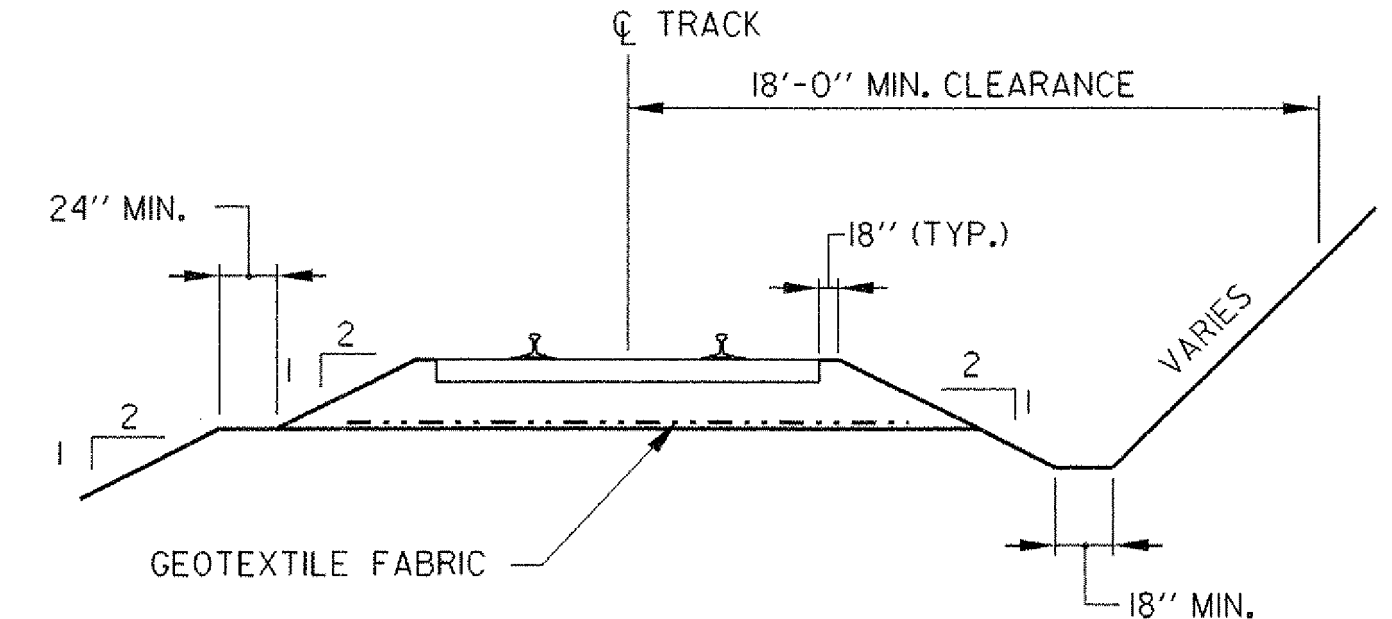
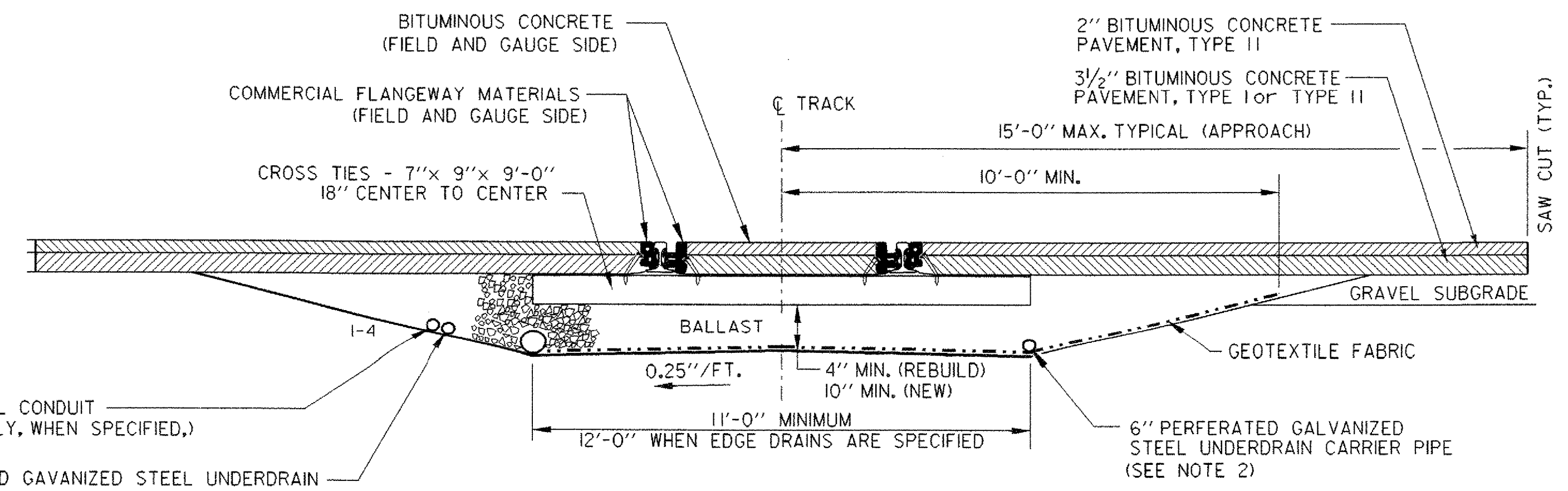


