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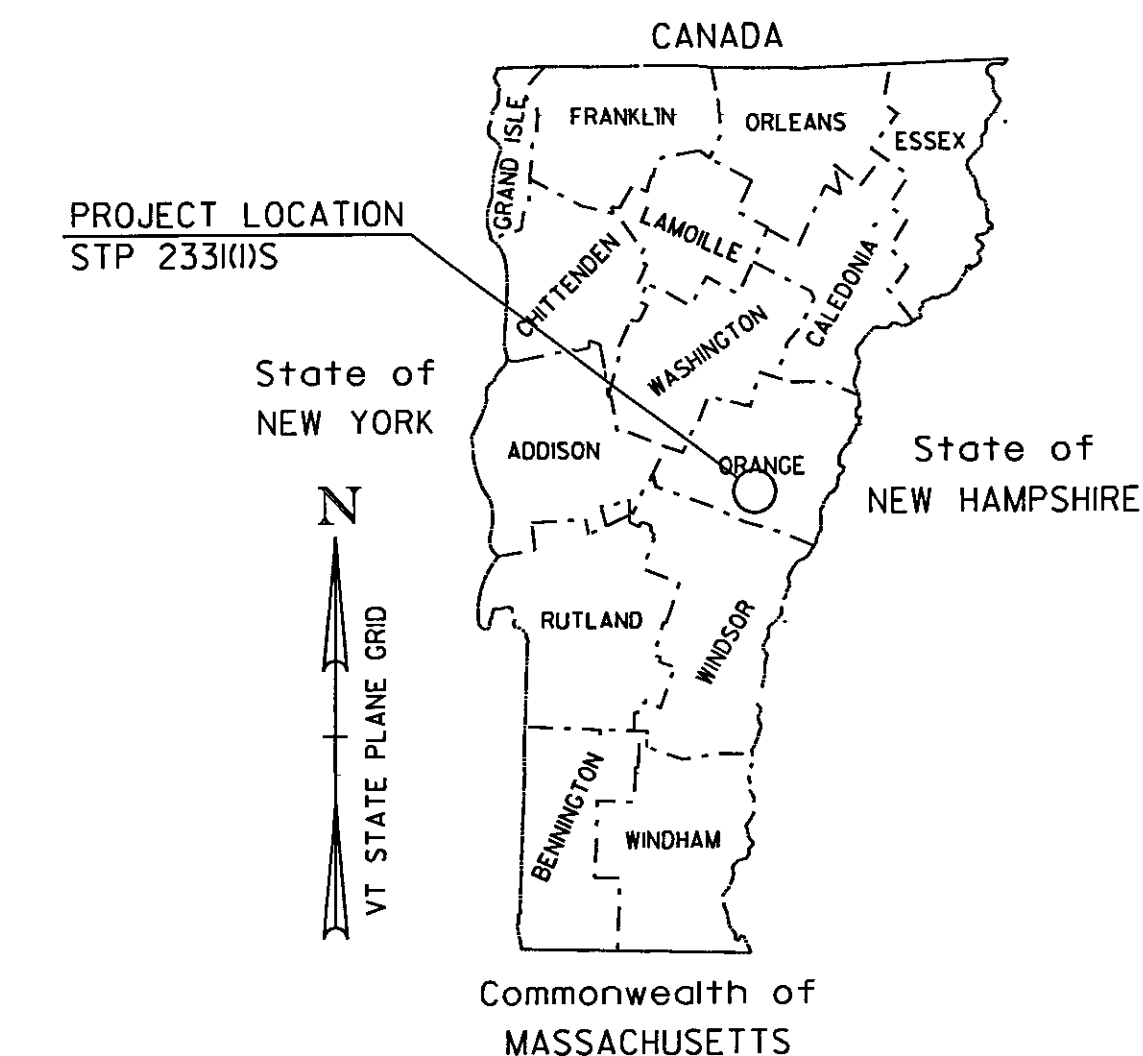
STATE OF VERMONT AGENCY OF TRANSPORTATION



PROPOSED IMPROVEMENT TOWNS OF CHELSEA AND VERSHIRE COUNTY OF ORANGE VT ROUTE 113

BEGINNING IN THE TOWN OF CHELSEA AT THE INTERSECTION OF VT-110 AND VT-113 AT MILE MARKER 0.008 = STA. 0+40.00 AND EXTENDING EASTERLY ALONG VT ROUTE 113 FOR A DISTANCE OF 36,920 FT (6.992 MILES) TO AN ENDING POINT IN THE TOWN OF VERSHIRE AT MILE MARKER 3.560 = STA. 187+96.80.

STATION TO STATION DATA	LENGTH (FEET)	(MILES)
TOWN OF CHELSEA STA. 0+40.00 TO STA. 181+63.20 (MM 0.008 TO MM 3.440)	18,123.20	3.432
TOWN OF VERSHIRE STA. 0+00.00 TO STA. 187+96.80 (MM 0.000 TO MM 3.560)	18,796.80	3.560
TOTAL ROADWAY LENGTH =	36,920.00	6.992
TOTAL PROJECT LENGTH =	36,920.00	6.992



RECORD PLANS

CONTRACTOR: PIKE INDUSTRIES, INC. - BERLIN, VT

RESIDENT ENGINEER: CHARLIE HARDING

CONSTRUCTION BEGAN: APRIL 19, 2010

CONSTRUCTION COMPLETE: OCTOBER 11, 2010

RECORD PLANS BY: CHARLIE HARDING & MATT BIRCHARD

I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.

BY *Charlie Harding* RESIDENT ENGINEER

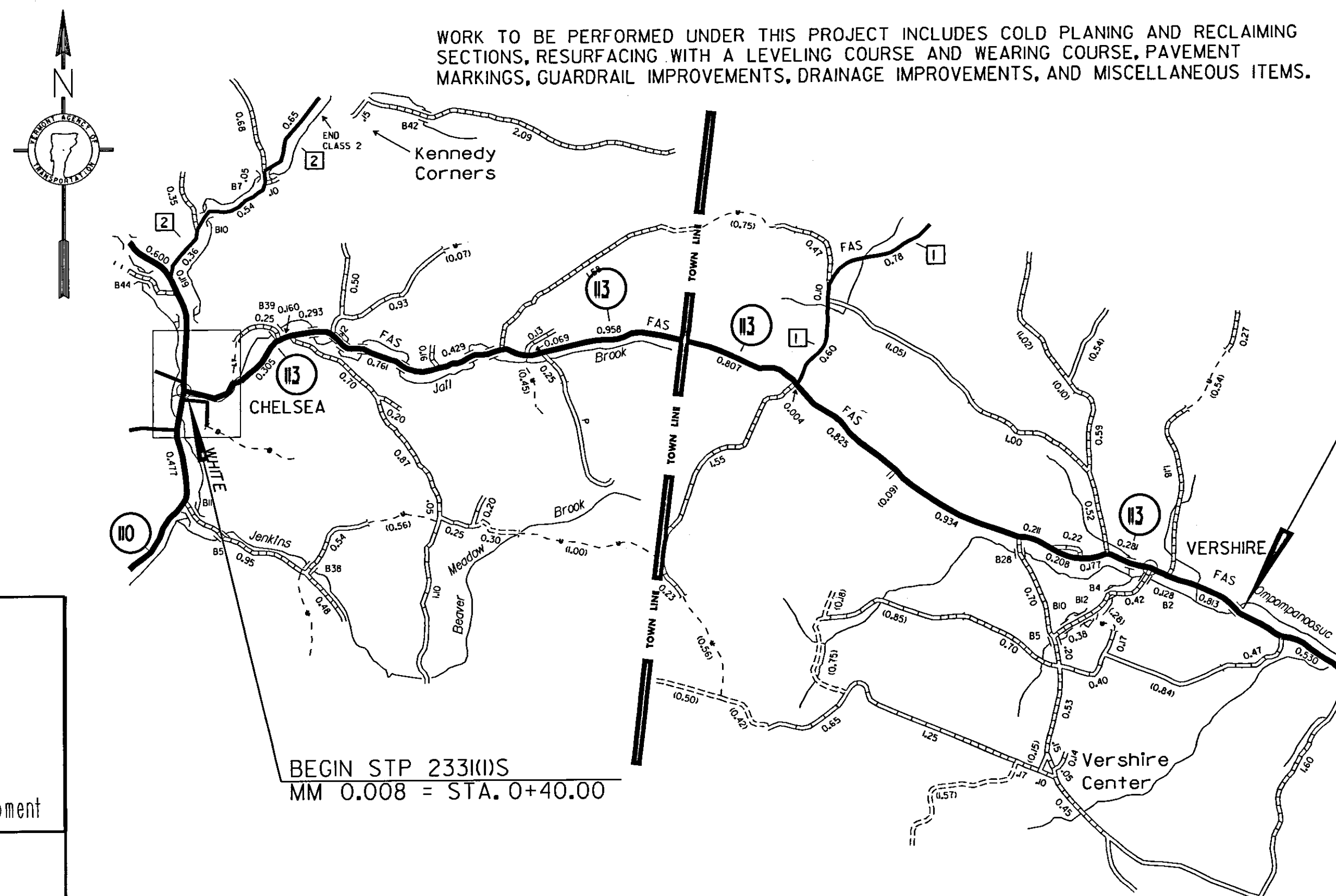
DATE *April 18, 2011*

NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.

TRAFFIC DATA

HIGHWAY SECTION	AADT		DHV		CUM. ESALS (2009-2019)	CUM. ESALS (2009-2029)
	2009	2019	2009	2019		
BEGIN OF PROJECT TO GOOSE GREEN ROAD (TH #11/MCIVER ROAD (TH #16) (MM 0.000 CHELSEA - MM 0.810 VERSHIRE)	860	910	120	130	154,000	340,000
GOOSE GREEN ROAD (TH #11/MCIVER ROAD (TH #16) TO END OF PROJECT (MM 0.810 VERSHIRE - MM 3.560 VERSHIRE)	1000	1100	140	150	113,000	263,000

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES COLD PLANING AND RECLAIMING SECTIONS, RESURFACING WITH A LEVELING COURSE AND WEARING COURSE, PAVEMENT MARKINGS, GUARDRAIL IMPROVEMENTS, DRAINAGE IMPROVEMENTS, AND MISCELLANEOUS ITEMS.



**BITUMINOUS CONCRETE PAVEMENT
SUPERPAVE MIXTURE DESIGN CRITERIA**

DESIGN LANE / DESIGN LIFE ESAL	340,000
DESIGN NUMBER OF GYRATIONS	50
PERFORMANCE GRADE ASPHALT BINDER	SEE SECTION 490 GENERAL SPECIAL PROVISIONS

CONVENTIONAL SYMBOLS

COUNTY LINE	
TOWN LINE	
LIMITS OF ACCESS	
POINT OF ACCESS	
FENCE LINE	
STONE WALL	
TRAVELED WAY	
GUARD RAIL	
RAILROAD	
SURVEY LINE	
CULVERT	
POWER POLE	
TELEPHONE POLE	
TREES	
CONTROL OF ACCESS	
PROPERTY LINE	
R.O.W. TAKING LINE	
SLOPE RIGHTS	
TOP OF CUT	
TOE OF SLOPE	

DuBois & King inc.

engineering planning management development

DATUM

VERTICAL N/A

HORIZONTAL N/A

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT.

CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

DIRECTOR OF PROGRAM DEVELOPMENT

APPROVED *[Signature]* DATE *10-16-09*

PROJECT MANAGER : KEVIN MARSHIA

PROJECT NAME : CHELSEA-VERSHIRE

PROJECT NUMBER : STP 2331 (I) S

SHEET 1 OF 55 SHEETS

PLOTTED: 16-OCT-2009

01c040_01

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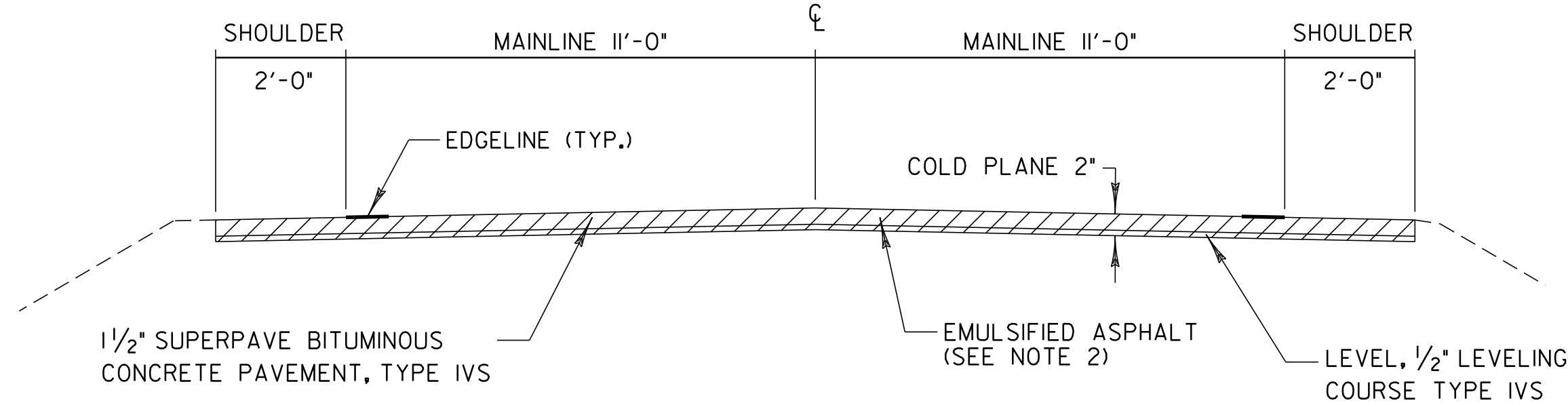
VAOT STANDARDS

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E-106	TRAFFIC CONTROL - MISCELLANEOUS DETAILS	03/01/04
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OF
SHEETS

PROJECT NAME: CHELSEA-VERSHIRE
PROJECT NUMBER: STP 2331(1)S

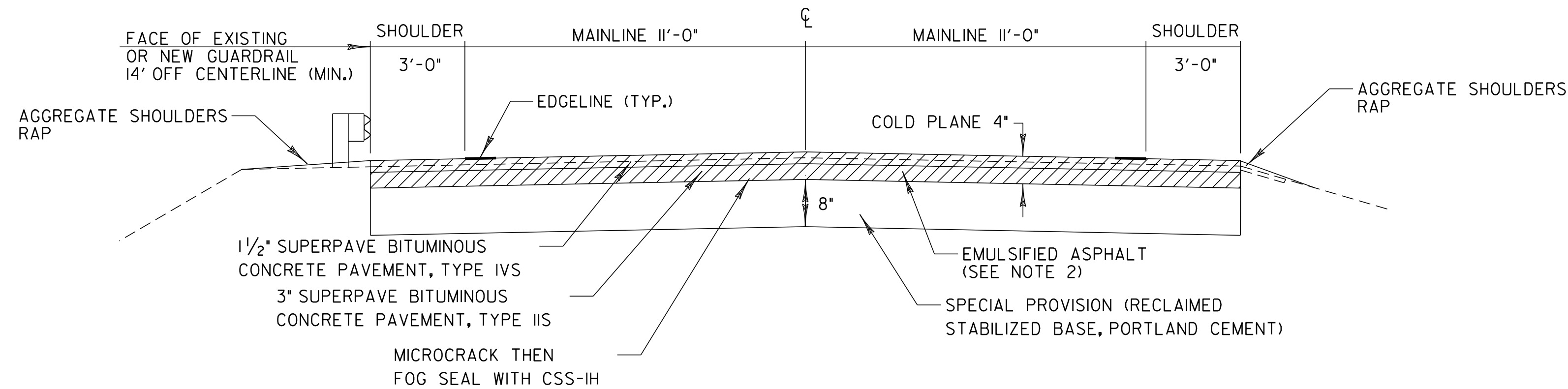
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PROJECT LEADER: CDL	DRAWN BY: SJL
DESIGNED BY: SJL	CHECKED BY: EPD
PLOT FILE: 01c040_02.i	SHEET 2 OF 55



COLD PLANE AND OVERLAY SECTION

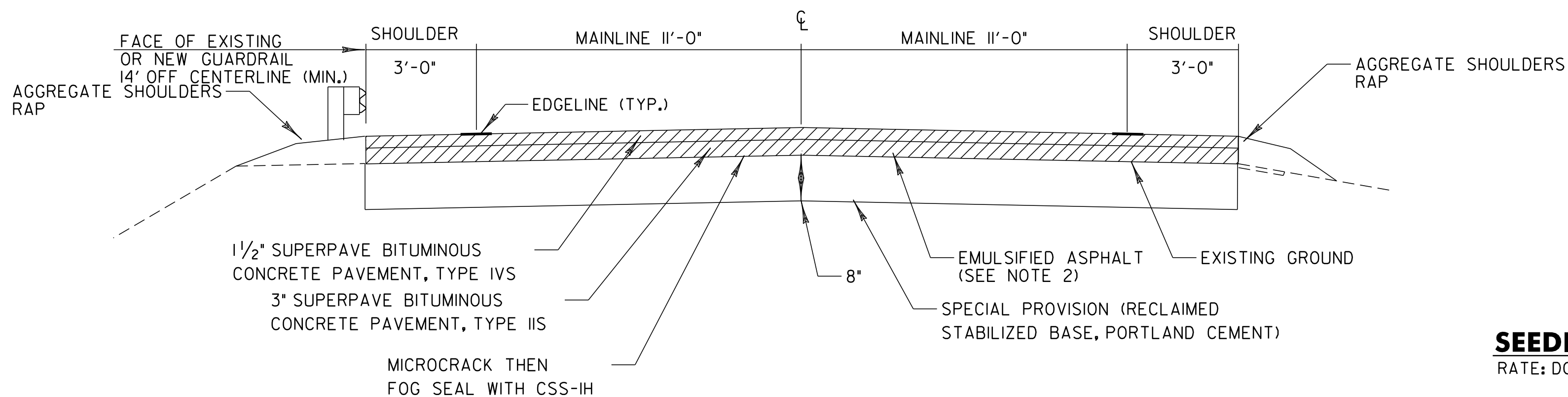
CHELSEA
STA. 0+40.00 TO STA. 8+71.20

NOTE: THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING PAVEMENT THICKNESS ON BRIDGES BEFORE ANY COLD PLANE OPERATIONS BEGIN



RECLAIMED STABILIZED TYPICAL SECTION

CHELSEA
STA. 8+71.20 TO 181+63.20
VERSHIRE
STA. 0+00.00 TO STA. 39+60.00



RECLAIMED STABILIZED TYPICAL SECTION

VERSHIRE
STA. 39+60.00 TO STA. 187+96.80

SEEDING FORMULA

RATE: DOUBLE IF HYDROSEEDING

% WT.	LBS./A.	NAME	PUR %	GERM %
38	32	CREeping RED FESCUE	98	90
29	24	SPARTAN HARD FESCUE	95	85
15	12	AZAY SHEEP'S FESCUE	95	87
15	12	ANNUAL RYE GRASS	95	90
3	--	INERTS	--	--
100.0	80 LB/A			

GENERAL NOTES

SEED MIXTURE: SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS SEED.

SEED: TO BE APPLIED PER SEEDING FORMULAS OR AS DIRECTED BY THE ENGINEER.

FERTILIZER: FORMULA 10-20-10, TO BE USED WITH SEED, APPLIED AT THE RATE OF 500 LBS./ACRE. (HYDRO SEEDERS MAY USE 19-19-19 FORMULA).

AGRICULTURAL LIMESTONE: TO BE APPLIED AT THE RATE OF 2 TONS/ACRE, OR AS DIRECTED BY THE ENGINEER.

TOPSOIL: TO BE USED WITH SEED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.

NOTES:

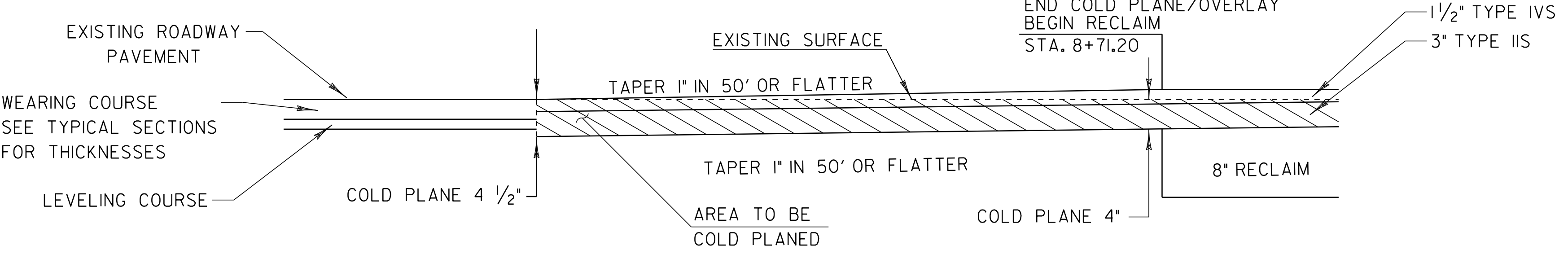
- THE WEARING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. THE LEVELING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. LEVELING HAS BEEN INCLUDED TO RESHAPE THE ROADWAY PRIOR TO PAVING THE TOP COURSE. ALL ASPHALT CEMENT USED IN THE SUPERPAVE BITUMINOUS CONCRETE PAVEMENT SHALL BE AS SPECIFIED IN THE SECTION 490 OF THE GENERAL SPECIAL PROVISIONS.
- EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES AND BETWEEN ALL COURSES OF PAVEMENT (INCLUDING RECLAIMED SURFACES) AT THE RATE OF 0.025 GAL/SY OR AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT WILL BE UNDER ITEM 404.65, EMULSIFIED ASPHALT.
- SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = +/- 1/4 INCH (TOTAL PAVEMENT THICKNESS EXCLUDING LEVELING).
- PRIOR TO RECLAIMING, ANY EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER WILL BE EXCAVATED TO THE DEPTH OF RECLAIMING OR AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATED MATERIAL WILL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT AS DIRECTED BY THE RESIDENT ENGINEER. THIS WORK WILL BE PAID FOR USING THE APPROPRIATE RENTAL ITEMS. THE METHOD OF REMOVAL AND THE USE OF RENTAL ITEMS SHALL BE APPROVED BY THE RESIDENT ENGINEER. PRIOR TO ANY WORK BEING DONE, MATERIAL REMOVAL SHALL BE REPLACED WITH ITEM 301.40 SUBBASE, RAP. AN ADDITIONAL QUANTITY OF 301.40 SUBBASE, RAP HAS BEEN INCLUDED TO CORRECT SUPPERELEVATION AND GRADATION DEFICIENCIES WITHIN THE RECLAIMED SECTION. AN ESTIMATED THICKNESS OF 2" HAS BEEN INCLUDED FOR THE ENTIRE RECLAIMED SURFACE AREA. IF THERE IS NOT SUFFICIENT AMOUNT OF COLD PLANE GRINDINGS AVAILABLE TO COMPLETE THESE REPAIRS, THE CONTRACTOR SHALL OBTAIN COLD PLANE GRINDINGS FROM ANOTHER SOURCE. THE COST ASSOCIATED WITH PROVIDING ADDITIONAL COLD PLANE GRINDINGS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 301.40 SUBBASE, RAP.
- COLD PLANING SHALL BE COMPLETED ACCORDING TO THE TYPICAL OR AS OTHERWISE NOTED ON THE PLANS. A FULL DEPTH BUTT JOINT SHALL BE CONSTRUCTED AT THE BEGINNING OF THE PROJECT, ALL SIDE ROAD APPROACHES AS DENOTED ON THE PROJECT PLANS OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. SAWCUTTING WILL NOT BE PAID FOR SEPARATELY. IT SHALL BE CONSIDERED INCIDENTAL TO ITEM 210.10 "COLD PLANING BITUMINOUS PAVEMENT"
- ALL DRIVES SHALL RECEIVE A PAVED APRON OF 4' OR AS DIRECTED BY THE RESIDENT ENGINEER. ANY AND ALL REQUIRED EXCAVATION IN DRIVE AREAS SHALL BE INCIDENTAL TO ITEM 900.675 SPECIAL PROVISION "HAND PLACED BITUMINOUS CONCRETE MATERIAL, DRIVES". THE NEW BITUMINOUS SURFACE SHALL BE CONSTRUCTED AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 900.675, SPECIAL PROVISION (HAND-PLACED BITUMINOUS CONCRETE MATERIAL, DRIVES).
- THREE FEET OF BACKING IS REQUIRED BEHIND THE FACE OF GUARDRAIL WITH 6 FOOT POSTS. IF THIS CANNOT BE OBTAINED THEN 8 FOOT POSTS SHALL BE USED. THESE ITEMS SHALL BE PAID UNDER ITEM 621.20 "STEEL BEAM GUARDRAIL, GALVANIZED" AND ITEM 621.205 "STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS".
- A QUANTITY FOR ITEM 604.412, "REHAB DI, CB OR MH CLASS I", ITEM 604.415, "REHAB DI, CB OR MH CLASS II", ITEM 604.418, "REHAB DI, CB OR MH CLASS III" AND ITEM 604.40, "CHANGING ELEVATION OF DROP INLETS, CATCH BASINS, OR MANHOLES HAVE BEEN INCLUDED IN THE QUANTITIES TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER. ALL DI'S SHALL BE RAISED OR REHABILITATED SUCH THAT THE NEW GRATE ELEVATION IS LEVEL WITH THE SURROUNDING TERRAIN. DRAINAGE STRUCTURES CALLING FOR REHAB HAVE BEEN EVENLY DISTRIBUTED BETWEEN ITEMS 604.412, 604.415 AND 604.418 FOR ESTIMATING PURPOSES.
- ALL EDGES OF PAVEMENT WITHIN THE COLD PLANE SECTION SHALL BE BACKED UP TO FULL HEIGHT WITH COLD PLANE GRINDINGS AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.13 AGGREGATE SHOULDERS, RAP. ADDITIONAL MATERIAL REQUIRED AFTER THE COLD PLANE GRINDINGS HAVE BEEN USED WILL BE PAID FOR UNDER ITEM 402.12 AGGREGATE SHOULDERS.
- ESTIMATED QUANTITIES OF ITEM 608.25, "ALL PURPOSE EXCAVATOR RENTAL, TYPE I", 608.37, "TRUCK RENTAL", AND 608.40, "LOADER RENTAL, TYPE I" HAVE BEEN INCLUDED FOR THE PROVISION OF CONSTRUCTING GUARDRAIL END SECTIONS WITH EXCAVATED DITCHING MATERIAL. AN ESTIMATED QUANTITY OF 203.30, "EARTH BORROW" HAS BEEN INCLUDED FOR USE WITH THE GUARDRAIL END SECTIONS. 25 CY OF ITEM 203.30, "EARTH BORROW" HAS BEEN INCLUDED FOR EACH NEW GUARDRAIL END SECTION. GUARDRAIL END SECTIONS SHALL BE CAPPED WITH AN ESTIMATED 3 INCH DEPTH OF ITEM 402.12, "AGGREGATE SHOULDERS" UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THE QUANTITIES INCLUDED REFLECT 5 TONS OF ITEM 402.12, "AGGREGATE SHOULDERS" FOR EACH NEW GUARDRAIL END SECTION. ITEM 653.20, "TEMPORARY EROSION MATTING" SHALL BE PLACED ON SLOPES GREATER THAN 1:6 CREATED BY THE GUARDRAIL END SECTIONS. THE QUANTITIES INCLUDED REFLECT 25 SY OF ITEM 653.20, "TEMPORARY EROSION MATTING" FOR EACH NEW GUARDRAIL END SECTION. PRIOR TO THE PLACEMENT OF TEMPORARY EROSION MATTING, THE AREA SHALL BE TOPSOILED AND SEEDED USING ITEM 651.35, "TOPSOIL" AND ITEM 651.15, "SEED".
- STABILIZING AGENT FOR THE RECLAIMED STABILIZED BASE WILL BE PORTLAND CEMENT. PAYMENT WILL BE MADE UNDER ITEM 900.680 "SPECIAL PROVISION (PORTLAND CEMENT FOR BASE STABILIZATION)".
- ESTIMATED QUANTITY FOR ITEM 619.17 YIELDING MARKER POSTS HAS BEEN INCLUDED TO DELINEATE PIPE INLETS, PIPE OUTLETS AND DROP INLETS LOCATED OUTSIDE THE PAVEMENT SURFACES OR AS DIRECTED BY THE RESIDENT ENGINEER.
- STEEL BEAM GUARDRAIL WITH STEEL POSTS SHALL BE USED ON THIS PROJECT.
- ITEM 605.10 6 INCH UNDERDRAIN PIPE IS AN ESTIMATED QUANTITY TO BE USED BY THE RESIDENT ENGINEER IF WET AREAS ARE ENCOUNTERED DURING RECLAIMING.
- ITEM 609.15 DUST AND ICE CONTROL WITH CALCIUM CHLORIDE IS AN ESTIMATED QUANTITY TO BE USED AS DUST CONTROL FOR SIDEROADS.
- IN AREAS WHERE A UTILITY POLE OR SIMILAR OBJECT IS LOCATED WITHIN 4 FEET BEHIND GUARDRAIL, A REDUCED POST SPACING SHALL BE USED. THE REDUCED SPACING IS DETAILED ON PROJECT TYPICAL SHEET 2.
- ITEM 204.20 TRENCH EXCAVATION OF EARTH HAS BEEN INCLUDED FOR THE REMOVAL OF THE EXISTING CULVERT AT STA. 115+95.

PROJECT TYPICAL SHEET 1

PROJECT NAME: CHELSEA-VERSHIRE
PROJECT NUMBER: STP 2331(1)S

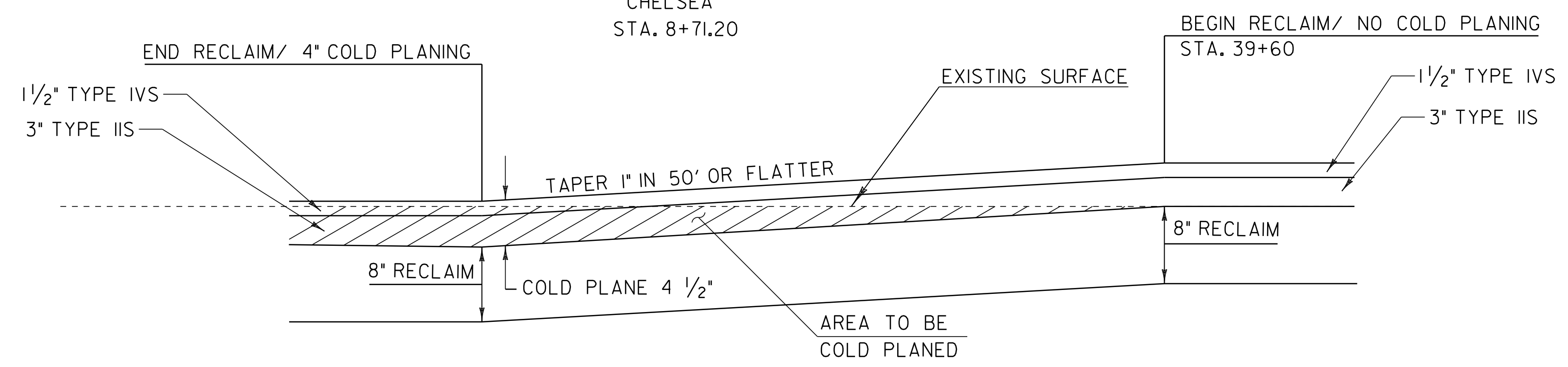
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DESIGNED BY: CDL CHECKED BY: EPD
PLOT FILE: 01c040_03.1 SHEET 3 OF 55

DETAILS ARE NOT TO SCALE



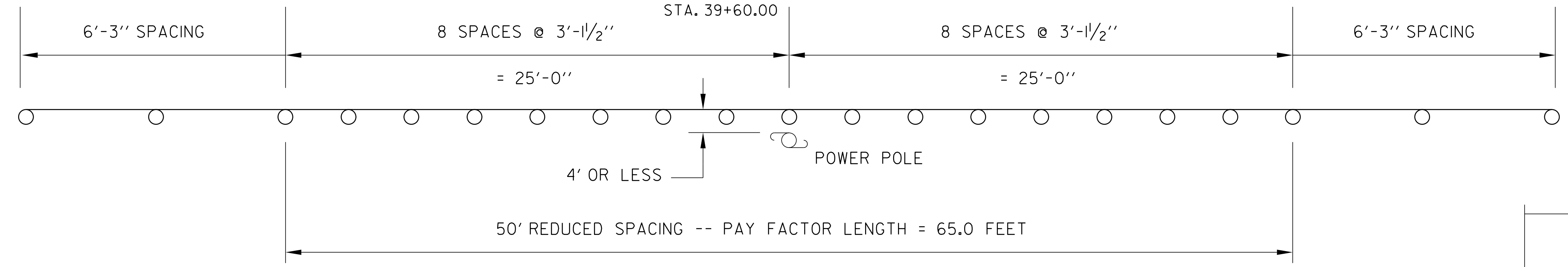
TRANSITION DETAIL

CHELSEA
STA. 8+71.20



TRANSITION DETAIL

CHELSEA
STA. 39+60.00

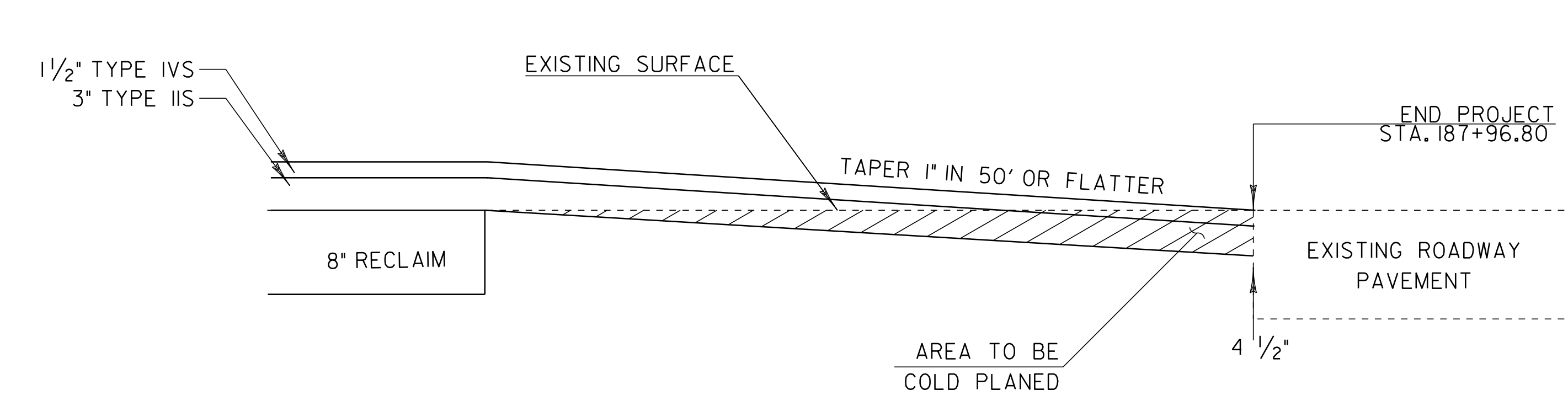


REDUCED SPACING GUARDRAIL DETAIL

OBJECTS WITHIN 4' OF GUARDRAIL

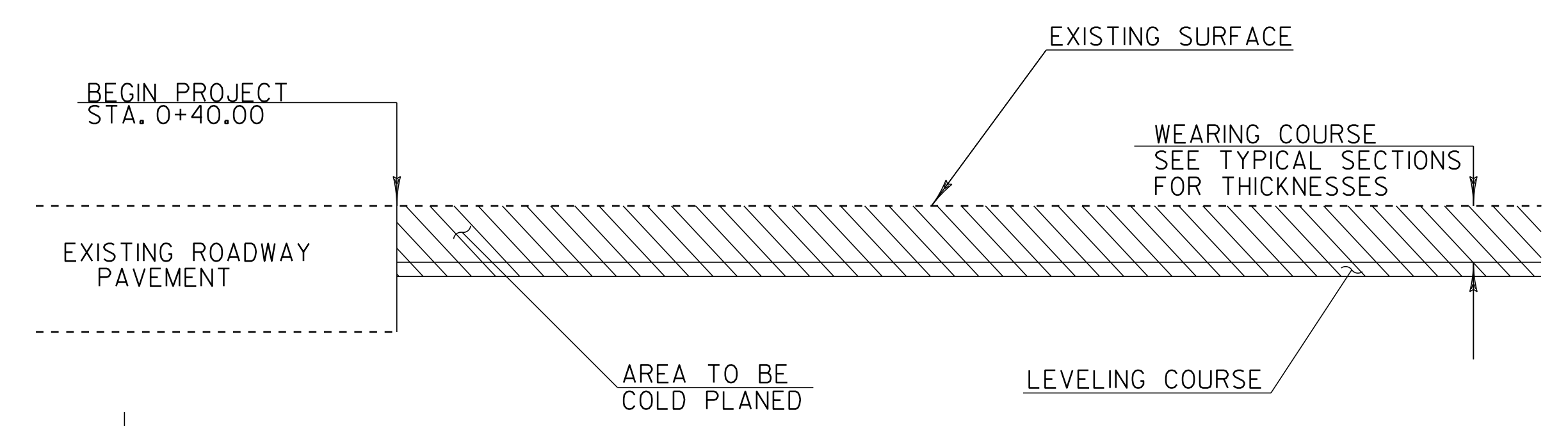
PROJECT PAVING LIMITS

TOWN AND ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING TON	NOTES
CHELSEA	0+40.00	8+71.20	2' - 11' - 11' - 2'	1 1/2"	67.54	COLD PLANE 2", LEVEL, 1 1/2" TYPE IVS
	8+71.20	10+00.00	2' - 11' - 11' - 2'	1 1/2"	-	COLD PLANE 4", RECLAIM 8", CEMENT STABILIZED BASE 8", MICROCRACK THEN FOG SEAL WITH CSS-IH, 3" TYPE IIS, 1 1/2" TYPE IVS. TRANSITION IN AND OUT OF BRIDGES AS NECESSARY.
	10+00.00	11+00.00	2 1/2' - 11' - 11' - 2 1/2'	1 1/2"	-	COLD PLANE 4", RECLAIM 8", CEMENT STABILIZED BASE 8", MICROCRACK THEN FOG SEAL WITH CSS-IH, 3" TYPE IIS, 1 1/2" TYPE IVS. TRANSITION IN AND OUT OF BRIDGES AS NECESSARY.
	11+00	181+63.20	3' - 11' - 11' - 3'	1 1/2"	-	COLD PLANE 4", RECLAIM 8", CEMENT STABILIZED BASE 8", MICROCRACK THEN FOG SEAL WITH CSS-IH, 3" TYPE IIS, 1 1/2" TYPE IVS. TRANSITION IN AND OUT OF BRIDGES AS NECESSARY.
VERSHIRE	0+00.00	39+60.00	3' - 11' - 11' - 3'	1 1/2"	-	COLD PLANE 4", RECLAIM 8", CEMENT STABILIZED BASE 8", MICROCRACK THEN FOG SEAL WITH CSS-IH, 3" TYPE IIS, 1 1/2" TYPE IVS. TRANSITION IN AND OUT OF BRIDGES AS NECESSARY.
	39+60.00	187+96.80	3' - 11' - 11' - 3'	1 1/2"	-	RECLAIM 8", CEMENT STABILIZED BASE 8", MICROCRACK, THEN FOG SEAL WITH CSS-IH, 3" TYPE IIS, 1 1/2" TYPE IVS



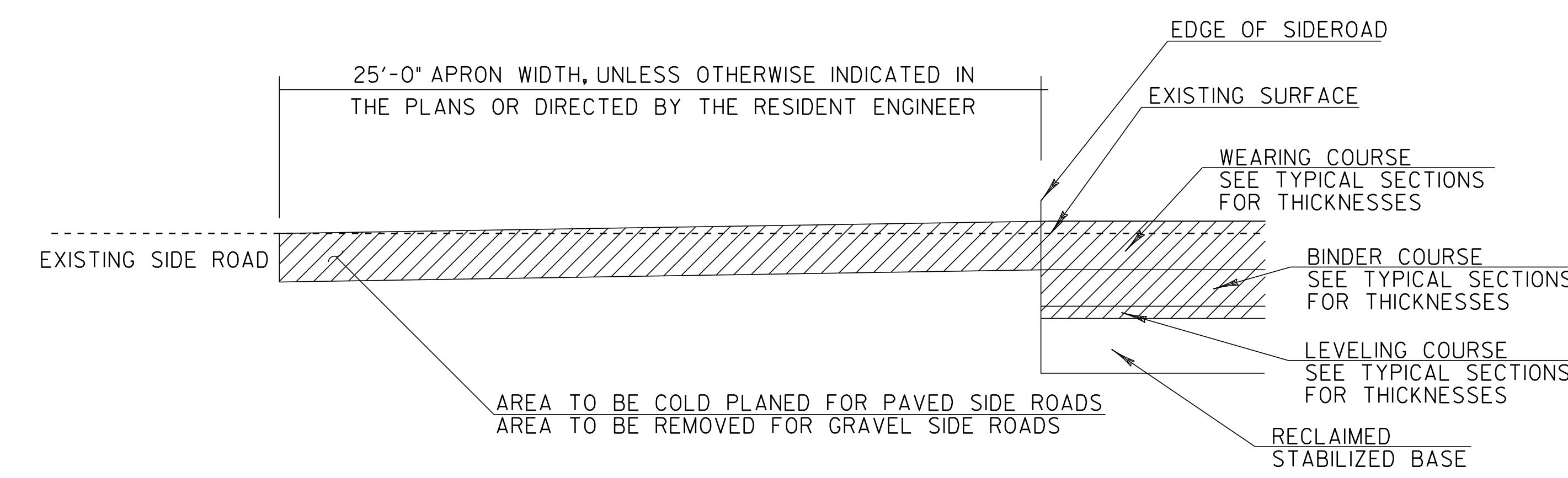
APPROACH AREA DETAIL (END PROJECT)

VERSHIRE
STA. 187+96.80



APPROACH AREA DETAIL (BEGIN PROJECT)

CHELSEA
STA. 0+40.00



TRANSITION AREA FOR SIDE ROADS

SEE LAYOUT SHEETS FOR LOCATIONS OF ALL SIDE ROADS

CHELSEA
STA. 8+71.20 TO 181+63.20
VERSHIRE
STA. 0+00.00 TO STA. 39+60.00

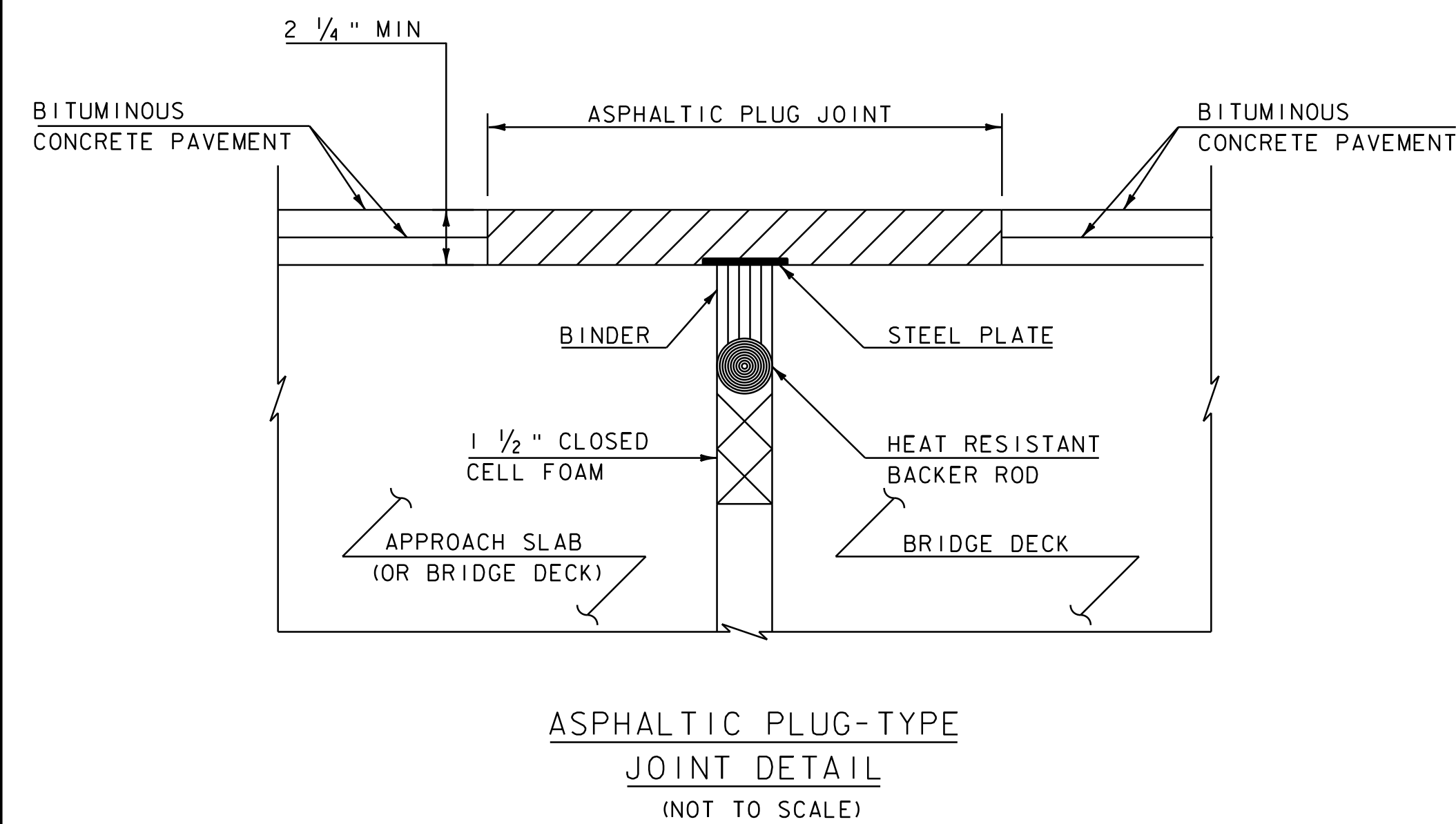
NOTE:

- I. TRANSITION AT SIDE ROADS SHALL HAVE A TAPER LENGTH OF 25', UNLESS SHOWN OTHERWISE ON THE PLANS. SEE PLANS FOR LOCATION OF SIDE ROADS. A BUTT JOINT WILL BE LOCATED AT THIS LOCATION.

