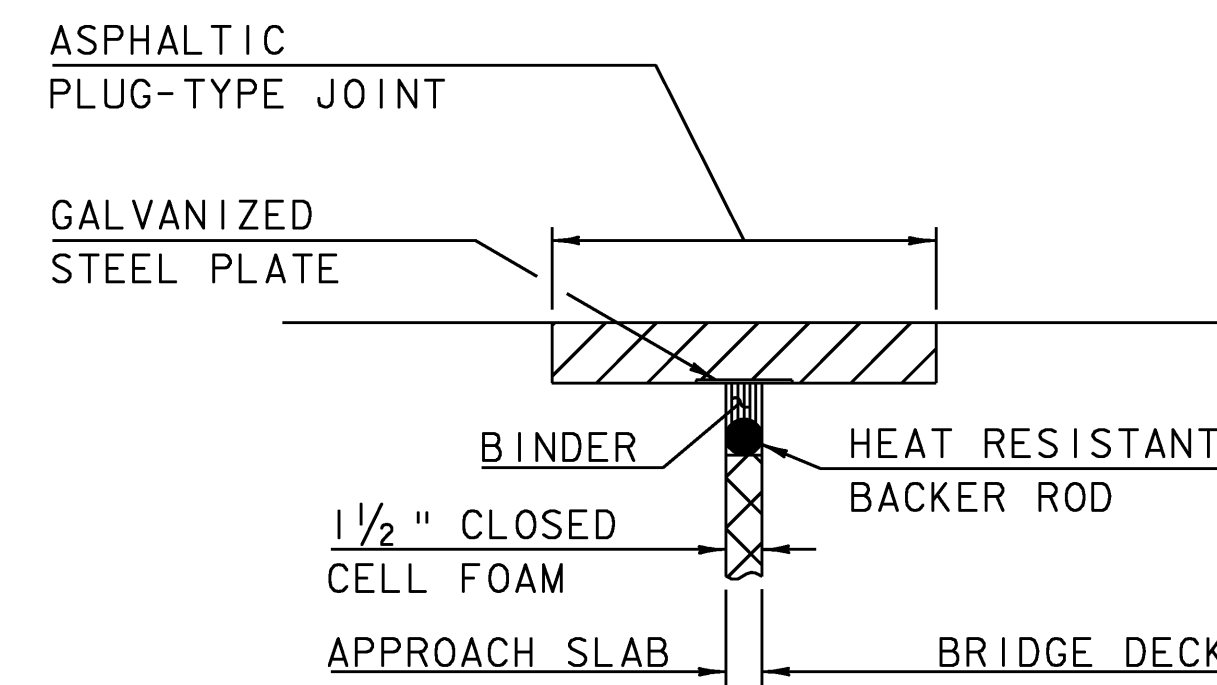
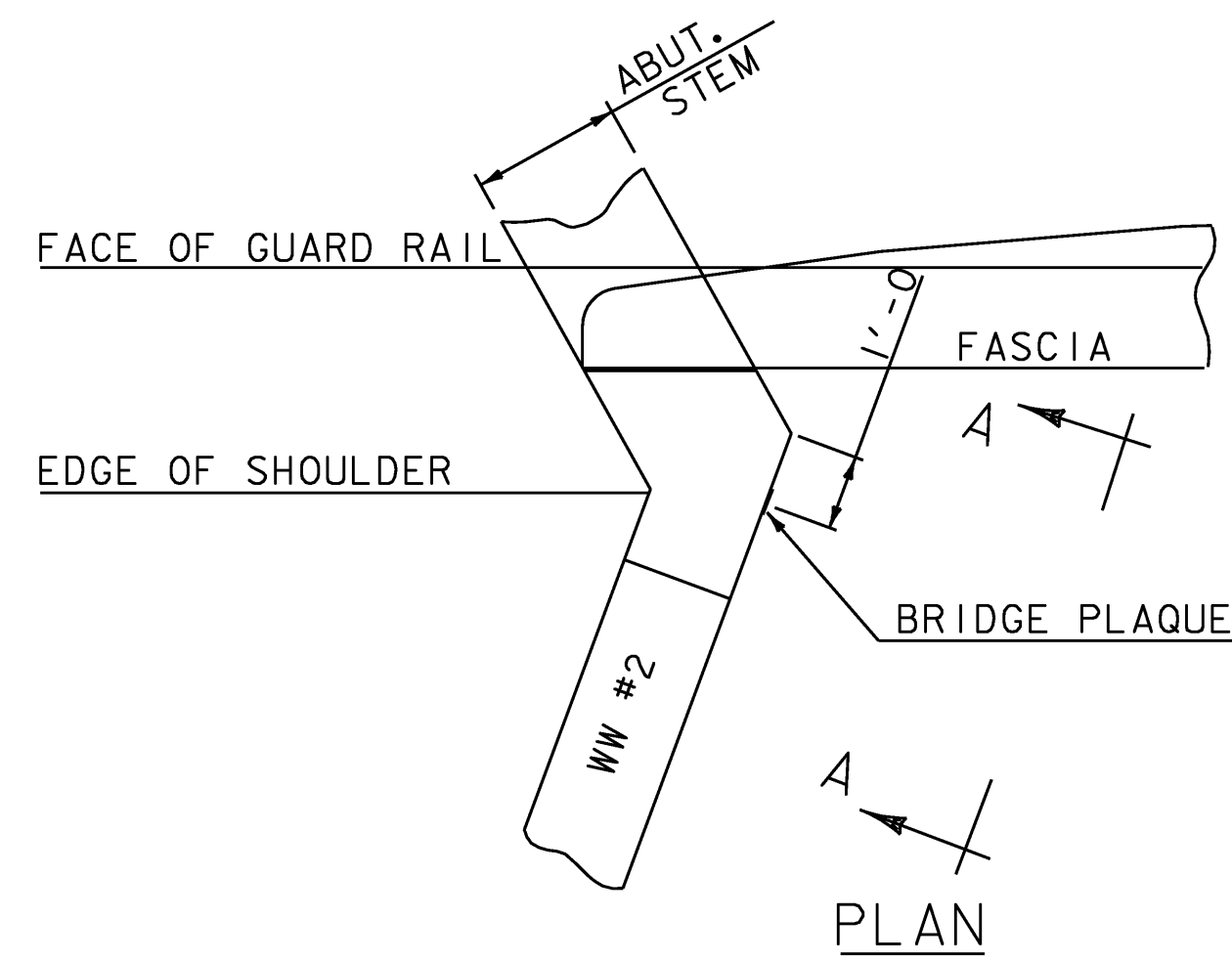
