAVEMENT SHALL BE PG 58-34.
2. EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES, BETWEEN ALL COURSES OF PAVEMENT AND ON COLD PLANED SURFACES AT THE RATE OF 0.025 GAL/SY OR AS DIRECTED BY THE RESIDENT ENGINEER.
3. SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = +/- 1/4" (TOTAL PAVEMENT THICKNESS EXCLUDING LEVELING).
4. ALL EDGES OF PAVEMENT AND TREATED TIMBER CURB SHALL BE BACKED UP TO FULL HEIGHT WITH COLD PLANE GRINDINGS AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.I3 AGGREGATE SHOULDERS, RAP. ADDITIONAL AGGREGATE MATERIAL REQUIRED AFTER THE COLD PLANE GRINDINGS ARE USED WILL BE PAID FOR UNDER ITEM 402.I2 AGGREGATE SHOULDERS.
5. EARTH BORROW SHALL BE USED FOR THE CONSTRUCTION OF MANUFACTURED TERMINAL FLARES WHICH SHALL BE CAPPED WITH AN ESTIMATED 3" DEPTH OF AGGREGATE SHOULDER MATERIAL AS DIRECTED BY THE RESIDENT ENGINEER. THE QUANTITIES INCLUDED REFLECT 5 TONS OF AGGREGATE SHOULDERS, RAP AND 25 CUBIC YARDS OF EARTH BORROW FOR EACH GUARDRAIL TERMINAL.
6. THE PROPOSED GUARDRAIL SHALL BE INSTALLED IN A LOCATION THAT MAXIMIZES THE DISTANCE FROM THE CENTER OF THE ROAD TO THE FACE OF GUARDRAIL. 3' OF BACKING IS REQUIRED BEHIND THE FACE OF GUARDRAIL WITH 6' POSTS. IF THIS CANNOT BE OBTAINED, THEN 8' POSTS SHALL BE USED.
7. ALL DRIVES, MAILBOX TURNOUTS AND GRAVEL PULLOUTS SHALL RECEIVE A PAVED APRON AS DIRECTED BY THE RESIDENT ENGINEER. ANY AND ALL REQUIRED EXCAVATION OR GRADING IN DRIVE AREAS SHALL BE AS DIRECTED AND WILL BE PAID FOR UNDER THE APPLICABLE RENTAL ITEM(S). IF REQUIRED, A NEW DRIVEWAY SUBBASE SHALL BE CONSTRUCTED AND WILL BE PAID FOR UNDER ITEM 30I.28 SUBBASE OF CRUSHED GRAVEL, FINE GRADED. A BITUMINOUS SURFACE SHALL BE CONSTRUCTED AS DIRECTED AND WILL BE PAID FOR UNDER ITEM 490.30 SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (PG 58-34). ESTIMATED QUANTITIES OF THE ABOVE ITEMS HAVE BEEN INCLUDED TO PAY FOR THIS WORK.
8. AN ESTIMATED QUANTITY OF ITEM 6I9.I7 YIELDING MARKER POSTS HAS BEEN INCLUDED TO DELINEATE PIPE INLETS, PIPE OUTLETS AND DROP INLETS LOCATED OUTSIDE OF THE PAVEMENT SURFACE OR AS DIRECTED BY THE RESIDENT ENGINEER.
9. EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER SHALL BE EXCAVATED TO A DEPTH OF 3" OR AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATION WILL BE PAID FOR AS ALL PURPOSE EXCAVATOR OR POWER GRADER RENTAL. MATERIAL REMOVED SHALL BE REPLACED WITH SUBBASE OF CRUSHED GRAVEL, FINE GRADED. EXCAVATED MATERIAL SHALL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT AS DIRECTED BY THE RESIDENT ENGINEER.



**OVERLAY TYPICAL SECTION**

U.S. ROUTE 4 KILLINGTON STA. 401+28.00 TO KILLINGTON STA. ~~449+50.00~~ 448+00  
 U.S. ROUTE 4 BRIDGEWATER STA. 5+33.00 TO BRIDGEWATER STA. ~~298+00.00~~ 297+00  
 U.S. ROUTE 4 BRIDGEWATER STA. ~~304+50.00~~ 305+60 TO BRIDGEWATER STA. 318+0I.44



**COLD PLANE TYPICAL SECTION**

U.S. ROUTE 4 KILLINGTON STA. ~~448+00~~ 449+50.00 TO KILLINGTON STA. 450+70.08  
 U.S. ROUTE 4 BRIDGEWATER STA. 0+00.00 TO BRIDGEWATER STA. 5+33.00  
 U.S. ROUTE 4 BRIDGEWATER STA. ~~298+00.00~~ 297+00 TO BRIDGEWATER STA. ~~304+50.00~~ 305+60