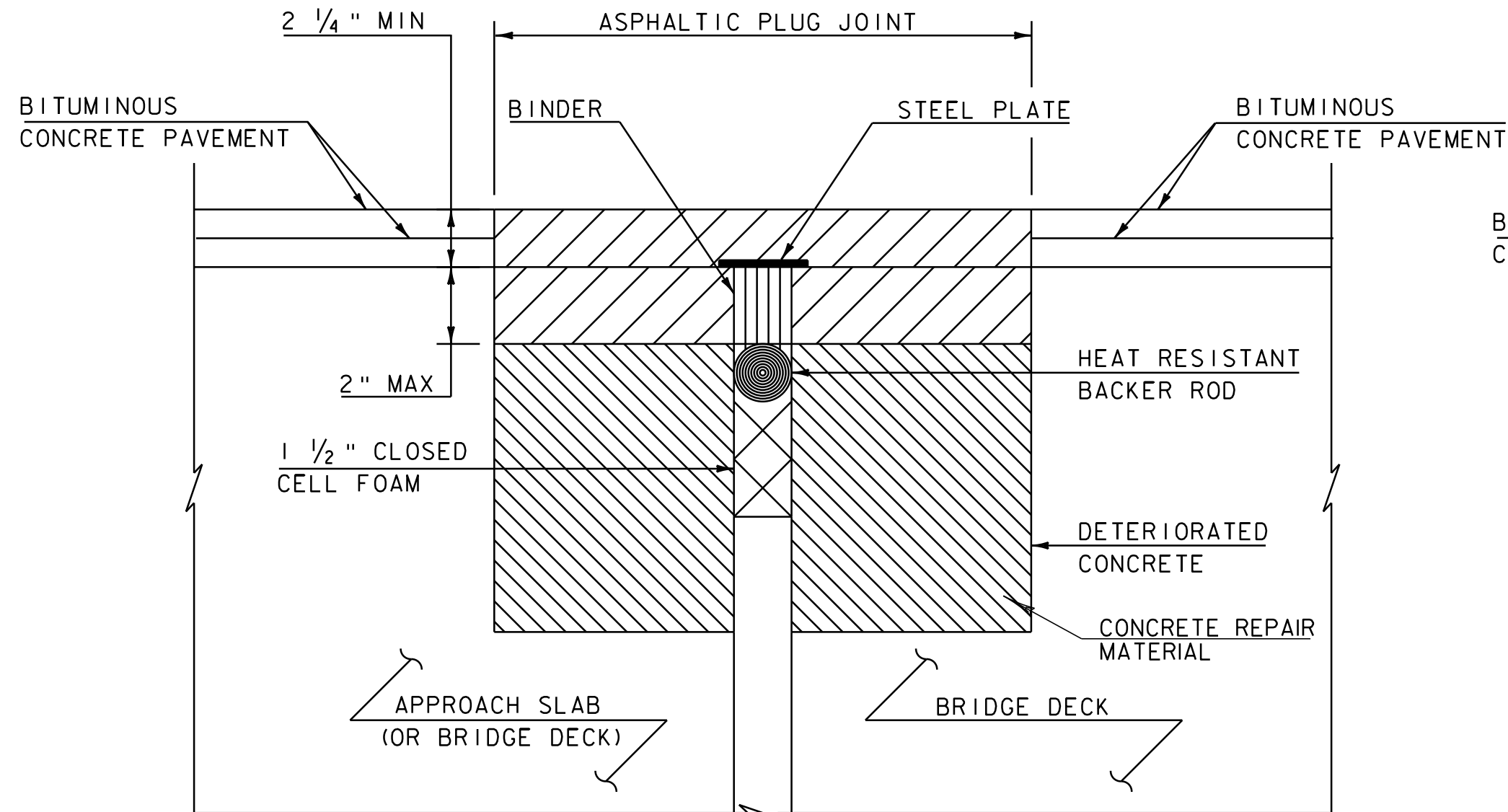
PROJECT TYPICAL SHEET 2

DETAILS ARE NOT TO SCALE

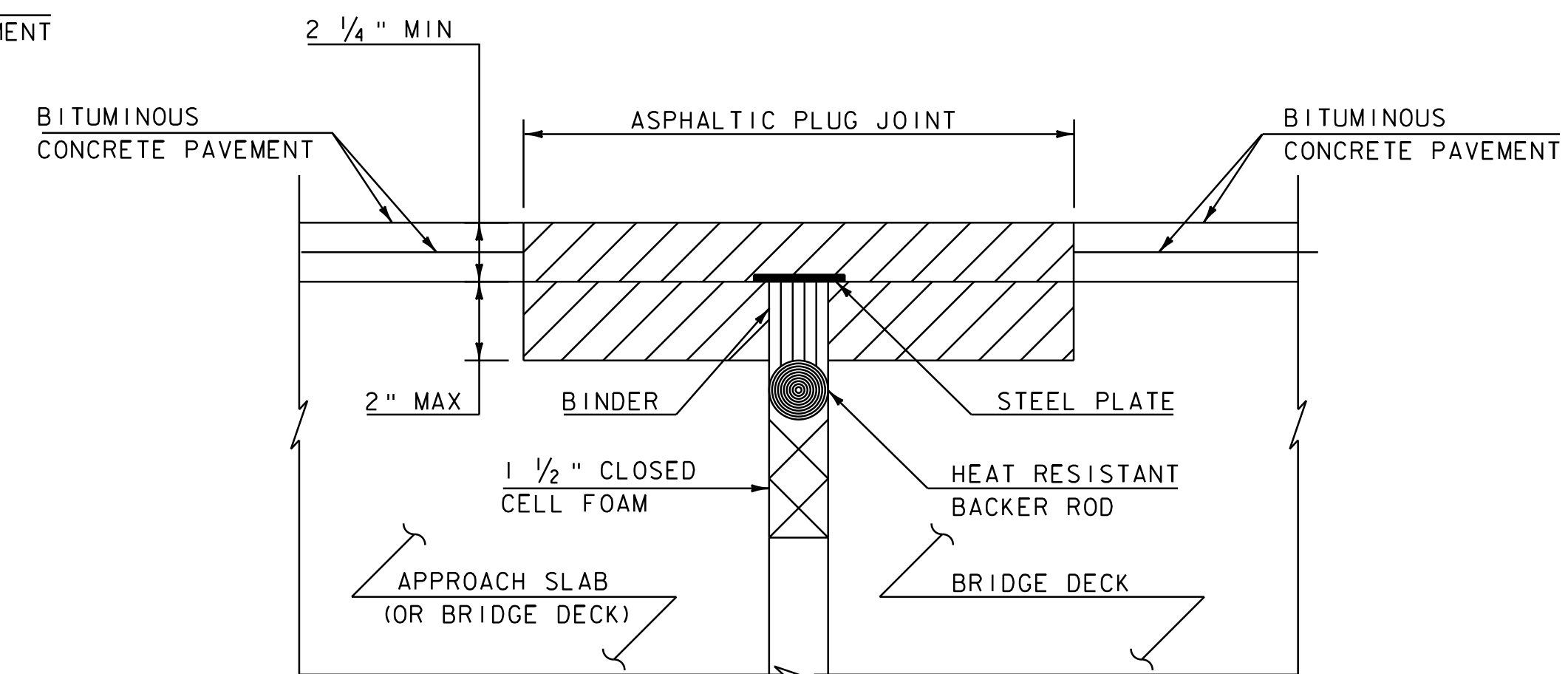
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PROJECT NUMBER: STP 2331(1)S	DRAWN BY: SJL
FILE NAME: 01c040.dgn	DESIGNED BY: CDL
PROJECT LEADER: CDL	CHECKED BY: EPD
DESIGNED BY: CDL	SHEET 4 OF 55
PLOT FILE: 01c040_04.i	



ASPHALTIC PLUG-TYPE JOINT DETAIL (NOT TO SCALE)



ASPHALTIC PLUG-TYPE JOINT DETAIL REMOVAL OF > 2" DETERIORATED CONCRETE (NOT TO SCALE)



ASPHALTIC PLUG-TYPE JOINT DETAIL REMOVAL OF < 2" DETERIORATED CONCRETE (NOT TO SCALE)

ASPHALTIC PLUG JOINT NOTES

1. INSTALLATION

- A. LOCATE THE JOINT CENTRALLY OVER THE DECK OVERLAY EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.
- B. REMOVE THE BITUMINOUS CONCRETE PAVEMENT FULL DEPTH AS SHOWN ON THE PLANS. THE PAVEMENT SHALL BE DRY AND SAW CUT TO THE LIMITS REQUIRED TO PLACE THE JOINT. A PNUMATIC HAMMER AND CHISEL MAY BE USED ADJACENT TO THE CURB ONLY WHEN SAW CUTTING IS NOT POSSIBLE.
- C. BLAST CLEAN THE JOINT AREA OF DEBRIS, ASPHALT AND SHEET MEMBRANE. THOROUGHLY DRY THE JOINT AREA WITH COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.
- D. REPAIR SPALLED AND DEFECTIVE CONCRETE WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.
- E. PLACE PROPERLY SIZED HEAT RESISTANT BACKER ROD IN THE MOVEMENT GAP ALLOWING FOR 1" +/- OF BINDER ABOVE THE ROD.
- F. HEAT AND PLACE THE BINDER MATERIAL AS RECOMMENDED BY THE MANUFACTURER.
- G. PLACE 1/4" THICK BY 8" WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE THE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRESTAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER. THE STEEL PLATES MAY BE OMITTED WHERE THE ENGINEER DETERMINES THAT THE APPROACH SLAB OR BRIDGE DECK WILL PROVIDE INADEQUATE SUPPORT AND WHERE VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.
- H. HEAT AND MIX THE BINDER MATERIAL AND AGGREGATE AS RECOMMENDED BY THE MANUFACTURER.
- I. INSTALLATION OF MATERIAL, COMPACTION, AND TOP COATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- J. IMMEDIATELY AFTER TOP COATING, CAST AN ANTI-SKID MATERIAL OVER THE JOINT TO REDUCE THE RISK OF TRACKING.
- K. ONCE THE JOINT REACHES 82 DEG C (180 DEG F) +/-, WATER MAYBE USED TO EXPEDITE THE COOLING PROCESS.
- L. PROTECTED JOINT FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 51 DEG C (125 DEG F) +/-.

2. WEATHER LIMITATIONS. (APPLY BINDER MATERIAL ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL OR AS RECOMMENDED BY THE MANUFACTURER):

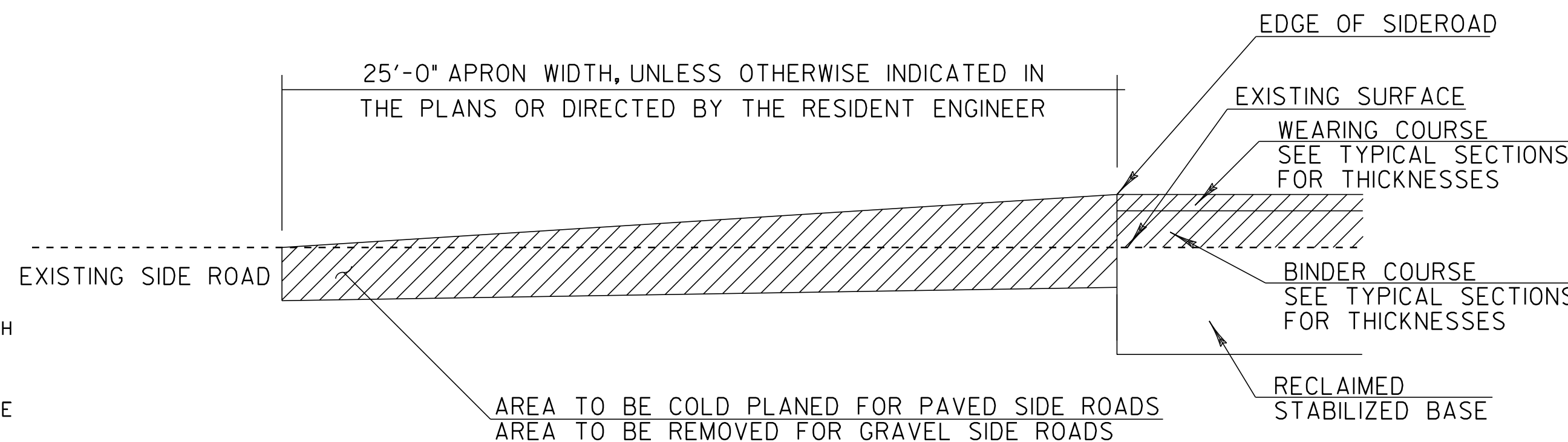
- A. THE AMBIENT AIR TEMPERATURE IS AT LEAST 10 DEG C (50 DEG F) AND RISING.
- B. THE ROAD SURFACE IS DRY.
- C. WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF SATISFACTORY WORK.

NOTES:

1. UPON ENCOUNTERING GREATER THAN 2" AVERAGE OF DETERIORATED CONCRETE, THE CONTRACTOR SHALL REMOVE THE DETERIORATED MATERIAL AND REPLACE IT WITH RAPID SETTING CONCRETE REPAIR MATERIAL WITH COARSE AGGREGATE FORMED TO EXISTING ELEVATION.
2. REMOVAL OF THE DETERIORATED CONCRETE WILL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 580.20 "RAPID SETTING CONCRETE REPAIR MATERIAL WITH COARSE AGGREGATE".
3. REINFORCING STEEL NOT SHOWN FOR CLARITY.

NOTES:

1. UPON ENCOUNTERING UP TO 2" AVERAGE OF DETERIORATED CONCRETE, THE CONTRACTOR SHALL REMOVE THE DETERIORATED MATERIAL AND REPLACE IT WITH THE ASPHALTIC PLUG JOINT MATERIAL AS DIRECTED BY THE RESIDENT ENGINEER.
2. REMOVAL OF THE DETERIORATED CONCRETE WILL NOT BE PAID SEPARATELY BUT WILL BE CONSIDERED INCIDENTAL TO THE UNIT BID PRICE FOR THE ITEM 516.10. THE ADDITIONAL PLUG JOINT MATERIAL BELOW THE DESIGN DEPTH TO REPLACE THE DETERIORATED CONCRETE WILL BE CONSIDERED INCIDENTAL TO THE UNIT BID PRICE FOR THE ITEM 516.10.



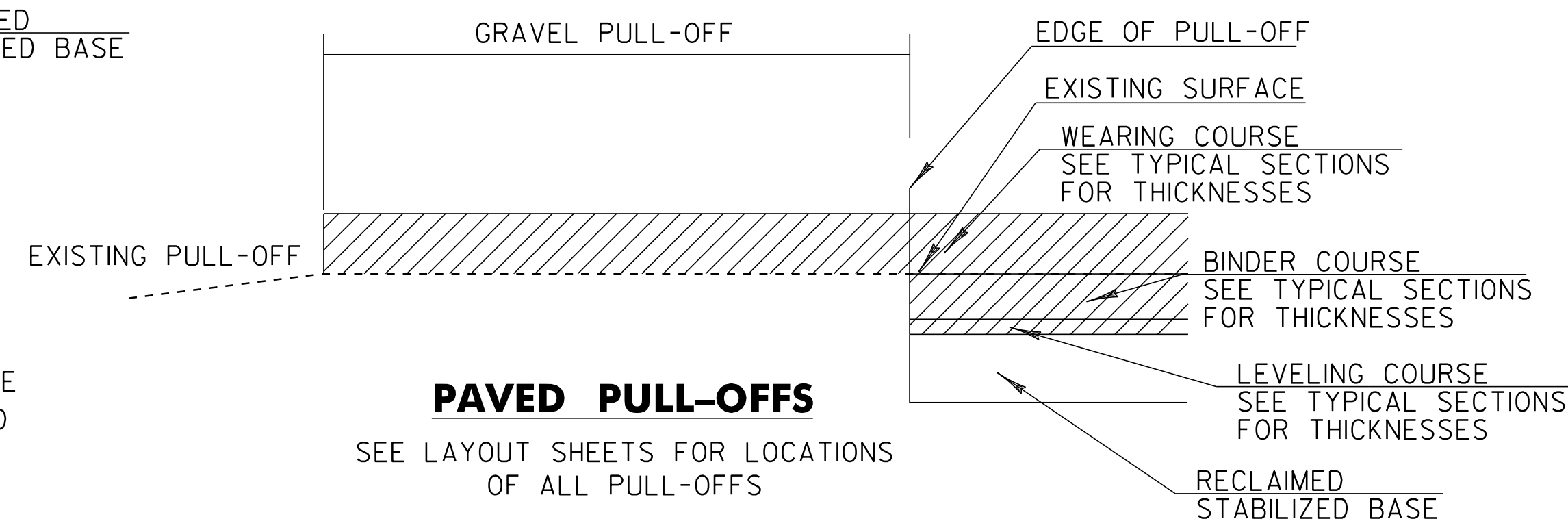
TRANSITION AREA FOR SIDE ROADS

SEE LAYOUT SHEETS FOR LOCATIONS OF ALL SIDE ROADS

VERSHIRE
STA. 39+60.00 TO STA. 187+96.80

NOTE:

1. TRANSITION AT SIDE ROADS SHALL HAVE A TAPER LENGTH OF 25', UNLESS SHOWN OTHERWISE ON THE PLANS. SEE PLANS FOR LOCATION OF SIDE ROADS. A BUTT JOINT WILL BE LOCATED AT THIS LOCATION.

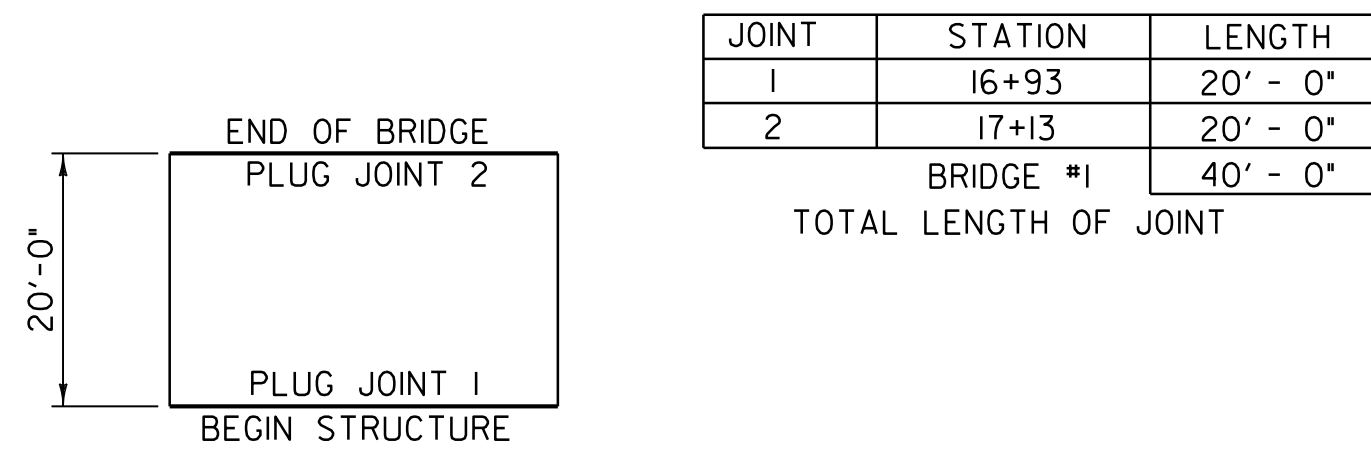


PAVED PULL-OFFS

SEE LAYOUT SHEETS FOR LOCATIONS OF ALL PULL-OFFS

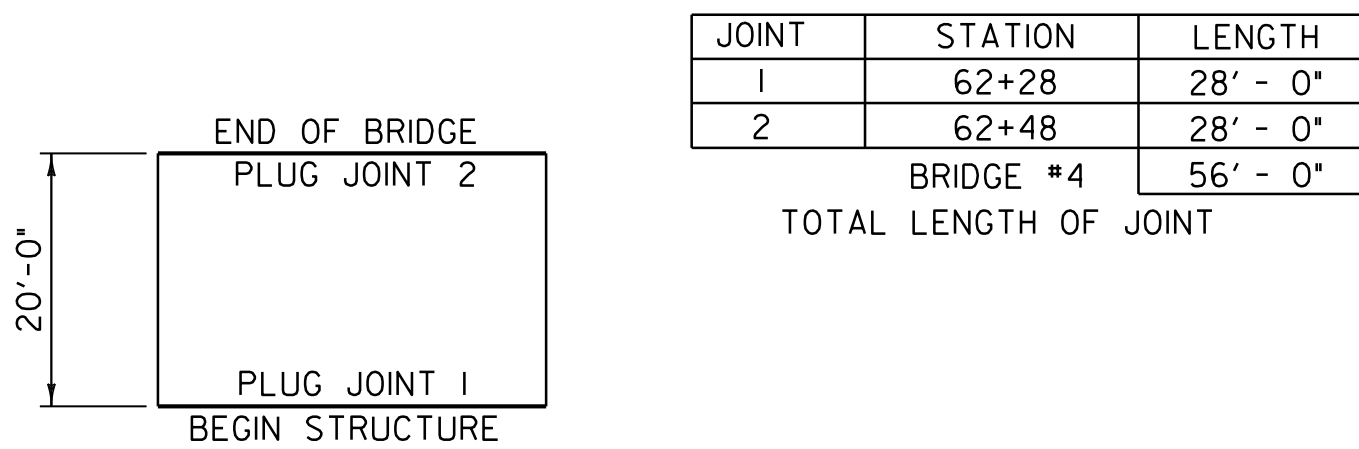
DETAILS ARE NOT TO SCALE

ASPHALTIC PLUG JOINT DETAIL SHEET	PROJECT NAME: CHelsea-VERSHIRE	PLOT DATE: 28-OCT-2011 3:35
	PROJECT NUMBER: STP 2331(1)S	DRAWN BY: SJL
	FILE NAME: 01c040.dgn	DESIGNED BY: CDL
	PROJECT LEADER: CDL	CHECKED BY: EPD
	PLOT FILE: 01c040_05.i	SHEET 5 OF 55



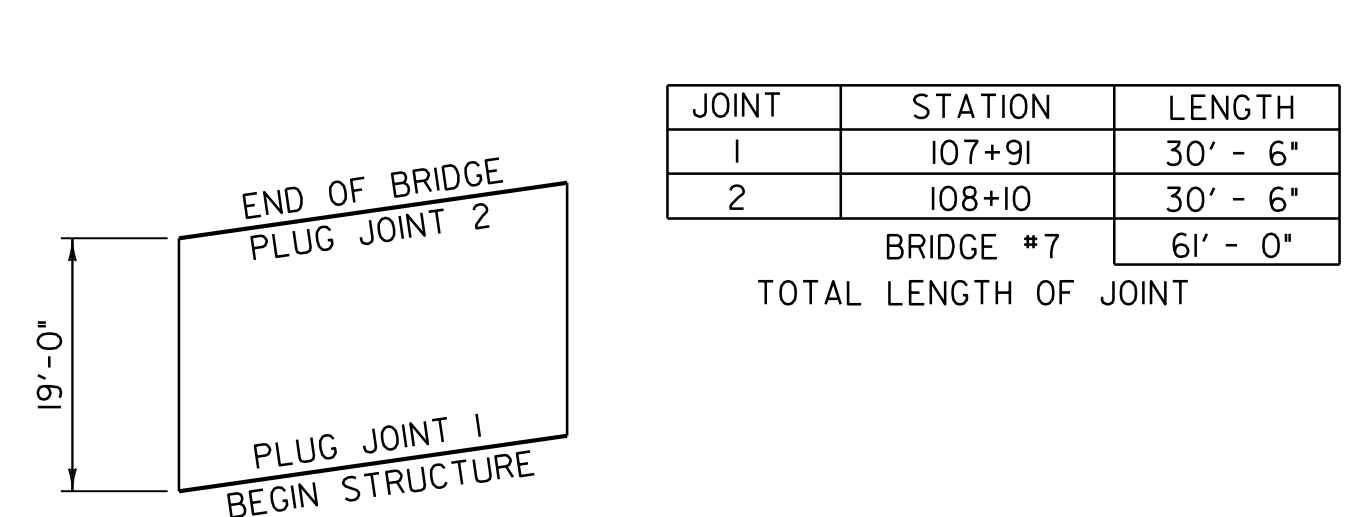
JOINT	STATION	LENGTH
1	16+93	20' - 0"
2	17+13	20' - 0"
BRIDGE #1 TOTAL LENGTH OF JOINT		

BRIDGE #1
CHELSEA STA. 17+03 (MM 0.323)
COLD PLANE AND PAVE



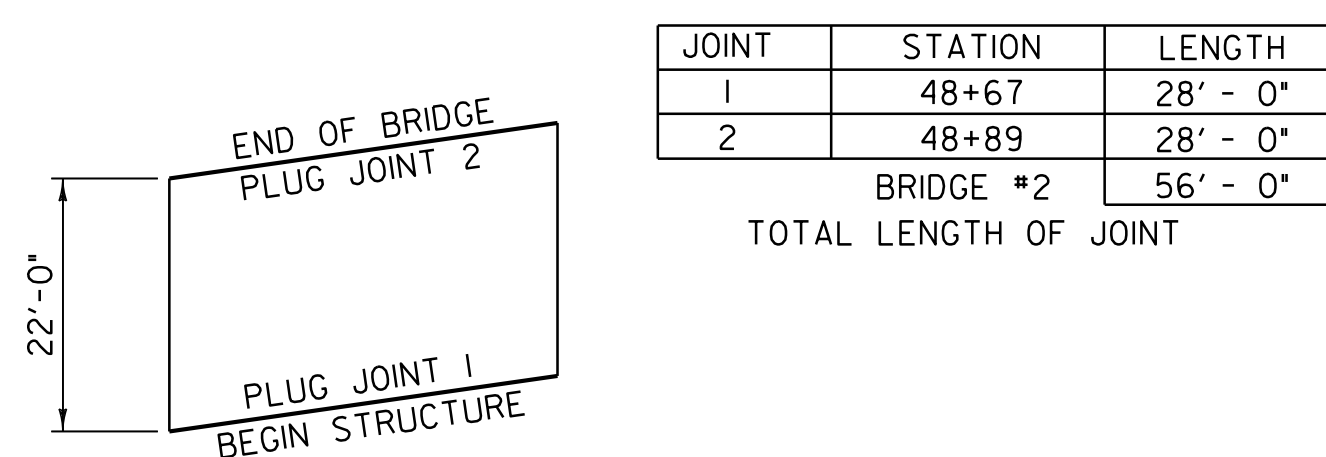
JOINT	STATION	LENGTH
1	62+28	28' - 0"
2	62+48	28' - 0"
BRIDGE #4 TOTAL LENGTH OF JOINT		

BRIDGE #4
CHELSEA STA. 62+36 (MM 1.181)
COLD PLANE AND PAVE



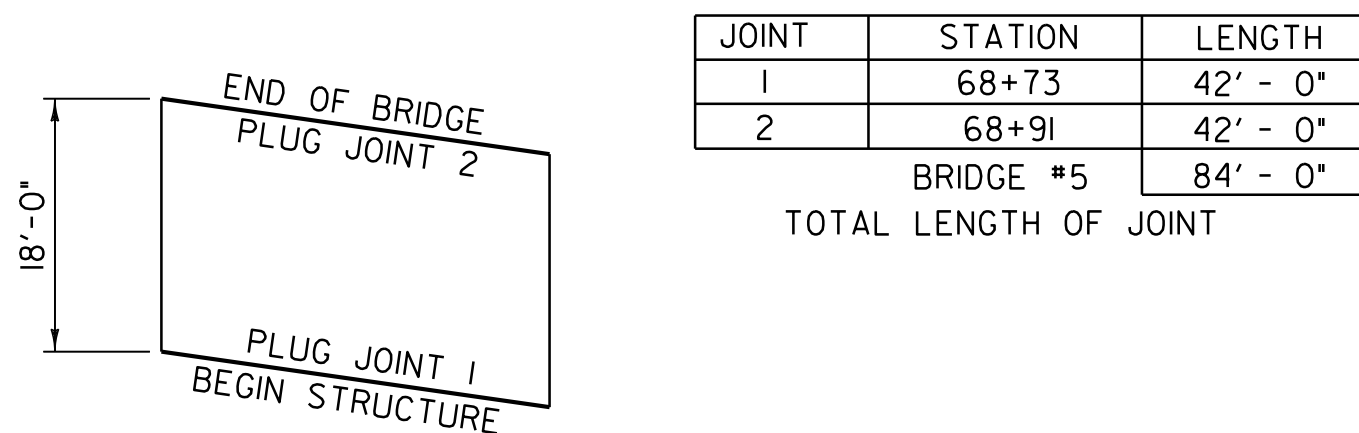
JOINT	STATION	LENGTH
1	107+91	30' - 6"
2	108+10	30' - 6"
BRIDGE #7 TOTAL LENGTH OF JOINT		

BRIDGE #7
CHELSEA STA. 108+02 (MM 2.046)
COLD PLANE AND PAVE



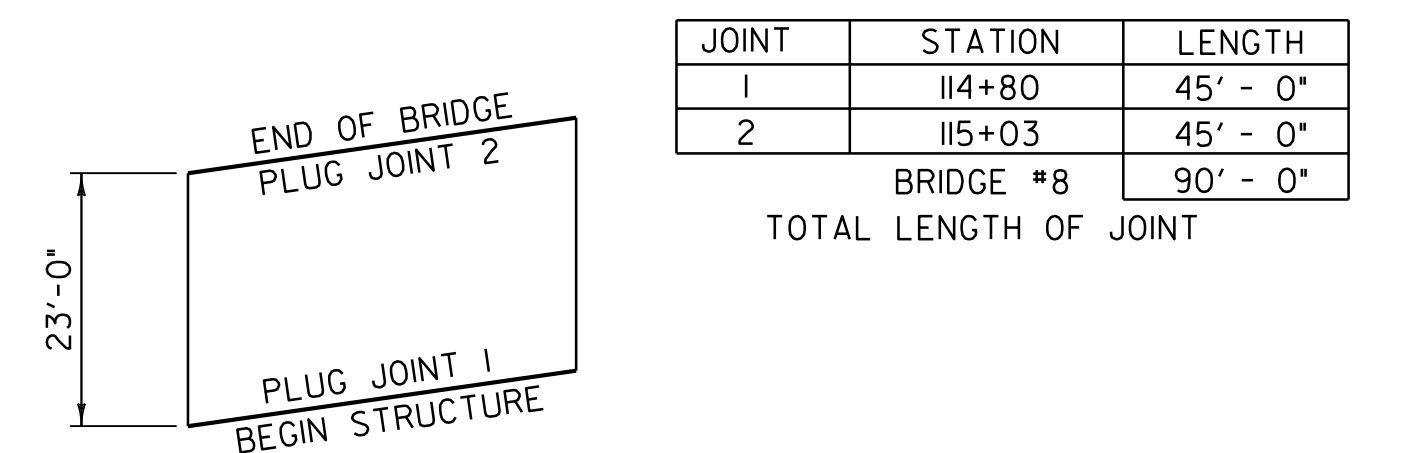
JOINT	STATION	LENGTH
1	48+67	28' - 0"
2	48+89	28' - 0"
BRIDGE #2 TOTAL LENGTH OF JOINT		

BRIDGE #2
CHELSEA STA. 48+79.50 (MM 0.924)
COLD PLANE AND PAVE



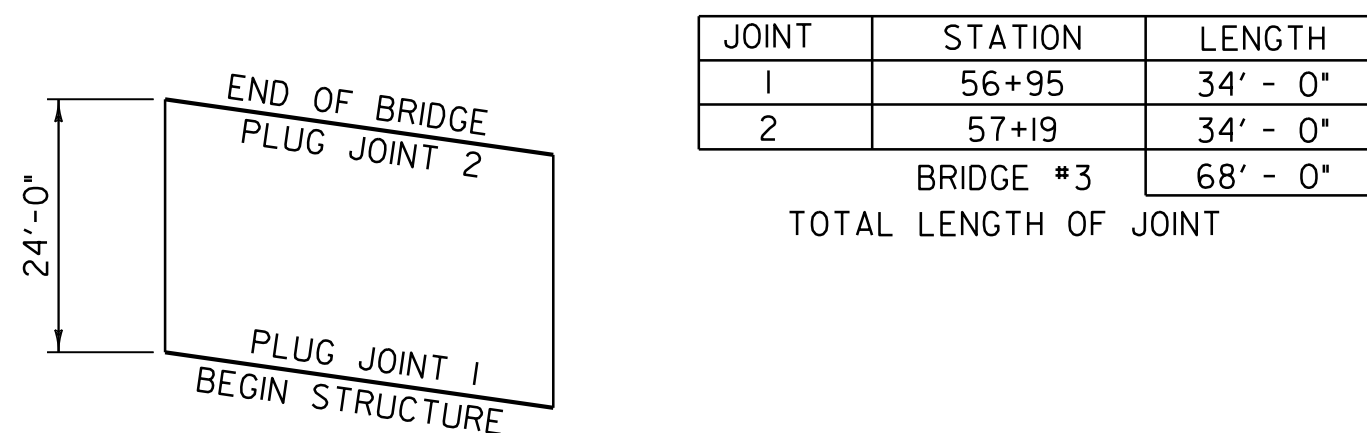
JOINT	STATION	LENGTH
1	68+73	42' - 0"
2	68+91	42' - 0"
BRIDGE #5 TOTAL LENGTH OF JOINT		

BRIDGE #5
CHELSEA STA. 68+80 (MM 1.303)
COLD PLANE AND PAVE



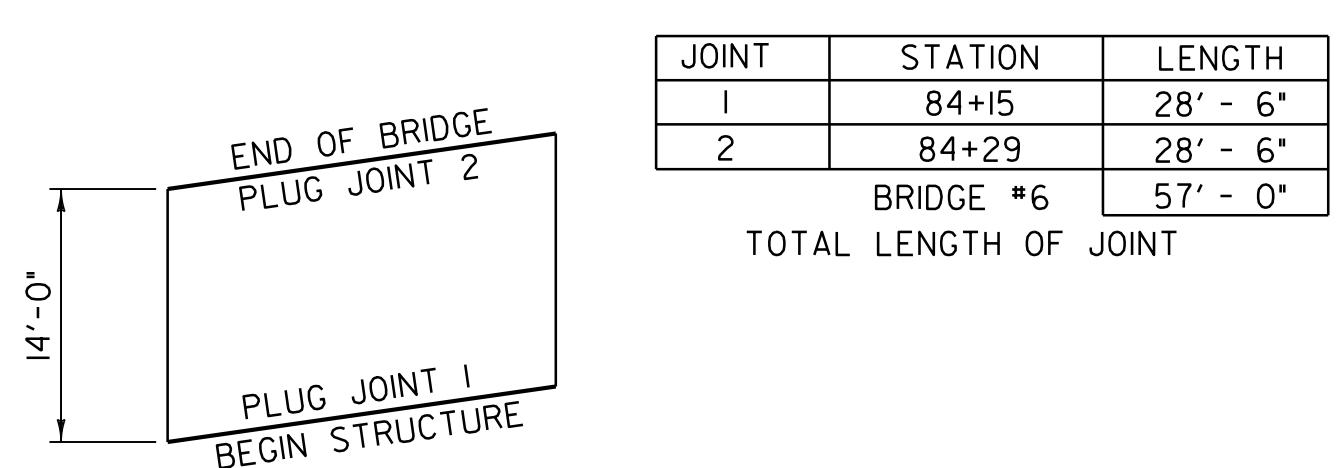
JOINT	STATION	LENGTH
1	114+80	45' - 0"
2	115+03	45' - 0"
BRIDGE #8 TOTAL LENGTH OF JOINT		

BRIDGE #8
CHELSEA STA. 115+00 (MM 2.178)
COLD PLANE AND PAVE



JOINT	STATION	LENGTH
1	56+95	34' - 0"
2	57+19	34' - 0"
BRIDGE #3 TOTAL LENGTH OF JOINT		

BRIDGE #3
CHELSEA STA. 57+06 (MM 1.081)
COLD PLANE AND PAVE



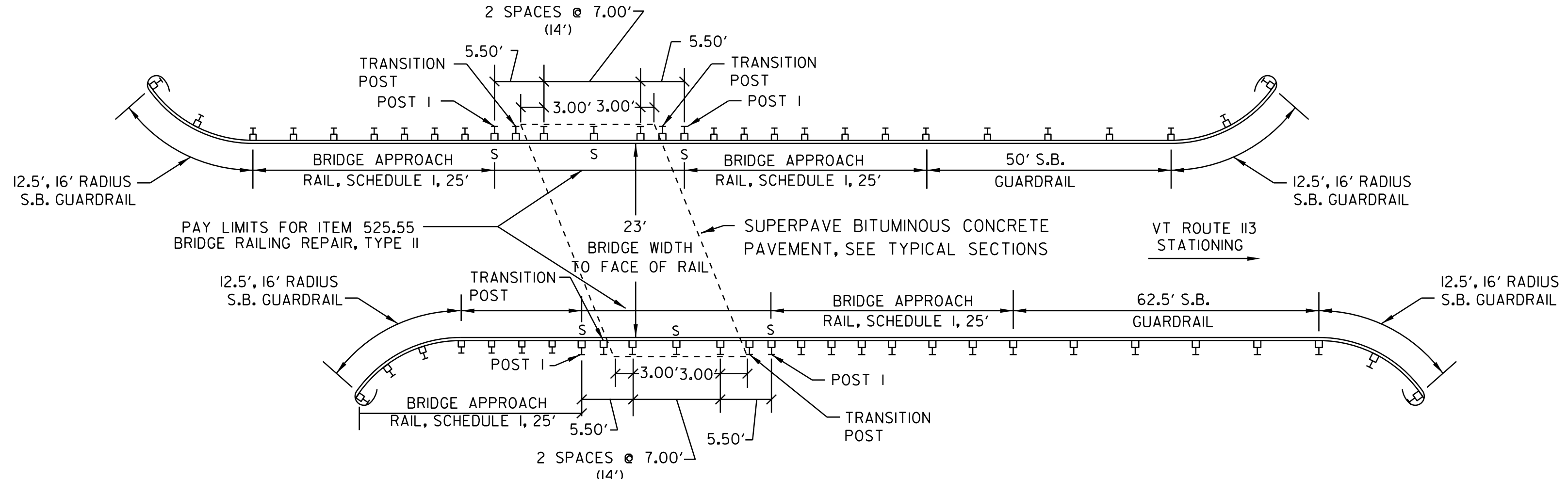
JOINT	STATION	LENGTH
1	84+15	28' - 6"
2	84+29	28' - 6"
BRIDGE #6 TOTAL LENGTH OF JOINT		

BRIDGE #6
CHELSEA STA. 84+21 (MM 1.595)
COLD PLANE AND PAVE

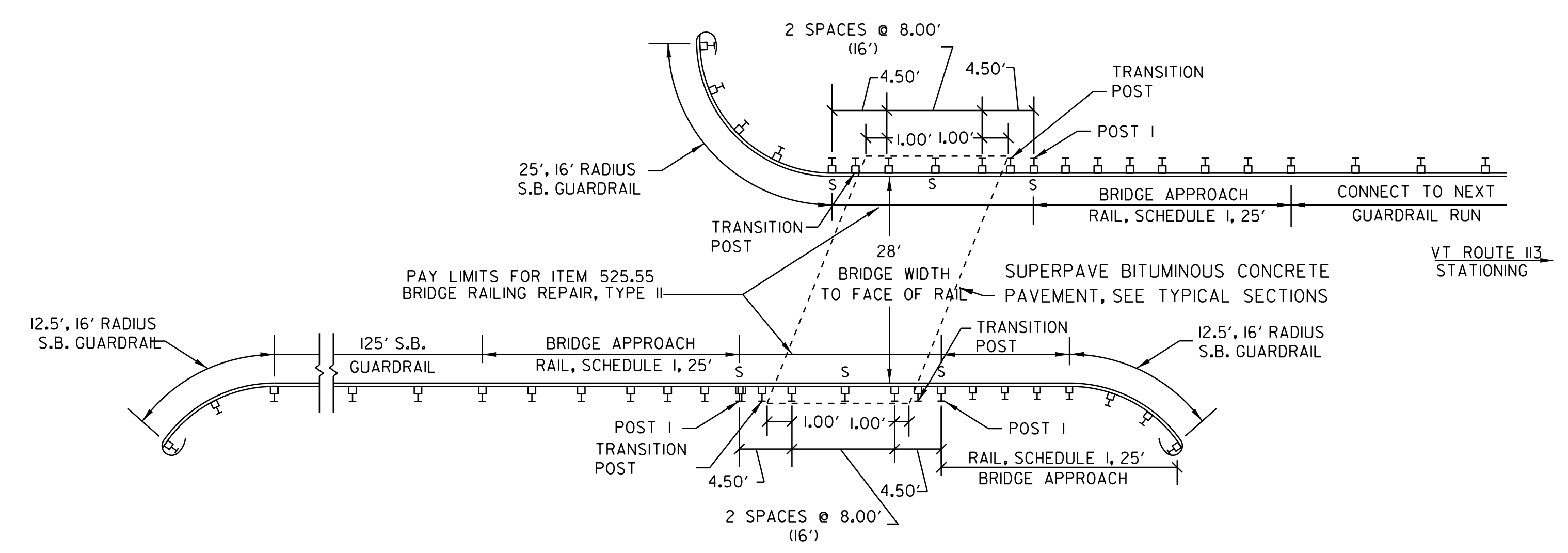
BRIDGE #	516.10	580.20
	BRIDGE EXPANSION JOINT, ASPHALT PLUG	RAPID SETTING CONCRETE REPAIR MATERIAL WITH COARSE AGGREGATE
	LF	CF
1	40	10
2	56	10
3	68	10
4	56	10
5	84	10
6	57	10
7	61	10
8	90	10
SUBTOTAL	512	80
ROUNDING	-	20
TOTAL	512	100

DETAILS ARE NOT TO SCALE

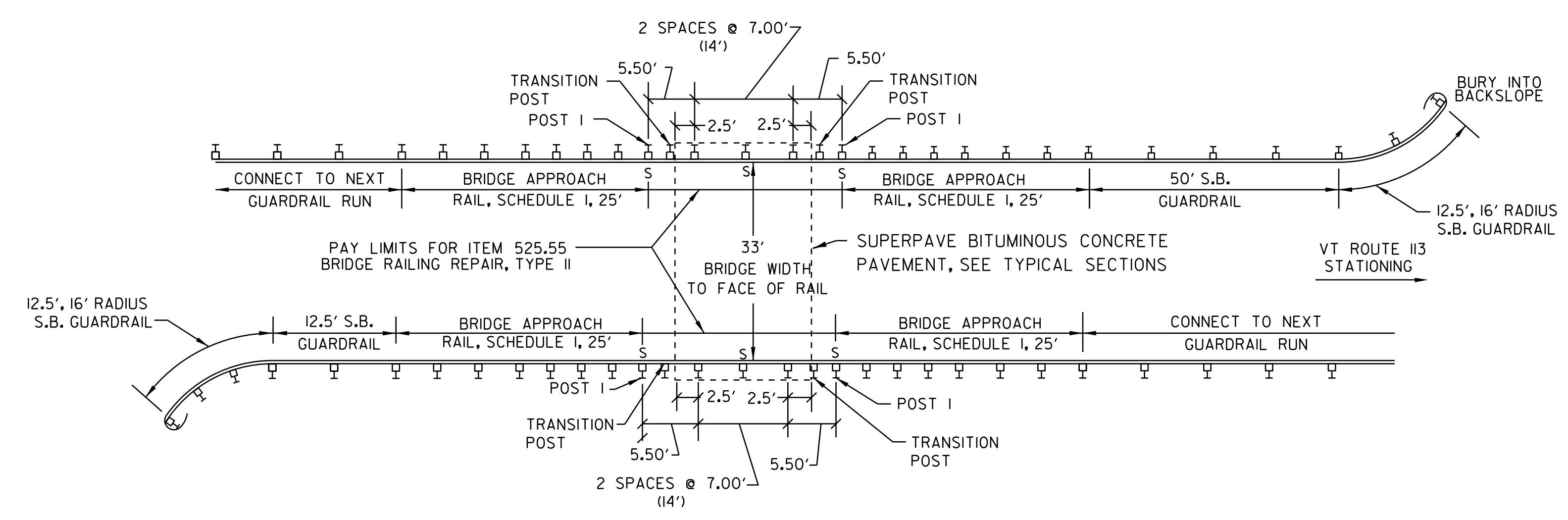
BRIDGE LOCATION AND DETAIL SHEET	PROJECT NAME: CHELSEA - VERSHIRE	
	PROJECT NUMBER: STP 2331(1)S	
	FILE NAME: 01c040.dgn	PLOT DATE: 28-OCT-2011 13:35
	PROJECT LEADER: CDL	DRAWN BY: SJL
DESIGNED BY: SJL	CHECKED BY: EPD	
PLOT FILE: 01c040_06.i	SHEET 6 OF 55	



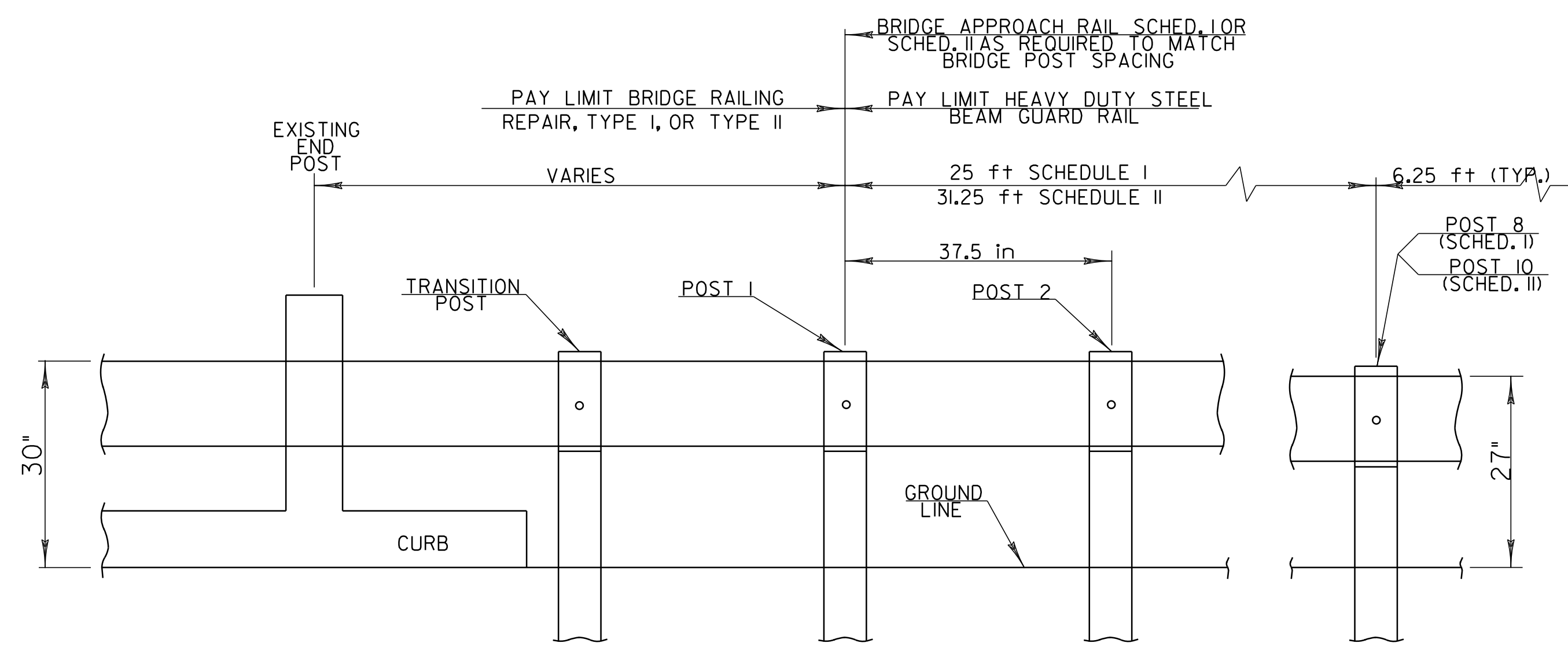
CHELSEA BRIDGE #2
FIELD STA. 48+79.50 - MM 0.924



CHELSEA BRIDGE #3
FIELD STA. 57+06 - MM 1.081



CHELSEA BRIDGE #4
FIELD STA. 62+36 - MM 1.181



BRIDGE APPROACH RAILING

NOTES:

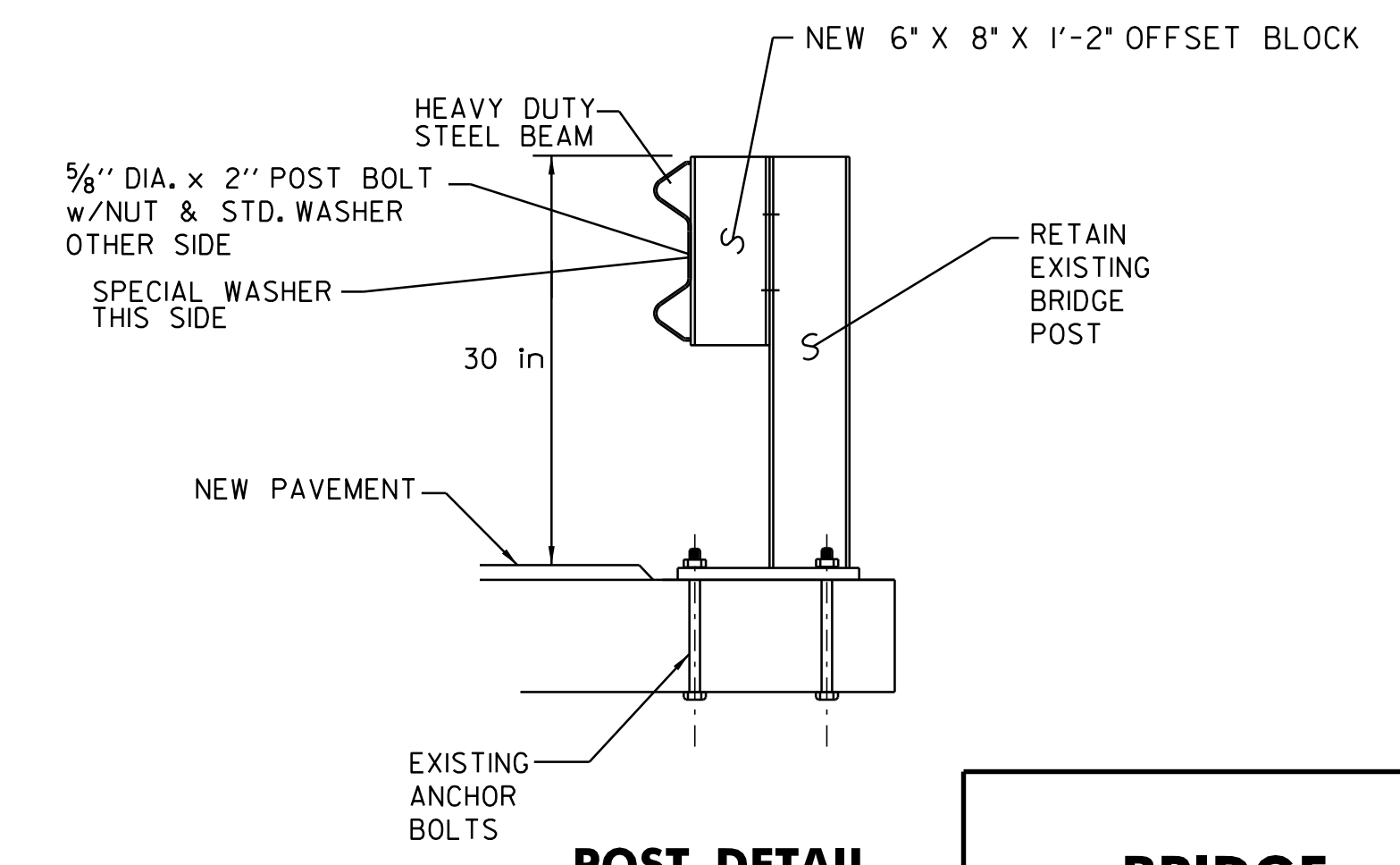
- BRIDGE RAIL SHALL BE HEAVY DUTY STEEL BEAM RAIL.
- BRIDGE APPROACH RAIL HEIGHT SHALL BE TRANSITIONED TO NORMAL ROADWAY RAIL HEIGHT IN 25 FEET.
- APPROACH RAILING SHALL BE HEAVY DUTY STEEL BEAM FOR LENGTHS OF 25 FEET OR 31.25 FEET DETERMINED BY THE SCHEDULE. SEE BRIDGE DETAILS.
- FOR BRIDGE RAILING, THE TRANSITION POST SHALL HAVE AN OFFSET BLOCK AND BE LOCATED AS CLOSE AS PRACTICAL TO THE MID-POINT BETWEEN THE BRIDGE END POST AND APPROACH RAIL POST I.
- SPLICES SHALL LAP IN DIRECTION OF TRAFFIC FLOW.