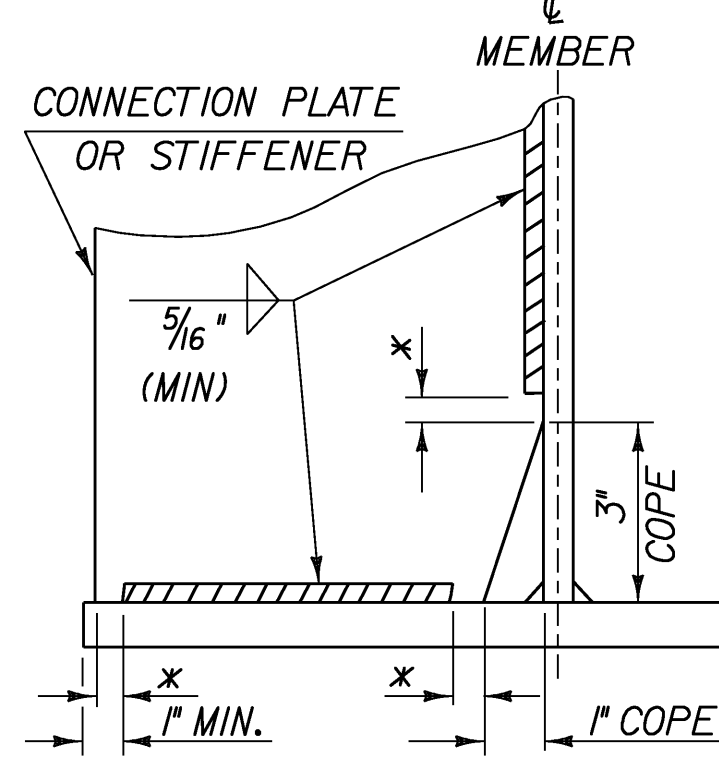