**TYPICAL PLAN VIEW  
VT 25A CROSSING**  
NOT TO SCALE



**TYPICAL SECTION  
RAILROAD MAINLINE**  
NOT TO SCALE



**TYPICAL TRANSVERSE SECTION  
VT 25A CROSSING**  
NOT TO SCALE

**GENERAL NOTES**

1. ON VT 25A THE EXISTING SIGNAL CONDUIT IS TO BE REPLACED WITH NEW WIRE AS DETAILED IN THE CONTRACT.
2. NEW UNDERDRAIN CARRIER PIPE SHALL CONNECT TO NEW DROP INLET. PERFORATIONS TO BE PLACED NEAR FLOW LINE OF PIPE.
3. ALL RAIL JOINTS WITHIN THE CROSSING AREA AND 50'-0" BEYOND WILL BE CROPPED AND WELDED IN ACCORDANCE WITH THE LATEST REVISION OF A.R.E.M.A. SPECIFICATIONS AT AN OFF-SITE ELECTRIC WELDING PLANT. WELDING CAN BE DONE IN FIELD UTILIZING THERMITE WELDING. WELDING JOINTS SHALL BE GROUND TO CONFORM TO THE SHAPE OF THE RAIL ON GAUGE AND FIELD SIDES.
4. TIE SPACING UNDER CONTINUOUS WELDED RAIL AREA SHALL BE 18 INCHES ON CENTER OR AS REQUIRED IN CROSSING PANEL AREA BY MANUFACTURER.
5. NEW 7"x 9"x 9'-0" AND 7"x9"x8'-6" TIES SHALL BE USED IN CROSSING AREA AS SHOWN.
6. TIE PLATES SHALL BE NEW 14 INCH PLATES, MANUFACTURED FOR THE RAIL SIZE USED. PLATES SHALL BE INSPECTED AND APPROVED BY THE RAILROAD AND THE ENGINEER. RAIL FASTENERS SHALL BE CUT TRACK SPIKES.
7. BALLAST SHALL EXTEND 18" BEYOND END OF TIES AND SLOPED 1:2 TO THE ROADBED. (SEE TYPICAL SECTION)
8. TYPE AND DESIGN OF COMMERCIAL FLANGEWAY MATERIALS SHALL RECEIVE APPROVAL FROM THE ENGINEER AND THE RAILROAD.
9. MANUFACTURERS SPECIFICATIONS SHALL BE FOLLOWED FOR THE INSTALLATION OF COMMERCIAL FLANGEWAY MATERIALS.
10. INSTALLATION OF INSULATED JOINTS: THE MAXIMUM STAGGER BETWEEN INSULATED JOINTS SHALL BE 4'-6", MINIMUM SHALL BE 3'-6".
11. APPROACH ASPHALT ROADWAY PAVING SHALL FOLLOW LATEST EDITION OF THE AGENCY'S STANDARD SPECIFICATION FOR CONSTRUCTION AND SHALL BE INSTALLED WITH PAVING MACHINE WITH MINIMUM 3" LIFTS (UNLESS OTHERWISE DIRECTED BY THE ENGINEER) AND SHALL BE LAID PARALLEL TO CROSSING TO MINIMIZE APPROACH SETTLEMENT.
12. EXISTING TRACK IS BOLTED RAIL, TRANSITION RAIL SHALL BE NEW AND MATCH RAIL SECTION THROUGH CROSSING.
13. CONTRACTOR SHALL ADD BALLAST, LINE, TAMP, AND SURFACE TRACK IN APPROACH AREAS TO OBTAIN A SMOOTH TRANSITION BETWEEN EXISTING AND PROPOSED TRACK TO THE SATISFACTION OF THE ENGINEER AND RAILROAD. THIS WORK IS INCIDENTAL TO THE RAIL-HIGHWAY CROSSING ITEM.

**RAILROAD CROSSING DETAILS**

PROJECT NAME:	FAIRLEE	FILE NAME:	98b18/railroaddetails.dgn	PLOT DATE:	07-MAY-2008
PROJECT NUMBER:	STP 040-I(2)	PROJECT LEADER:	J. KAPLAN	DRAWN BY:	VAOT
		DESIGNED BY:	G. SHANGRAW	CHECKED BY:	J. ROYER
			sbl18rxx.i		SHEET 21 OF 28