BRIDGE APPROACH RAILING

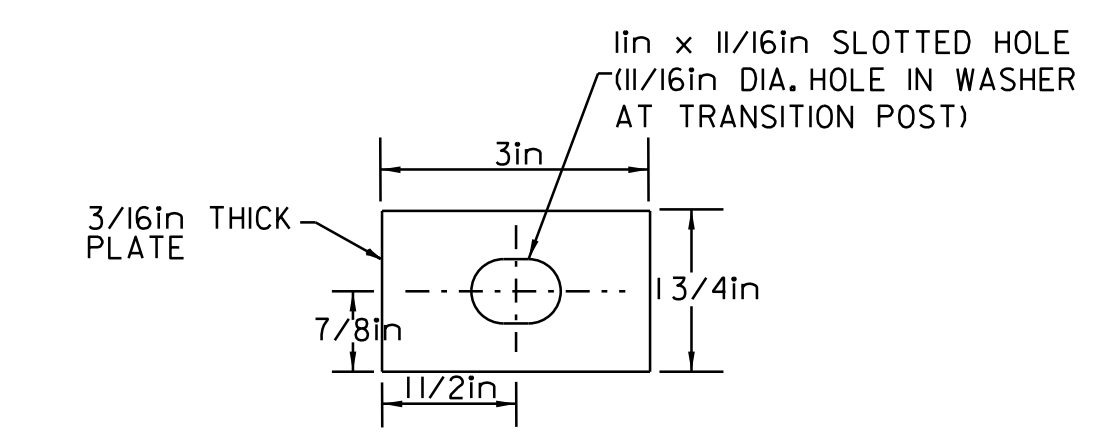
WHEN A RAIL PANEL SPLICE OCCURS AT POST NO. 1 USE SCHEDULE I FOR APPROACH RAILING WHEN A RAIL PANEL SPLICE OCCURS AT BRIDGE END POST USE SCHEDULE II FOR APPROACH RAILING.

SCHEDULE I		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1/2"	1.4 x 12.5 ft
2	3'-1/2"	
3	3'-1/2"	
4	3'-1/2"	
5	4'-2"	1.2 x 12.5 ft
6	4'-2"	
7	4'-2"	
8	4'-2"	1.0 (TYP.)
9	6'-3" (TYP.)	

SCHEDULE II		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1/2"	1.4 x 18.75 ft
2	3'-1/2"	
3	3'-1/2"	
4	3'-1/2"	
5	3'-1/2"	
6	3'-1/2"	
7	4'-2"	1.2 x 12.5 ft
8	4'-2"	
9	4'-2"	
10	4'-2"	1.0 (TYP.)
11	6'-3" (TYP.)	



POST DETAIL



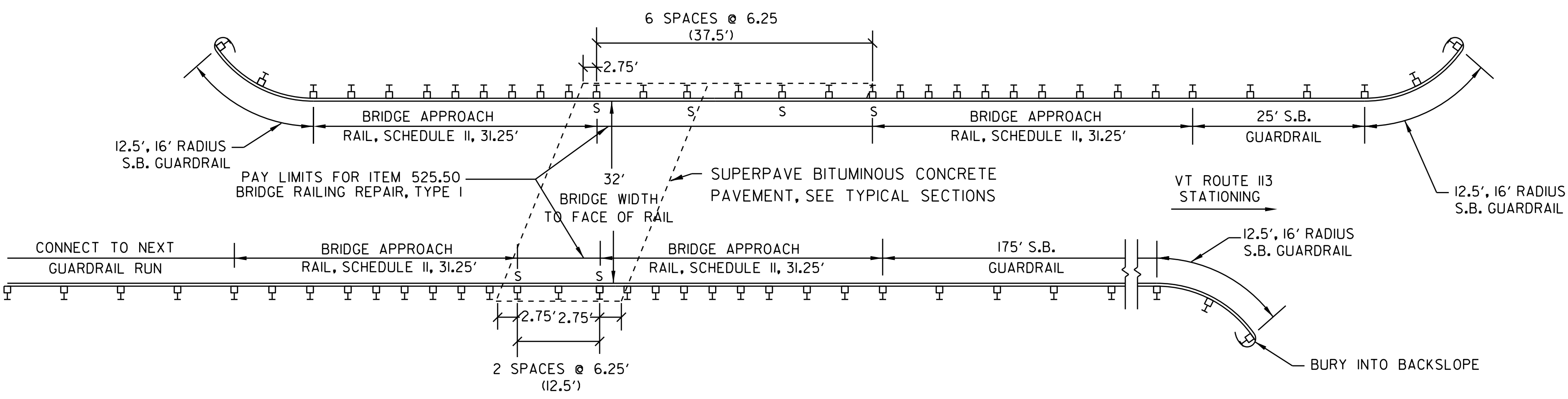
SPECIAL WASHER DETAIL

(MATERIAL SHALL MEET AASHTO M270M/M270 GRADE 345W)

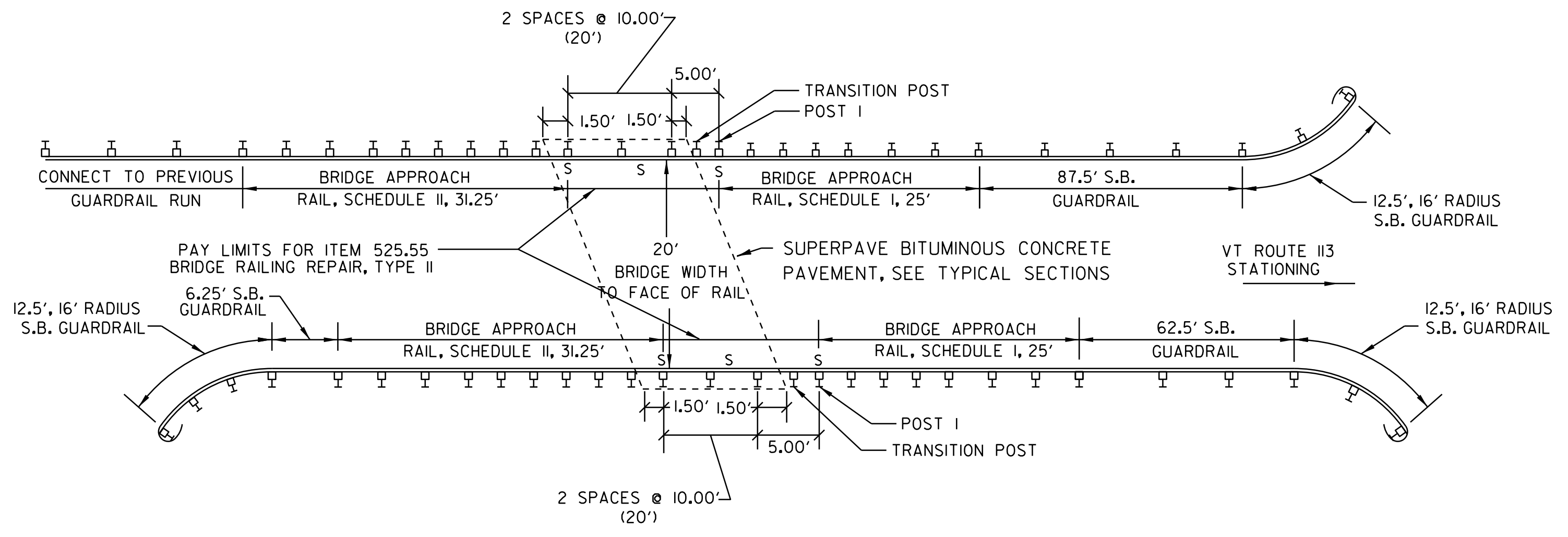
DETAILS ARE NOT TO SCALE

BRIDGE DETAIL SHEET 1

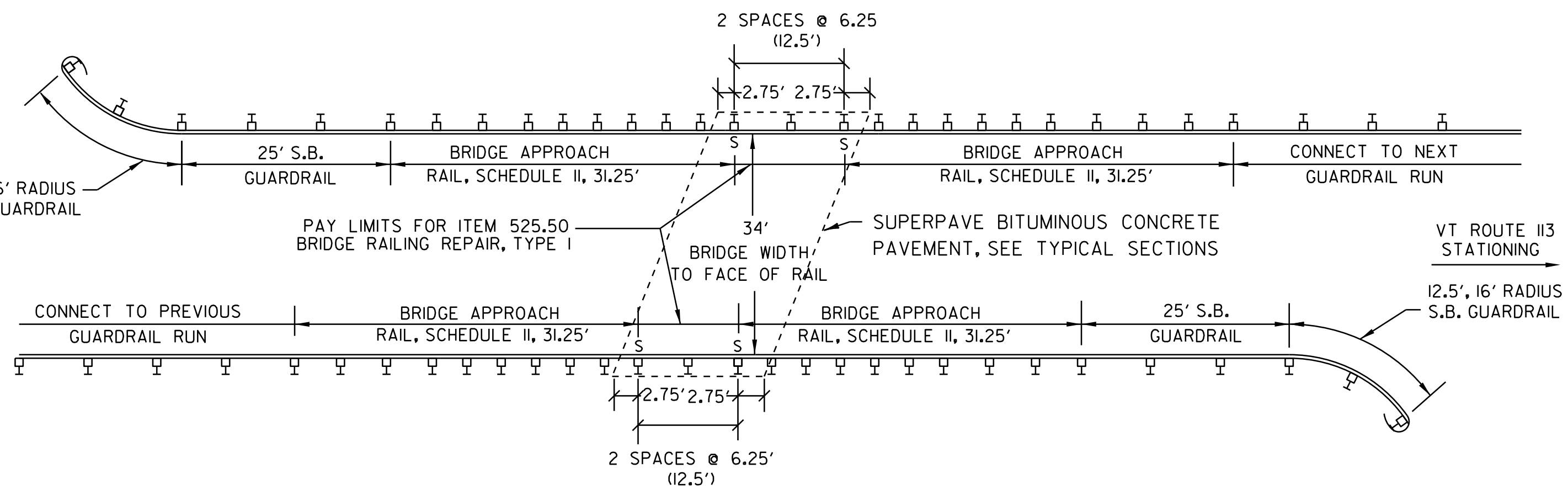
PROJECT NAME: CHELSEA-VERSHIRE	FILE NAME: 01c040.dgn	PLOT DATE: 28-OCT-2011 13:35
PROJECT NUMBER: STP 2331(1)S	PROJECT LEADER: CDL	DRAWN BY: SJL
	DESIGNED BY: RHB	CHECKED BY: EPD
	PLOT FILE: 01c040_07.i	SHEET 7 OF 55



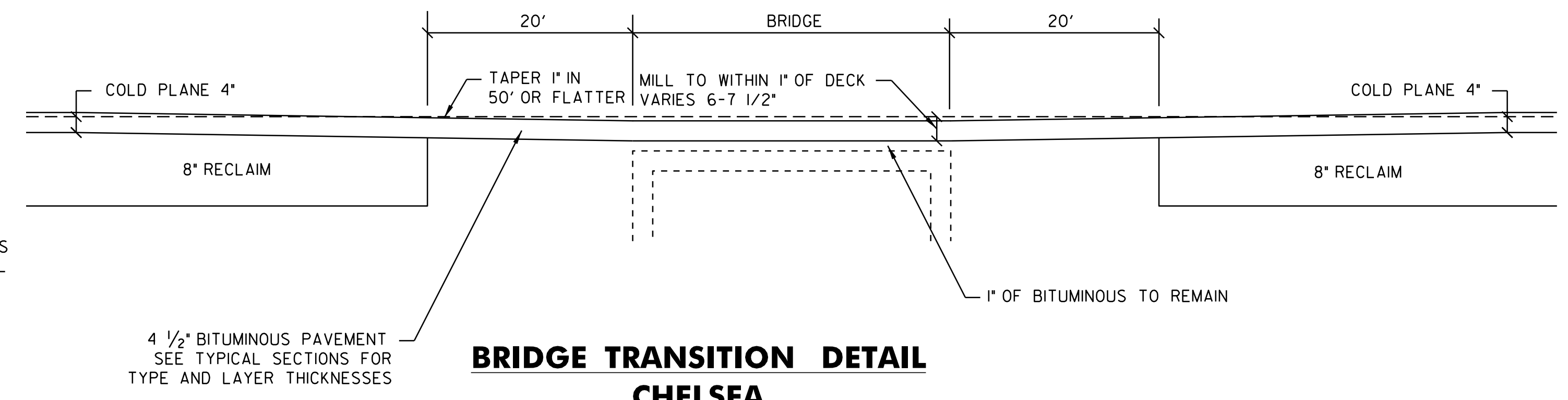
CHELSEA BRIDGE #5
FIELD STA. 68+80 - MM 1.303



CHELSEA BRIDGE #6
FIELD STA. 84+21 - MM 1.595

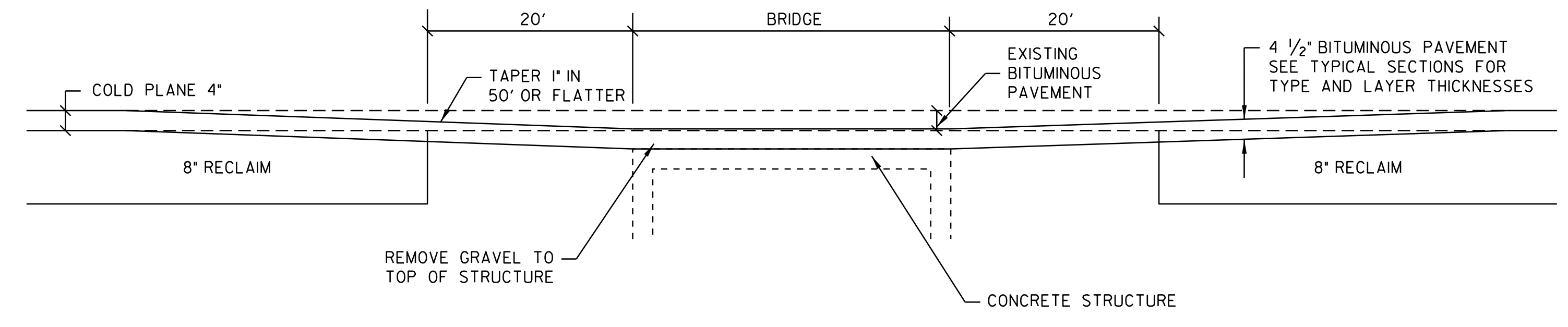


CHELSEA BRIDGE #7
FIELD STA. 108+02 - MM 2.046



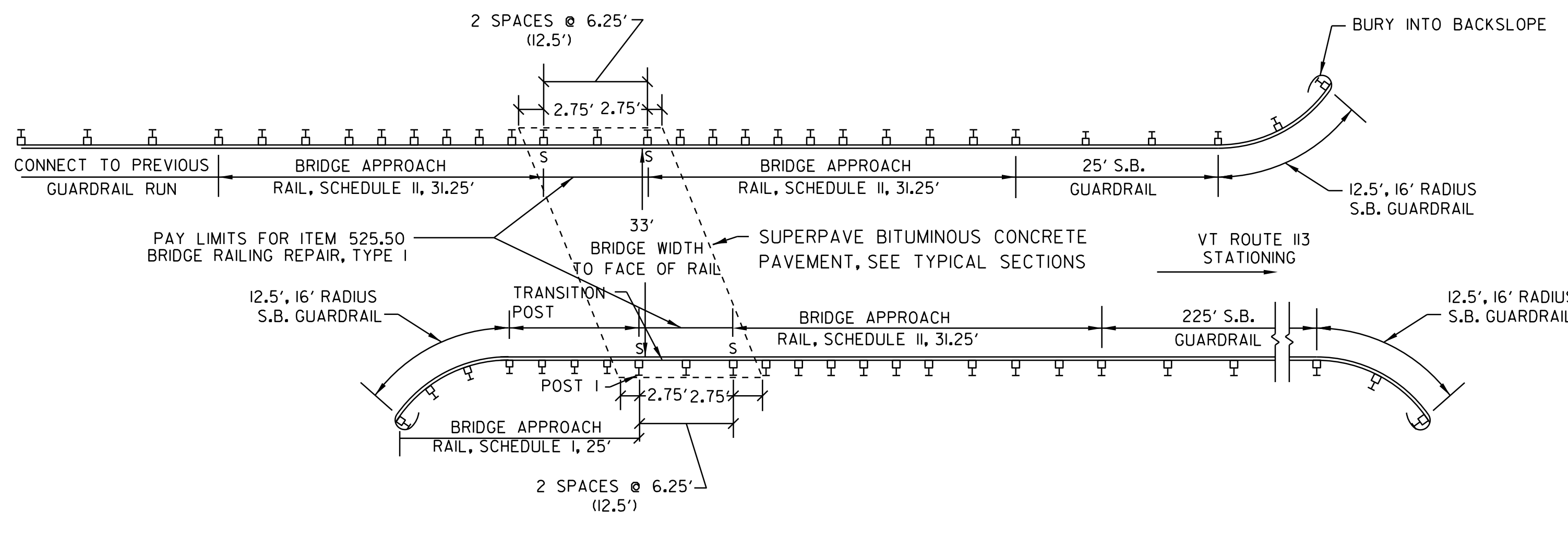
BRIDGE TRANSITION DETAIL
CHELSEA

BRIDGE #1	STA. 17+05.00 = MM 0.323
BRIDGE #2	STA. 48+79.50 = MM 0.924
BRIDGE #4	STA. 62+36.00 = MM 1.181
BRIDGE #5	STA. 68+80.00 = MM 1.303
BRIDGE #6	STA. 84+20.00 = MM 1.595
BRIDGE #7	STA. 108+02.00 = MM 2.046
BRIDGE #8	STA. 115+00.00 = MM 2.178



BRIDGE TRANSITION DETAIL WITH GRAVEL
CHELSEA

BRIDGE #3	STA. 57+06.00 = MM 1.081
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CHELSEA BRIDGE #8
FIELD STA. 115+00 - MM 2.178

DETAILS ARE NOT TO SCALE

BRIDGE DETAIL SHEET 2	PROJECT NAME: CHELSEA - VERSHIRE	PLOT DATE: 28-OCT-2011 13:35
	PROJECT NUMBER: STP 2331(1)S	DRAWN BY: SJL
	FILE NAME: 01c040.dgn	CHECKED BY: EPD
	PROJECT LEADER: CDL	SHEET 8 OF 55
	DESIGNED BY: RHB	
PLOT FILE: 01c040_08.i		

**STATE OF VERMONT
AGENCY OF TRANSPORTATION**

ITEM DETAIL SUMMARY SHEET 1

BEGIN STATION	END STATION	POS.	203.15 COMMON EXCAV.	203.30 EARTH BORROW	204.22 TRENCH EXCAV. OF EARTH, EXP. (N.A.B.I.)	402.12 AGG. SHOULD.	525.10 REMOVAL OF EXIST. RAILING	525.50 BRIDGE RAILING REPAIR, TYPE I	525.55 BRIDGE RAILING REPAIR, TYPE II	604.40 CHANGE ELEV. DI	604.412 REHAB. D. I. CLASS I	604.415 REHAB. D. I. CLASS II	604.418 REHAB. D. I. CLASS III	604.42 CHANGE ELEV. OF SMH	613.10 STONE FILL, TYPE I	616.47 BIT. CONC. GUTTERS AND TRAF. ISLANDS	619.17 YIELDING MARKER POSTS	621.20 STEEL BEAM G. R., GALV	621.205 STEEL BEAM G. R., GALV W/8FT POSTS	621.21 HD STEEL BEAM G. R. GALV.	621.50 MANUF. TERMINAL SECTION, FLARED	621.51 MANUF. TERMINAL SECTION, TANGENT	621.60 ANCHOR FOR STEEL BEAM RAIL	621.80 REMOVE & DISP. OF G. R.	676.10 DEL.INE. WITH STEEL POSTS	676.12 REMOVAL OF EXIST. DELINE.	REMARKS	
CHELSEA																												
0+00	71+90.63	LT&RT	50		1					1	5	4	3	3	2064	12.75	74											ESTIMATED QUANTITIES TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER
8+80.00	9+42.50	RT		25		5												14.5				1	1	65	2	2	INSTALL TANGENT MTS AT STA. 8+80 AND 12'-6' LONG, 16' RADIUS WITH ANCHOR AT STA. 9+30	
9+52.00	11+99.00	RT																154	100				2	254	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 9+52 AND STA. 11+86.5	
13+83.00	17+35.00	RT																266.5	100				2	292	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 13+83 AND STA. 17+22.5	
21+38.50	26+76.00	LT																					2	467	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 21+38.5 AND STA. 26+63.5	
29+07.50	32+07.50	LT																304					2	304	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 29+07.5 AND STA. 31+95	
33+60.00	37+35.00	LT																379					2	304	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 33+60 AND STA. 37+22.5	
48+27.00	49+77.00	LT					25		25									79		50			2	88	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 48+27 AND STA. 49+64.5	
48+44.50	49+94.50	RT					25		25									77		52			2	100	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 48+44.5 AND STA. 49+82	
55+26.00	57+38.50	RT					25		25									139.5		52			2	113	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 55+26 AND STA. 57+26	
56+82.00	63+36.00	LT					50		50									512.5		100			2	216	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 56+82.00 AND STA. 63+23.5 BURIED IN BACKSLOPE	
61+73.50	70+92.25	RT					37.5	12.5	25									772.75		112.5			2	194	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 61+73.5 AND STA. 70+79.75	
68+43.00	69+93.00	LT					37.5	37.5										50		62.5			2	113	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 68+43 AND STA. 69+80.5	
73+62.50	85+49.50	LT					25		25									1110.25		56.25			2	754	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 73+62.5 AND STA. 85+37	
83+71.50	85+46.50	RT					25		25									97.75		56.25			2	154	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 83+71.50 AND STA. 85+34.00	
86+12.00	89+99.50	RT																391.5					2	354	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 86+12 AND STA. 89+87	
91+19.00	93+94.00	RT																279					2	267	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 91+19 AND STA. 93+81.5	
97+01.50	108+68.00	RT					12.5	12.5										1104		62.5			2	373	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 97+01.50 AND STA. 108+55.50	
107+36.00	115+61.00	LT					25	25										677		125			2	528	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 107+36 AND BURIED IN BACKSLOPE AT STA. 115+61	
114+84.75	117+91.00	RT					12.5	12.5										164.5	75	50			2	298	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 114+84.75 AND STA. 117+78.50	
120+63.00	124+50.50	RT																391.5					2	392	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 120+63 AND STA. 124+38	
128+14.00	129+76.50	RT																166.5					2	154	2	2	INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 128+14 AND STA. 129+64	
SUBTOTAL			50	25	1	5	300	100	200	1	5	4	3	3	2064	12.75	74	7130.75	816.5	779	0	1	41	5784	42	42		

ITEM DETAIL SUMMARY SHEET 1	PROJECT NAME: CHELSEA-VERSHIRE
	PROJECT NUMBER: STP 233I(1)S
	FILE NAME: 01c040.dgn
	PLOT FILE: 01c040.I3.I
	PLOT DATE: 28-OCT-2011 13:36
	DRAWN BY: SJL
	CHECKED BY: EPD
	SHEET 13 OF 55

**STATE OF VERMONT
AGENCY OF TRANSPORTATION**

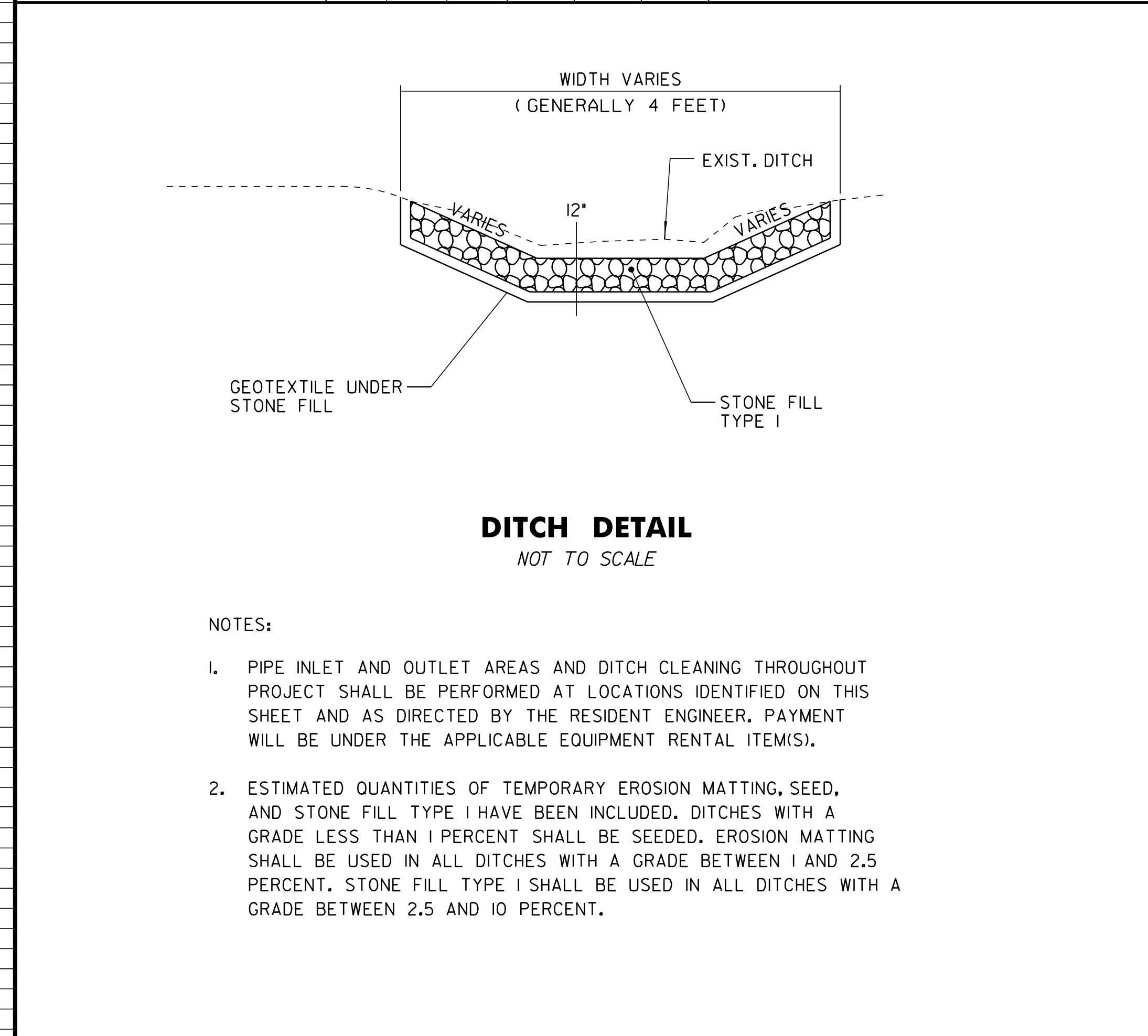
ITEM DETAIL SUMMARY SHEET 2

BEGIN STATION	END STATION	POS.	203.15 COMMON EXCAV.	203.30 EARTH BORROW	204.22 TRENCH EXCAV. OF EARTH, EXP. (N.A.B.I.)	402.12 AGG. SHOULD.	525.10 REMOVAL OF EXIST. RAILING	525.50 BRIDGE RAILING REPAIR TYPE I	525.55 BRIDGE RAILING REPAIR TYPE II	604.40 CHANGE ELEV. D1	604.412 REHAB. D. I. CLASS I	604.415 REHAB. D. I. CLASS II	604.418 REHAB. D. I. CLASS III	604.42 CHANGE ELEV. OF SMH	613.10 STONE FILL, TYPE I	616.47 BIT. CONC. GUTTERS AND TRAF. ISLANDS	619.17 YIELDING MARKER POSTS	621.20 STEEL BEAM G. R., GALV	621.205 STEEL BEAM G. R., GALV W/8FT POSTS	621.21 HD STEEL BEAM G. R., GALV.	621.50 MANUF. TERMINAL SECTION, FLARED	621.51 MANUF. TERMINAL SECTION, TANGENT	621.60 ANCHOR FOR STEEL BEAM RAIL	621.80 REMOVE & DISP. OF G. R.	676.10 DEL INE. WITH STEEL POSTS	676.12 REMOVAL OF EXIST. DEL INE.	REMARKS
			CY	CY	CY	TON	LF	LF	LF	EACH	EACH	EACH	EACH	EACH	CY	TON	EACH	LF	LF	LF	EACH	EACH	EACH	LF	EACH	EACH	
135+60.50	153+75.00	RT		50		10												1739.5			2			275	2	2	INSTALL FLARED MTS AT STA. 135+60.5 AND STA. 153+37.5
158+25.00	161+00.00	RT		50		10												200			2				2		INSTALL FLARED MTS AT STA. 158+25 AND STA. 160+62.5
164+74.00	168+36.50	RT		50		10												287.5			2			88	2	2	INSTALL FLARED MTS AT STA. 164+74 AND STA. 167+99
166+65.00	169+27.50	LT		50		10												187.5			2				2		INSTALL FLARED MTS AT STA. 166+65 AND STA. 168+90
VERSHIRE																	72										
0+00	84+69.03	LT&RT	50								1	1															ESTIMATED QUANTITIES TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER
96+76.00	103+26.00	RT		50		10												575			2			650	2	2	INSTALL FLARED MTS AT STA. 96+76 AND STA. 102+88.5
109+42.50	114+05.00	RT		50		10												387.5			2			363	2	2	INSTALL FLARED MTS AT STA. 109+42.5 AND STA. 113+67.5
110+89.00	112+14.00	LT		50		10												50			2			125	2	2	INSTALL FLARED MTS AT STA. 110+89 AND STA. 111+76.5
116+90.00	131+14.50	RT		25		10												1389			1		1	1015	2	2	INSTALL FLARED MTS AT STA. 116+90 AND STA. 130+77
133+15.00	135+27.50	RT		25		10												175			1		1	215	2	2	INSTALL FLARED MTS AT STA. 133+15 AND STA. 134+90
136+07.50	146+20.00	RT		50		10												937.5			2			1013	2	2	INSTALL FLARED MTS AT STA. 136+07.5 AND 12' -6' LONG, 16' RADIUS WITH ANCHOR AT STA. 167+39.5
146+67.00	155+04.50	RT		50		10													762.5		2			838	2	2	INSTALL FLARED MTS AT STA. 146+67 AND STA. 154+67
161+89.50	167+52.00	RT		25		5												527			1		1	375	2	2	INSTALL FLARED MTS AT STA. 161+89.5 AND 12' -6' LONG, 16' RADIUS WITH ANCHOR AT STA. 167+39.5
164+51.00	166+63.50	LT		25		5												177			1		1	213	2	2	INSTALL FLARED MTS AT STA. 164+51 AND 12' -6' LONG, 16' RADIUS WITH ANCHOR AT STA. 166+51
168+13.00	171+00.50	LT																291.5					2	175	2	2	INSTALL 12' -6' LONG, 16' RADIUS WITH ANCHOR AT STA. 168+13 AND STA. 170+88
171+37.50	181+50.00	RT																					2	975	2	2	INSTALL 12' -6' LONG, 16' RADIUS WITH ANCHOR AT STA. 171+37 AND STA. 181+37.5
174+77.00	177+27.00	LT																254					2	250	2	2	INSTALL 12' -6' LONG, 16' RADIUS WITH ANCHOR AT STA. 174+77 AND STA. 177+14.5
182+22.00	184+44.00	RT																252					3	252	1	1	INSTALL 12' -6' LONG, 16' RADIUS WITH ANCHOR AT STA. 182+26, STA. 182+44 AND STA. 184+31.5
SUBTOTAL, THIS SHEET			50	550	-	120	-	-	-	-	1	1	-	-	0	72	7430	1779	0	22	0	13	6822	33	29		
SUBTOTAL, SHEET 13			50	25	1	5	300	100	200	1	5	4	3	3	2064	12.75	74	7130.75	816.5	779.0	0	1	41	5784	42	42	
ROUNDING			-	-	-	-	-	-	-	-	-	-	-	36	0.25	-	14.25	-	-	-	-	-	-	-	-	-	
GRAND TOTAL			100	575	1	125	300	100	200	1	5	5	4	3	2100	13	146	14575	2595.5	779.0	22	1	54	12606	75	71	

ITEM DETAIL SUMMARY SHEET 2	PROJECT NAME: CHLSEA-VERSHIRE
	PROJECT NUMBER: STP 233I(1)S
	FILE NAME: 01c040.dgn
	PLOT DATE: 28-OCT-2011 13:36
	PROJECT LEADER: CDL
	DRAWN BY: SJL
	DESIGNED BY: RHB
	CHECKED BY: EPD
	PLOT FILE: 01c040_14.i
	SHEET 14 OF 55

LOCATION				FEET OF DITCHING			MISC. ITEMS			REMARKS	
SITE	STATION	STATION	POS.	PERCENT GRADE			653.20	613.10	649.31		
				0-1	1-2.5	2.5-10	TEMP. EROS. MATT.	STONE FILL TYP. I	GEOT. UNDER STONE FILL		
							SY	CY	SY		
VT 113 CHELSEA											
1	22+10	22+25	RT			15		2	7		
2	22+50	26+60	RT			410		61	182		
3	27+65	42+00	RT			1435		213	638	EXTEND DITCHING 50' UP DENSMORE RD	
4	44+10	44+60	RT			50		22			
5	44+90	48+20	RT			330		147			
6	50+95	56+60	LT			565		84	251		
7	57+40	60+35	RT			295		44	131		
8	63+10	66+35	LT			325		144			
9	66+85	68+45	LT			160		24	71		
10	68+90	73+00	RT			410		61	182		
11	73+00	73+65	LT			65		29			
12	77+30	77+60	RT			30		4	13		
13	78+35	83+15	RT			480		71	213		
14	87+25	96+60	LT			935		416			
15	97+20	101+75	LT			455		202			
16	103+75	107+40	LT			365		54	162		
17	108+60	111+55	RT			295		44	131		
18	111+75	114+50	RT			275		41	122		
19	115+15	118+25	LT			310		138			
20	119+10	119+30	LT			20		9			
21	121+70	127+50	LT			580		86	258		
22	127+75	136+90	LT			915		136	407		
23	137+75	140+90	LT			315		140			
24	141+50	146+95	LT			545		81	242		
25	152+65	155+50	LT			285		127			
26	159+20	161+75	LT			255		38	113		
27	162+05	162+25	LT			20		9			
28	163+95	164+55	LT			60		27			
29	167+45	168+75	LT			130		19	58		
30	168+75	171+10	LT			235		104			
31	171+40	171+50	LT			10		4			
32	172+40	174+00	LT			160		71			
33	174+80	175+55	RT			75		33			
34	175+75	177+50	RT			175		26	78		
CHELSEA SUBTOTAL						3650	7335	1622	1089	3259	
VT 113 VERSHIRE											
35	3+85	4+85	LT			100		44			
36	5+50	6+25	LT			75		33			
37	6+50	7+60	LT			110		49			
38	7+60	8+30	LT			70		10	31		
39	11+75	14+90	LT			315		47	140		
40	18+75	21+90	LT			315		47	140		

LOCATION				FEET OF DITCHING			MISC. ITEMS			REMARKS	
SITE	STATION	STATION	POS.	PERCENT GRADE			653.20	613.10	649.31		
				0-1	1-2.5	2.5-10	TEMP. EROS. MATT.	STONE FILL TYP. I	GEOT. UNDER STONE FILL		
							SY	CY	SY		
78	169+85	173+20	LT			335		51	50	149	
79	177+25	179+55	LT			230		102			
80	179+95	182+55	LT			260		39	116		
81	187+05	187+55	LT			50		22			
VERSHIRE SUBTOTAL						8475	6240	3766	925	2772	
CHELSEA SUBTOTAL						3650	7335	1622	1089	3259	
								1825			GUARDRAIL ENDS/MISC DISTURBED AREAS EROSION AREAS
ROUNDING								137	36	119	
TOTAL						12125	13575	7350	2065	6150	



629.20 ADJUST ELEVATION OF VALVE BOX

STA. 0+48 RT
STA. 4+04 LT
STA. 4+87 LT

604.412, 604.415, 604.418 REHAB, DROP
INLETS, CATCH BASINS, OR MANHOLES, CLASS I,
CLASS II OR CLASS III

STA. 0+91 RT "TYPE D GRATE"
STA. 4+73 LT "TYPE D GRATE"
STA. 6+93 RT "TYPE D GRATE"
STA. 7+24 LT "TYPE D GRATE"
STA. 8+03 RT "TYPE D GRATE"
STA. 8+25 LT "TYPE D GRATE"
STA. 10+41 LT "TYPE B GRATE"
STA. 13+02 LT "TYPE B GRATE"

604.42 CHANGE ELEVATION OF
SEWER MANHOLES

STA. 2+66 RT
STA. 5+43 RT
STA. 8+24 RT

616.47 BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS

STA. 5+10 - STA. 8+50 LT

621.51 MANUFACTURED TERMINAL SECTION, TANGENT

~~STA. 8+80.00 - STA. 9+30.00 RT NOT USED~~

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

STA. 8+80.00 - STA. 9+42.50 RT
STA. 9+52.00 - STA. 12+02.00 RT
STA. 13+83.00 - STA. 15+75.00 RT

~~8+80.00 RT~~

621.20 STEEL BEAM GUARDRAIL, GALVANIZED

STA. 9+30.00 - STA. 9+42.50 RT
STA. 9+52.00 - STA. 10+21.00 RT
~~STA. 11+21.00 - STA. 11+99.00 RT~~
STA. 15+33.50 - STA. 15+75.00 RT

621.60 ANCHOR FOR STEEL BEAM RAIL

STA. 9+30.00 RT STA. 11+86.50 RT
STA. 9+52.00 RT STA. 13+83.00 RT
~~STA. 8+80.00 RT STA. 15+10.00 RT~~

676.10 DELINEATOR WITH STEEL POST

STA. 8+80.00 RT STA. 11+99.00 RT
STA. 9+42.50 RT STA. 13+83.00 RT
STA. 9+52.00 RT

675.50 REMOVING SIGNS

AS SHOWN - 18

675.60 ERECTING SALVAGED SIGNS

AS SHOWN - 14

Curve # 3
Delta= 20°51'40.91"
D= 14°19'26.20"
T= 73.64'
L=145.64'
R= 400.00'
BANKING 0.080 FT/FT
170' RUNOFF (40 MPH)
SEE SUPERELEVATION SHEET 1
FOR BANKING DETAIL

621.205 STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS

~~STA. 10+21.00 - STA. 11+21.00 RT NOT USED~~

STA. 13+83.00 - STA. 15+33.50 RT

Curve # 2

Delta= 41°53'27.40"

D= 16°22'12.80"

T= 133.97'

L=255.90'

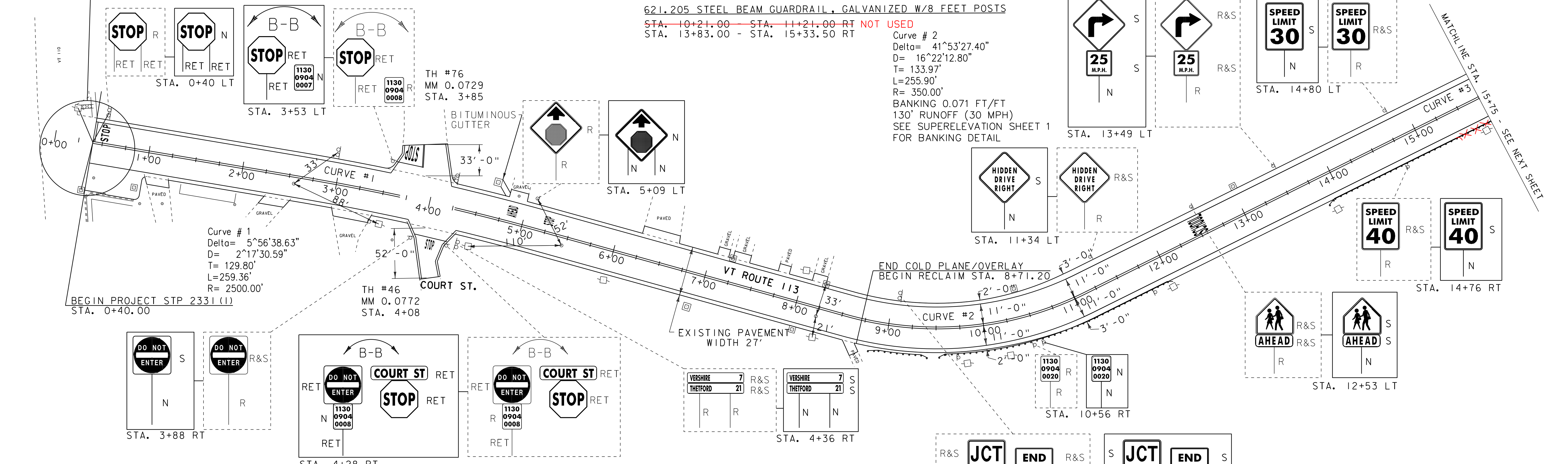
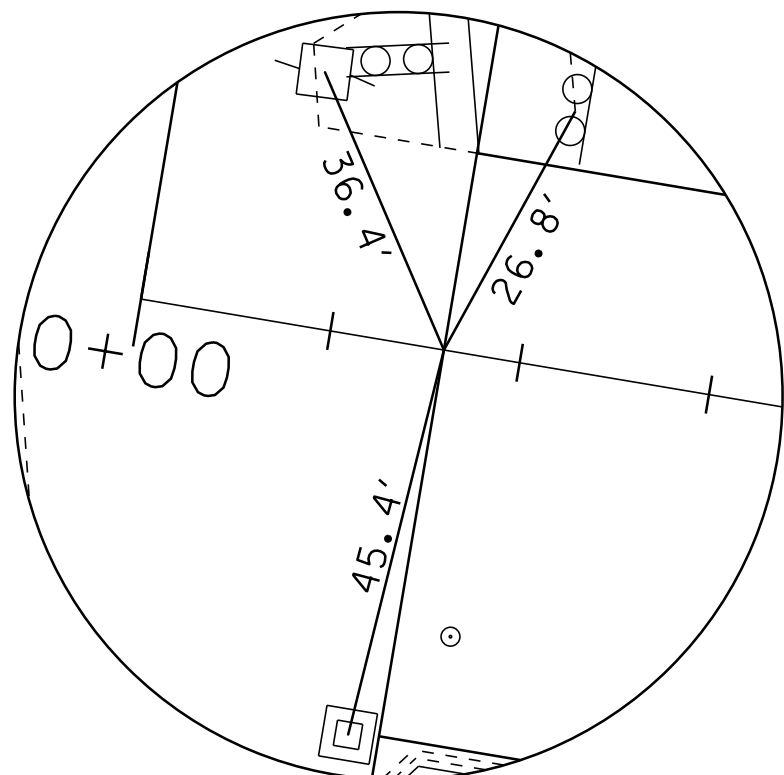
R= 350.00'

BANKING 0.071 FT/FT

130' RUNOFF (30 MPH)

SEE SUPERELEVATION SHEET 1

FOR BANKING DETAIL



Curve # 1
Delta= 5°56'38.63"
D= 2°17'30.59"
T= 129.80'
L=259.36'
R= 2500.00'

END COLD PLANE/OVERLAY
BEGIN RECLAIM STA. 8+71.20

EXISTING PAVEMENT
WIDTH 27'

MATCH LINE STA. 15+75 - SEE NEXT SHEET

SIGN LEGEND
R = REMOVE
R&S = REMOVE AND SALVAGE
S = SALVAGE
RET = RETAIN
N = NEW
B-B = BACK TO BACK

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
STA. 0+40 - STA. 15+75 (SOLID LT & RT)
(WITH EDGELINE BREAKS AT TOWN ROADS)
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
STA. 0+40 - STA. 15+75 (SOLID LT & RT)
(WITH CENTERLINE BREAKS AT TOWN ROADS)
STA. 3+85 LT (CENTERLINE TH#76)
646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
STA. 0+40 - STA. 15+75 (SOLID LT & RT)
(WITH EDGELINE BREAKS AT TOWN ROADS)
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
STA. 0+40 - STA. 15+75 (SOLID LT & RT)
(WITH CENTERLINE BREAKS AT TOWN ROADS)
STA. 3+85 LT (CENTERLINE TH#76)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC
STA. 0+43 LT
STA. 3+57 - STA. 3+85 LT
STA. 3+95 - STA. 4+27 RT
646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
STA. 0+52 LT "STOP" STA. 4+87 LT "AHEAD"
STA. 3+71 LT "STOP" STA. 5+27 LT "STOP"
STA. 4+10 RT "STOP" STA. 12+53 LT "SCHOOL"
646.682 TEMPORARY 24 INCH STOP BAR, PAINT
STA. 0+43 LT
STA. 3+57 - STA. 3+85 LT
STA. 3+95 - STA. 4+27 RT
646.692 TEMPORARY LETTER OR SYMBOL, PAINT
STA. 0+52 LT "STOP" STA. 4+87 LT "AHEAD"
STA. 3+71 LT "STOP" STA. 5+27 LT "STOP"
STA. 4+10 RT "STOP" STA. 12+53 LT "SCHOOL"

LEGEND
□ UTILITY POLE
○ HYDRANT
⊗ CATCH BASIN / DROP INLET

NOT TO SCALE

PROJECT LAYOUT SHEET 1

PROJECT NAME: CHelsea-VERSHIRE
PROJECT NUMBER: STP 2331(1)S
FILE NAME: 01c040.dgn
PROJECT LEADER: CDL
DESIGNED BY: BMB
PLOT FILE: 01c040_16.1
PLOT DATE: 28-OCT-2011 13:36
DRAWN BY: BMB
CHECKED BY: EPD
SHEET 16 OF 55

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

STA. 15+75.00 - STA. 15+85.00 RT
STA. 16+59.00 - STA. 17+34.00 RT
STA. 16+88.00 - STA. 17+63.00 LT
STA. 21+51.00 - STA. 26+13.50 LT
STA. 29+07.50 - STA. 31+75.00 LT

621.20 STEEL BEAM GUARDRAIL, GALVANIZED

~~STA. 15+75.00 - STA. 17+34.00 RT~~
~~STA. 16+88.00 - STA. 17+63.00 LT~~
~~STA. 29+07.50 - STA. 31+75.00 LT~~
~~STA. 16+44.00 - STA. 17+35.00 RT~~
~~STA. 16+88.00 - STA. 18+00.00 LT~~

621.205 STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS

~~STA. 21+38.50 - STA. 26+76.00 LT~~
~~STA. 29+47.00 - STA. 31+75.00 LT~~
~~STA. 21+51.00 - STA. 26+17.00 LT~~

621.60 ANCHOR FOR STEEL BEAM RAIL

~~STA. 16+88.00 LT~~
~~STA. 17+21.50 RT~~
~~STA. 17+51.50 LT~~
~~STA. 21+38.50 LT~~
~~STA. 26+63.50 LT~~
~~STA. 29+07.50 LT~~
~~STA. 21+51.00 LT~~
~~STA. 26+05.00 LT~~
~~STA. 16+43.50 RT~~

676.10 DELINEATOR WITH STEEL POST

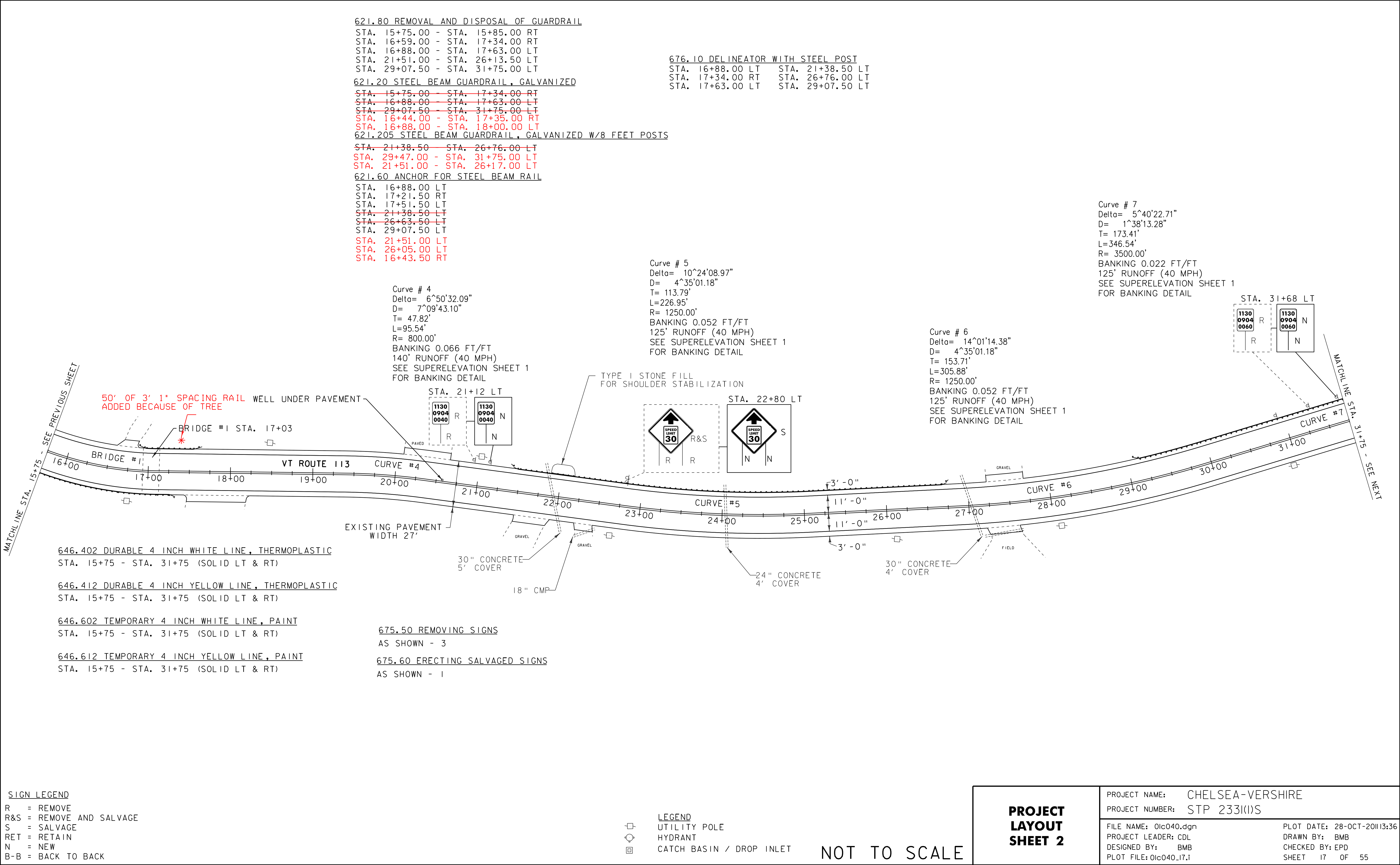
STA. 16+88.00 LT STA. 21+38.50 LT
STA. 17+34.00 RT STA. 26+76.00 LT
STA. 17+63.00 LT STA. 29+07.50 LT