BRIDGE PLAQUE
(NOT TO SCALE)



ASPHALTIC PLUG-TYPE
JOINT DETAIL
(NOT TO SCALE)



**WELD TERMINATION AND COPING
DETAILS FOR STEEL MEMBERS**

*NO WELD FOR 3/8" MIN. 7/8" MAX. (EXCEPT MUST MAINTAIN 1" MINIMUM FROM EDGE OF FLANGE)

JOINT SEALER, HOT OR COLD POURED. SHALL BE SLIGHTLY OVER FILLED THEN WIPED FLUSH WITH A "V" OR "U" SHAPED SQUEEGEE TO PROVIDE A 1 1/4" WIPE ZONE EACH SIDE OF JOINT.

TOP COURSE OF PAVEMENT

3/8"

1 1/4" MIN. WIPE ZONE (TYP.)

ROADWAY SURFACE

CONCRETE SURFACES TO BE SANDBLASTED ON BOTH SIDES OF JOINT

7/8" Ø HEAT RESISTANT FOAM BACKER ROD. COMPRESSION FIT REQUIRED TO ENSURE THAT THE ROD POSITION IS MAINTAINED DURING FILLING OPERATION. COST TO BE INCLUDED WITH UNIT PRICE BID FOR JOINT SEALER.

3/4" SAW CUT*

1/4" WIDE X 1/2" DEEP SAW CUT INTO BOTTOM COURSE OF PAVEMENT TO BE MADE DURING THE SAME WORKDAY AS PLACEMENT.

SAWED PAVEMENT JOINT DETAIL
(NOT TO SCALE)

ASPHALTIC PLUG JOINT NOTES

1. INSTALLATION

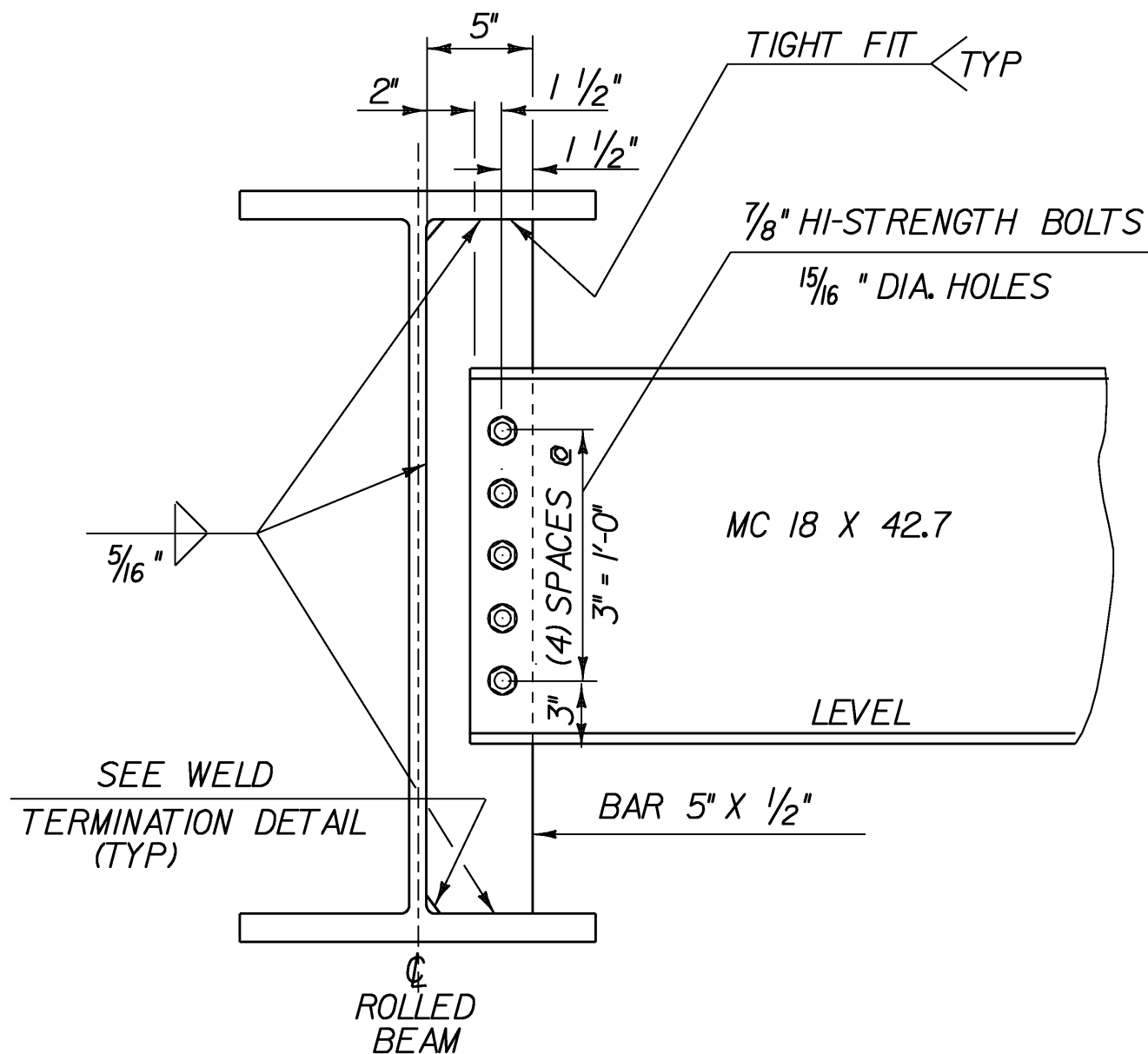
- A. LOCATE THE JOINT CENTRALLY OVER THE DECK OVERLAY EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.
- B. REMOVE THE BITUMINOUS CONCRETE PAVEMENT FULL DEPTH AS SHOWN ON THE PLANS. THE PAVEMENT SHALL BE DRY AND SAW CUT TO THE LIMITS REQUIRED TO PLACE THE JOINT. A PNUMATIC HAMMER AND CHISEL MAY BE USED ADJACENT TO THE CURB ONLY WHEN SAW CUTTING IS NOT POSSIBLE.
- C. BLAST CLEAN THE JOINT AREA OF DEBRIS AND ASPHALT. THOROUGHLY DRY THE JOINT AREA WITH COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.
- D. REPAIR SPALLED AND DEFECTIVE CONCRETE WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.
- E. PLACE PROPERLY SIZED HEAT RESISTANT BACKER ROD IN THE MOVEMENT GAP ALLOWING FOR 1" +/- OF BINDER ABOVE THE ROD.
- F. HEAT AND PLACE THE BINDER MATERIAL AS RECOMMENDED BY THE MANUFACTURER.
- G. PLACE 1/4" THICK BY 8" WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE THE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRESTAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER. THE STEEL PLATES MAY BE OMITTED WHERE THE ENGINEER DETERMINES THAT THE APPROACH SLAB OR BRIDGE DECK WILL PROVIDE INADEQUATE SUPPORT AND WHERE VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.
- H. HEAT AND MIX THE BINDER MATERIAL AND AGGREGATE AS RECOMMENDED BY THE MANUFACTURER.
- I. INSTALLATION OF MATERIAL, COMPACTION, AND TOP COATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- J. IMMEDIATELY AFTER TOP COATING, CAST AN ANTI-SKID MATERIAL OVER THE JOINT TO REDUCE THE RISK OF TRACKING.
- K. PROTECT JOINT FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 51 DEG C (125 DEG F) +/-.