**PROJECT PAVING LIMITS**

TOWN & ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING TON	NOTES
<b>KILLINGTON:</b>						
U.S. ROUTE 4	401+28.00	436+4I.50	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	442	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	436+4I.50	438+28.00	3'-0" - 12'-0" - 12'-0" - 3'-0"	1 1/4"	-	BR 34 COLD PLANE I" & PAVE WITH 1 1/4" TYPE IIIS
U.S. ROUTE 4	438+28.00	449+50.00	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	127	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	449+50.00	450+70.08	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	15	COLD PLANE 2 1/4", LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
<b>BRIDGEWATER:</b>						
U.S. ROUTE 4	0+00.00	2+55.00	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	32	COLD PLANE 2 1/4", LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	2+55.00	3+I4.00	3'-0" - 12'-0" - 12'-0" - 3'-0"	1 3/4"	6	COLD PLANE 2 1/4", LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	3+I4.00	5+I6.00	3'-0" - 12'-0" - 12'-0" - 3'-0"	1 1/4"	-	BR 35 COLD PLANE I" & PAVE WITH 1 1/4" TYPE IIIS
U.S. ROUTE 4	5+I6.00	5+33.00	3'-0" - 12'-0" - 12'-0" - 3'-0"	1 3/4"	2	COLD PLANE 2 1/4", LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	5+33.00	66+42.00	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	77I	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	66+42.00	67+80.00	3'-0" - 12'-0" - 12'-0" - 3'-0"	1 1/4"	-	BR 38 COLD PLANE I" & PAVE WITH 1 1/4" TYPE IIIS
U.S. ROUTE 4	67+80.00	84+66.50	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	209	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	84+66.50	86+04.00	3'-0" - 12'-0" - 12'-0" - 3'-0"	1 1/4"	-	BR 39 COLD PLANE I" & PAVE WITH 1 1/4" TYPE IIIS
U.S. ROUTE 4	86+04.00	118+39.00	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	406	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	118+39.00	119+40.50	2'-9" - 12'-0" - 12'-0" - 2'-9"	1 1/4"	-	BR 40 COLD PLANE I" & PAVE WITH 1 1/4" TYPE IIIS
U.S. ROUTE 4	119+40.50	149+9I.00	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	382	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	149+9I.00	151+57.00	2'-9" - 12'-0" - 12'-0" - 2'-9"	1 1/4"	-	BR 42 COLD PLANE I" & PAVE WITH 1 1/4" TYPE IIIS
U.S. ROUTE 4	151+57.00	237+29.00	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	1080	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	237+29.00	238+62.50	4'-3" - 12'-0" - 12'-0" - 4'-3"	1 1/4"	-	BR 44 COLD PLANE I" & PAVE WITH 1 1/4" TYPE IIIS
U.S. ROUTE 4	238+62.50	282+04.50	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	546	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	282+04.50	283+76.00	2'-9" - 12'-0" - 12'-0" - 2'-9"	1 1/4"	-	BR 45 COLD PLANE I" & PAVE WITH 1 1/4" TYPE IIIS
U.S. ROUTE 4	283+76.00	298+00.00	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	181	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	298+00.00	304+50.00	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	83	COLD PLANE 2 1/4", LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS
U.S. ROUTE 4	304+50.00	318+0I.44	8'-3" - 12'-0" - 12'-0" - 8'-3"	1 3/4"	171	LEVEL WITH 1/2" TYPE IVS & PAVE WITH 3/4" TYPE IIIS

**CONSERVATION SEED MIX**

RURAL AREA - SEED MIXTURE				
% WT.	LBS./A.	NAME	PUR. %	GERM. %
37.5	22.5	CREeping RED FESCUE	98	85
37.5	22.5	TALL FESCUE	95	90
5.0	3.0	RED TOP	95	90
15.0	9.0	BIRDSFOOT TREFOIL	98	85
5.0	3.0	ANNUAL RYEGRASS	95	85
100.0	60.0			

SEED MIXTURE:  
SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS WEED SEED.

SEED:  
TO BE APPLIED PER SEEDING FORMULA OR AS DIRECTED BY THE RESIDENT ENGINEER.

FERTILIZER:  
FORMULA 10-20-10 TO BE USED WITH SEED APPLIED AT THE RATE OF 500 LBS/ACRE (HYDRO SEEDERS MAY USE 19-19-19 FORMULA).

AGRICULTURAL LIMESTONE:  
TO BE APPLIED AT THE RATE OF 2 TONS/ACRE OR AS DIRECTED BY THE RESIDENT ENGINEER.

HAY MULCH:  
TO BE APPLIED ON EARTH SLOPES AT THE RATE OF 2 TONS/ACRE OR AS DIRECTED BY THE RESIDENT ENGINEER.

TOPSOIL:  
TO BE USED WITH SEED AS INDICATED ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.

NOT TO SCALE

**PROJECT TYPICAL SHEET**

PROJECT NAME: KILLINGTON - BRIDGEWATER  
PROJECT NUMBER: NH 2502(I)S

FILE NAME: p05b036.dgn PLOT DATE: 15-MAY-2008 14  
PROJECT LEADER: D.E.G. DRAWN BY: C.A.K.  
DESIGNED BY: J.S.P. CHECKED BY: D.E.G.  
IPARM FILE: p05b036+ts0I.I SHEET 3 OF 74