Curve # 4
Delta= 6°50'32.09"
D= 7°09'43.10"
T= 47.82'
L=95.54'
R= 800.00'
BANKING 0.066 FT/FT
140' RUNOFF (40 MPH)
SEE SUPERELEVATION SHEET 1
FOR BANKING DETAIL

Curve # 5
Delta= 10°24'08.97"
D= 4°35'01.18"
T= 113.79'
L=226.95'
R= 1250.00'
BANKING 0.052 FT/FT
125' RUNOFF (40 MPH)
SEE SUPERELEVATION SHEET 1
FOR BANKING DETAIL

Curve # 7
Delta= 5°40'22.71"
D= 1°38'13.28"
T= 173.41'
L=346.54'
R= 3500.00'
BANKING 0.022 FT/FT
125' RUNOFF (40 MPH)
SEE SUPERELEVATION SHEET 1
FOR BANKING DETAIL

Curve # 6
Delta= 14°01'14.38"
D= 4°35'01.18"
T= 153.71'
L=305.88'
R= 1250.00'
BANKING 0.052 FT/FT
125' RUNOFF (40 MPH)
SEE SUPERELEVATION SHEET 1
FOR BANKING DETAIL



646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
STA. 15+75 - STA. 31+75 (SOLID LT & RT)

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
STA. 15+75 - STA. 31+75 (SOLID LT & RT)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
STA. 15+75 - STA. 31+75 (SOLID LT & RT)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
STA. 15+75 - STA. 31+75 (SOLID LT & RT)

675.50 REMOVING SIGNS
AS SHOWN - 3

675.60 ERECTING SALVAGED SIGNS
AS SHOWN - 1

SIGN LEGEND

R = REMOVE
R&S = REMOVE AND SALVAGE
S = SALVAGE
RET = RETAIN
N = NEW
B-B = BACK TO BACK

LEGEND

□ UTILITY POLE
○ HYDRANT
□ CATCH BASIN / DROP INLET

PROJECT LAYOUT SHEET 2

PROJECT NAME: CHELSEA-VERSHIRE
PROJECT NUMBER: STP 2331(1)S

FILE NAME: 01c040.dgn
PROJECT LEADER: CDL
DESIGNED BY: BMB
PLOT FILE: 01c040_17.1

PLOT DATE: 28-OCT-2011 13:36
DRAWN BY: BMB
CHECKED BY: EPD
SHEET 17 OF 55

NOT TO SCALE

604.412, 604.415, 604.418 REHAB, DROP
INLETS, CATCH BASINS, OR MANHOLES, CLASS I,
CLASS II OR CLASS III
STA. 36+55 RT "TYPE D GRATE"

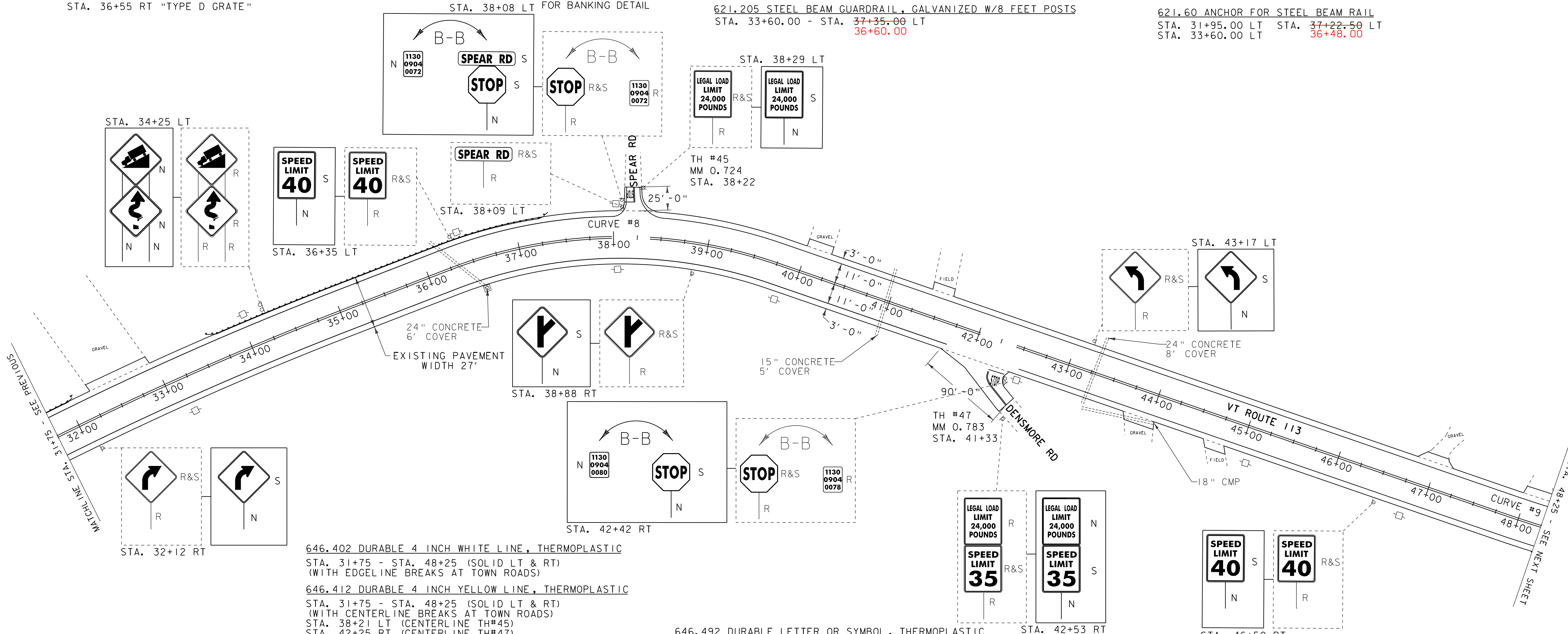
Curve # 8
Delta= 40°11'29.45"
D= 11°27'32.96"
T= 182.93'
L=350.74'
R= 500.00'
BANKING 0.078 FT/FT
170' RUNOFF (40 MPH)
SEE SUPERELEVATION SHEET 1
FOR BANKING DETAIL

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
STA. 31+75.00 - STA. 32+07.50 LT
STA. 33+60.00 - STA. 36+60.00 LT
621.20 STEEL BEAM GUARDRAIL, GALVANIZED
STA. 31+75.00 - STA. 32+07.50 LT

676.10 DELINEATOR WITH STEEL POST
STA. 32+07.50 LT STA. 37+35.00 LT
STA. 33+60.00 LT

621.205 STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS
STA. 33+60.00 - STA. ~~37+35.00~~ LT
~~36+60.00~~

621.60 ANCHOR FOR STEEL BEAM RAIL
STA. 31+95.00 LT STA. ~~37+22.50~~ LT
STA. 33+60.00 LT ~~36+48.00~~



646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
STA. 31+75 - STA. 48+25 (SOLID LT & RT)
(WITH EDGELINE BREAKS AT TOWN ROADS)

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
STA. 31+75 - STA. 48+25 (SOLID LT & RT)
(WITH CENTERLINE BREAKS AT TOWN ROADS)
STA. 38+21 LT (CENTERLINE TH#45)
STA. 42+25 RT (CENTERLINE TH#47)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
STA. 31+75 - STA. 48+25 (SOLID LT & RT)
(WITH EDGELINE BREAKS AT TOWN ROADS)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
STA. 31+75 - STA. 48+25 (SOLID LT & RT)
(WITH CENTERLINE BREAKS AT TOWN ROADS)
STA. 38+21 LT (CENTERLINE TH#45)
STA. 42+25 RT (CENTERLINE TH#47)

646.482 DURABLE 24 INCH STOPBAR, THERMOPLASTIC
STA. 38+11 - STA. 38+21 LT
STA. 42+25 - STA. 42+39 RT

646.682 TEMPORARY 24 INCH STOPBAR, PAINT
STA. 38+11 - STA. 38+21 LT
STA. 42+25 - STA. 42+39 RT

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
STA. 38+15 LT "STOP"
STA. 42+40 RT "STOP"

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
STA. 38+15 LT "STOP"
STA. 42+40 RT "STOP"

675.50 REMOVING SIGNS
AS SHOWN - 15

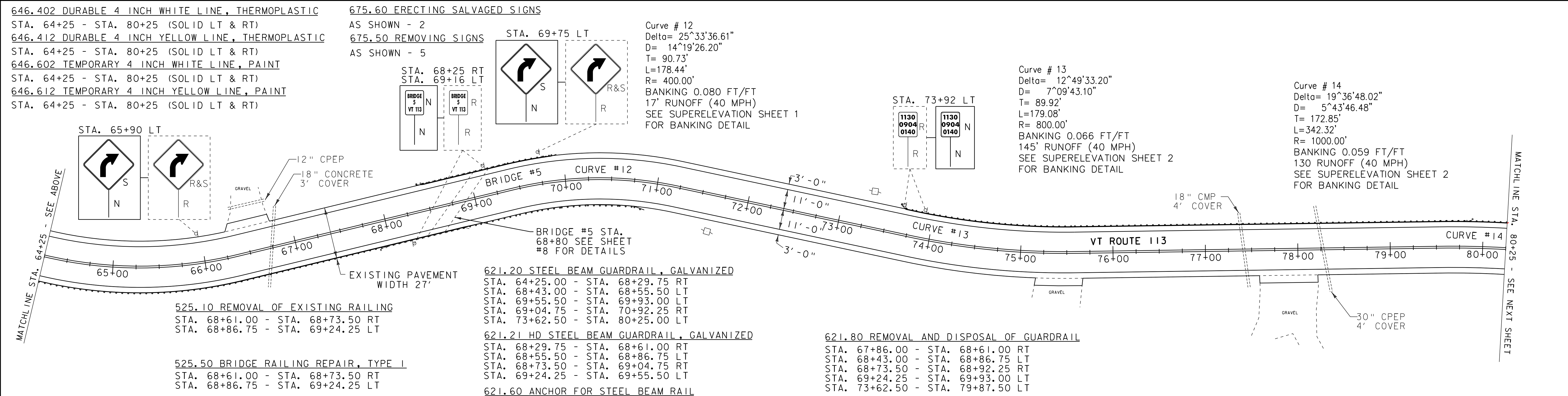
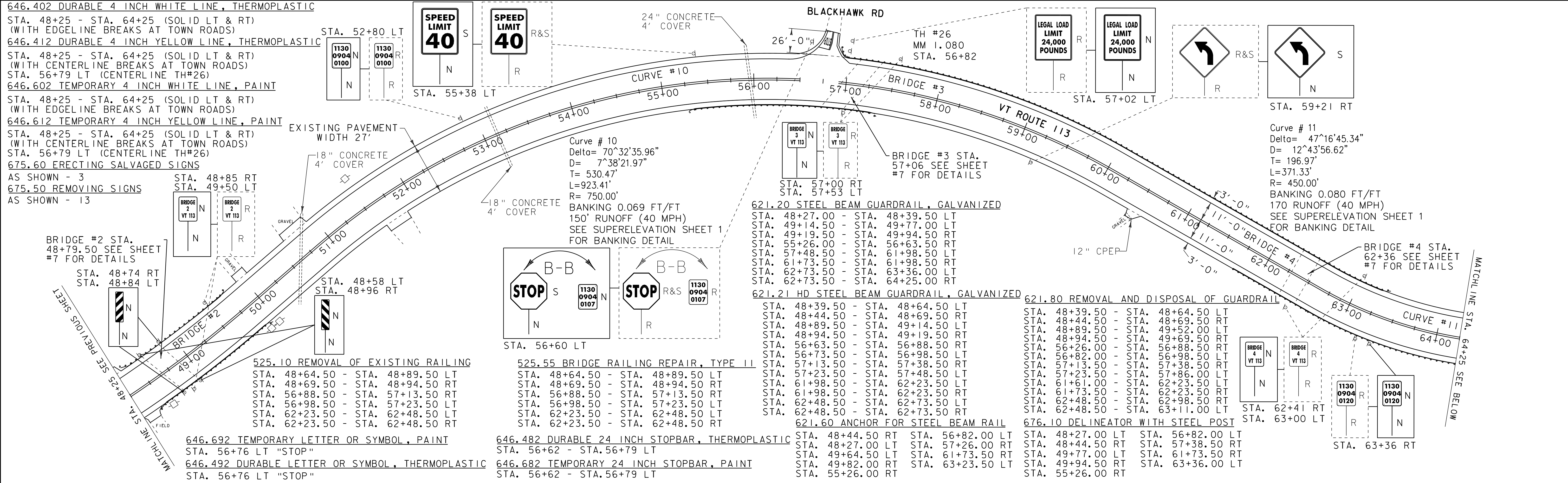
675.60 ERECTING SALVAGED SIGNS
AS SHOWN - 10

SIGN LEGEND
R = REMOVE
R&S = REMOVE AND SALVAGE
S = SALVAGE
RET = RETAIN
N = NEW
B-B = BACK TO BACK

LEGEND
□ = UTILITY POLE
○ = HYDRANT
⊗ = CATCH BASIN / DROP INLET

NOT TO SCALE

PROJECT LAYOUT SHEET 3	PROJECT NAME: CHLSEA-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
	PROJECT NUMBER: STP 2331(1)S	DRAWN BY: BMB
	FILE NAME: 01c040.dgn	DESIGNED BY: BMB
	PROJECT LEADER: CDL	CHECKED BY: EPD
	PLOT FILE: 01c040_18.1	SHEET 18 OF 55

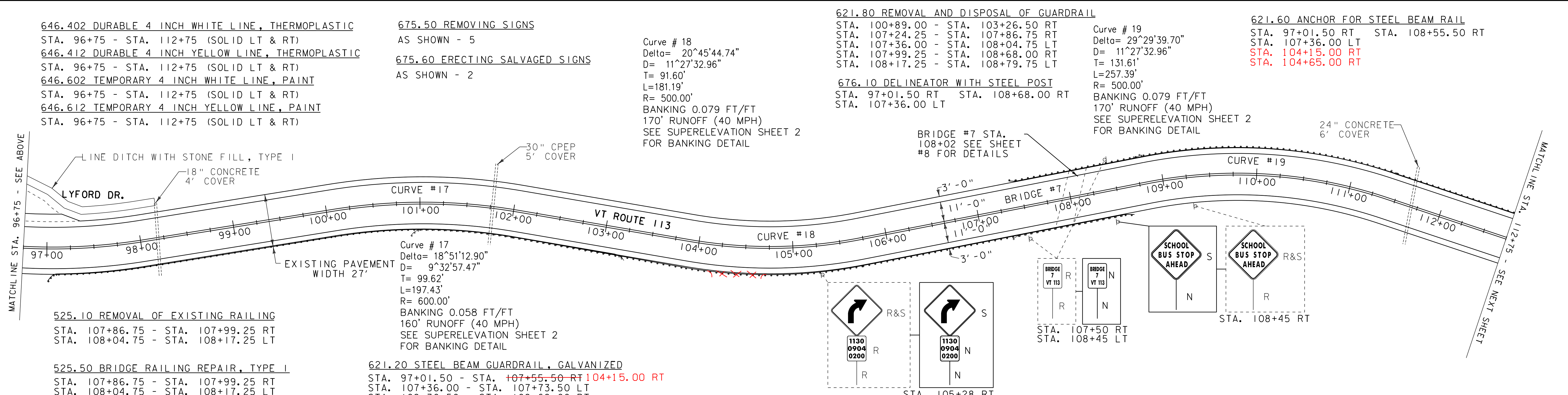
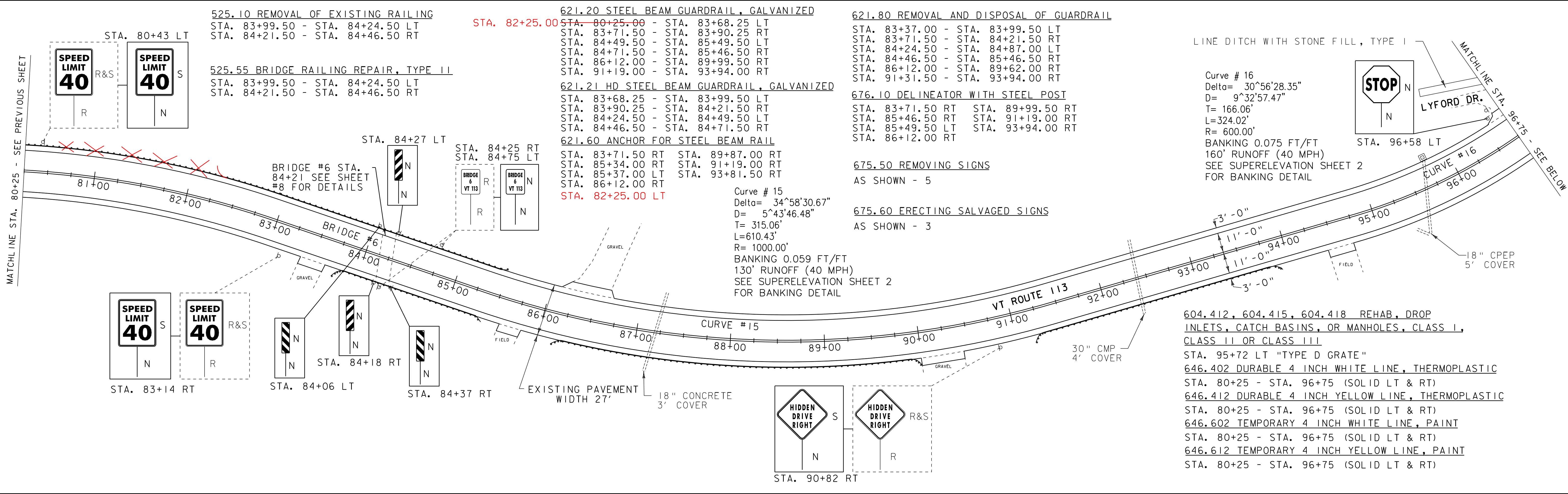


SIGN LEGEND
 R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

LEGEND
 ○ = UTILITY POLE
 ⊙ = HYDRANT
 □ = CATCH BASIN / DROP INLET

NOT TO SCALE

PROJECT LAYOUT SHEET 4	PROJECT NAME: CHLSEA-VERSHIRE	FILE NAME: 01c040.dgn	PLOT DATE: 28-OCT-2011 3:36
	PROJECT NUMBER: STP 2331(1)S	PROJECT LEADER: CDL	DRAWN BY: BMB
		DESIGNED BY: BMB	CHECKED BY: EPD
		PLOT FILE: 01c040_19.i	SHEET 19 OF 55



SIGN LEGEND
 R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

LEGEND
 □ = UTILITY POLE
 ○ = HYDRANT
 □ = CATCH BASIN / DROP INLET

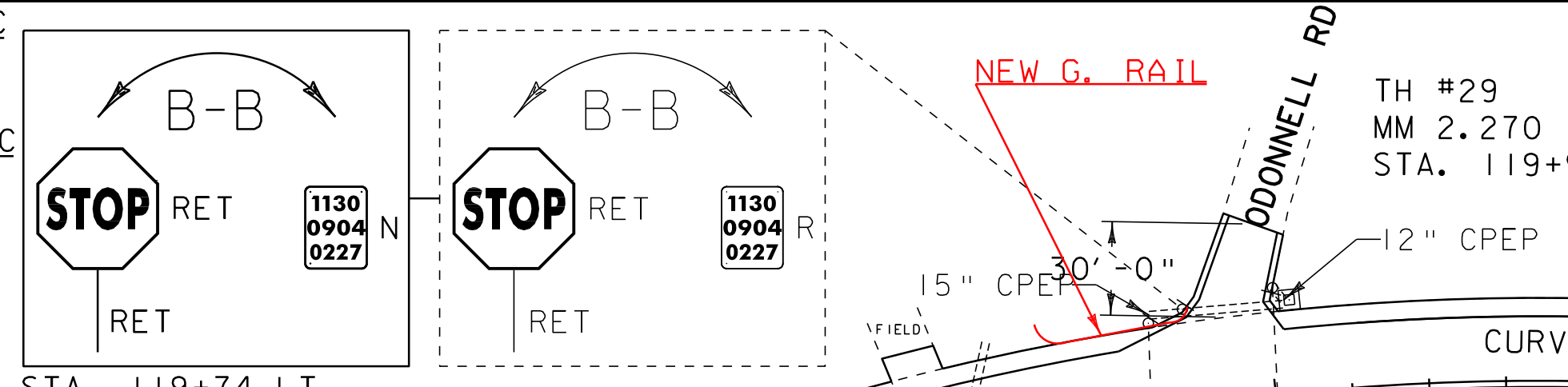
PROJECT LAYOUT SHEET 5

PROJECT NAME: CHelsea-VERSHIRE
 PROJECT NUMBER: STP 2331(1)S
 FILE NAME: 01c040.dgn
 PROJECT LEADER: CDL
 DESIGNED BY: BMB
 PLOT FILE: 01c040_20.i

PLOT DATE: 28-OCT-2011 13:36
 DRAWN BY: BMB
 CHECKED BY: BMB
 SHEET 20 OF 55

NOT TO SCALE

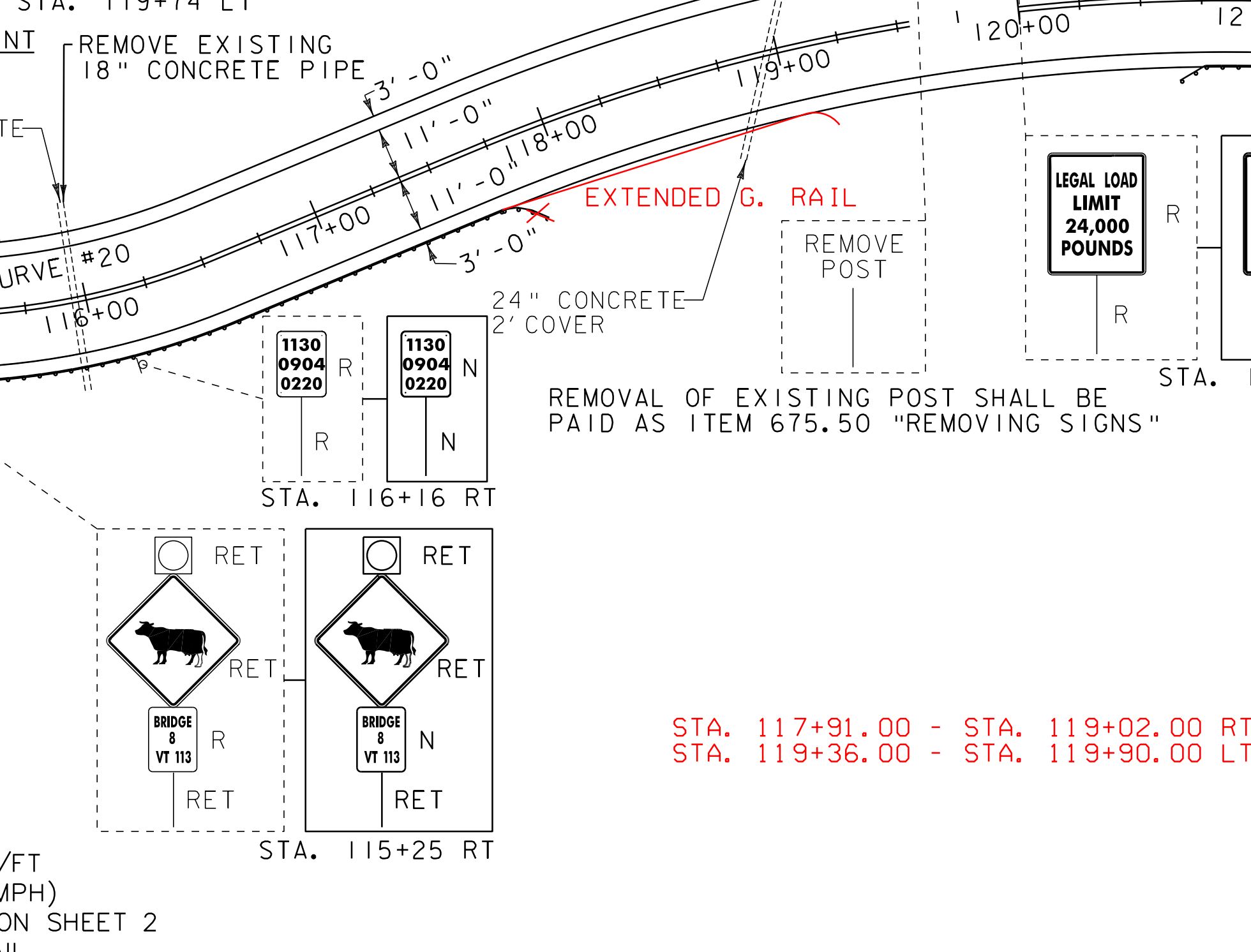
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 112+75 - STA. 129+25 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 112+75 - STA. 129+25 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 112+75 - STA. 129+25 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)



621.60 ANCHOR FOR STEEL BEAM RAIL
 STA. 114+84.75 RT STA. 120+63.00 RT
 STA. 115+48.50 LT STA. 124+38.00 RT
 STA. 117+78.50 RT STA. 128+14.00 RT
 STA. 119+36.00 LT STA. 119+90 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 112+75.00 - STA. 114+79.75 LT
 STA. 114+84.75 - STA. 115+09.75 RT
 STA. 114+92.25 - STA. 115+29.75 LT
 STA. 115+22.25 - STA. 117+91.00 RT
 STA. 120+63.00 - STA. 124+50.50 RT
 STA. 128+26.50 - STA. 129+25.00 RT

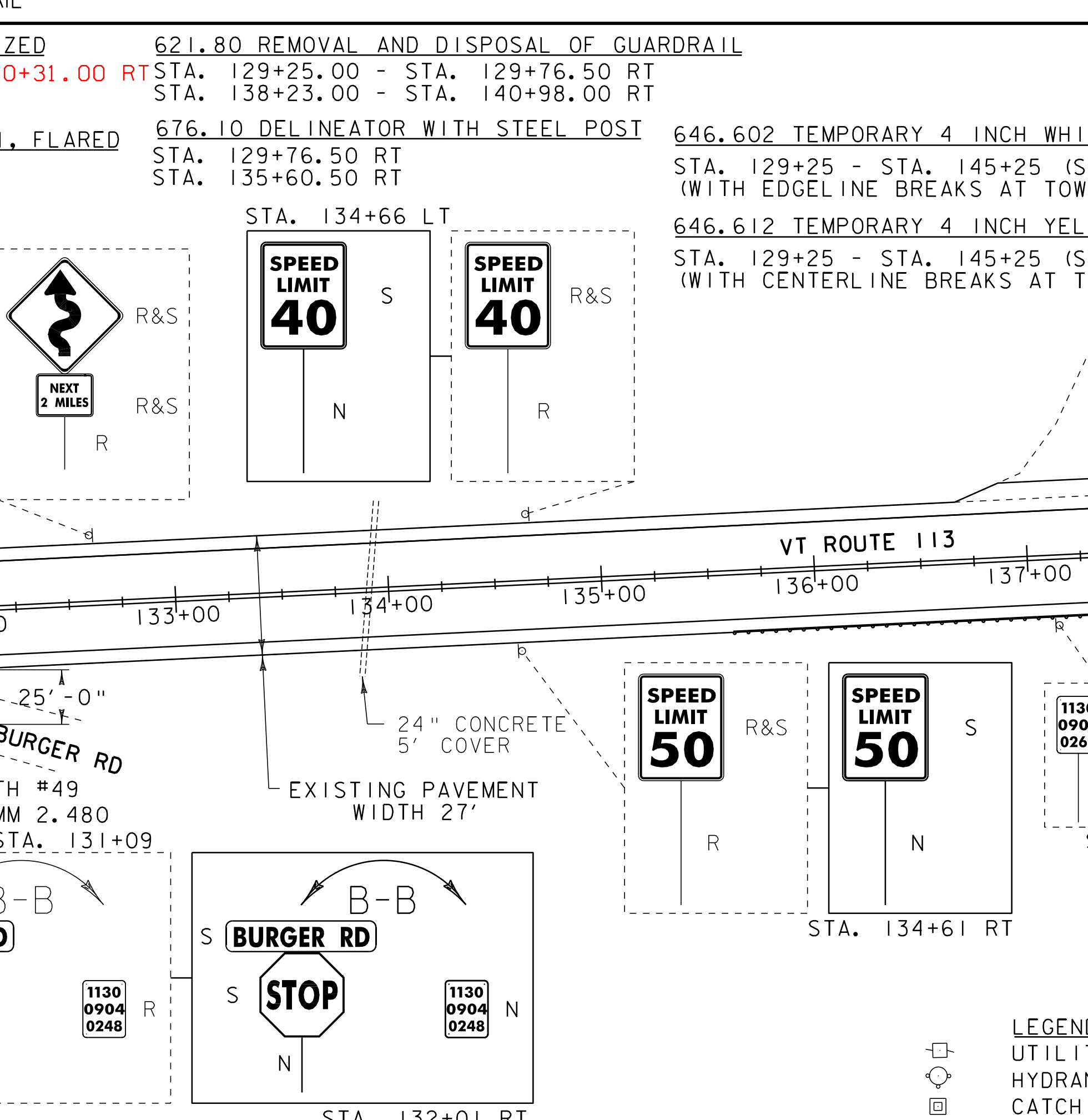
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 112+75 - STA. 129+25 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 675.50 REMOVING SIGNS
 AS SHOWN - 8
 675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 1



Curve # 21
 Delta= 44°29'14.43"
 D= 7°09'43.10"
 T= 327.19'
 L=621.16'
 R= 800.00'
 BANKING 0.066 FT/FT
 145' RUNOFF (40 MPH)
 SEE SUPERELEVATION SHEET 2
 FOR BANKING DETAIL

Curve # 22
 Delta= 34°32'41.28"
 D= 7°09'43.10"
 T= 248.75'
 L=482.34'
 R= 800.00'
 BANKING 0.066 FT/FT
 145' RUNOFF (40 MPH)
 SEE SUPERELEVATION SHEET 3
 FOR BANKING DETAIL

621.20 STEEL BEAM GUARDRAIL, GALVANIZED
 STA. 129+25.00 - STA. 130+31.00 RT
 STA. 135+98.00 - STA. 145+25.00 RT
 621.50 MANUFACTURED TERMINAL SECTION, FLARED
 STA. 135+60.50 - STA. 135+98.00 RT
 621.60 ANCHOR FOR STEEL BEAM RAIL
 STA. 129+64.00 RT
 STA. 130+19.00 RT



621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 129+25.00 - STA. 129+76.50 RT
 STA. 138+23.00 - STA. 140+98.00 RT
 676.10 DELINEATOR WITH STEEL POST
 STA. 129+76.50 RT
 STA. 135+60.50 RT

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 129+25 - STA. 145+25 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 129+25 - STA. 145+25 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 129+25 - STA. 145+25 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 129+25 - STA. 145+25 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)

PROJECT LAYOUT SHEET 6

PROJECT NAME: CHelsea-VERSHIRE
 PROJECT NUMBER: STP 2331(1)S

FILE NAME: 01c040.dgn
 PROJECT LEADER: CDL
 DESIGNED BY: BMB
 PLOT FILE: 01c040_21.i

PLOT DATE: 28-OCT-2011 13:36
 DRAWN BY: BMB
 CHECKED BY: EPD
 SHEET 21 OF 55

LEGEND
 UTILITY POLE
 HYDRANT
 CATCH BASIN / DROP INLET

NOT TO SCALE

SIGN LEGEND
 R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

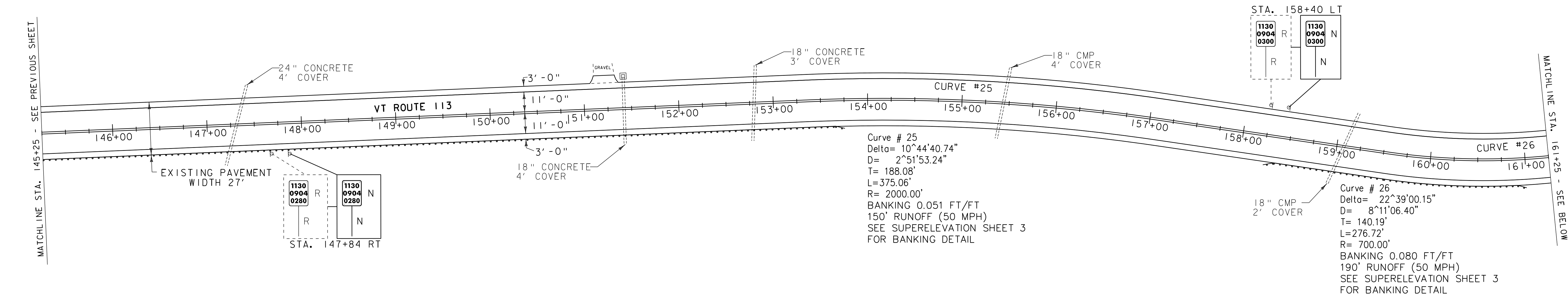
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 145+25 - STA. 161+25 (SOLID LT & RT)
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 145+25 - STA. 161+25 (SOLID LT & RT)
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 145+25 - STA. 161+25 (SOLID LT & RT)
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 145+25 - STA. 161+25 (SOLID LT & RT)

675.50 REMOVING SIGNS
 AS SHOWN - 2

604.412, 604.415, 604.418 REHAB, DROP
 INLETS, CATCH BASINS, OR MANHOLES, CLASS I,
 CLASS II OR CLASS III
 STA. 151+42 LT "TYPE D GRATE"

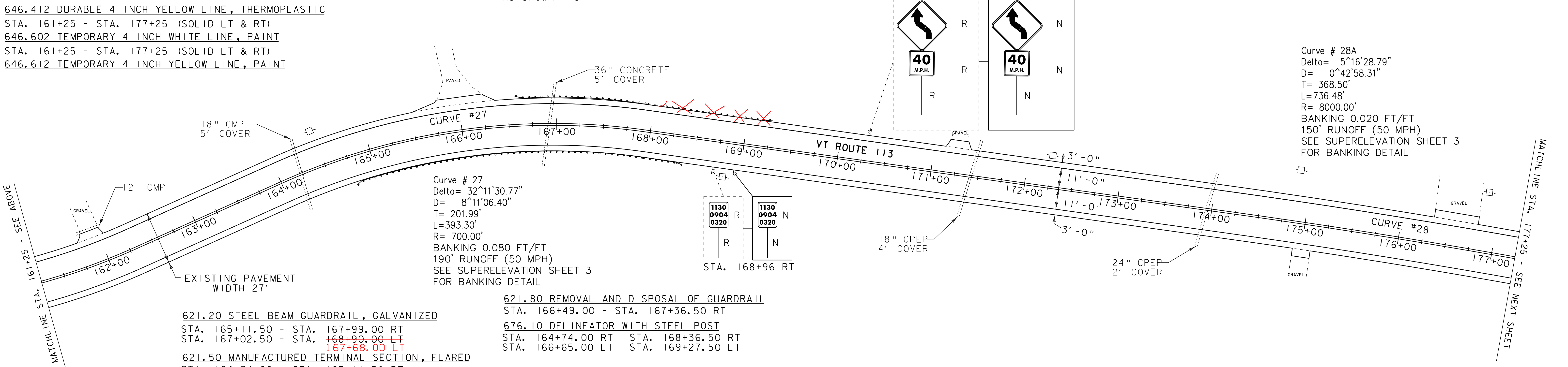
621.20 STEEL BEAM GUARDRAIL, GALVANIZED
 STA. 145+25.00 - STA. 153+37.50 RT
 STA. 158+62.50 - STA. 160.62.50 RT
 621.50 MANUFACTURED TERMINAL SECTION, FLARED
 STA. 153+37.50 - STA. 153+75.00 RT
 STA. 158+25.00 - STA. 158+62.50 RT
 STA. 160+62.50 - STA. 161+00.00 RT

676.10 DELINEATOR WITH STEEL POST
 STA. 153+75.00 RT
 STA. 158+25.00 RT
 STA. 161+00.00 RT



646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 161+25 - STA. 177+25 (SOLID LT & RT)
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 161+25 - STA. 177+25 (SOLID LT & RT)
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 161+25 - STA. 177+25 (SOLID LT & RT)
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 161+25 - STA. 177+25 (SOLID LT & RT)

675.50 REMOVING SIGNS
 AS SHOWN - 3



621.20 STEEL BEAM GUARDRAIL, GALVANIZED

STA. 165+11.50 - STA. 167+99.00 RT
 STA. 167+02.50 - STA. 168+90.00 LT
 STA. 167+68.00 LT

621.50 MANUFACTURED TERMINAL SECTION, FLARED

STA. 164+74.00 - STA. 165+11.50 RT
 STA. 166+65.00 - STA. 167+02.50 LT
 STA. 167+99.00 - STA. 168+36.50 RT
 STA. 168+90.00 - STA. 169+27.50 LT
 STA. 167+68.00 - STA. 168+05.50 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

STA. 166+49.00 - STA. 167+36.50 RT

676.10 DELINEATOR WITH STEEL POST

STA. 164+74.00 RT STA. 168+36.50 RT
 STA. 166+65.00 LT STA. 169+27.50 LT

SIGN LEGEND
 R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

LEGEND
 □ = UTILITY POLE
 ⊕ = HYDRANT
 □ = CATCH BASIN / DROP INLET

NOT TO SCALE

PROJECT LAYOUT SHEET 7

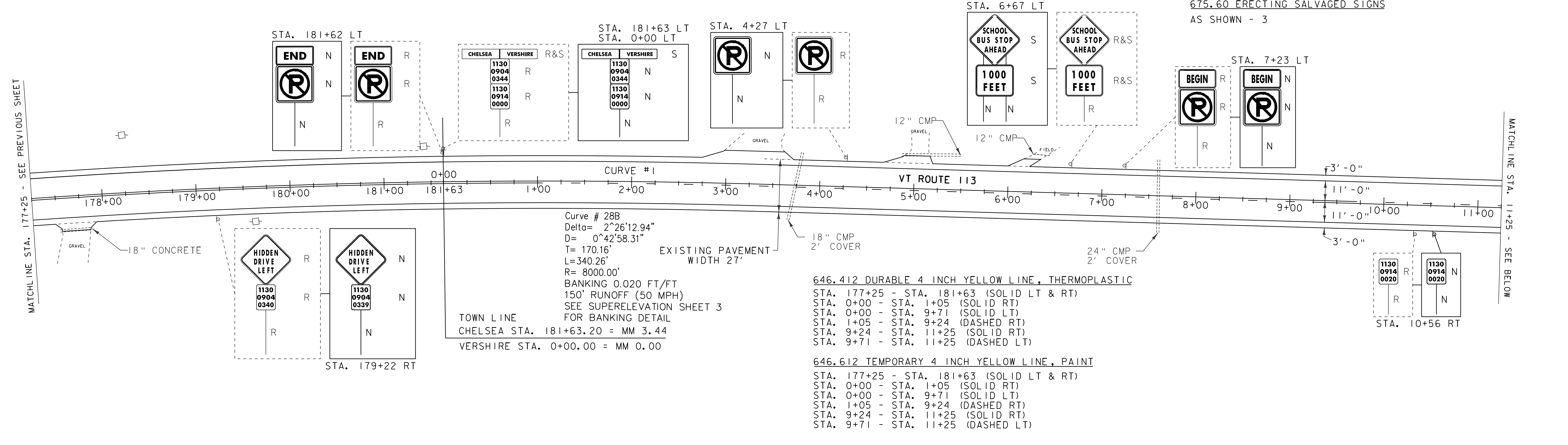
PROJECT NAME: CHelsea-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
PROJECT NUMBER: STP 2331(1)S	DRAWN BY: BMB
FILE NAME: 01c040.dgn	CHECKED BY: EPD
PROJECT LEADER: CDL	SHEET 22 OF 55
DESIGNED BY: BMB	
PLOT FILE: 01c040_22.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 177+25 - STA. 181+63 (SOLID LT & RT)
 STA. 0+00 - STA. 11+25 (SOLID LT & RT)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 177+25 - STA. 181+63 (SOLID LT & RT)
 STA. 0+00 - STA. 11+25 (SOLID LT & RT)

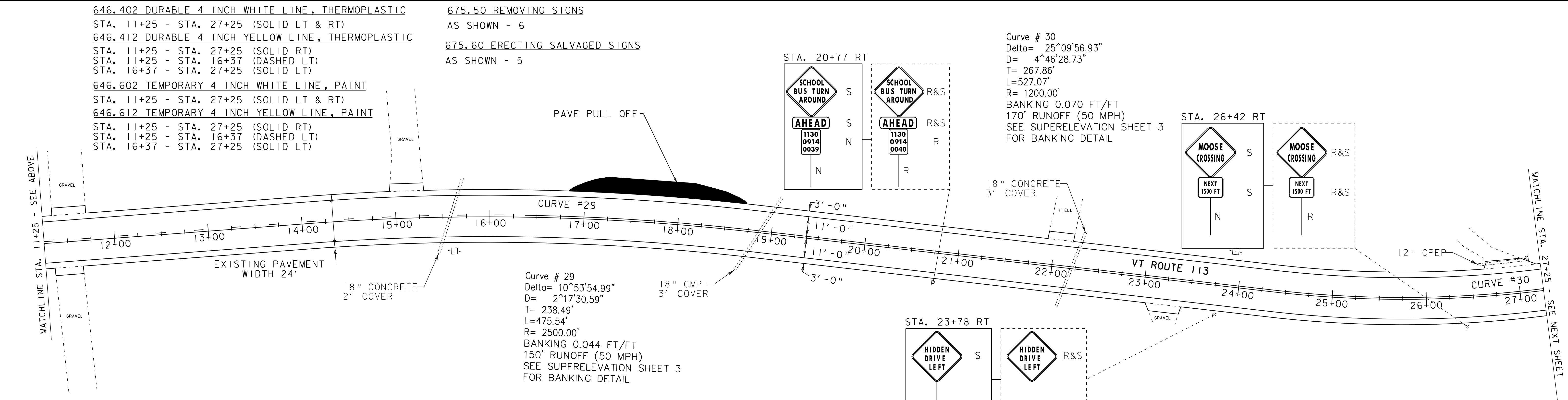
675.50 REMOVING SIGNS
 AS SHOWN - 13

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 3



646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 11+25 - STA. 27+25 (SOLID LT & RT)
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 11+25 - STA. 27+25 (SOLID RT)
 STA. 11+25 - STA. 16+37 (DASHED LT)
 STA. 16+37 - STA. 27+25 (SOLID LT)
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 11+25 - STA. 27+25 (SOLID LT & RT)
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 11+25 - STA. 27+25 (SOLID RT)
 STA. 11+25 - STA. 16+37 (DASHED LT)
 STA. 16+37 - STA. 27+25 (SOLID LT)

675.50 REMOVING SIGNS
 AS SHOWN - 6
 675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 5



SIGN LEGEND
 R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

LEGEND
 □ = UTILITY POLE
 ○ = HYDRANT
 □ = CATCH BASIN / DROP INLET

NOT TO SCALE

PROJECT LAYOUT SHEET 8

PROJECT NAME: CHLSEA-VERSHIRE
 PROJECT NUMBER: STP 2331(1)S
 FILE NAME: 01c040.dgn
 PROJECT LEADER: CDL
 DESIGNED BY: BMB
 PLOT FILE: 01c040_23.i
 PLOT DATE: 28-OCT-2011 3:36
 DRAWN BY: BMB
 CHECKED BY: EPD
 SHEET 23 OF 55

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC

STA. 27+25 - STA. 43+50 (SOLID LT & RT)
(WITH EDGELINE BREAKS AT TOWN ROADS)

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC

STA. 27+25 - STA. 43+50 (SOLID LT & RT)
(WITH CENTERLINE BREAKS AT TOWN ROADS)

STA. 42+60 RT (CENTERLINE TH#16)
STA. 42+90 LT (CENTERLINE TH#1)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT

STA. 27+25 - STA. 43+50 (SOLID LT & RT)
(WITH EDGELINE BREAKS AT TOWN ROADS)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT

STA. 27+25 - STA. 43+50 (SOLID LT & RT)
(WITH CENTERLINE BREAKS AT TOWN ROADS)

STA. 42+60 RT (CENTERLINE TH#16)
STA. 42+90 LT (CENTERLINE TH#1)

646.482 DURABLE 24 INCH STOPBAR, THERMOPLASTIC

STA. 42+60 - STA. 42+92 LT
STA. 42+62 - STA. 42+81 RT

646.682 TEMPORARY 24 INCH STOPBAR, PAINT

STA. 42+60 - STA. 42+92 LT
STA. 42+62 - STA. 42+81 RT

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC

STA. 42+80 LT "STOP"
STA. 42+70 RT "STOP"

646.692 TEMPORARY LETTER OR SYMBOL, PAINT

STA. 42+80 LT "STOP"
STA. 42+70 RT "STOP"

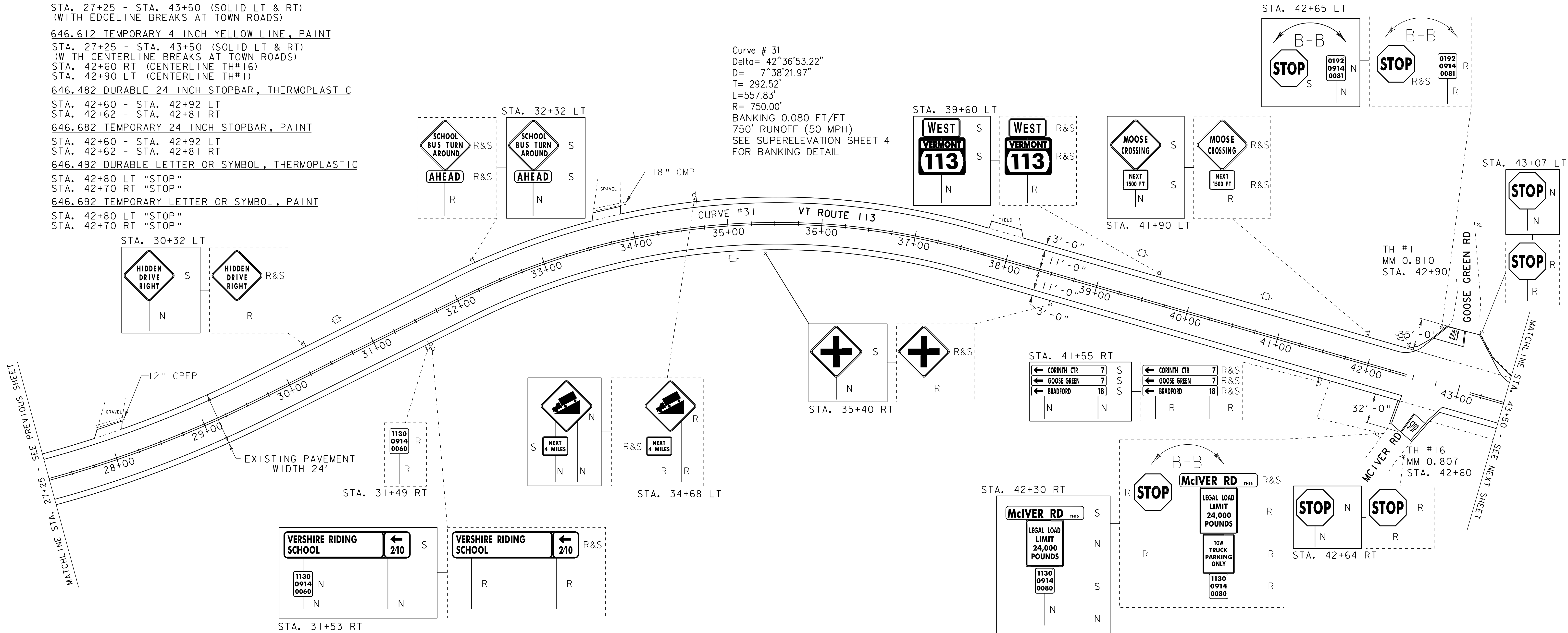
675.50 REMOVING SIGNS

AS SHOWN - 24

675.60 ERECTING SALVAGED SIGNS

AS SHOWN - 15

Curve # 31
Delta= 42°36'53.22"
D= 7°38'21.97"
T= 292.52'
L=557.83'
R= 750.00'
BANKING 0.080 FT/FT
750' RUNOFF (50 MPH)
SEE SUPERELEVATION SHEET 4
FOR BANKING DETAIL



LEGEND
□ UTILITY POLE
○ HYDRANT
□ CATCH BASIN / DROP INLET

NOT TO SCALE

PROJECT LAYOUT SHEET 9

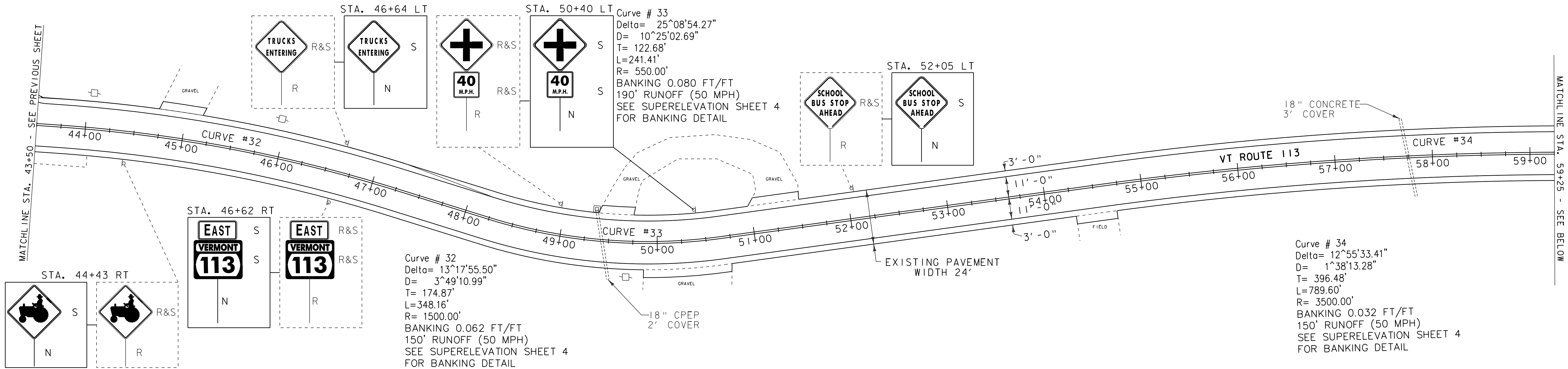
PROJECT NAME: CHELSEA-VERSHERE
PROJECT NUMBER: STP 2331(1)S