2. WEATHER LIMITATIONS. (APPLY BINDER MATERIAL ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL OR AS RECOMMENDED BY THE MANUFACTURER):

- A. THE AMBIENT AIR TEMPERATURE IS AT LEAST 10 DEG C (50 DEG F) AND RISING.
- B. THE ROAD SURFACE IS DRY.
- C. WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF SATISFACTORY WORK.

THE BRIDGE PLAQUE WILL BE SUPPLIED BY THE AGENCY OF TRANSPORTATION AND SHALL BE INSTALLED BY THE CONTRACTOR AT ABUTMENT #1 ON THE RIGHT SIDE AS SHOWN OR AS DIRECTED BY THE ENGINEER.

PAYMENT FOR INSTALLATION OF THE BRIDGE PLAQUE SHALL BE INCIDENTAL TO THE ADJACENT CONCRETE.



NOTE: HI-STRENGTH BOLTS, NUTS AND WASHERS SHALL CONFORM TO AASHTO DESIGNATION M164.

INTERMEDIATE DIAPHRAGMS

*JOINT IS TO BE LOCATED ACCURATELY BY STRING LINING, OR OTHER MEANS, PRIOR TO PAVING, SO THAT THE SAW CUTS WILL BE MADE DIRECTLY OVER THE END OF CONCRETE DECK. JOINT SHALL BE CUT DRY IN A SINGLE PASS AND BE SEALED WITHIN 24 HOURS OR PRIOR TO EXPOSURE TO TRAFFIC. JOINT SHALL BE CLEANED PRIOR TO APPLYING THE JOINT SEALER. PAYMENT INCIDENTAL TO 406.25

SUPERSTRUCTURE DETAILS (2)

PROJECT NAME: CANAAN	PLOT DATE: 18-AUG-2008
PROJECT NUMBER: ER ST 0271(16)	DRAWN BY: L. DUQUETTE
FILE NAME: 04c098/struc./s04c098sup	CHECKED BY: S. SCRIBNER
DESIGNED BY: S.SCRIBNER	SHEET 43 OF 66