FILE NAME: 01c040.dgn
PROJECT LEADER: CDL
DESIGNED BY: BMB
PLOT FILE: 01c040_24.i

PLOT DATE: 28-OCT-2011 13:36
DRAWN BY: BMB
CHECKED BY: EPD
SHEET 24 OF 55

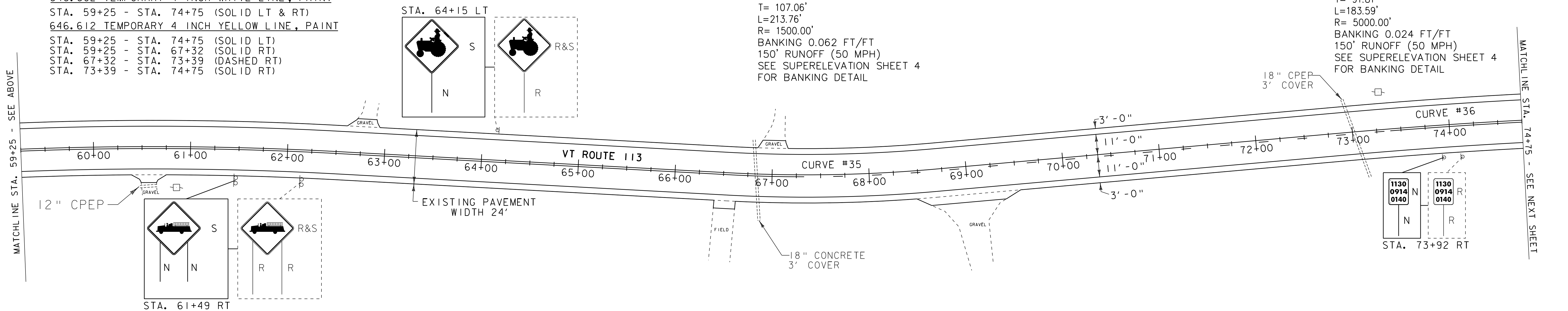
646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 43+50 - STA. 59+25 (SOLID LT & RT)
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 43+50 - STA. 59+25 (SOLID LT & RT)
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 43+50 - STA. 59+25 (SOLID LT & RT)
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 43+50 - STA. 59+25 (SOLID LT & RT)

675.50 REMOVING SIGNS
 AS SHOWN - 7
 675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 7



646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 59+25 - STA. 74+75 (SOLID LT & RT)
 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 59+25 - STA. 74+75 (SOLID LT)
 STA. 59+25 - STA. 67+32 (SOLID RT)
 STA. 67+32 - STA. 73+39 (DASHED RT)
 STA. 73+39 - STA. 74+75 (SOLID RT)
 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 59+25 - STA. 74+75 (SOLID LT & RT)
 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 59+25 - STA. 74+75 (SOLID LT)
 STA. 59+25 - STA. 67+32 (SOLID RT)
 STA. 67+32 - STA. 73+39 (DASHED RT)
 STA. 73+39 - STA. 74+75 (SOLID RT)

675.50 REMOVING SIGNS
 AS SHOWN - 3
 675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 2

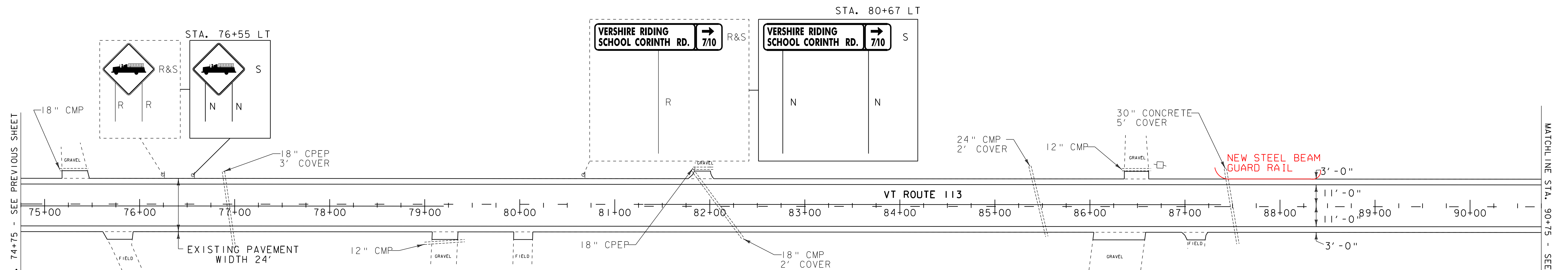


SIGN LEGEND
 R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

LEGEND
 □ = UTILITY POLE
 ○ = HYDRANT
 □ = CATCH BASIN / DROP INLET

NOT TO SCALE

PROJECT LAYOUT SHEET 10	PROJECT NAME: CHLSEA-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
	PROJECT NUMBER: STP 2331(1)S	DRAWN BY: BMB
	FILE NAME: 01c040.dgn	CHECKED BY: EPD
	PROJECT LEADER: CDL	SHEET 25 OF 55
DESIGNED BY: BMB		
PLOT FILE: 01c040_25.i		

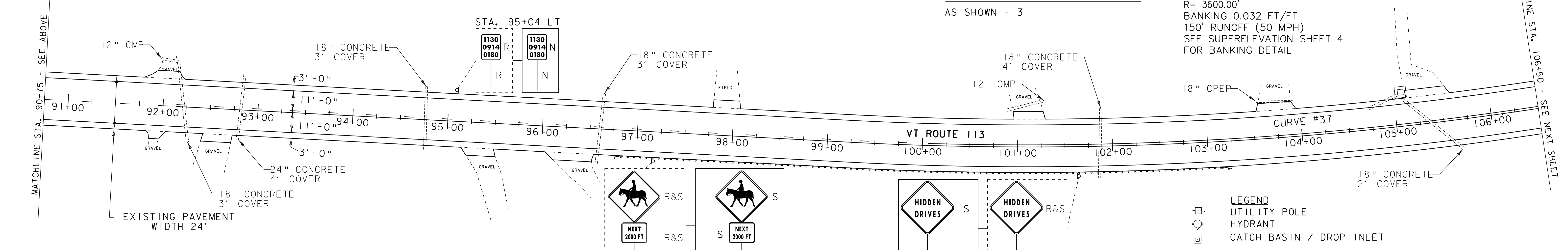


- | | | | |
|---|---|--|---|
| 646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
STA. 74+75 - STA. 90+75 (SOLID LT & RT) | 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
STA. 74+75 - STA. 90+75 (SOLID LT & RT) | 675.50 REMOVING SIGNS
AS SHOWN - 2 | 621.20 STEEL BEAM GUARD RAIL GALVANIZED
STA. 87+70.50 LT - STA. 87+95.50 LT |
| 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
STA. 74+75 - STA. 74+98 (SOLID LT)
STA. 74+75 - STA. 79+20 (SOLID RT)
STA. 74+98 - STA. 81+31 (DASHED LT)
STA. 81+31 - STA. 90+75 (DASHED RT)
STA. 81+31 - STA. 87+38 (SOLID LT) | 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
STA. 74+75 - STA. 74+98 (SOLID LT)
STA. 74+75 - STA. 79+20 (SOLID RT)
STA. 74+98 - STA. 81+31 (DASHED LT)
STA. 81+31 - STA. 90+75 (DASHED RT)
STA. 81+31 - STA. 87+38 (SOLID LT) | 675.60 ERECTING SALVAGED SIGNS
AS SHOWN - 2 | 621.50 MANUFACTURED TERMINAL SECTION FLARED
STA. 87+33.00 LT - STA. 87+70.50 LT
STA. 87+95.50 LT - STA. 88+33.00 LT |

- | | | | |
|---|---|---|---|
| 646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
STA. 90+75 - STA. 106+50 (SOLID LT & RT) | 646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
STA. 90+75 - STA. 106+50 (SOLID LT & RT) | 604.412, 604.415, 604.418 REHAB, DROP
INLETS, CATCH BASINS, OR MANHOLES, CLASS I,
CLASS II OR CLASS III | 621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
STA. 96+76.00 - STA. 103+26.00 RT |
| 646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
STA. 90+75 - STA. 92+13 (DASHED RT)
STA. 92+13 - STA. 100+06 (DASHED LT)
STA. 92+13 - STA. 106+50 (SOLID RT)
STA. 100+06 - STA. 106+50 (SOLID LT) | 646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
STA. 90+75 - STA. 92+13 (DASHED RT)
STA. 92+13 - STA. 100+06 (DASHED LT)
STA. 92+13 - STA. 106+50 (SOLID RT)
STA. 100+06 - STA. 106+50 (SOLID LT) | STA. 105+08 LT "TYPE D GRATE" | 676.10 DELINEATOR WITH STEEL POST
STA. 96+76.00 RT
STA. 103+26.00 RT |

- | | |
|--|--|
| 621.20 STEEL BEAM GUARDRAIL, GALVANIZED
STA. 97+13.50 - STA. 102+88.50 RT | 675.50 REMOVING SIGNS
AS SHOWN - 4 |
| 621.50 MANUFACTURED TERMINAL SECTION, FLARED
STA. 96+76.00 - STA. 97+13.50 RT
STA. 102+88.50 - STA. 103+26.00 RT | 675.60 ERECTING SALVAGED SIGNS
AS SHOWN - 3 |

Curve # 37
Delta= 20°05'38.79"
D= 1°35'29.58"
T= 637.83'
L=1262.55'
R= 3600.00'
BANKING 0.032 FT/FT
150' RUNOFF (50 MPH)
SEE SUPERELEVATION SHEET 4
FOR BANKING DETAIL



SIGN LEGEND
R = REMOVE
R&S = REMOVE AND SALVAGE
S = SALVAGE
RET = RETAIN
N = NEW
B-B = BACK TO BACK

LEGEND
□ UTILITY POLE
○ HYDRANT
□ CATCH BASIN / DROP INLET

PROJECT LAYOUT SHEET 11

PROJECT NAME: CHLSEA-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
PROJECT NUMBER: STP 2331(1)S	DRAWN BY: BMB
FILE NAME: 01c040.dgn	DESIGNED BY: BMB
PROJECT LEADER: CDL	CHECKED BY: EPD
PLOT FILE: 01c040_26.i	SHEET 26 OF 55

NOT TO SCALE

604.20 PRECAST CONCRETE CATCH BASIN WITH CAST IRON GRATE
 STA. 109+00 LT

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 106+50 - STA. 122+50 (SOLID LT & RT)

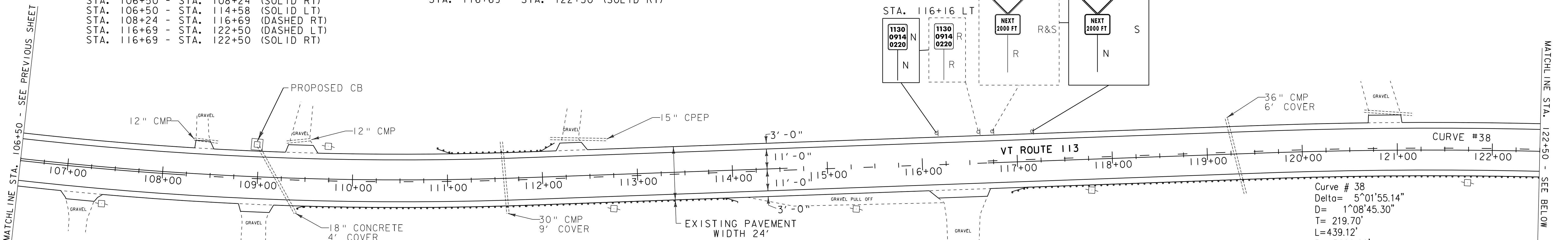
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 106+50 - STA. 108+24 (SOLID RT)
 STA. 106+50 - STA. 114+58 (SOLID LT)
 STA. 108+24 - STA. 116+69 (DASHED RT)
 STA. 116+69 - STA. 122+50 (DASHED LT)
 STA. 116+69 - STA. 122+50 (SOLID RT)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 106+50 - STA. 122+50 (SOLID LT & RT)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 106+50 - STA. 108+24 (SOLID RT)
 STA. 106+50 - STA. 114+58 (SOLID LT)
 STA. 108+24 - STA. 116+69 (DASHED RT)
 STA. 116+69 - STA. 122+50 (DASHED LT)
 STA. 116+69 - STA. 122+50 (SOLID RT)

675.50 REMOVING SIGNS
 AS SHOWN - 3

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 2



621.20 STEEL BEAM GUARDRAIL, GALVANIZED
 STA. 109+80.00 - STA. 113+67.50 RT
 STA. 111+26.50 - STA. 111+76.50 LT
 STA. 117+27.50 - STA. 122+50.00 RT

621.50 MANUFACTURED TERMINAL SECTION, FLARED
 STA. 109+42.50 - STA. 109+80.00 RT
 STA. 110+89.00 - STA. 111+26.50 LT
 STA. 111+76.50 - STA. 112+14.00 LT
 STA. 113+67.50 - STA. 114+05.00 RT
 STA. 116+90.00 - STA. 117+27.50 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 110+42.50 - STA. 114+05.00 RT
 STA. 110+89.00 - STA. 112+14.00 LT
 STA. 116+90.00 - STA. 121+65.00 RT

676.10 DELINEATOR WITH STEEL POST
 STA. 109+42.50 RT STA. 114+05.00 RT
 STA. 110+89.00 LT STA. 116+90.00 RT
 STA. 112+14.00 LT

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 122+50 - STA. 138+75 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 122+50 - STA. 138+75 (SOLID RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 STA. 122+50 - STA. 124+61 (DASHED LT)
 STA. 124+61 - STA. 138+75 (SOLID LT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 STA. 135+70 RT (CENTERLINE TH#17)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 122+50 - STA. 138+75 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 122+50 - STA. 138+75 (SOLID RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 STA. 122+50 - STA. 124+61 (DASHED LT)
 STA. 124+61 - STA. 138+75 (SOLID LT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 STA. 135+70 RT (CENTERLINE TH#17)

675.50 REMOVING SIGNS
 AS SHOWN - 4

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 2

621.20 STEEL BEAM GUARDRAIL, GALVANIZED
 STA. 122+50.00 - STA. 130+77.00 RT
 STA. 133+52.50 - STA. 134+90.00 RT
 STA. 136+45.00 - STA. 138+75.00 RT

621.50 MANUFACTURED TERMINAL SECTION, FLARED
 STA. 130+77.00 - STA. 131+14.50 RT
 STA. 133+15.00 - STA. 133+52.50 RT
 STA. 134+90.00 - STA. 135+27.50 RT
 STA. 136+07.50 - STA. 136+45.00 RT

646.482 DURABLE 24 INCH STOPBAR, THERMOPLASTIC
 STA. 135+66 - STA. 135+79 RT

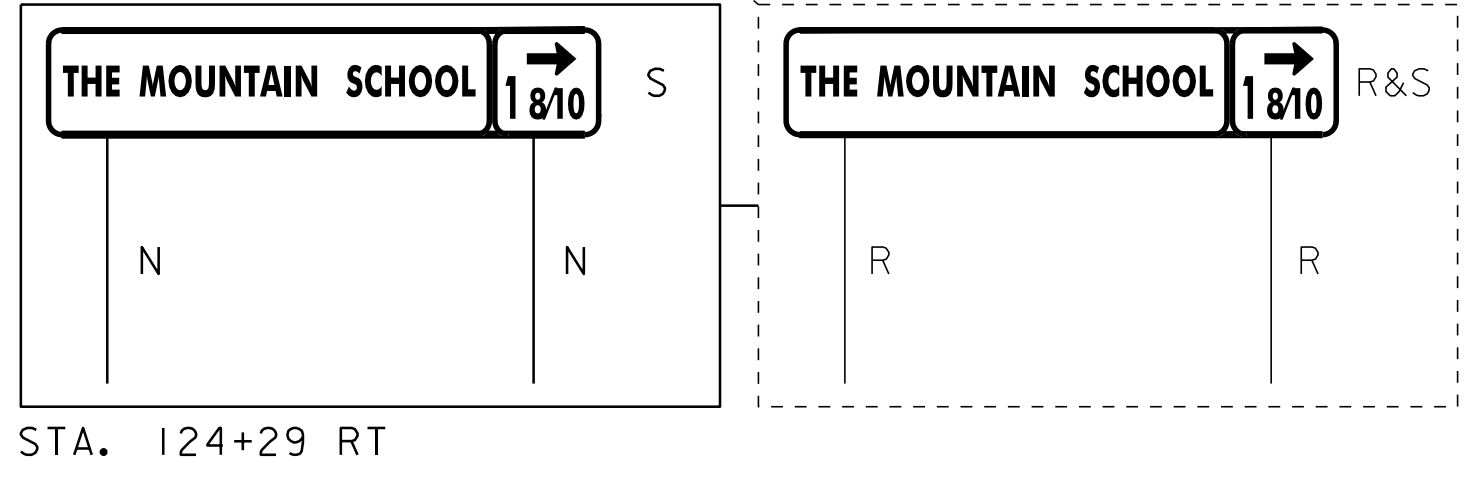
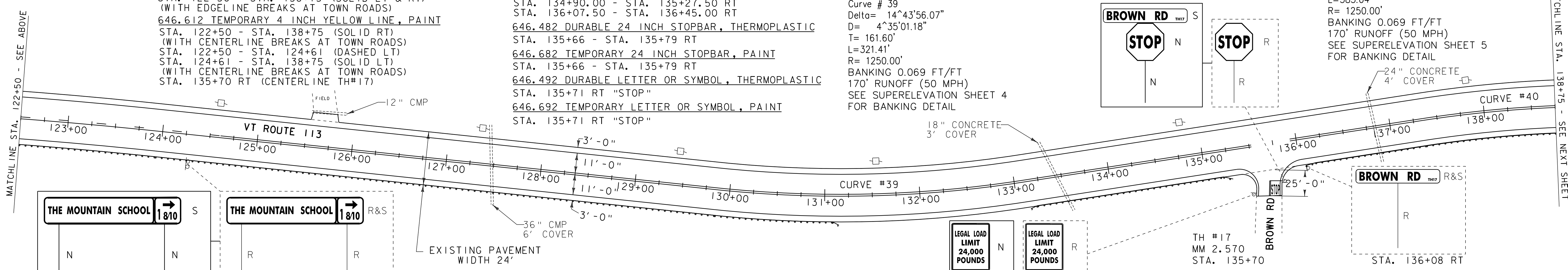
646.682 TEMPORARY 24 INCH STOPBAR, PAINT
 STA. 135+66 - STA. 135+79 RT

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 STA. 135+71 RT "STOP"

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 STA. 135+71 RT "STOP"

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 125+77.00 - STA. 131+14.50 RT
 STA. 133+15.00 - STA. 135+27.50 RT
 STA. 136+07.50 - STA. 138+75.00 RT

676.10 DELINEATOR WITH STEEL POST
 STA. 135+27.50 RT
 STA. 136+07.50 RT

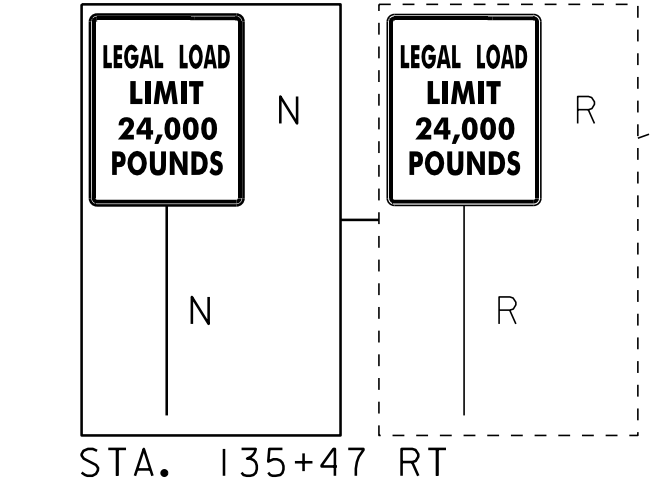


SIGN LEGEND

R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

LEGEND

□ = UTILITY POLE
 ○ = HYDRANT
 ⊗ = CATCH BASIN / DROP INLET



PROJECT LAYOUT SHEET 12

PROJECT NAME: CHelsea-VERSHIRE
 PROJECT NUMBER: STP 2331(1)S

FILE NAME: 01c040.dgn
 PROJECT LEADER: CDM
 DESIGNED BY: BMB
 PLOT FILE: 01c040_27.i

PLOT DATE: 28-OCT-2011 13:36
 DRAWN BY: BMB
 CHECKED BY: EPD
 SHEET 27 OF 55

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 138+75 - STA. 154+50 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)

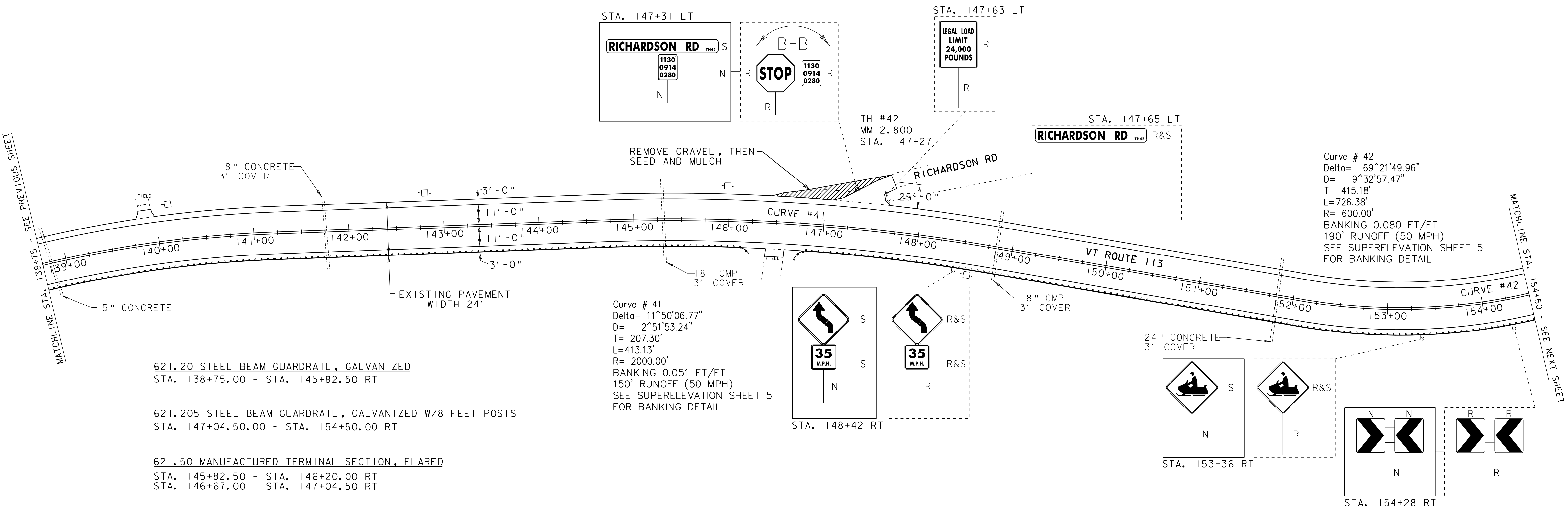
646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 138+75 - STA. 154+50 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 138+75 - STA. 154+50 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 138+75 - STA. 154+50 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)

675.50 REMOVING SIGNS
 AS SHOWN - 9

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 4



621.20 STEEL BEAM GUARDRAIL, GALVANIZED
 STA. 138+75.00 - STA. 145+82.50 RT

621.205 STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS
 STA. 147+04.50.00 - STA. 154+50.00 RT

621.50 MANUFACTURED TERMINAL SECTION, FLARED
 STA. 145+82.50 - STA. 146+20.00 RT
 STA. 146+67.00 - STA. 147+04.50 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 138+75.00 - STA. 146+20.00 RT
 STA. 146+67.00 - STA. 154+50.00 RT

676.10 DELINEATOR WITH STEEL POST
 STA. 146+20.00 RT
 STA. 146+67.00 RT

SIGN LEGEND
 R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

LEGEND
 UTILITY POLE
 HYDRANT
 CATCH BASIN / DROP INLET

NOT TO SCALE

PROJECT LAYOUT SHEET 13	PROJECT NAME: CHelsea-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
	PROJECT NUMBER: STP 2331(1)S	DRAWN BY: BMB
	FILE NAME: 01c040.dgn	CHECKED BY: EPD
	PROJECT LEADER: CDL	SHEET 28 OF 55
DESIGNED BY: BMB		
PLOT FILE: 01c040_28.i		

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 154+50 - STA. 171+75 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 154+50 - STA. 171+75 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 STA. 167+00 LT (CENTERLINE TH#6)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 154+50 - STA. 171+75 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 154+50 - STA. 171+75 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 STA. 167+00 LT (CENTERLINE TH#6)

675.50 REMOVING SIGNS
 AS SHOWN - 27

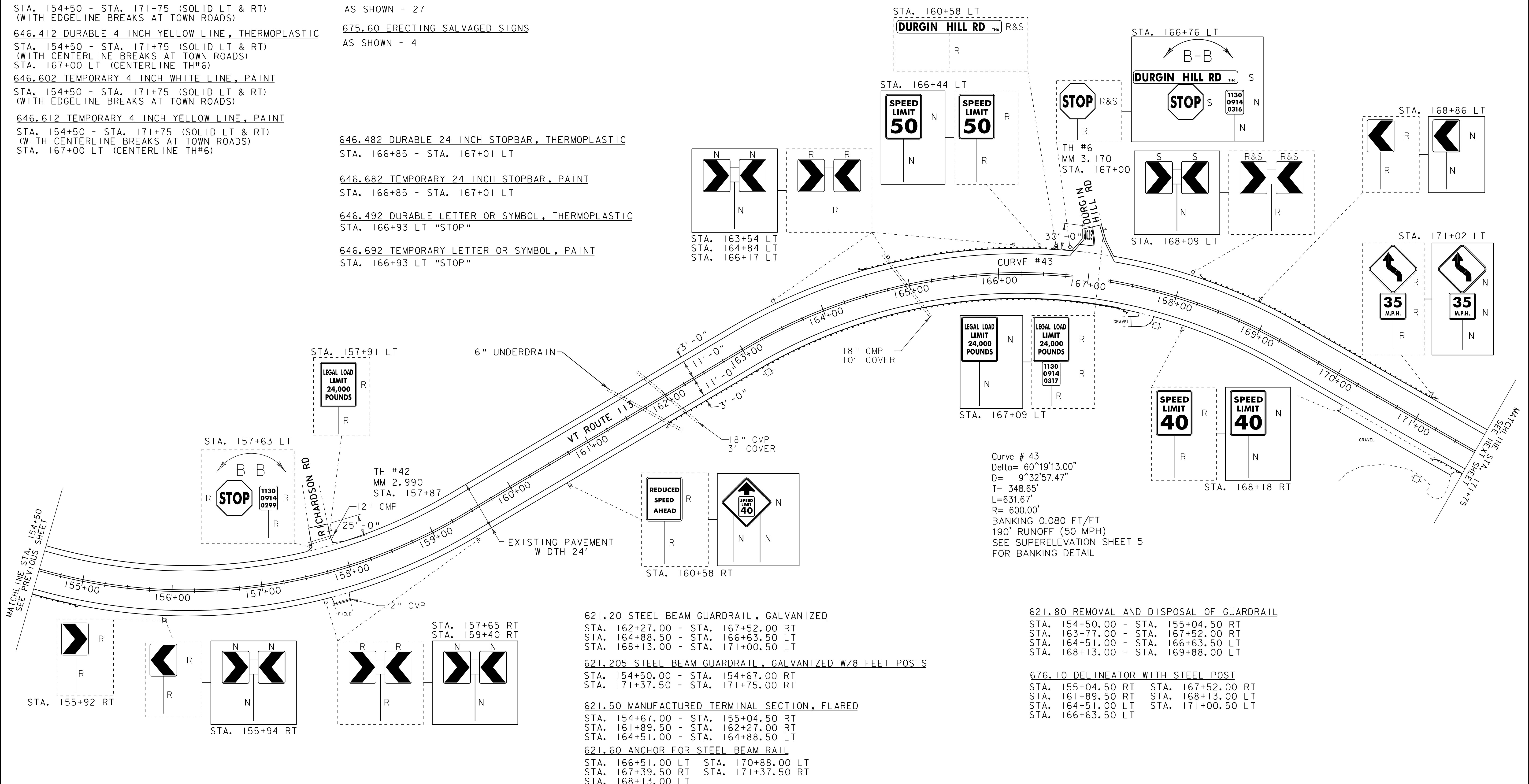
675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 4

646.482 DURABLE 24 INCH STOPBAR, THERMOPLASTIC
 STA. 166+85 - STA. 167+01 LT

646.682 TEMPORARY 24 INCH STOPBAR, PAINT
 STA. 166+85 - STA. 167+01 LT

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 STA. 166+93 LT "STOP"

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 STA. 166+93 LT "STOP"



621.20 STEEL BEAM GUARDRAIL, GALVANIZED
 STA. 162+27.00 - STA. 167+52.00 RT
 STA. 164+88.50 - STA. 166+63.50 LT
 STA. 168+13.00 - STA. 171+00.50 LT

621.205 STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS
 STA. 154+50.00 - STA. 154+67.00 RT
 STA. 171+37.50 - STA. 171+75.00 RT

621.50 MANUFACTURED TERMINAL SECTION, FLARED
 STA. 154+67.00 - STA. 155+04.50 RT
 STA. 161+89.50 - STA. 162+27.00 RT
 STA. 164+51.00 - STA. 164+88.50 LT

621.60 ANCHOR FOR STEEL BEAM RAIL
 STA. 166+51.00 LT STA. 170+88.00 LT
 STA. 167+39.50 RT STA. 171+37.50 RT
 STA. 168+13.00 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 154+50.00 - STA. 155+04.50 RT
 STA. 163+77.00 - STA. 167+52.00 RT
 STA. 164+51.00 - STA. 166+63.50 LT
 STA. 168+13.00 - STA. 169+88.00 LT

676.10 DELINEATOR WITH STEEL POST
 STA. 155+04.50 RT STA. 167+52.00 RT
 STA. 161+89.50 RT STA. 168+13.00 LT
 STA. 164+51.00 LT STA. 171+00.50 LT
 STA. 166+63.50 LT

SIGN LEGEND
 R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

LEGEND
 UTILITY POLE
 HYDRANT
 CATCH BASIN / DROP INLET

NOT TO SCALE

PROJECT LAYOUT SHEET 14	PROJECT NAME: CHLSEA-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
	PROJECT NUMBER: STP 2331(1)S	DRAWN BY: BMB
	FILE NAME: 01c040.dgn	CHECKED BY: EPD
	PROJECT LEADER: CDL	SHEET 29 OF 55
	DESIGNED BY: BMB	
	PLOT FILE: 01c040_29.i	

646.402 DURABLE 4 INCH WHITE LINE, THERMOPLASTIC
 STA. 171+75 - STA. 187+97 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)

646.412 DURABLE 4 INCH YELLOW LINE, THERMOPLASTIC
 STA. 171+75 - STA. 187+97 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 STA. 182+10 RT (CENTERLINE TH#3)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT
 STA. 171+75 - STA. 187+97 (SOLID LT & RT)
 (WITH EDGELINE BREAKS AT TOWN ROADS)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT
 STA. 171+75 - STA. 187+97 (SOLID LT & RT)
 (WITH CENTERLINE BREAKS AT TOWN ROADS)
 STA. 182+10 RT (CENTERLINE TH#3)

675.50 REMOVING SIGNS
 AS SHOWN - 11

675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 3

604.412, 604.415, 604.418 REHAB, DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I, CLASS II OR CLASS III
 STA. 187+06 LT "TYPE D GRATE"

621.20 STEEL BEAM GUARDRAIL, GALVANIZED
 STA. 174+77.00 - STA. 177+27.00 LT
 STA. 182+22.00 - STA. 184+44.00 RT

621.205 STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS
 STA. 171+75.00 - STA. 181+50.00 RT

621.60 ANCHOR FOR STEEL BEAM RAIL
 STA. 174+77.00 LT STA. 182+26.00 RT
 STA. 177+14.50 LT STA. 182+44.00 RT
 STA. 181+37.50 RT STA. 184+31.50 RT

616.35 TREATED TIMBER CURB
 STA. 180+00 - STA. 181+26 RT
 179+58

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 171+75.00 - STA. 181+50.00 RT
 STA. 174+77.00 - STA. 177+27.00 LT
 STA. 182+22.00 - STA. 184+44.00 RT

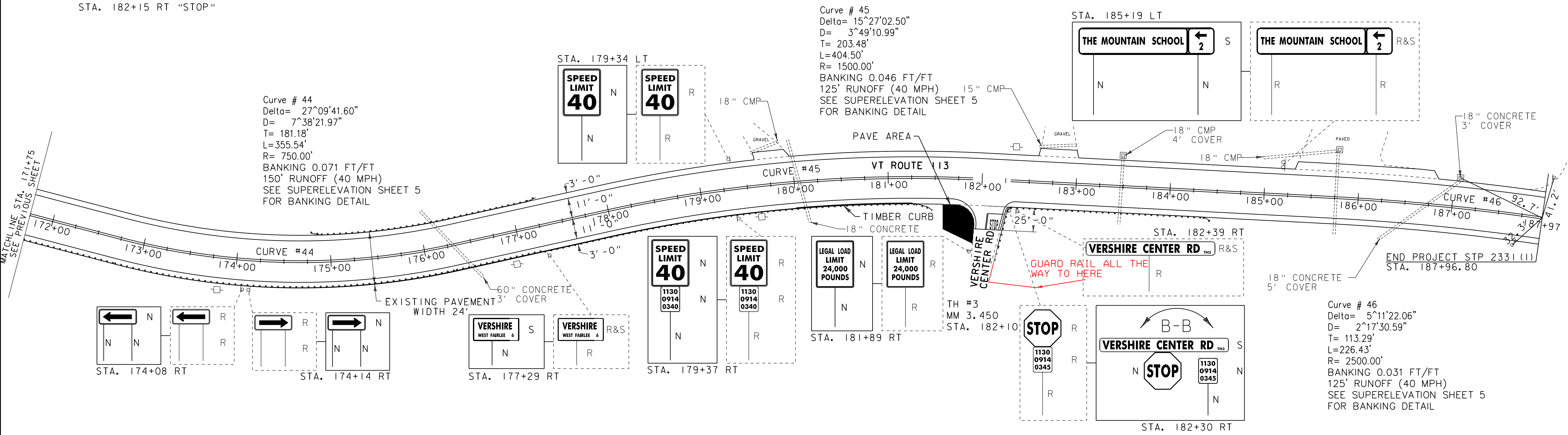
676.10 DELINEATOR WITH STEEL POST
 STA. 174+77.00 LT STA. 181+50.00 RT
 STA. 177+27.00 LT STA. 184+44.00 RT

646.482 DURABLE 24 INCH STOPBAR, THERMOPLASTIC
 STA. 182+08 - STA. 182+25 RT

646.682 TEMPORARY 24 INCH STOPBAR, PAINT
 STA. 182+08 - STA. 182+25 RT

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC
 STA. 182+15 RT "STOP"

646.692 TEMPORARY LETTER OR SYMBOL, PAINT
 STA. 182+15 RT "STOP"



Curve # 44
 Delta= 27°09'41.60"
 D= 7°38'21.97"
 T= 181.18'
 L=355.54'
 R= 750.00'
 BANKING 0.071 FT/FT
 150' RUNOFF (40 MPH)
 SEE SUPERELEVATION SHEET 5
 FOR BANKING DETAIL

Curve # 45
 Delta= 15°27'02.50"
 D= 3°49'10.99"
 T= 203.48'
 L=404.50'
 R= 1500.00'
 BANKING 0.046 FT/FT
 125' RUNOFF (40 MPH)
 SEE SUPERELEVATION SHEET 5
 FOR BANKING DETAIL

Curve # 46
 Delta= 5°11'22.06"
 D= 2°17'30.59"
 T= 113.29'
 L=226.43'
 R= 2500.00'
 BANKING 0.031 FT/FT
 125' RUNOFF (40 MPH)
 SEE SUPERELEVATION SHEET 5
 FOR BANKING DETAIL

SIGN LEGEND

R = REMOVE
 R&S = REMOVE AND SALVAGE
 S = SALVAGE
 RET = RETAIN
 N = NEW
 B-B = BACK TO BACK

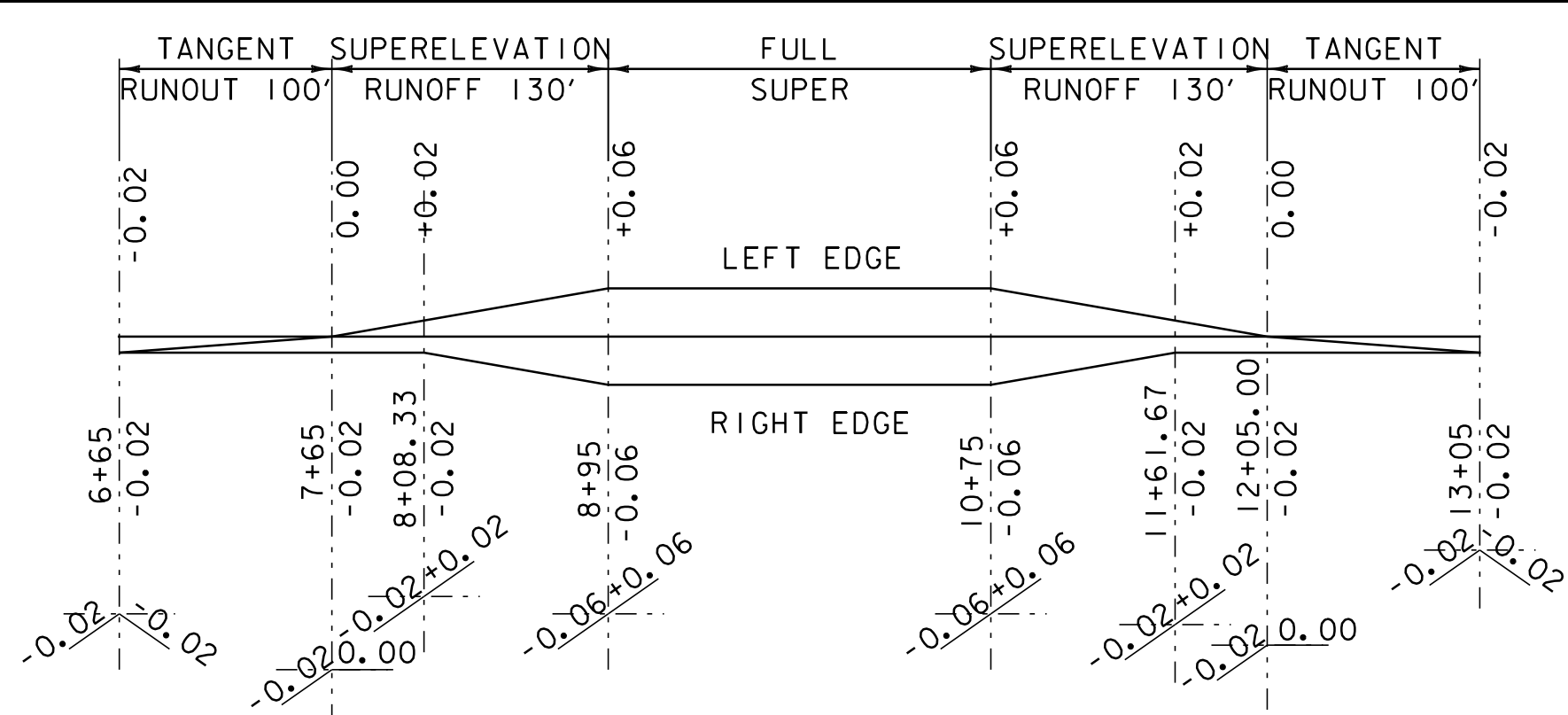
LEGEND

UTILITY POLE
 HYDRANT
 CATCH BASIN / DROP INLET

NOT TO SCALE

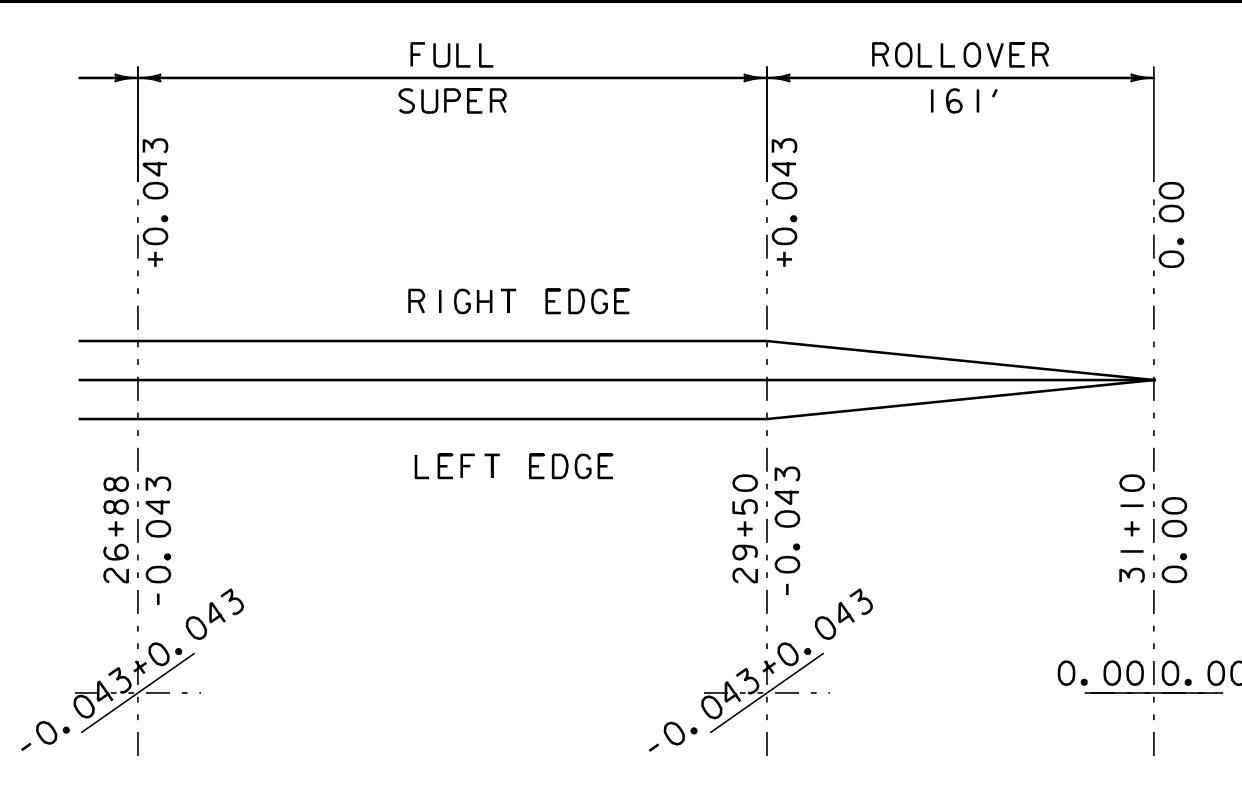
PROJECT LAYOUT SHEET 15

PROJECT NAME: CHLSEA-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
PROJECT NUMBER: STP 2331(1)S	DRAWN BY: BMB
FILE NAME: 01c040.dgn	CHECKED BY: EPD
PROJECT LEADER: CDL	SHEET 30 OF 55
DESIGNED BY: BMB	
PLOT FILE: 01c040_30.i	



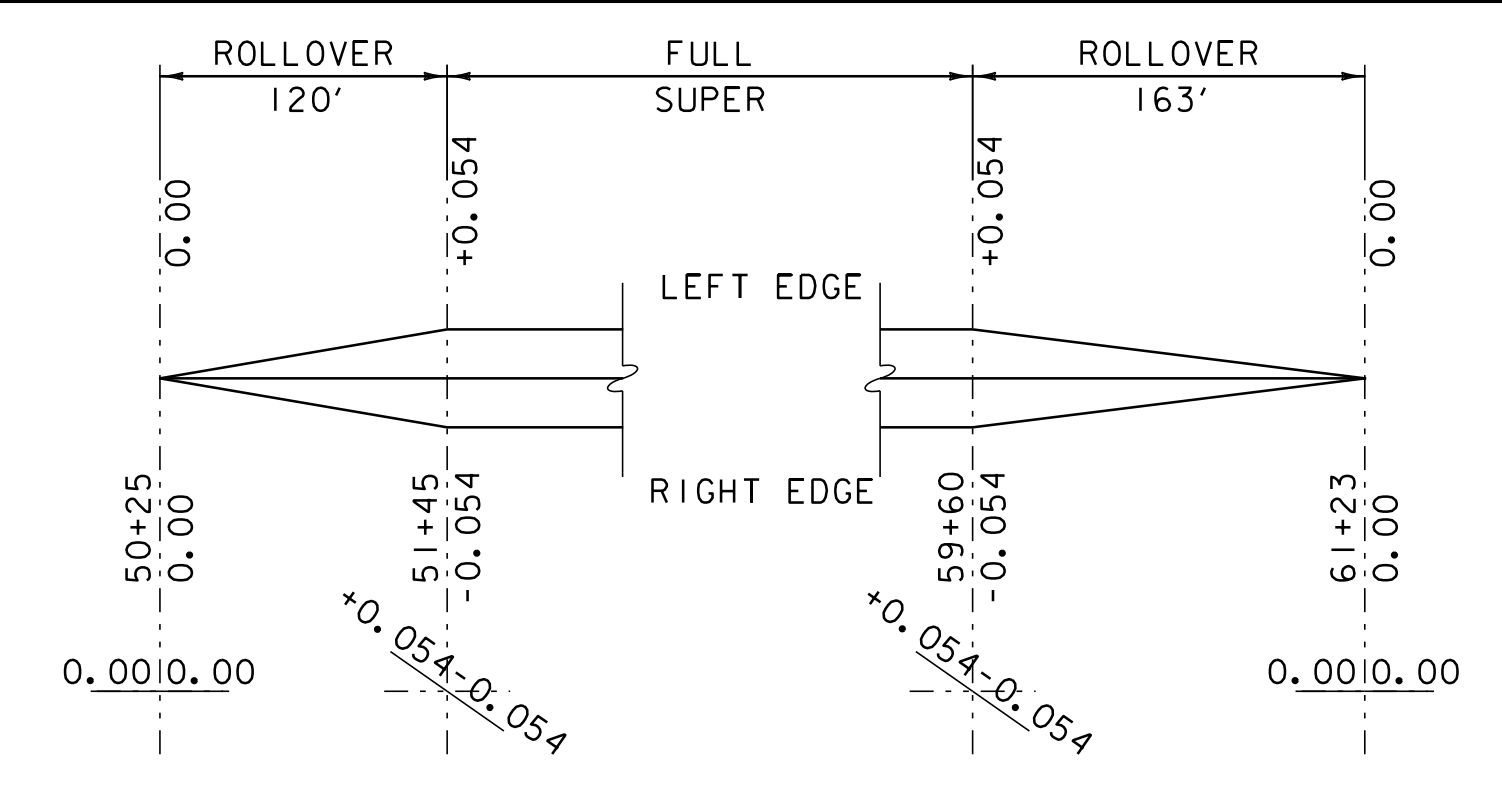
SUPERELEVATION DIAGRAM CURVE #2

NTS
 PC = 8+56.13 R = 350 FT - RT
 PT = 11+12.03 V = 30 MPH



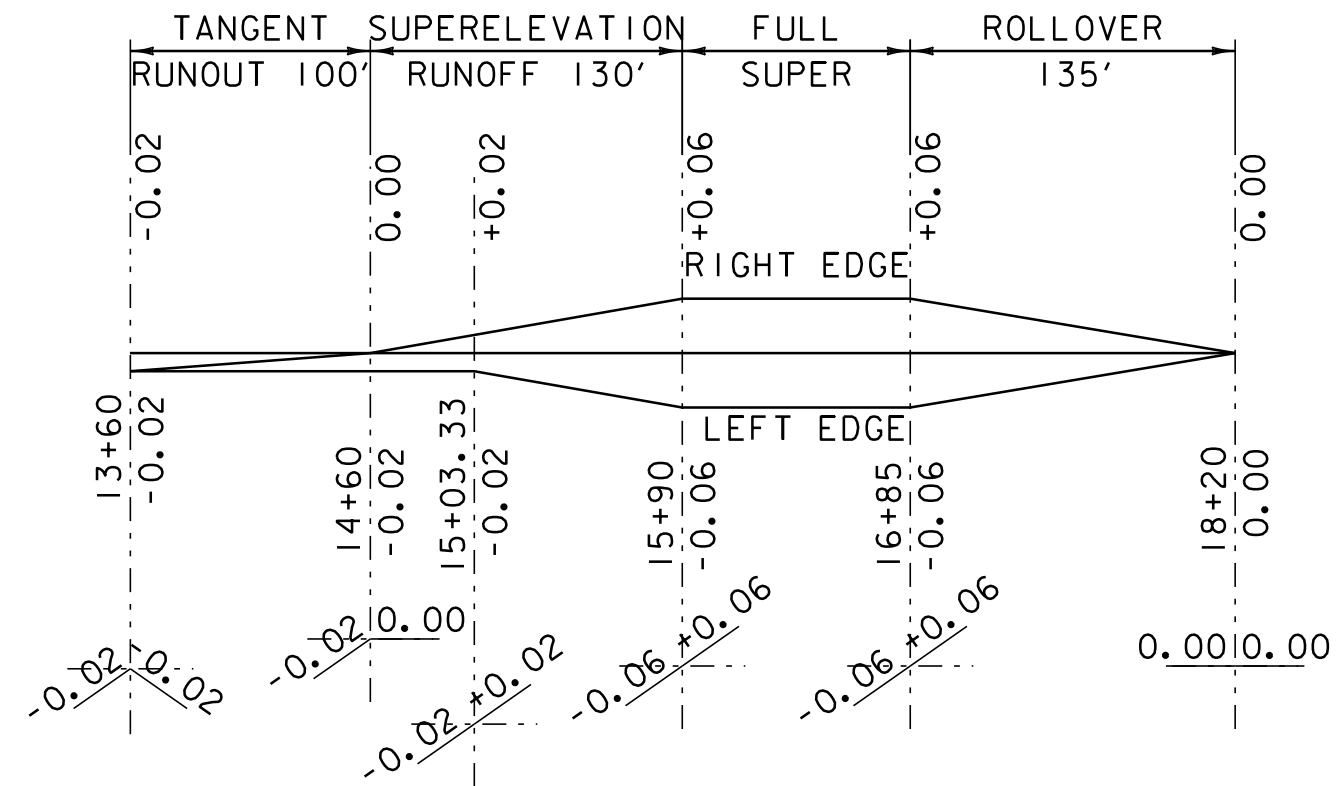
SUPERELEVATION DIAGRAM CURVE #6

NTS
 PC = 26+87.86 R = 1250 FT - LT
 PT = 29+93.74 V = 40 MPH



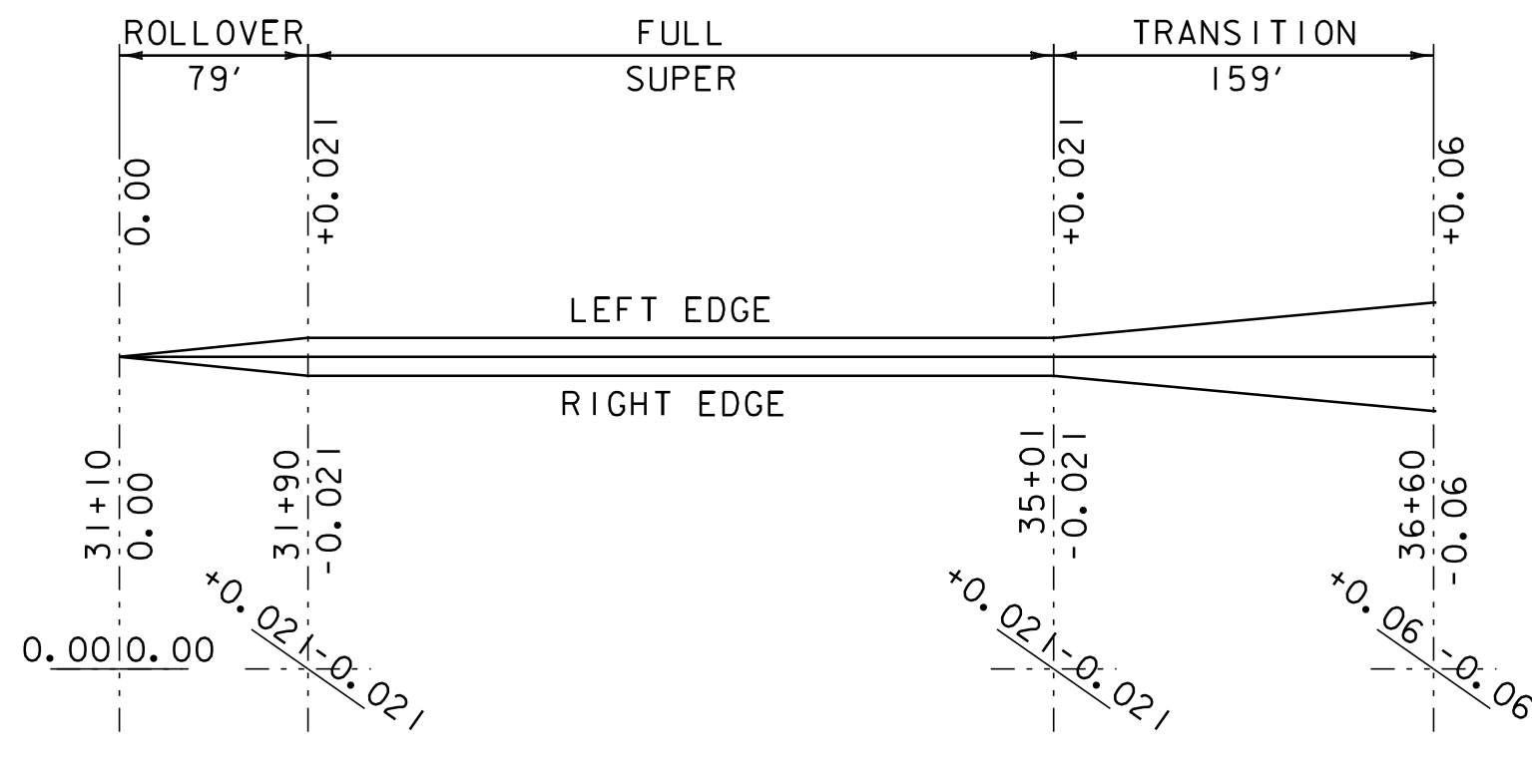
SUPERELEVATION DIAGRAM CURVE #10

NTS
 PC = 50+81.83 R = 750 FT - RT
 PT = 60+05.24 V = 40 MPH



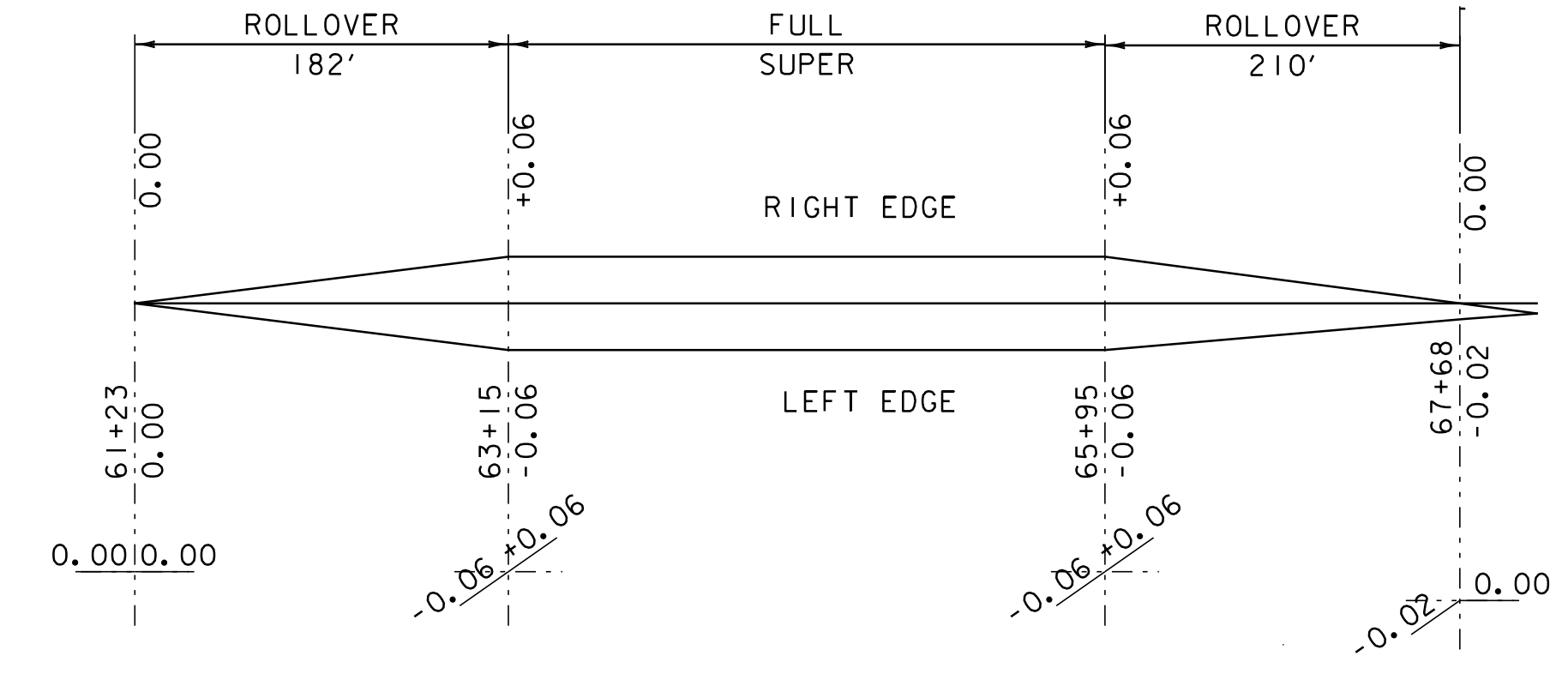
SUPERELEVATION DIAGRAM CURVE #3

NTS
 PC = 15+65.62 R = 400 FT - LT
 PT = 17+11.26 V = 40 MPH



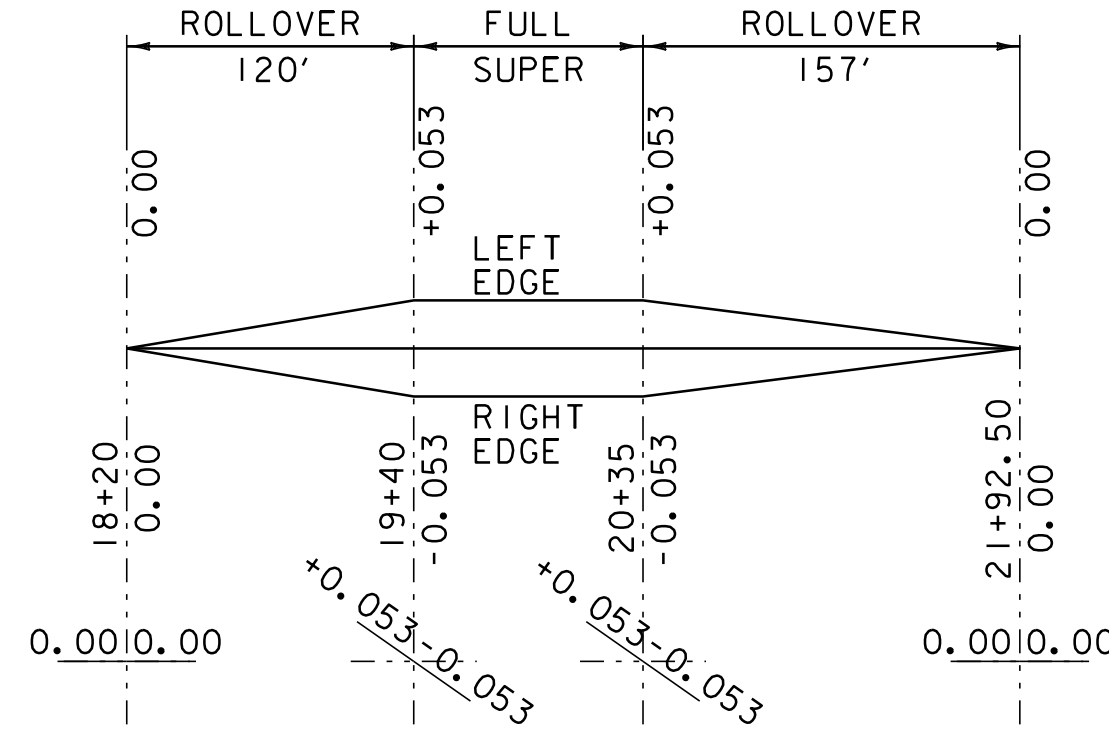
SUPERELEVATION DIAGRAM CURVE #7

NTS
 PC = 31+54.80 R = 3500 FT - RT
 PT = 35+01.34 V = 40 MPH



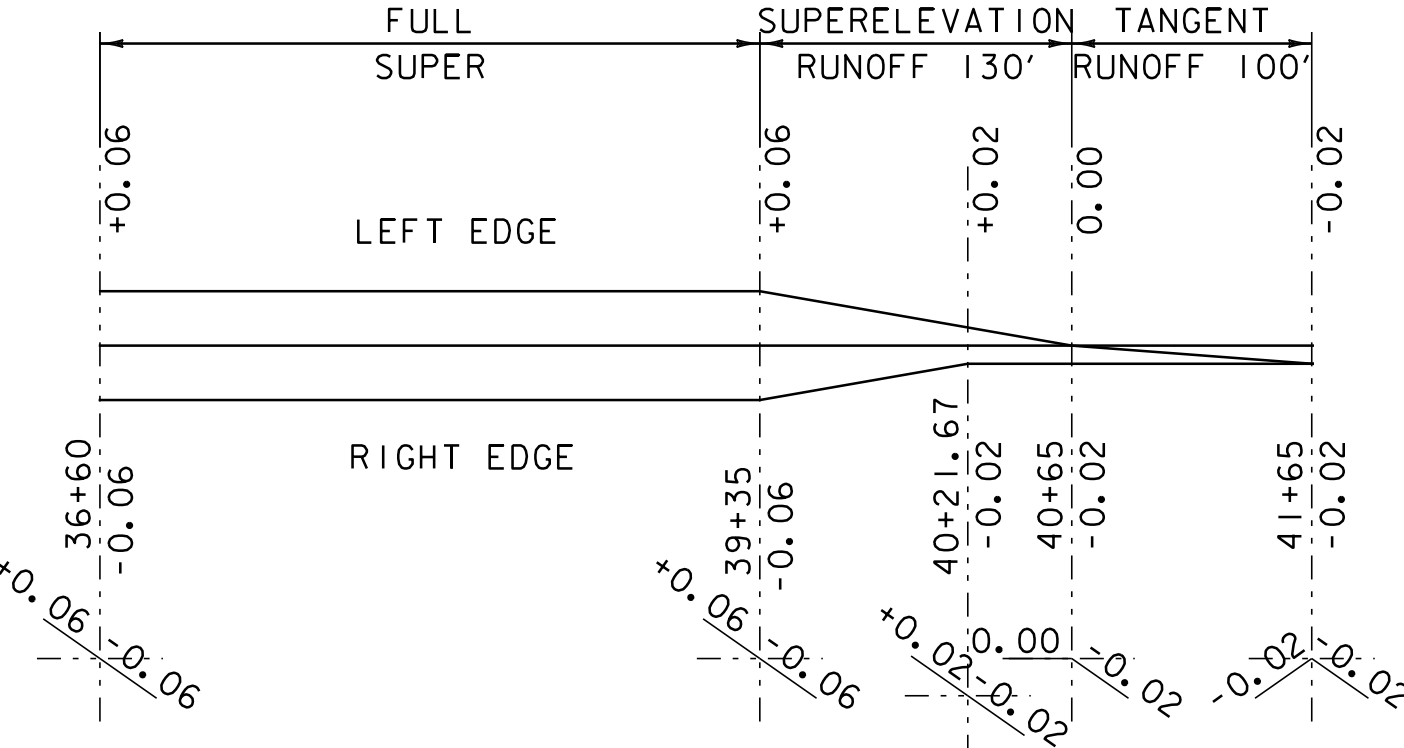
SUPERELEVATION DIAGRAM CURVE #11

NTS
 PC = 62+64.45 R = 450 FT - LT
 PT = 66+35.78 V = 40 MPH



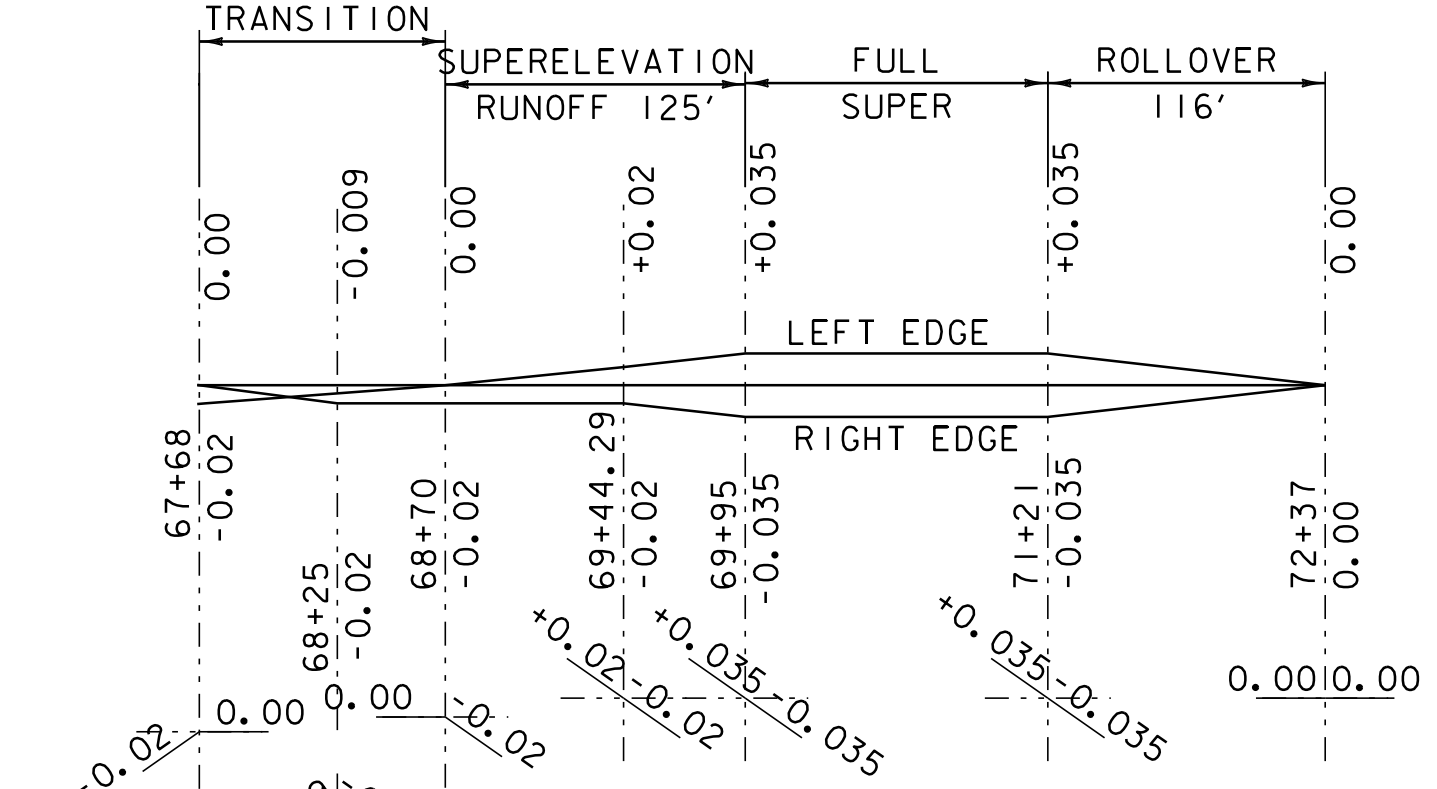
SUPERELEVATION DIAGRAM CURVE #4

NTS
 PC = 19+40.00 R = 800 FT - RT
 PT = 20+35.54 V = 40 MPH



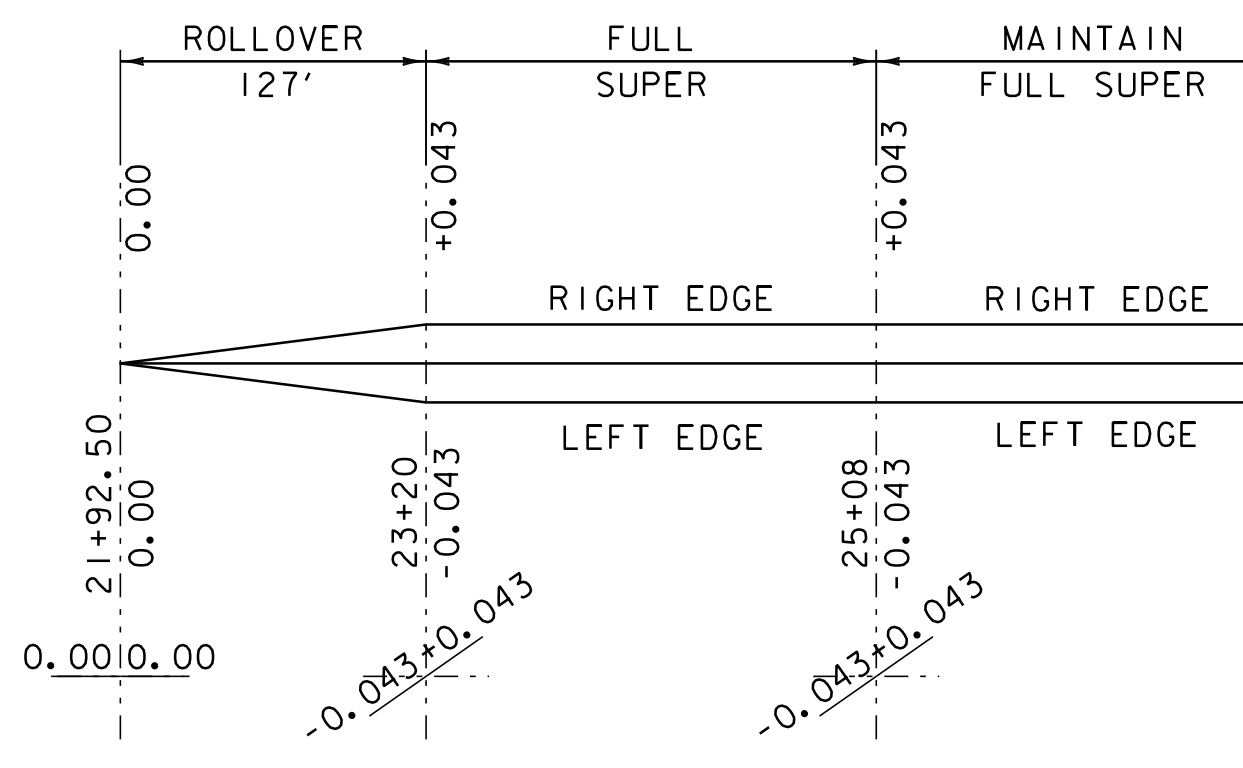
SUPERELEVATION DIAGRAM CURVE #8

NTS
 PC = 36+22.92 R = 500 FT - RT
 PT = 39+73.66 V = 40 MPH



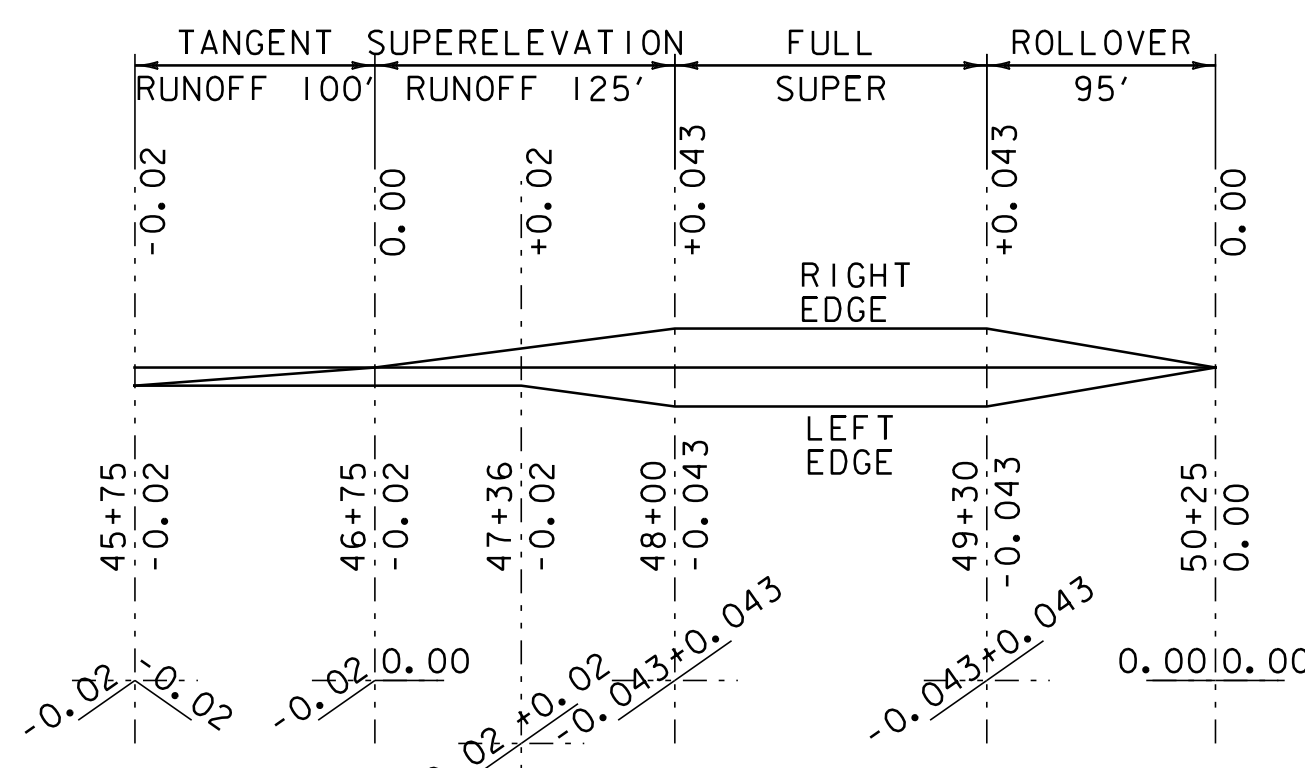
SUPERELEVATION DIAGRAM CURVE #12

NTS
 PC = 69+42.66 R = 400 FT - RT
 PT = 71+21.10 V = 40 MPH



SUPERELEVATION DIAGRAM CURVE #5

NTS
 PC = 22+80.67 R = 1250 FT - LT
 PT = 25+07.61 V = 40 MPH

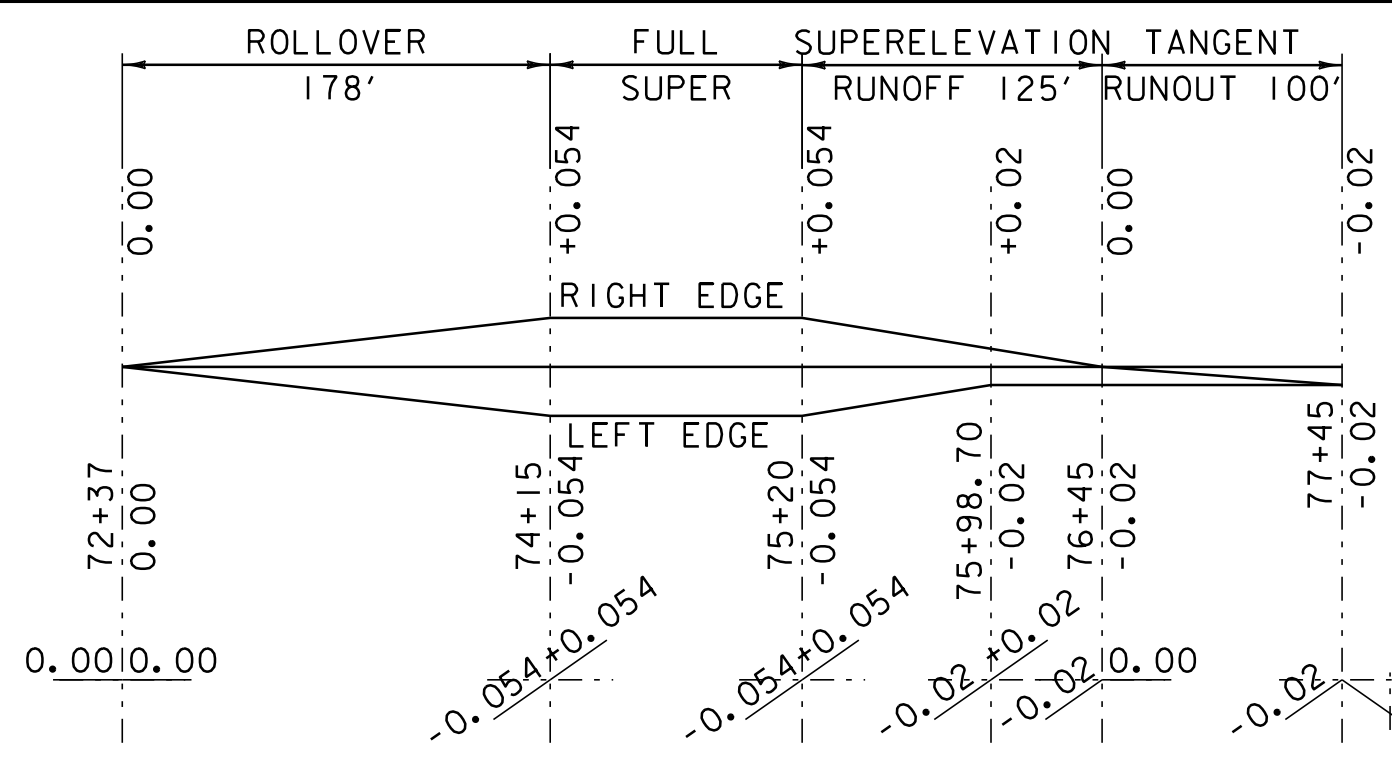


SUPERELEVATION DIAGRAM CURVE #9

NTS
 PC = 47+86.50 R = 1250 FT - LT
 PT = 49+79.51 V = 40 MPH

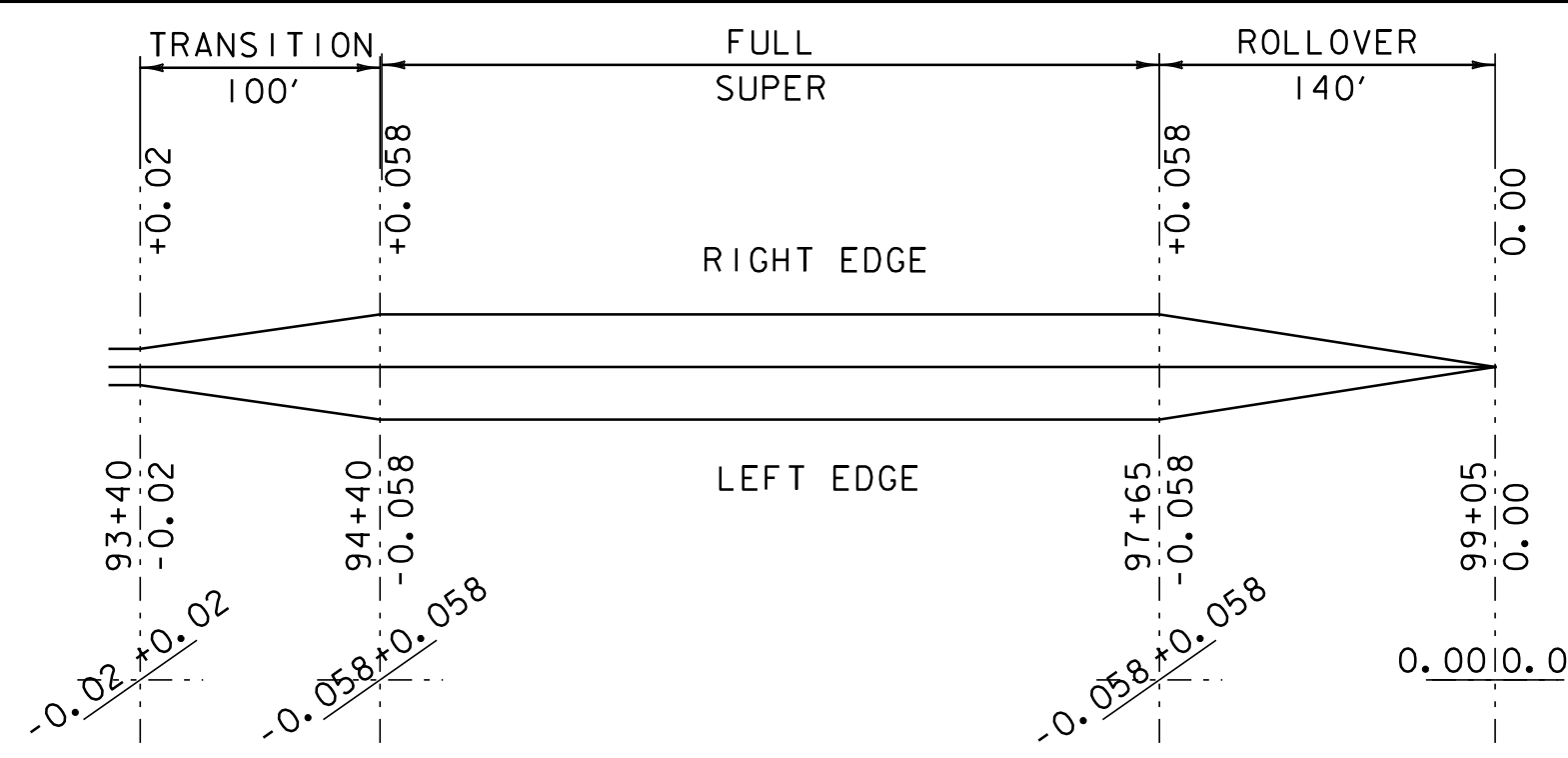
SUPERELEVATION BANKING DIAGRAM SHEET #1

PROJECT NAME: CHelsea-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
PROJECT NUMBER: STP 2331(I)S	DRAWN BY: SJL
FILE NAME: 01c040.DGN	CHECKED BY: CDL
DESIGNED BY: SJL	SHEET 31 OF 55
PLOT FILE: 01c040_31.1	



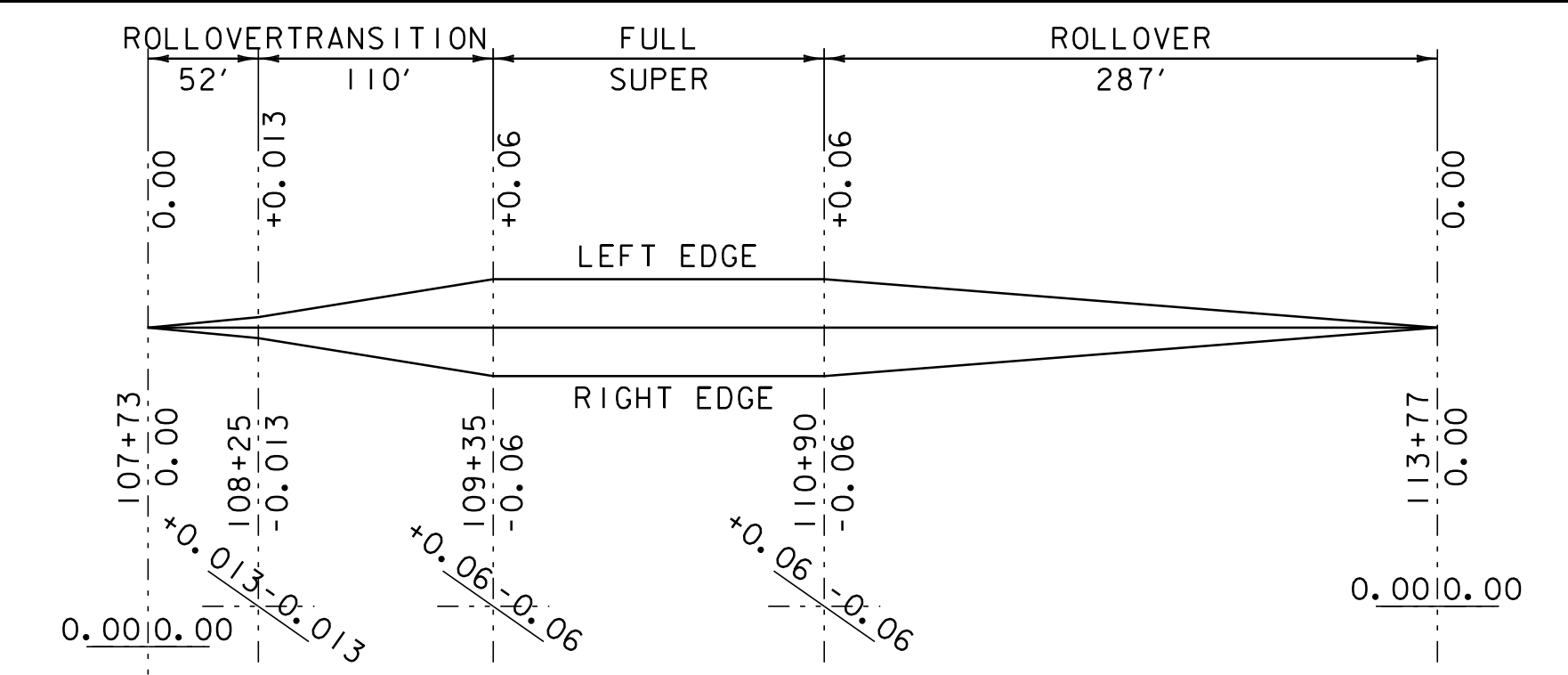
SUPERELEVATION DIAGRAM CURVE #13

NTS
 PC = 73+79.49 R = 800 FT - LT
 PT = 75+58.57 V = 40 MPH



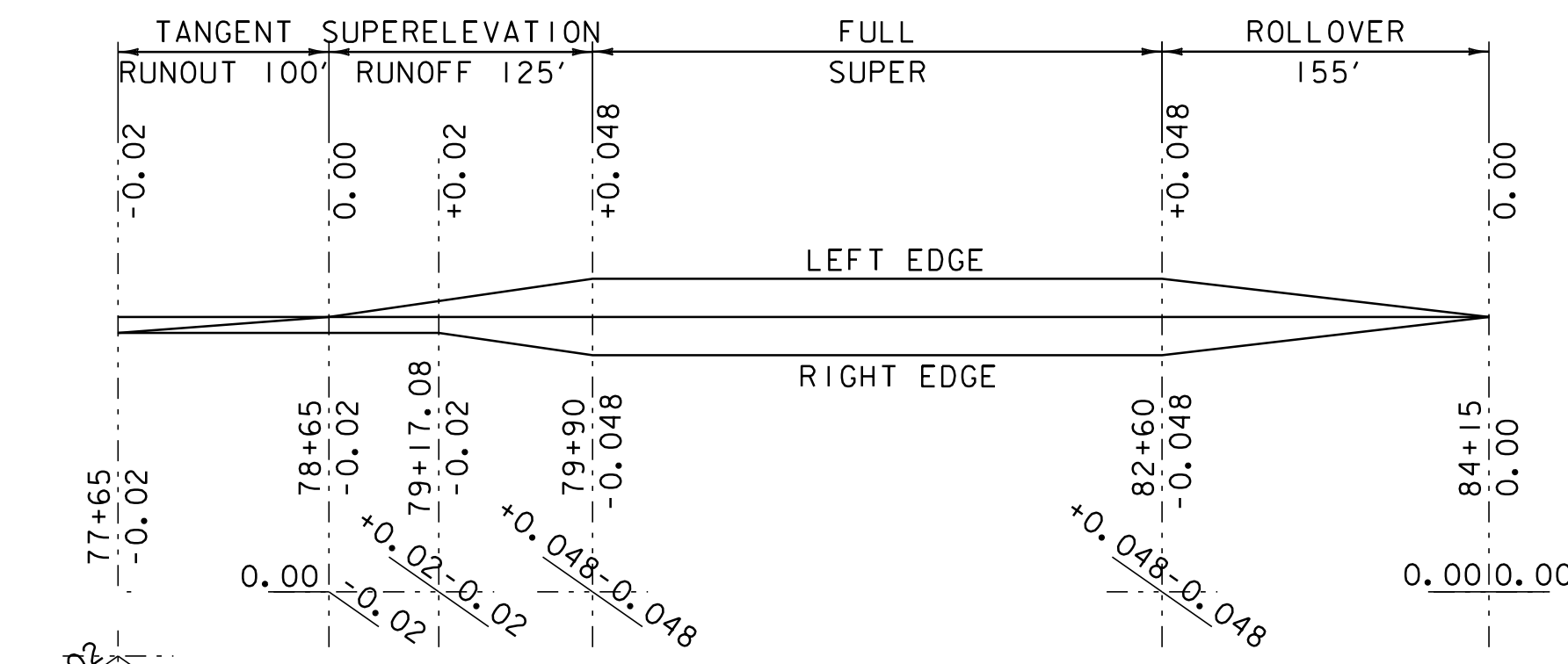
SUPERELEVATION DIAGRAM CURVE #16

NTS
 PC = 94+78.10 R = 600 FT - LT
 PT = 98+02.11 V = 40 MPH



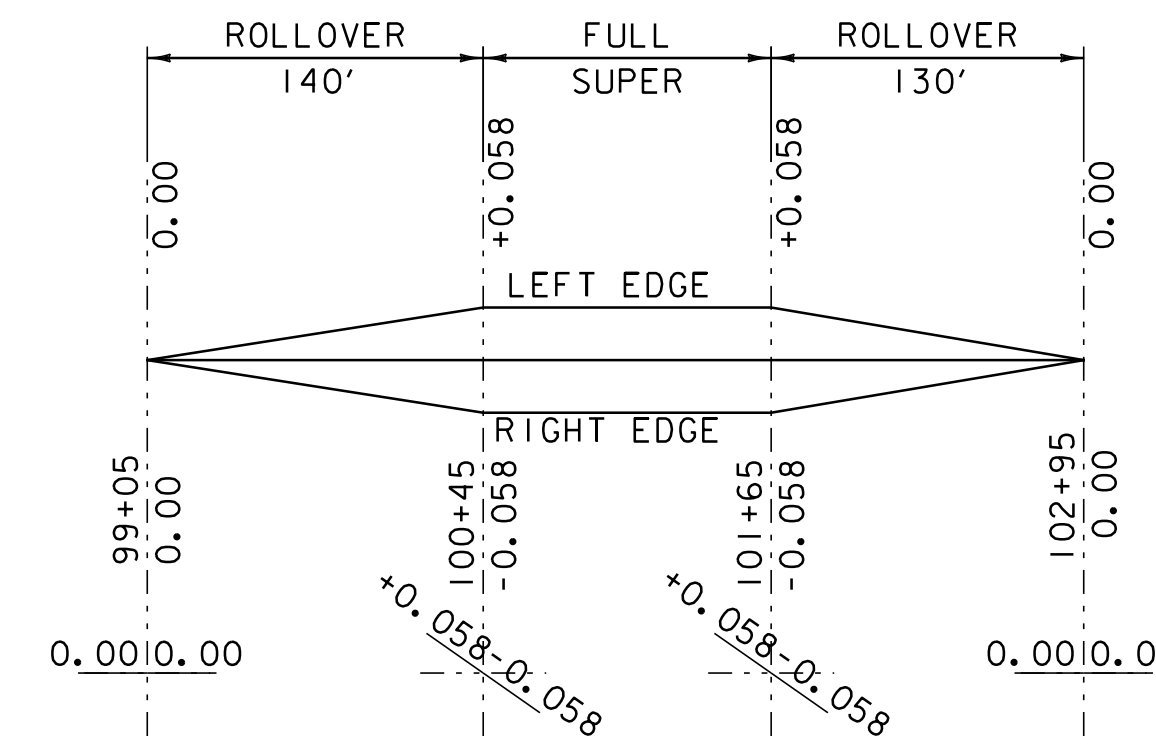
SUPERELEVATION DIAGRAM CURVE #19

NTS
 PC = 108+83.31 R = 500 FT - RT
 PT = 111+40.69 V = 40 MPH



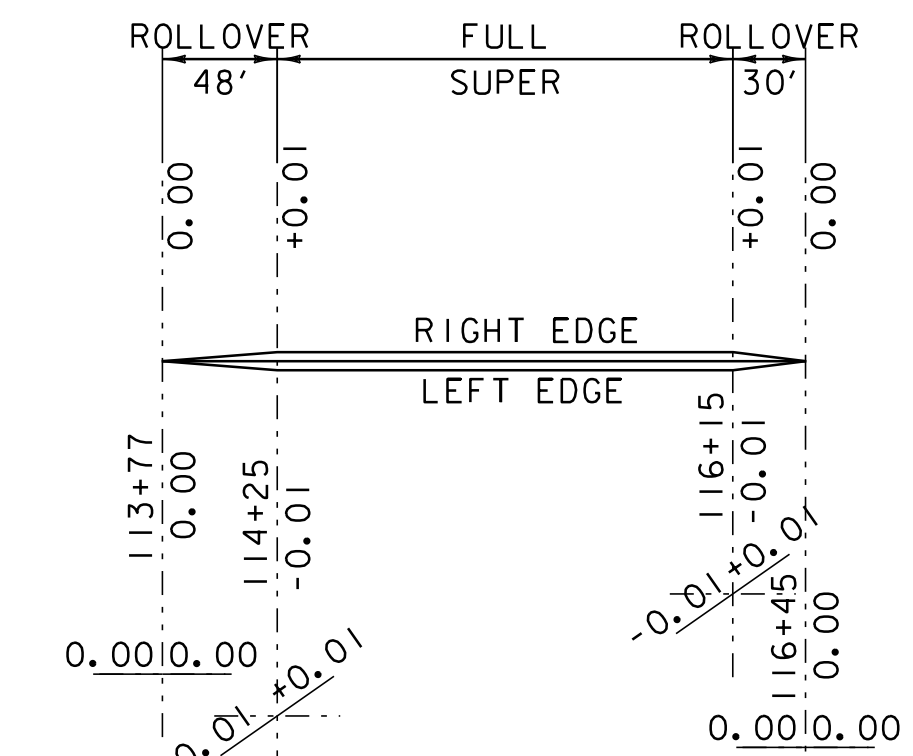
SUPERELEVATION DIAGRAM CURVE #14

NTS
 PC = 79+53.66 R = 1000 FT - RT
 PT = 82+95.98 V = 40 MPH



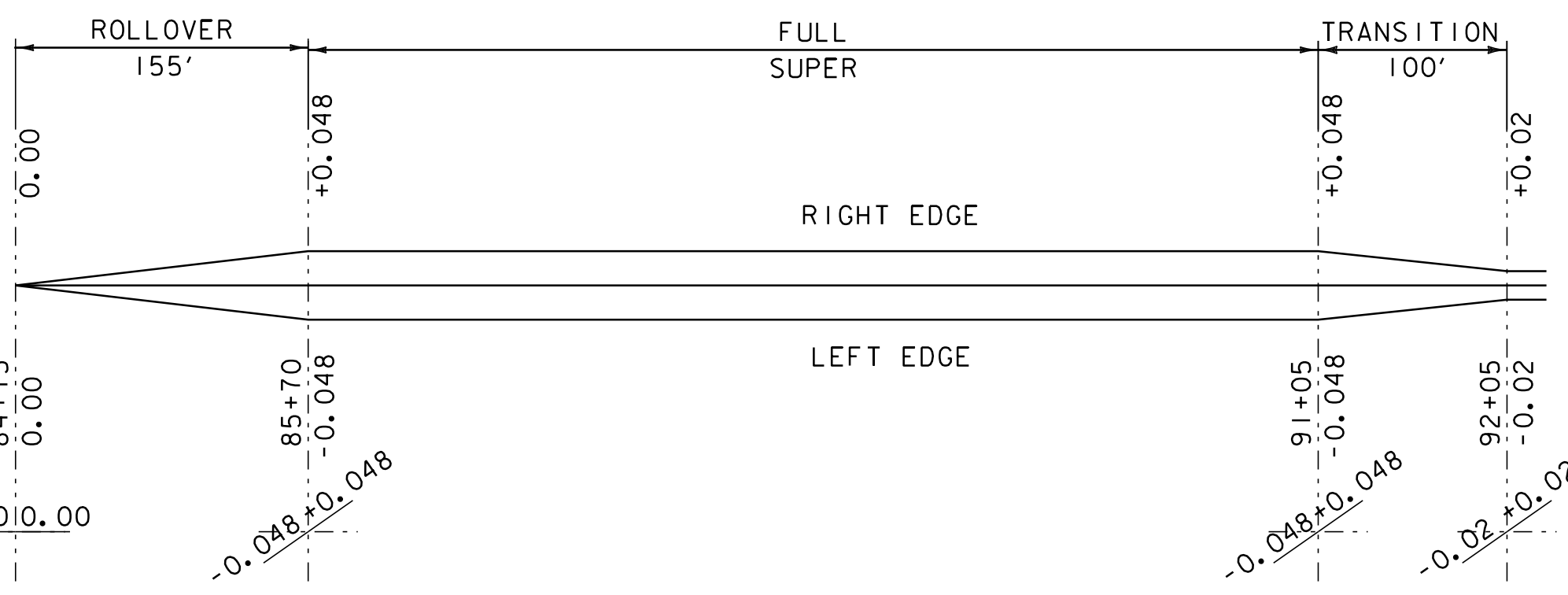
SUPERELEVATION DIAGRAM CURVE #17

NTS
 PC = 100+07.59 R = 600 FT - RT
 PT = 102+05.02 V = 40 MPH



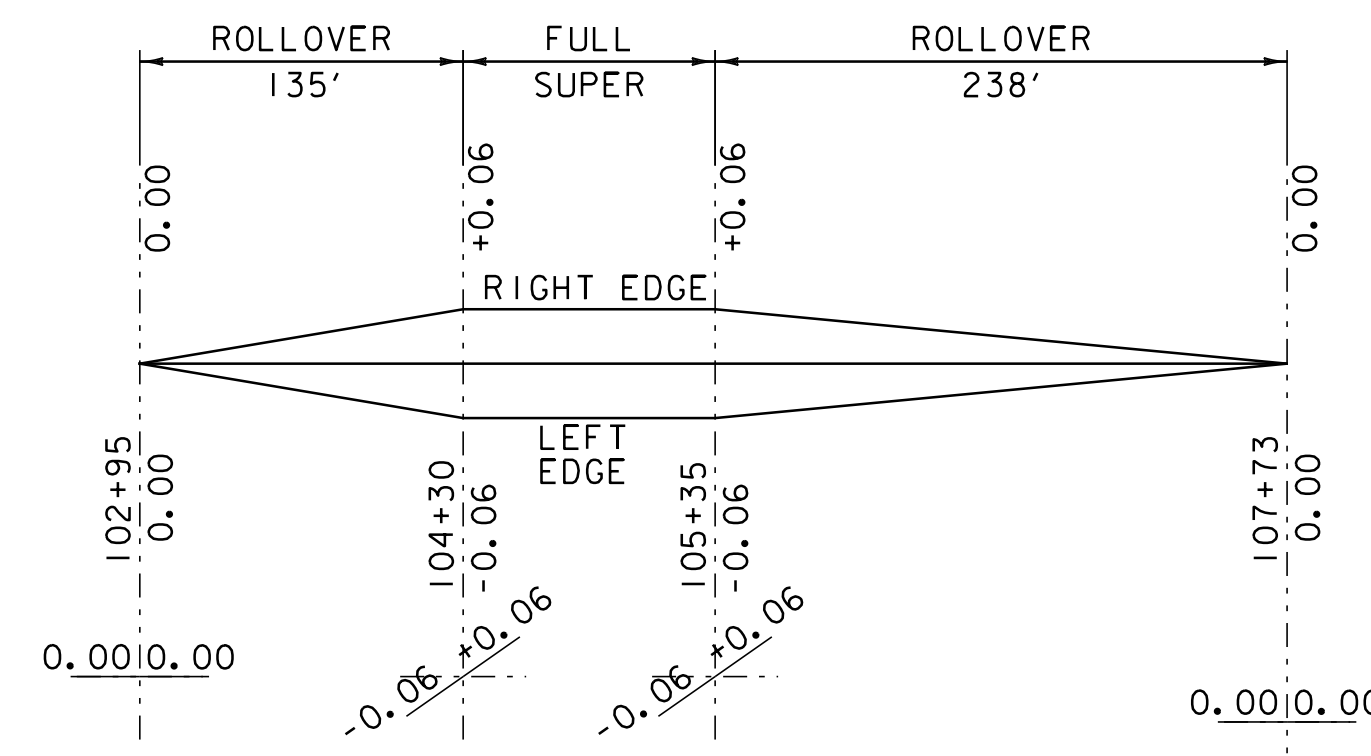
SUPERELEVATION DIAGRAM CURVE #20

NTS
 PC = 115+44.19 R = 300 FT - LT
 PT = 116+54.92 V = 40 MPH



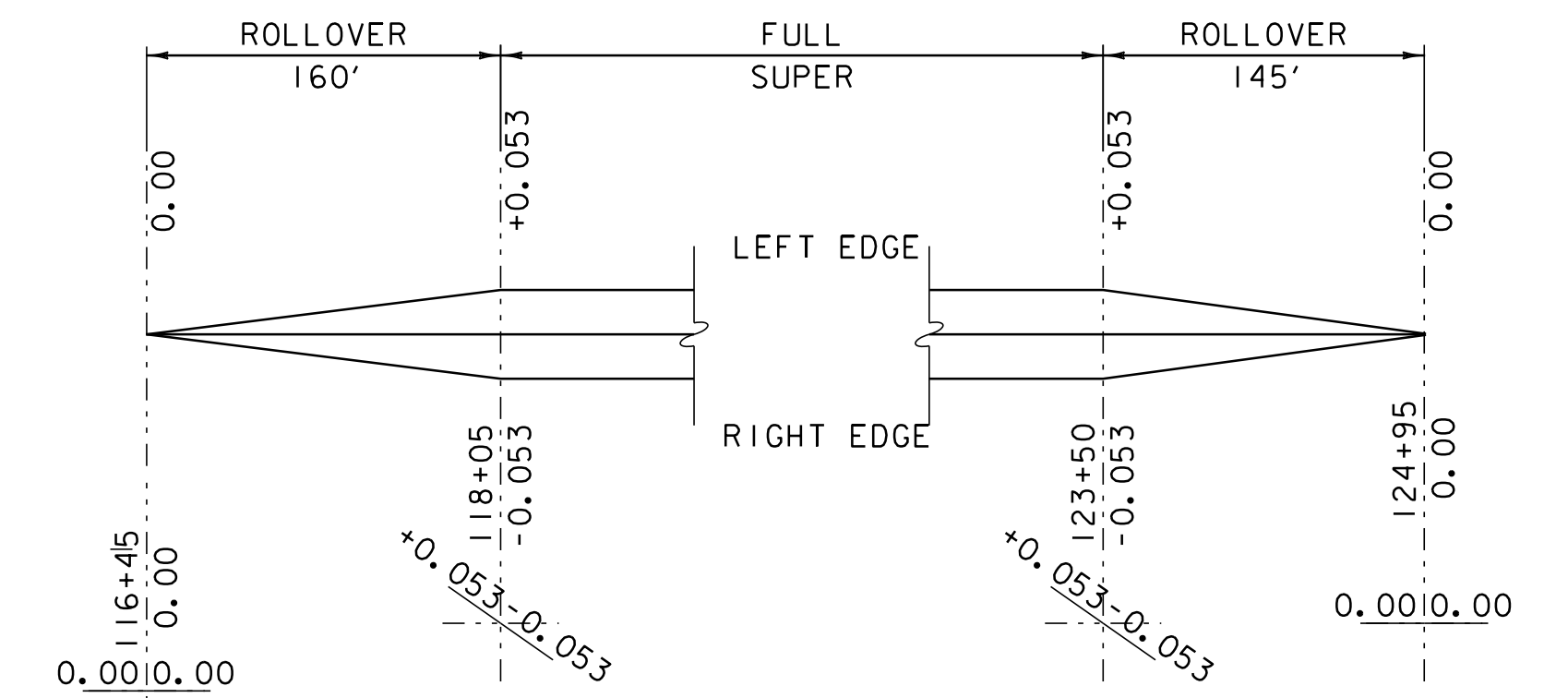
SUPERELEVATION DIAGRAM CURVE #15

NTS
 PC = 85+32.08 R = 1000 FT - LT
 PT = 91+42.51 V = 40 MPH



SUPERELEVATION DIAGRAM CURVE #18

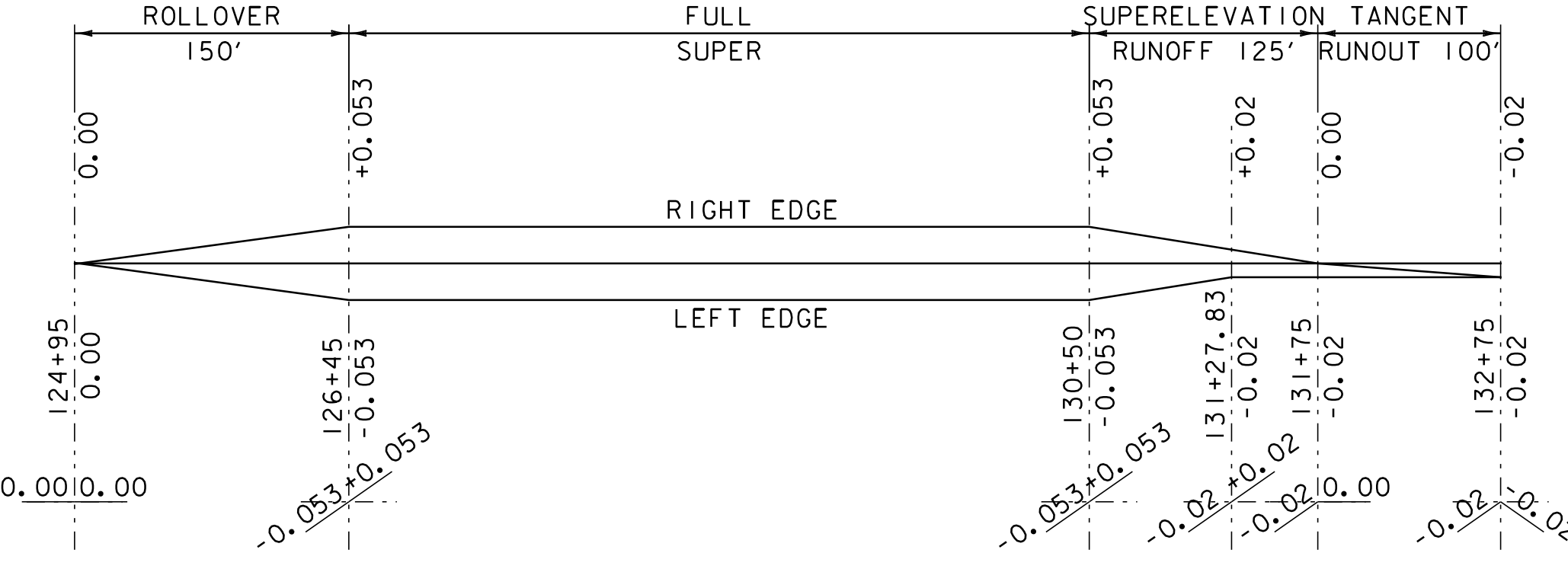
NTS
 PC = 103+90.94 R = 500 FT - LT
 PT = 105+72.13 V = 40 MPH



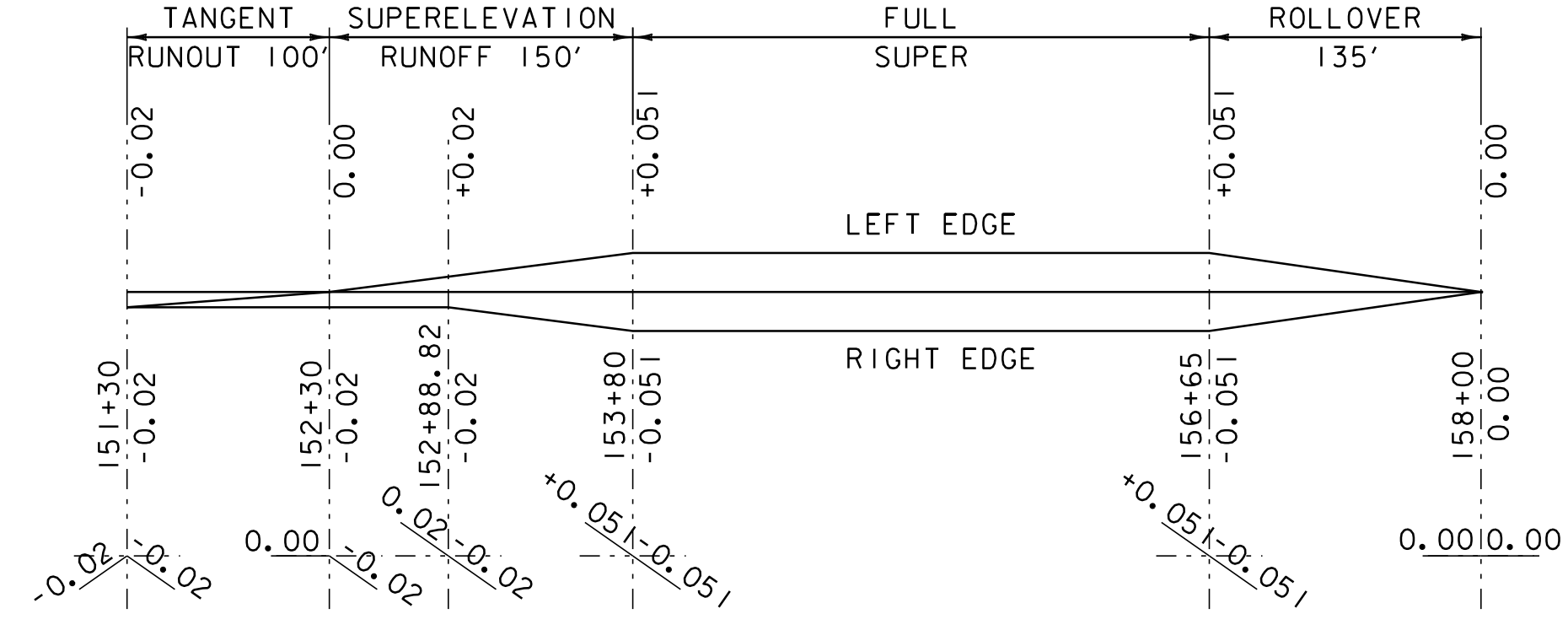
SUPERELEVATION DIAGRAM CURVE #21

NTS
 PC = 117+68.71 R = 800 FT - RT
 PT = 123+89.87 V = 40 MPH

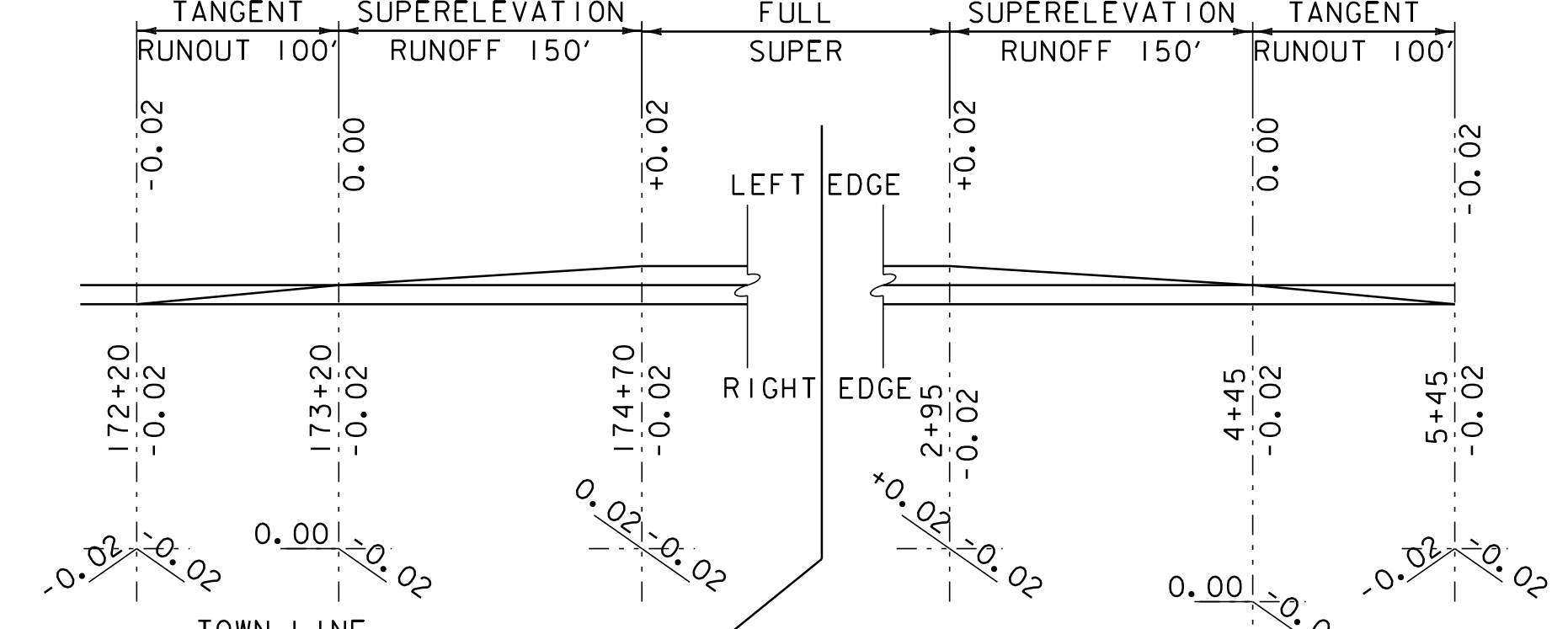
SUPERELEVATION BANKING DIAGRAM SHEET #2	PROJECT NAME: CHelsea-VERSHIRE	
	PROJECT NUMBER: STP 2331(I)S	
	FILE NAME: 01c040.DGN	PLOT DATE: 28-OCT-2011 3:36
	PROJECT LEADER: CDL	DRAWN BY: SJL
DESIGNED BY: SJL	CHECKED BY: CDL	
PLOT FILE: 01c040_32.i	SHEET 32 OF 55	



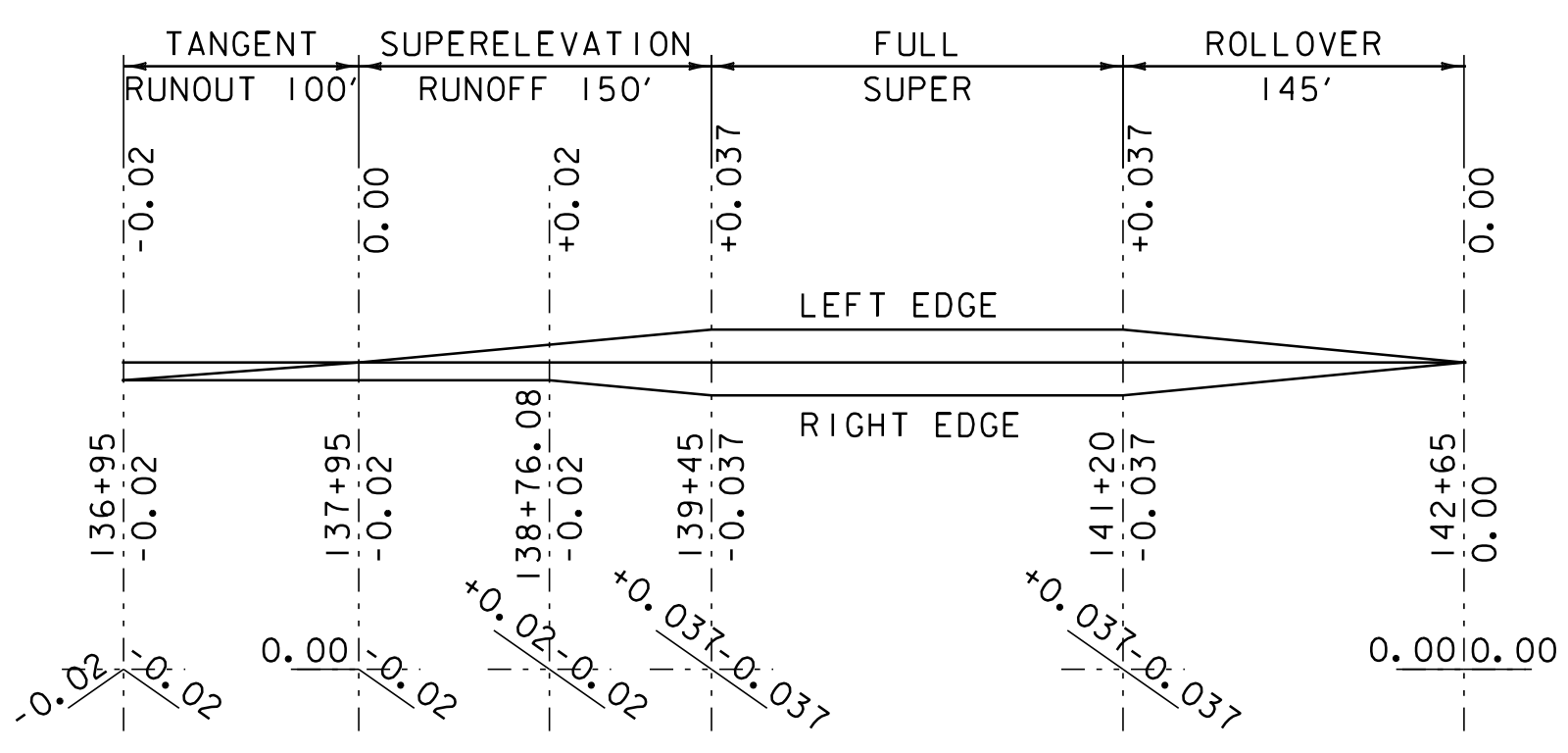
SUPERELEVATION DIAGRAM CURVE #22
 NTS
 PC = 126+04.62 R = 800 FT - LT
 PT = 130+86.96 V = 40 MPH



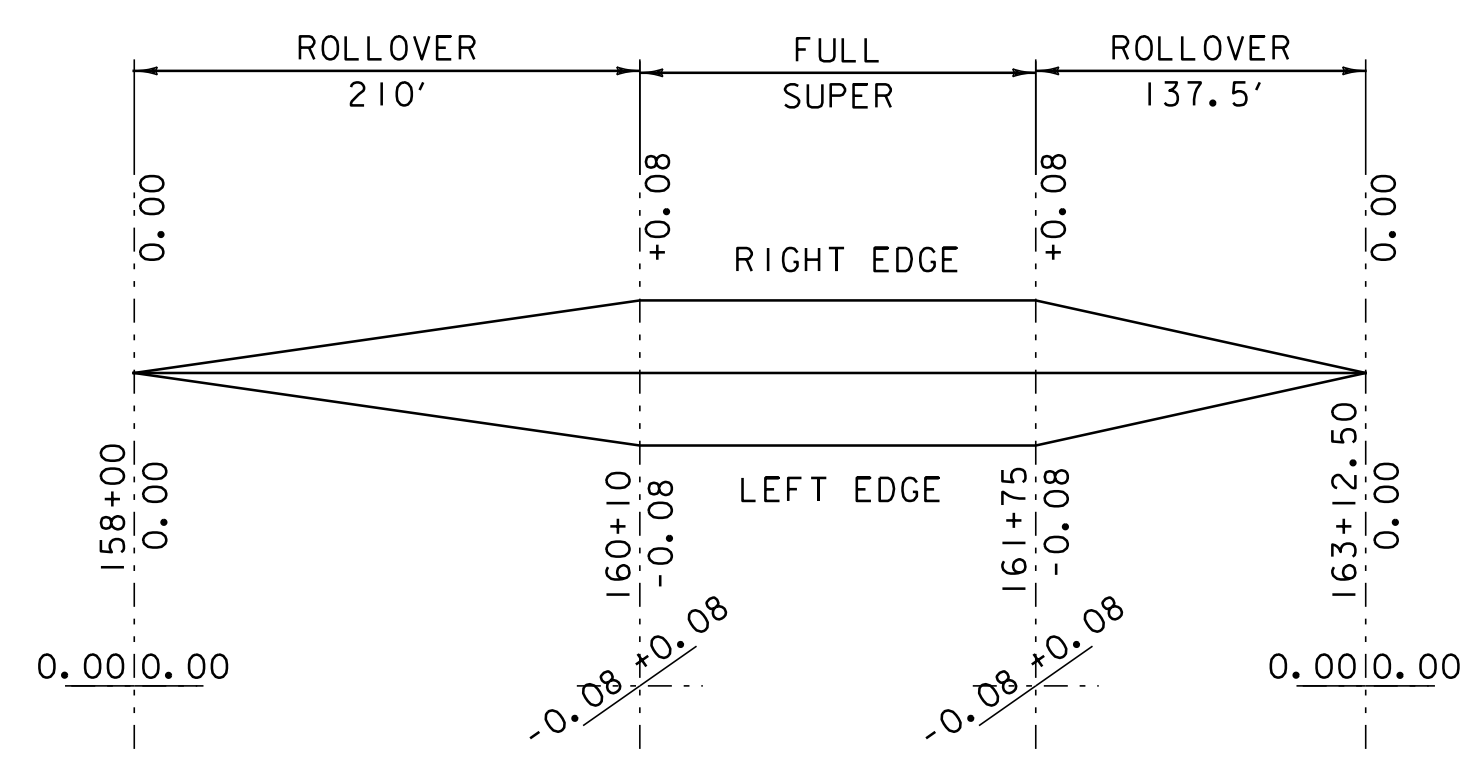
SUPERELEVATION DIAGRAM CURVE #25
 NTS
 PC = 153+33.64 R = 2000 FT - RT
 PT = 157+08.70 V = 50 MPH



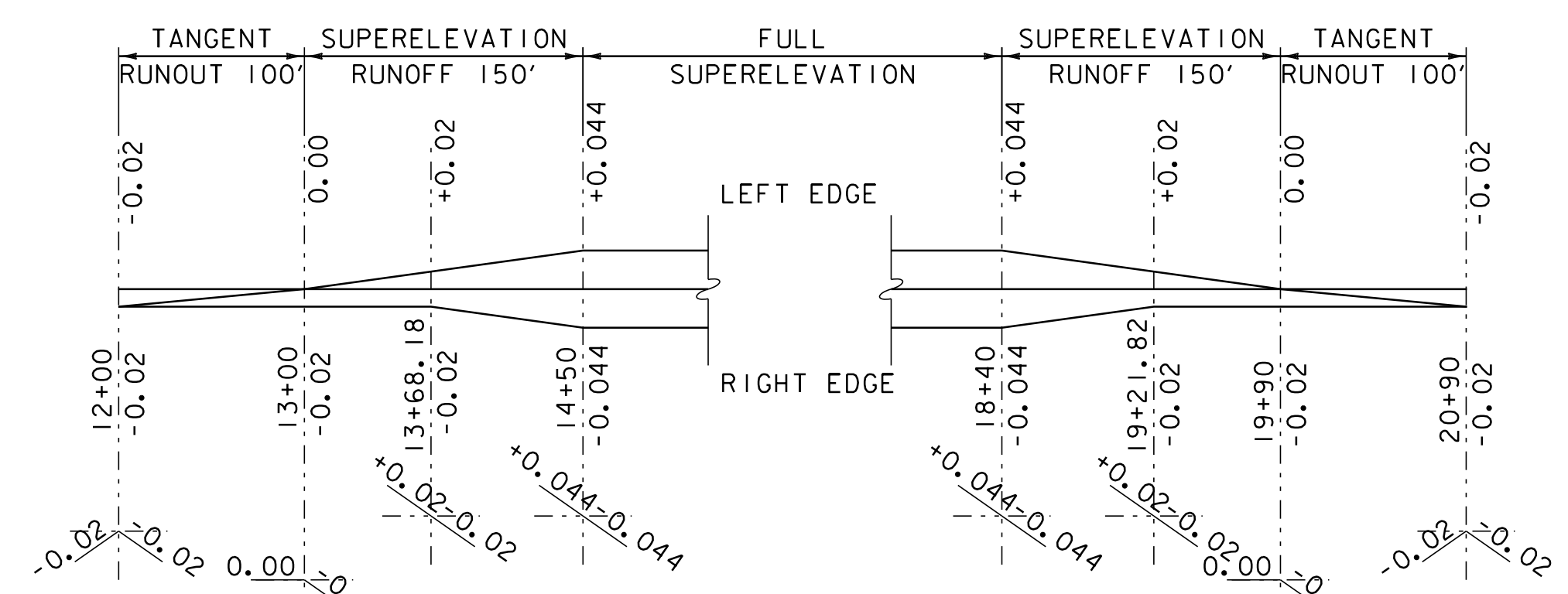
SUPERELEVATION DIAGRAM CURVE #28B
 NTS
 TOWN LINE
 STA. 181+63.20 = MM 0344
 STA. 0+00.00 = MM 0000
 PC = 174+26.72 R = 8000 FT - RT
 PT = 3+40.26 V = 50 MPH



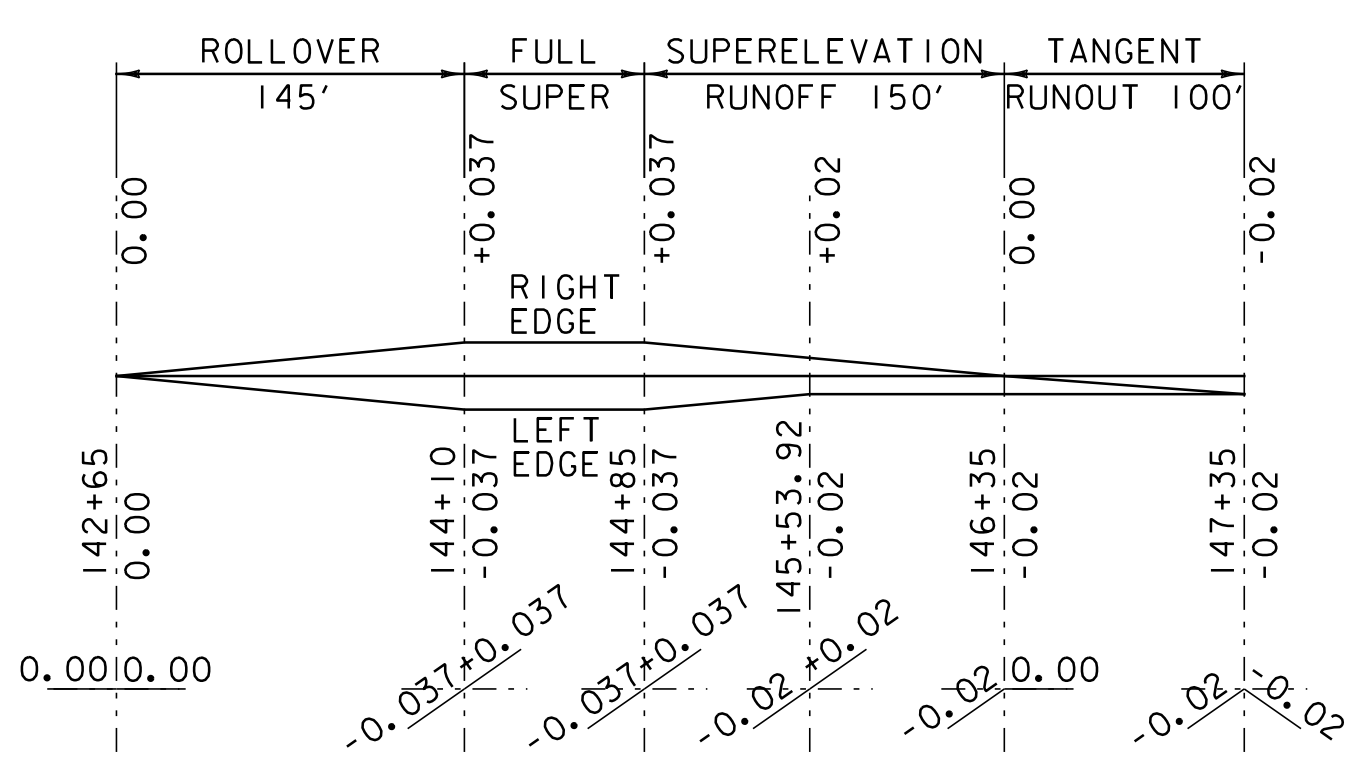
SUPERELEVATION DIAGRAM CURVE #23
 NTS
 PC = 139+02.23 R = 3000 FT - RT
 PT = 141+63.63 V = 50 MPH



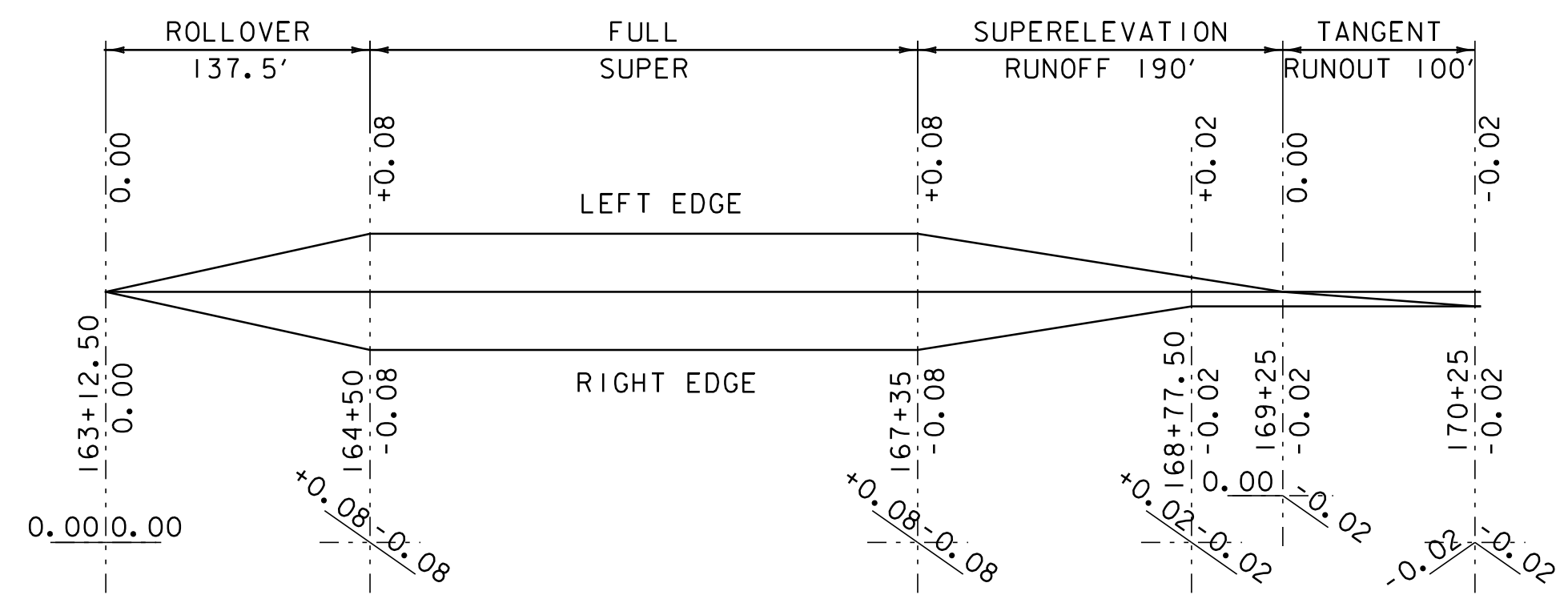
SUPERELEVATION DIAGRAM CURVE #26
 NTS
 PC = 159+54.12 R = 700 FT - LT
 PT = 162+30.85 V = 50 MPH



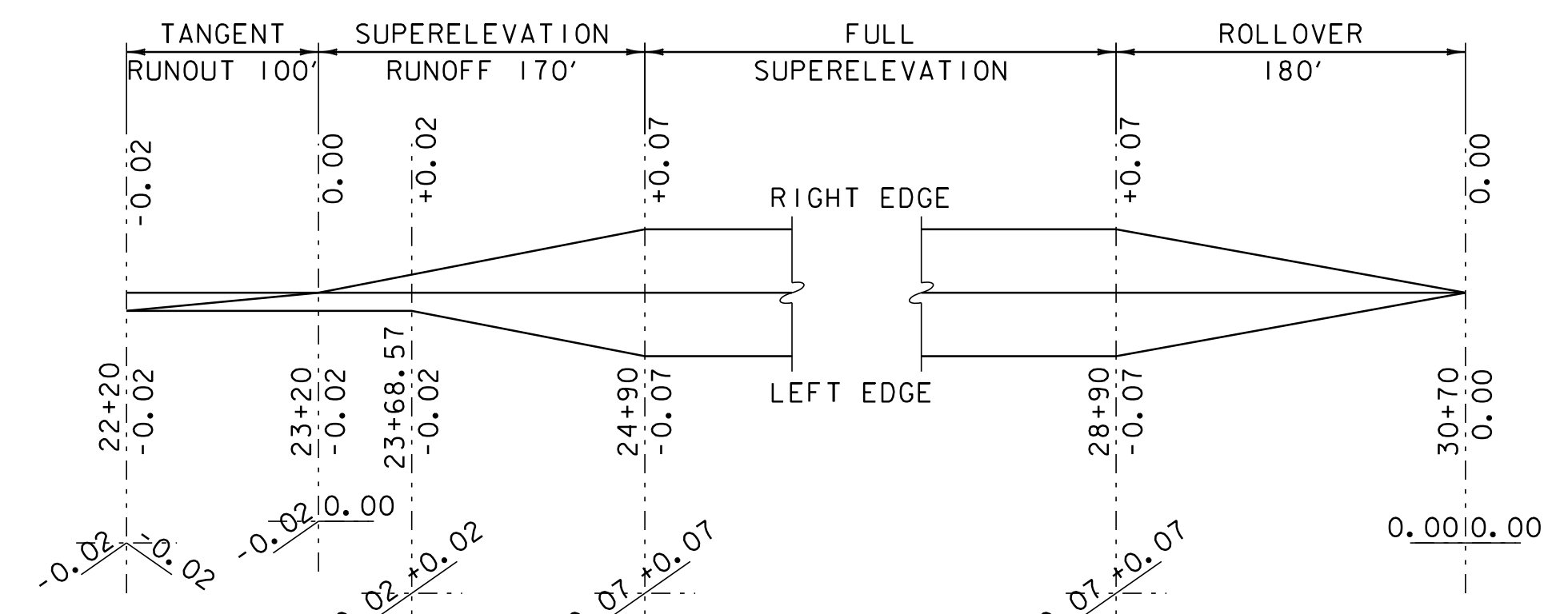
SUPERELEVATION DIAGRAM CURVE #29
 NTS
 PC = 14+07.13 R = 2500 FT - RT
 PT = 18+82.67 V = 50 MPH



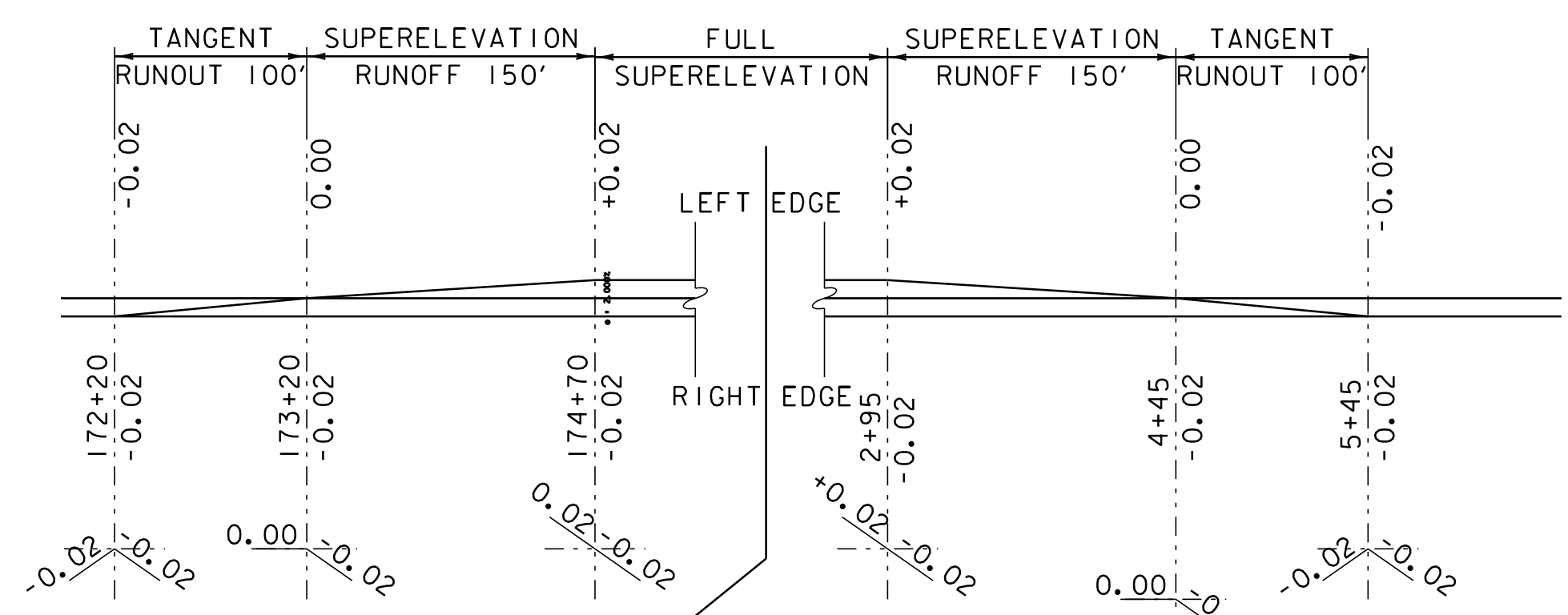
SUPERELEVATION DIAGRAM CURVE #24
 NTS
 PC = 143+81.89 R = 3000 FT - LT
 PT = 145+16.37 V = 50 MPH



SUPERELEVATION DIAGRAM CURVE #27
 NTS
 PC = 163+99.99 R = 700 FT - RT
 PT = 167+93.28 V = 50 MPH



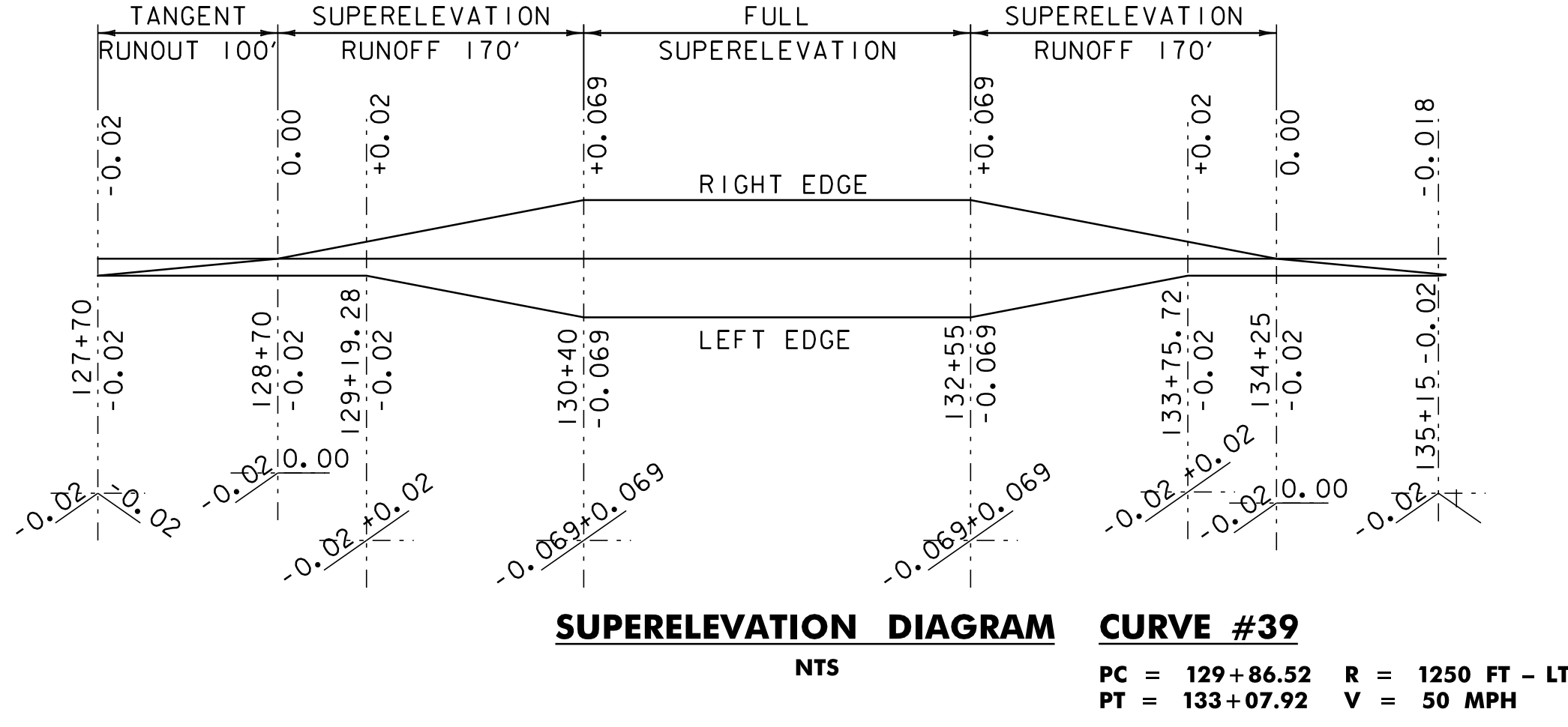
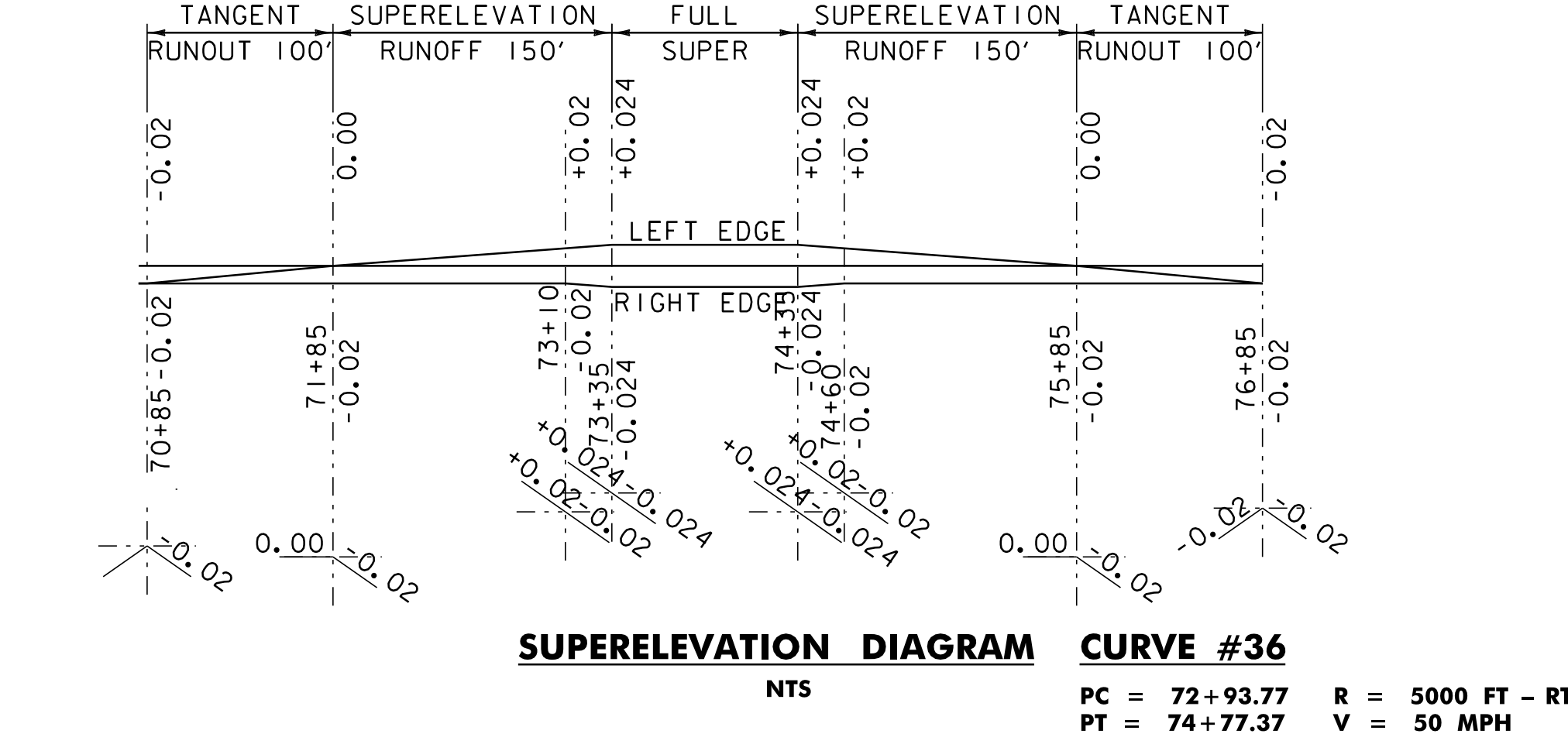
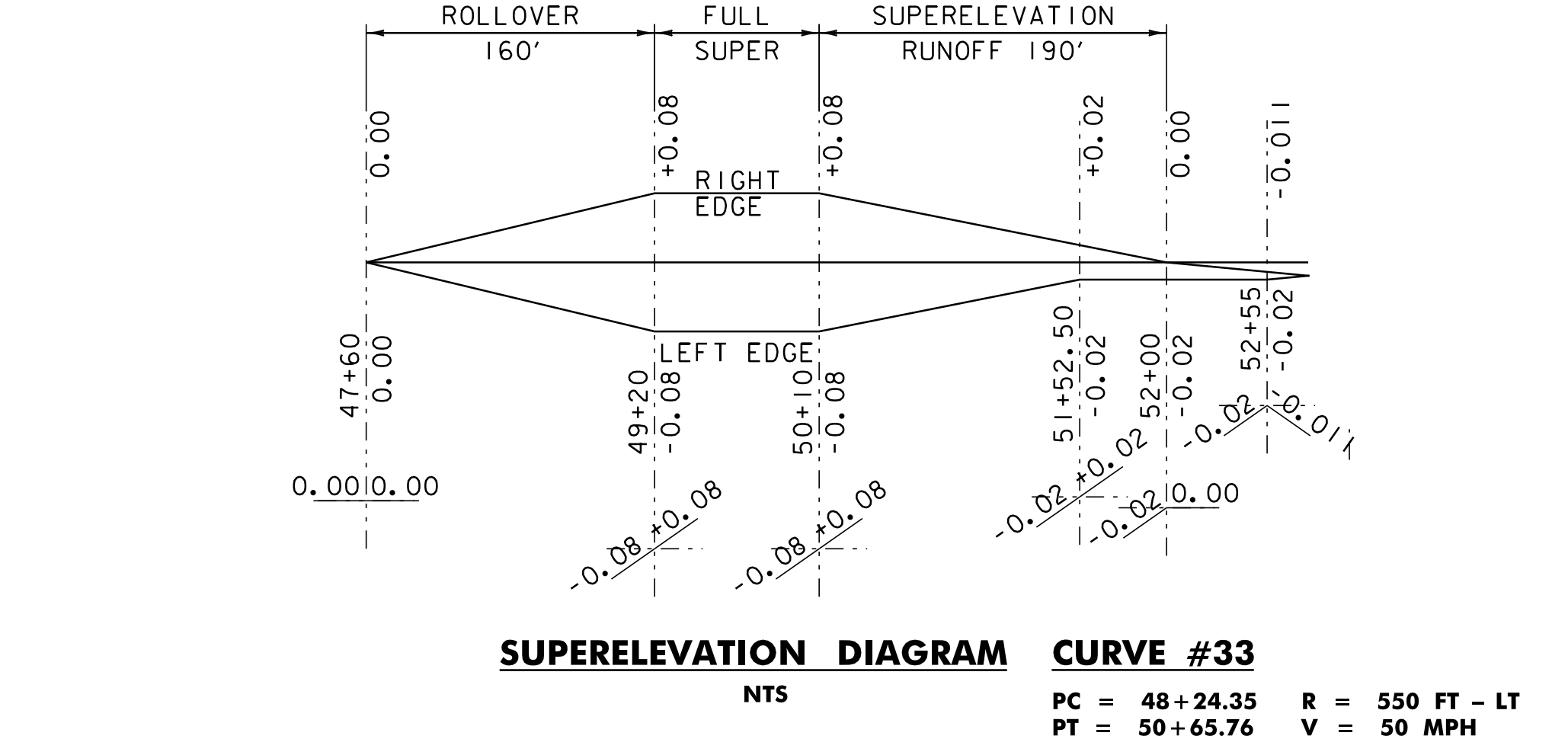
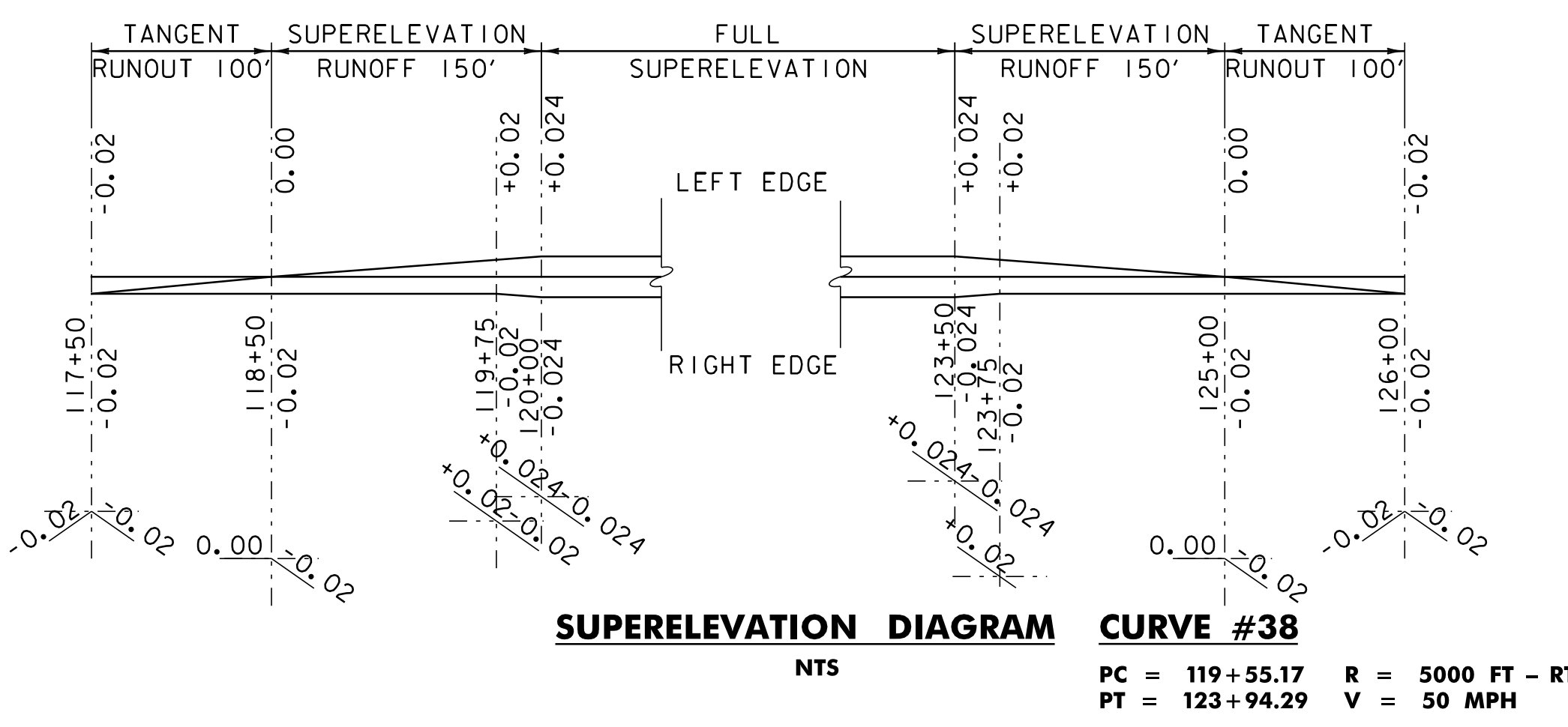
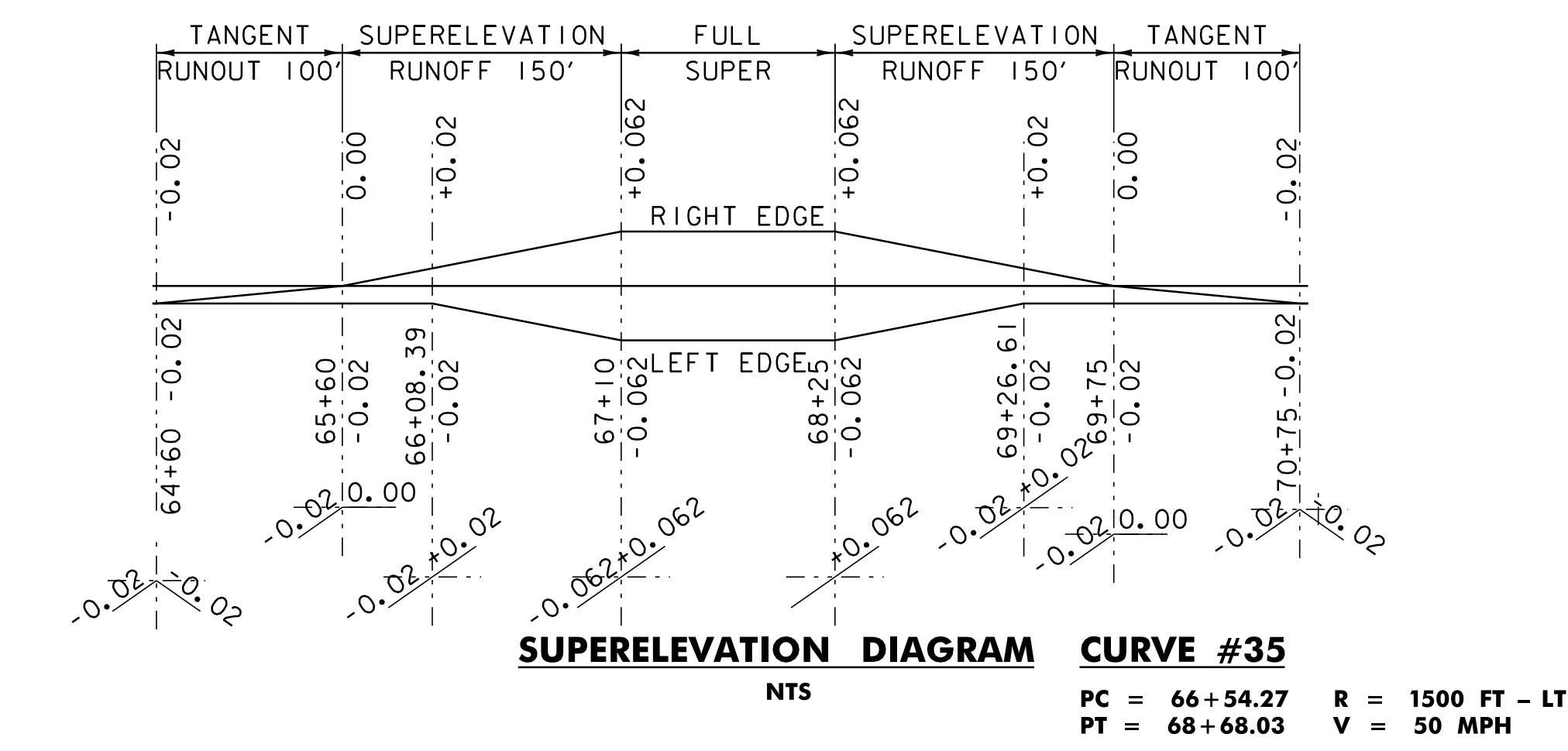
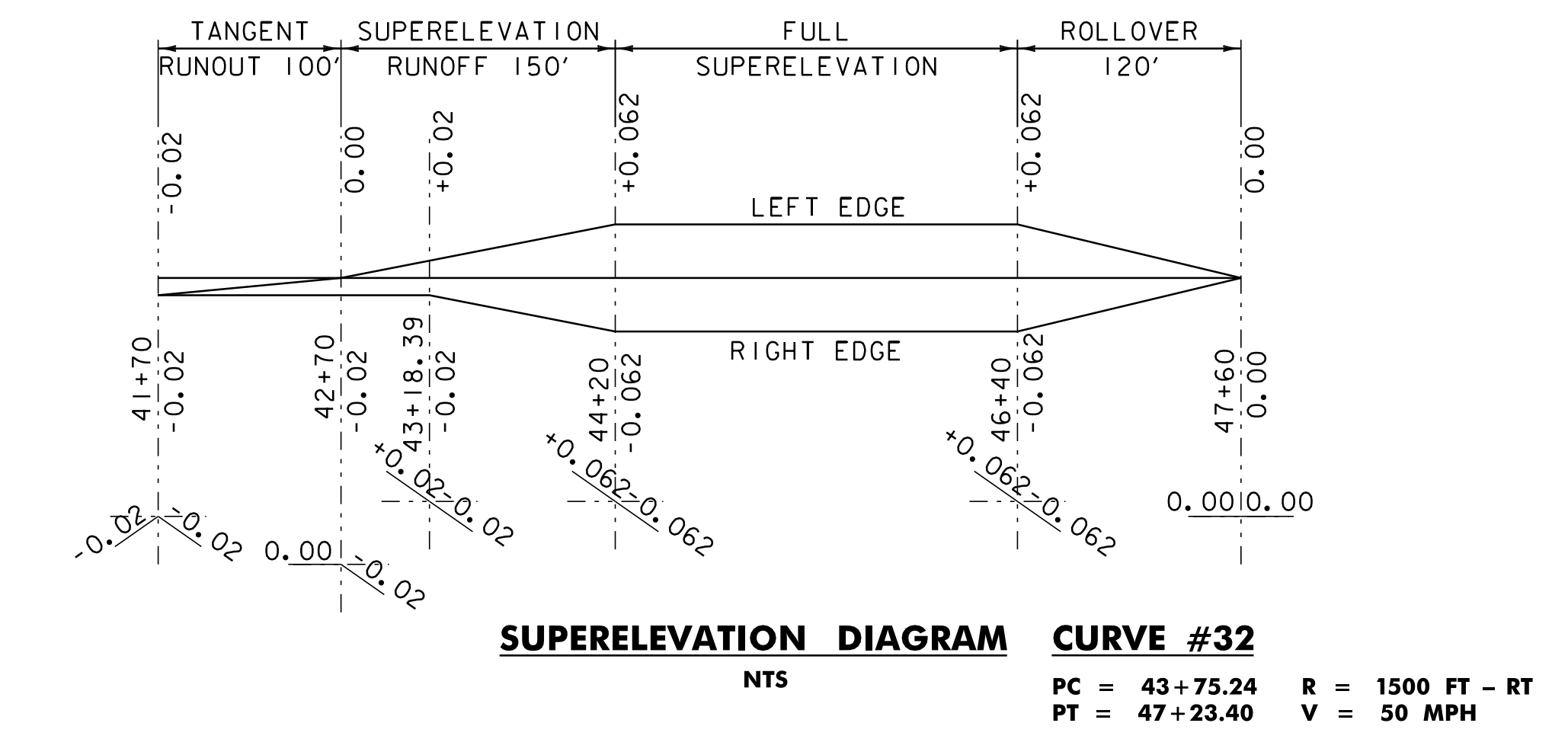
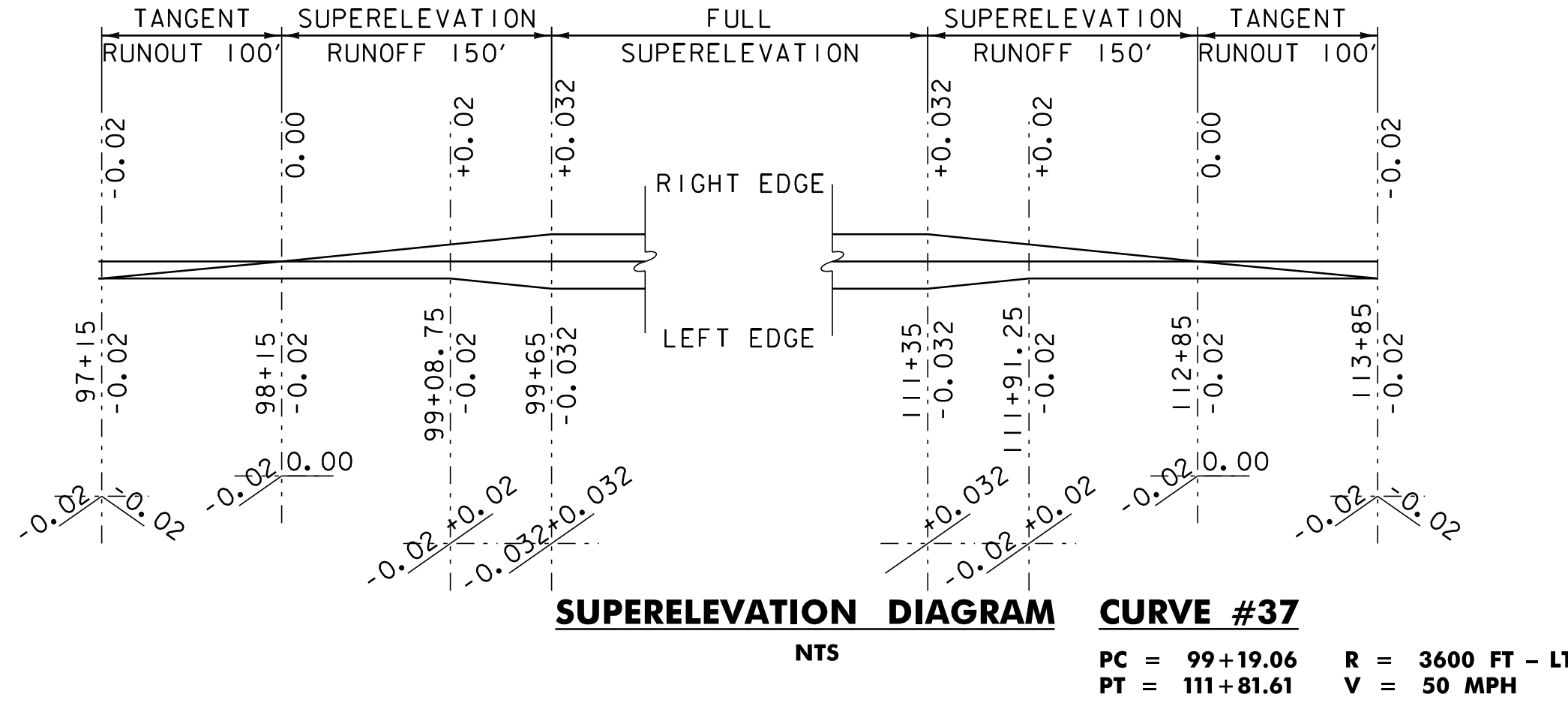
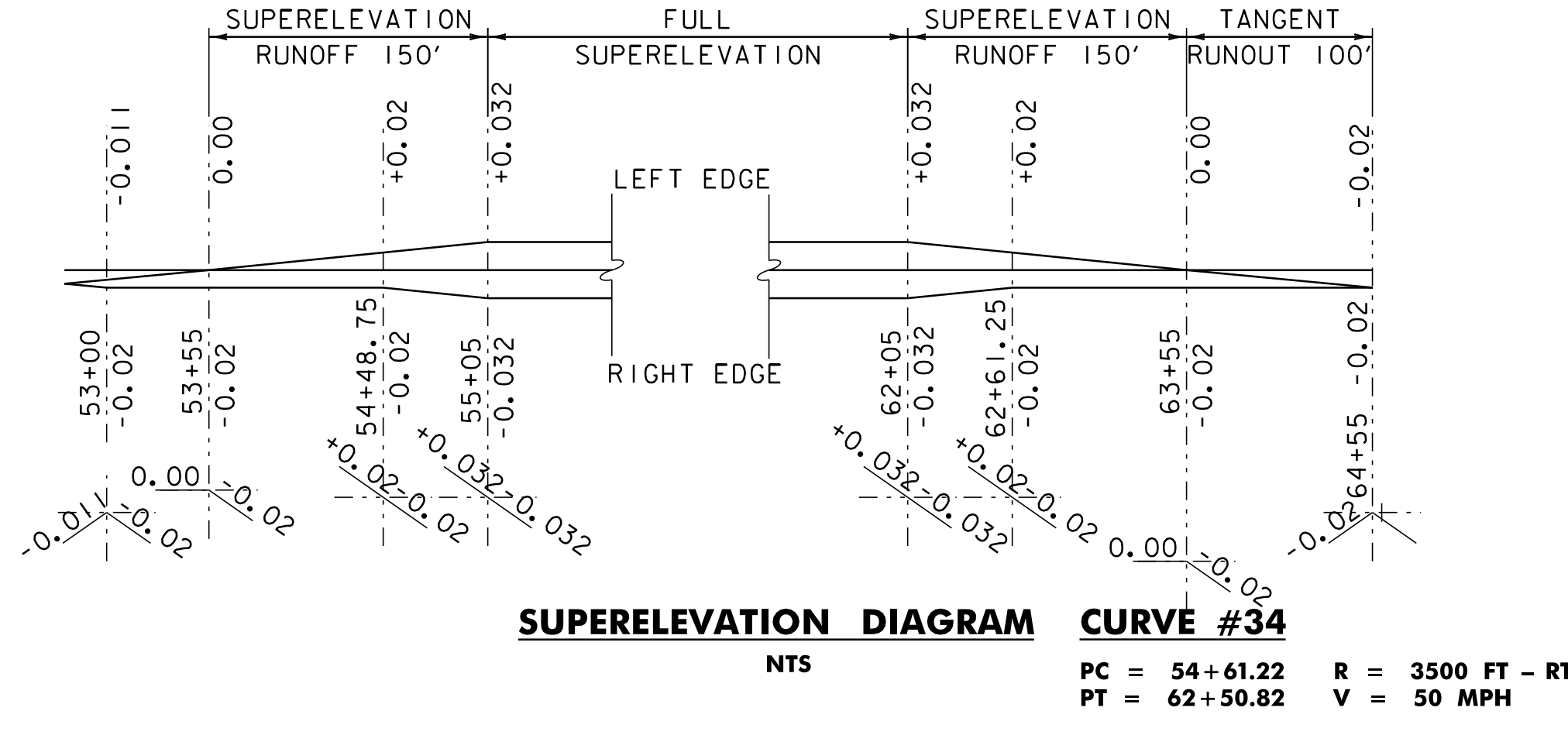
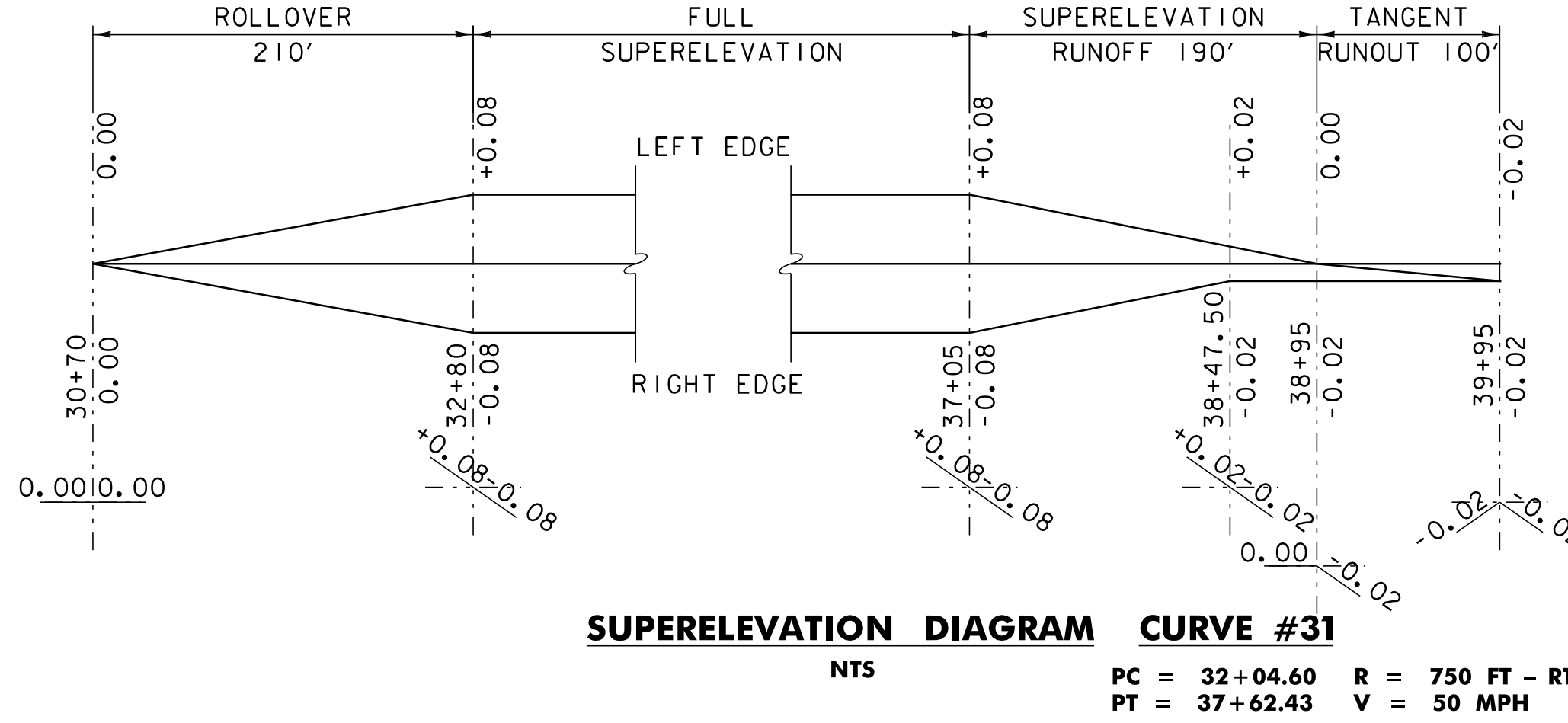
SUPERELEVATION DIAGRAM CURVE #30
 NTS
 PC = 24+37.02 R = 1200 FT - LT
 PT = 29+64.09 V = 50 MPH



SUPERELEVATION DIAGRAM CURVE #28A
 NTS
 TOWN LINE
 STA. 181+63.20 = MM 0344
 STA. 0+00.00 = MM 0000
 PC = 174+26.72 R = 8000 FT - RT
 PT = 3+40.26 V = 50 MPH

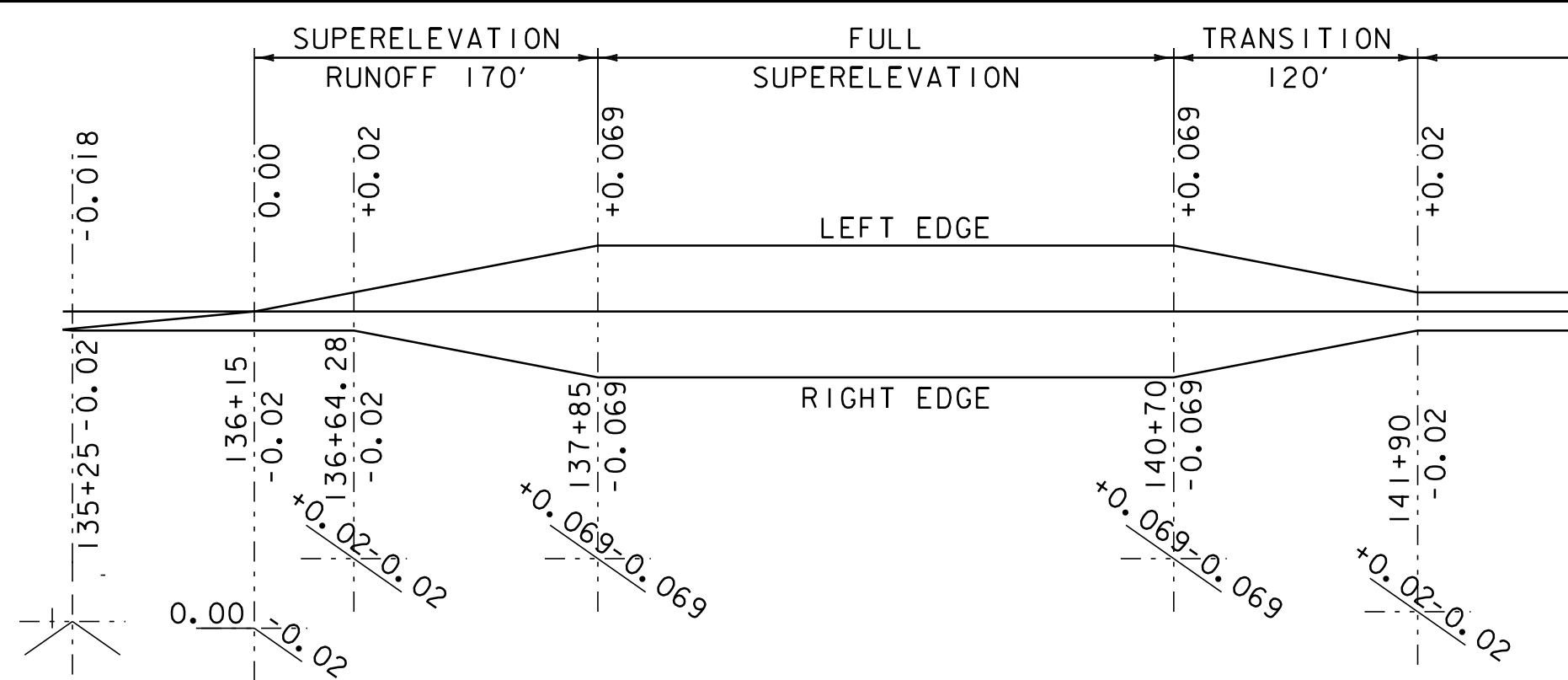
SUPERELEVATION BANKING SHEET #3

PROJECT NAME: CHelsea-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
PROJECT NUMBER: STP 2331(I)S	DRAWN BY: SJL
FILE NAME: 01c040.DGN	CHECKED BY: CDL
PROJECT LEADER: CDL	SHEET 33 OF 55
DESIGNED BY: SJL	
PLOT FILE: 01c040_33.1	

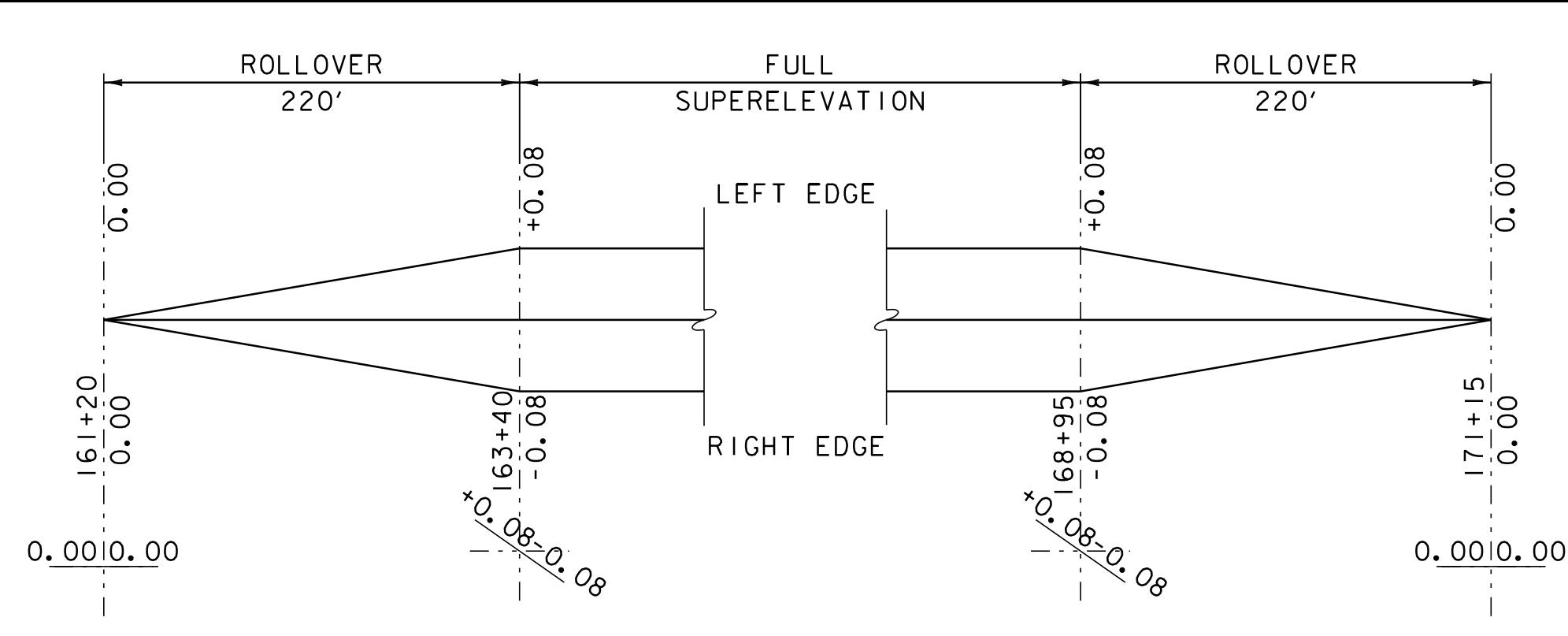


SUPERELEVATION BANKING SHEET #4

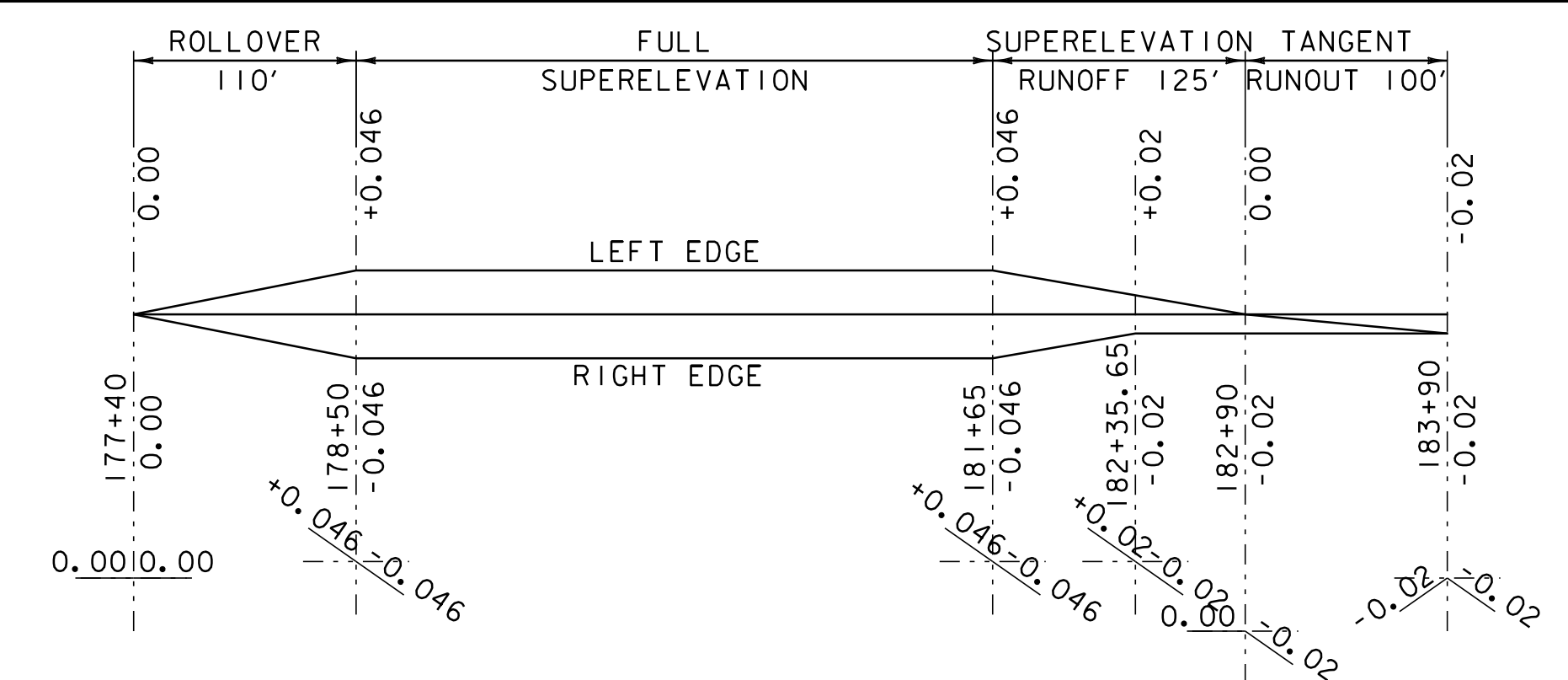
PROJECT NAME: CHLSEA-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
PROJECT NUMBER: STP 2331(I)S	DRAWN BY: SJL
FILE NAME: 01c040.DGN	CHECKED BY: CDL
DESIGNED BY: SJL	SHEET 34 OF 55
PLOT FILE: 01c040_34.i	



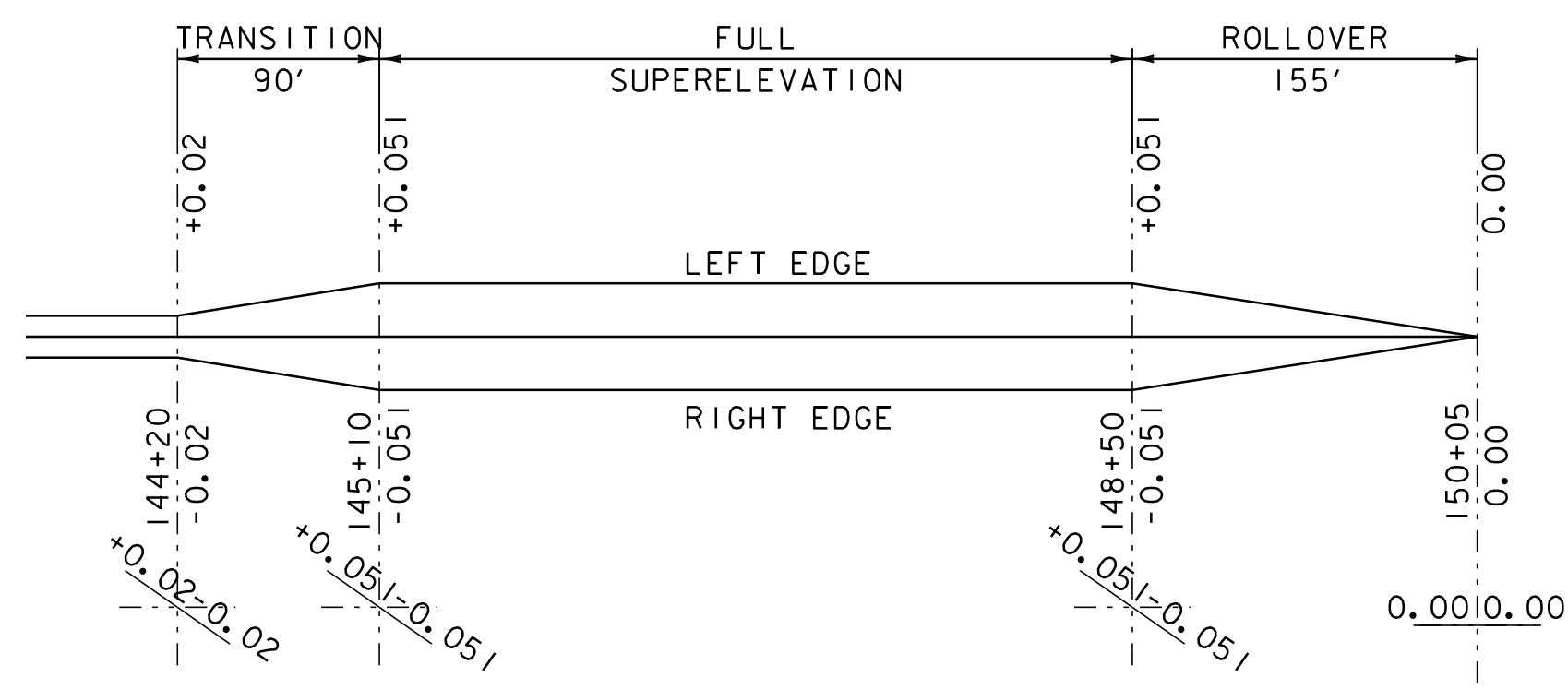
SUPERELEVATION DIAGRAM CURVE #40
 NTS
 PC = 137+35.25 R = 1250 FT - RT
 PT = 141+20.29 V = 50 MPH



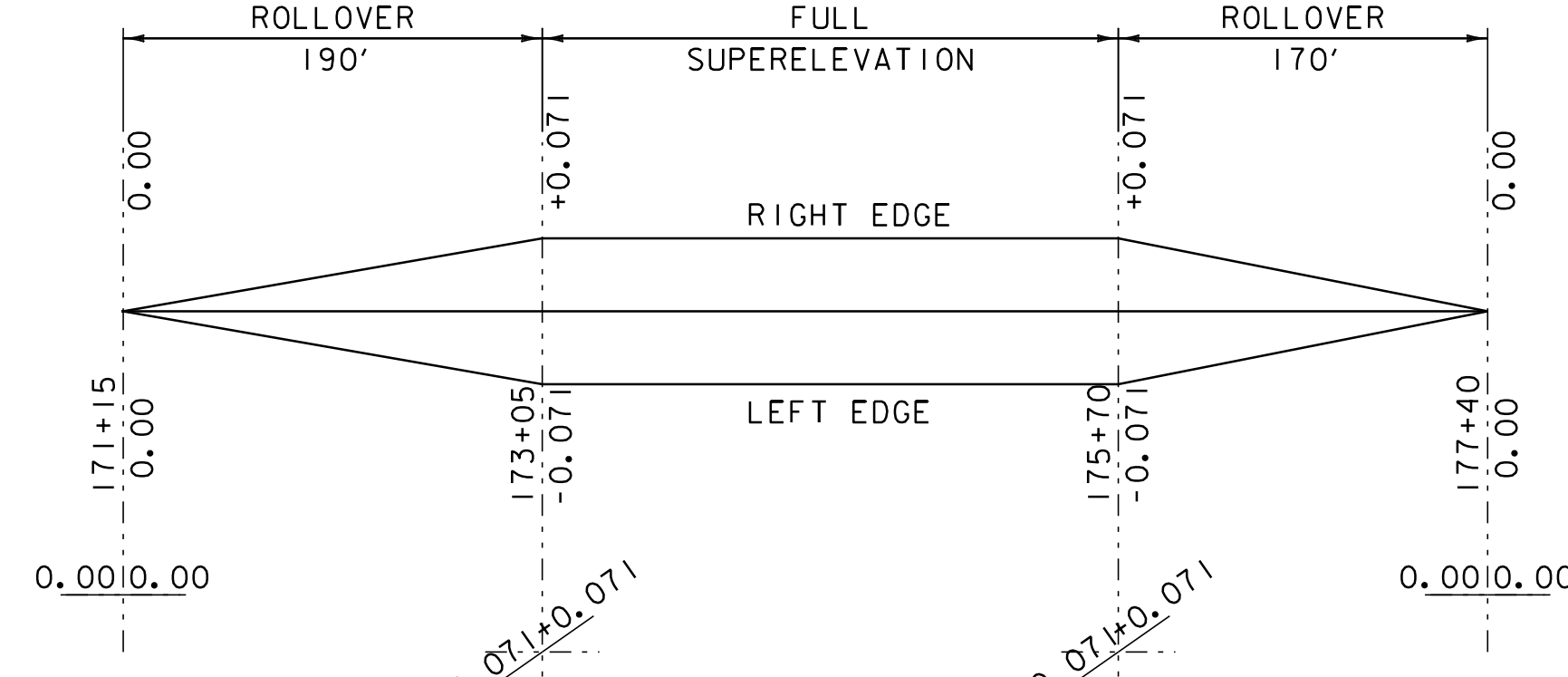
SUPERELEVATION DIAGRAM CURVE #43
 NTS
 PC = 163+01.97 R = 600 FT - RT
 PT = 169+33.65 V = 50 MPH



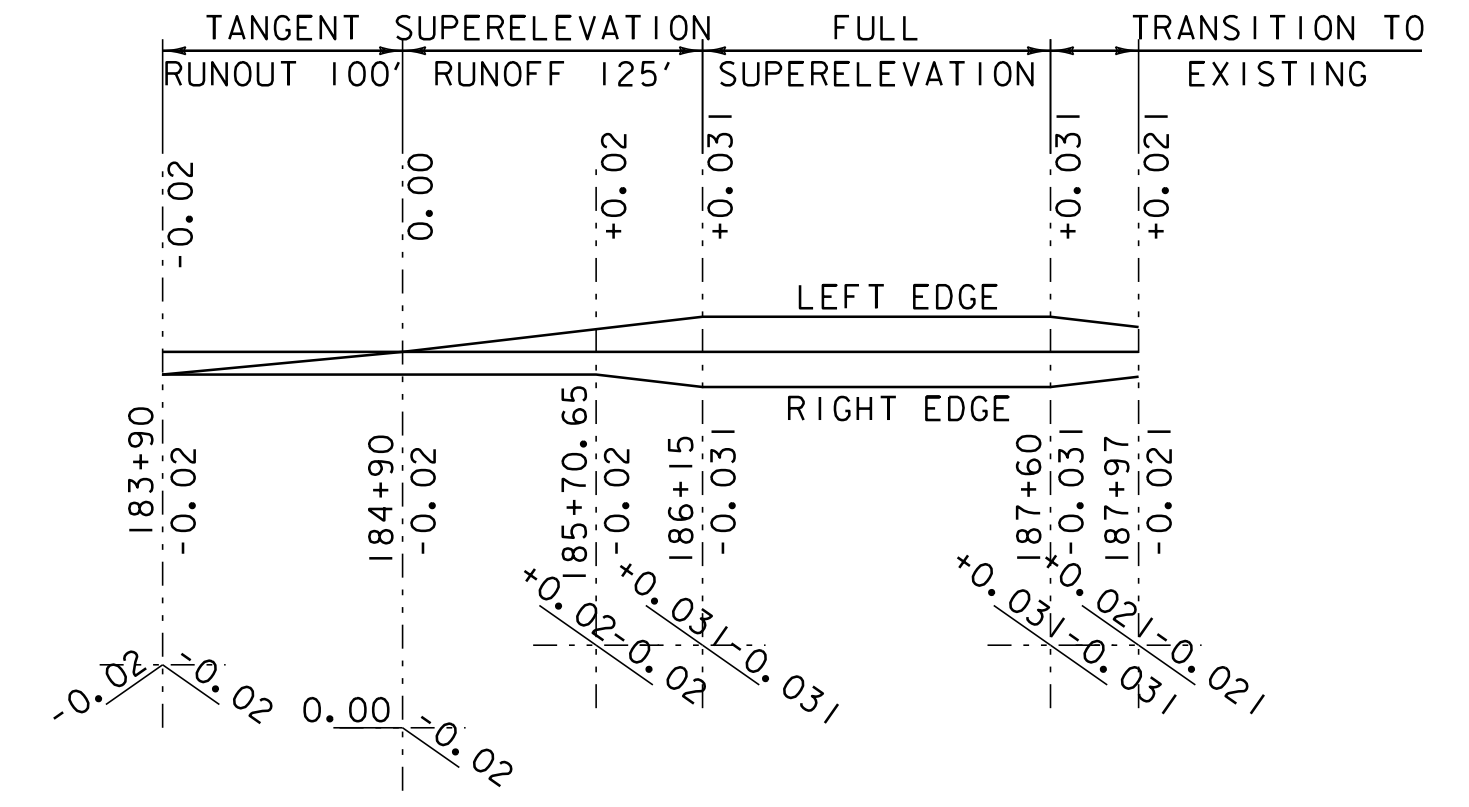
SUPERELEVATION DIAGRAM CURVE #45
 NTS
 PC = 177+98.23 R = 1500 FT - RT
 PT = 182+02.73 V = 40 MPH



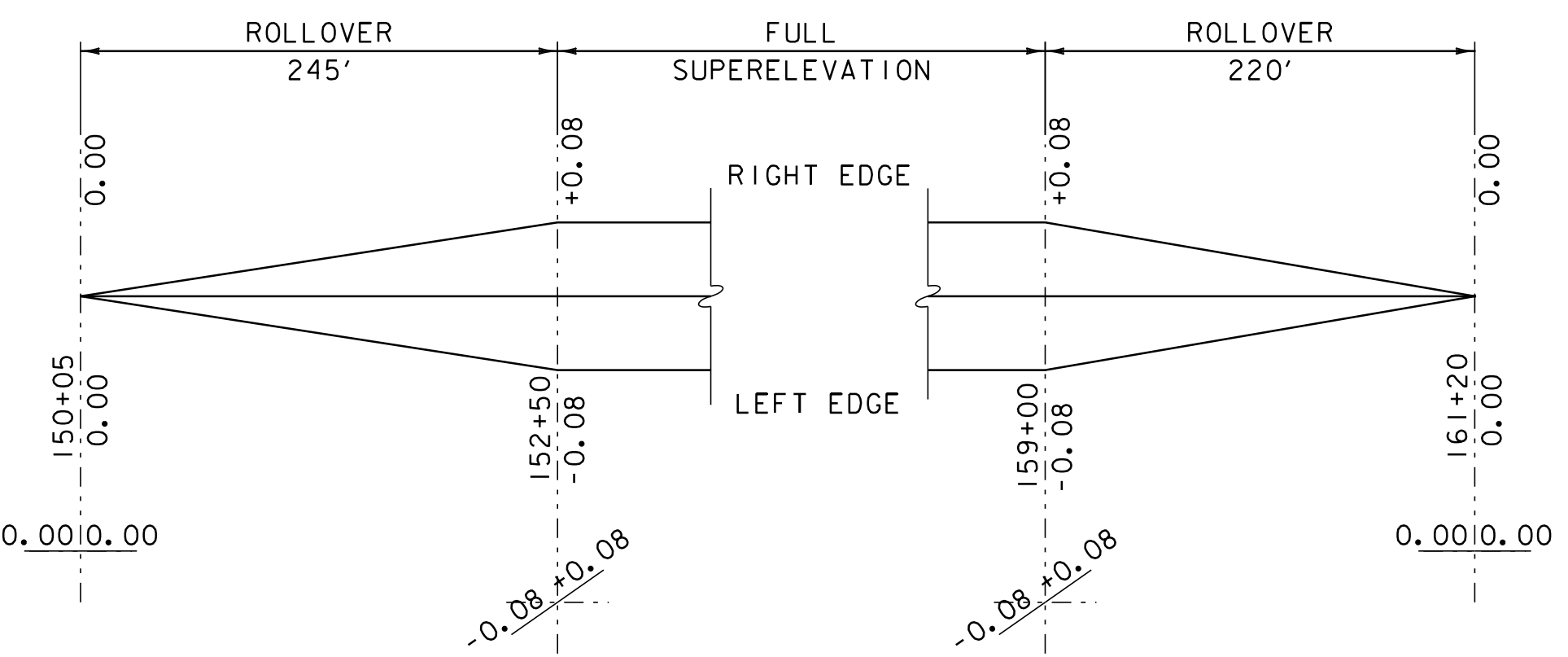
SUPERELEVATION DIAGRAM CURVE #41
 NTS
 PC = 144+65.45 R = 2000 FT - RT
 PT = 148+78.58 V = 50 MPH



SUPERELEVATION DIAGRAM CURVE #44
 NTS
 PC = 172+75.22 R = 750 FT - LT
 PT = 176+30.77 V = 40 MPH



SUPERELEVATION DIAGRAM CURVE #46
 NTS
 PC = 185+70.28 R = 2500 FT - RT
 PT = 187+96.72 V = 40 MPH










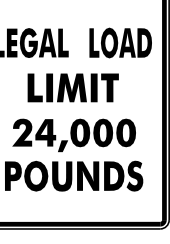


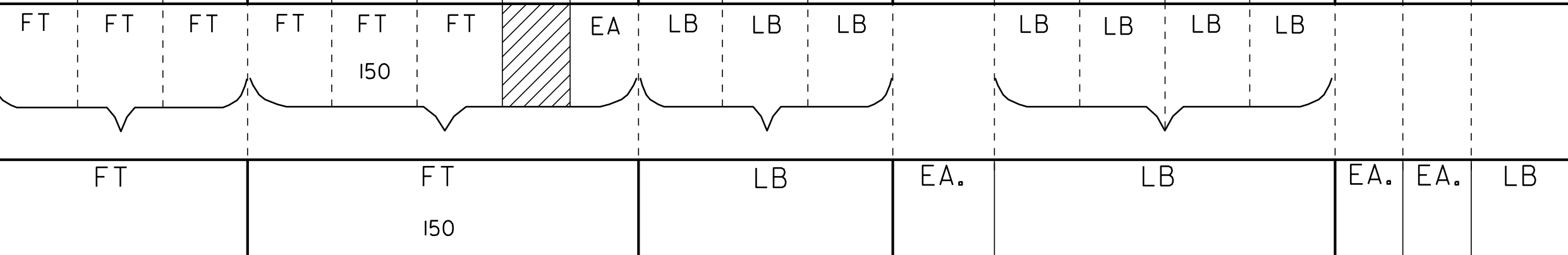
SUPERELEVATION DIAGRAM CURVE #42
 NTS
 PC = 152+11.87 R = 600 FT - LT
 PT = 159+38.25 V = 50 MPH

SUPERELEVATION BANKING DIAGRAM SHEET #5

PROJECT NAME: CHelsea-VERSHIRE	PLOT DATE: 28-OCT-2011 13:36
PROJECT NUMBER: STP 2331(I)S	DRAWN BY: SJL
FILE NAME: 01c040.DGN	CHECKED BY: CDL
PROJECT LEADER: CDL	SHEET 35 OF 55
DESIGNED BY: SJL	
PLOT FILE: 01c040_35.i	

TRAFFIC SIGN SUMMARY SHEET 18

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS			NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS																REMARKS	SIGN DETAIL							
											FLANGED CHANNEL				SQUARE STEEL (in)			TUBULAR ALUMINUM (in)			TUBULAR STEEL (in)				W-SHAPE STEEL			DETAIL ON SHEET NUMBER	STD. SHEET NUMBER						
		"A"	"B"	SALV SIGN	SALV TIS	1.75	2.0	2.5			3.0	4.0	4.0 MOD	FOUND-ATION	3.0	3.5	4.0	5.0	FTG. SIZE		WEIGHT	POST SIZE													
		EA	WIDTH (in)	HEIGHT (in)	RETAIN	SALVAGE	lb/ft	lb/ft			lb/ft	lb/ft	lb/ft		lb/ft	24"	30"	24"	30"	WEIGHT			POST SIZE												
STA. 168+86 LT VERSHIRE		1	18	24	3.00					1																			WI-8L		SHS				
STA. 171+02 LT VERSHIRE	 	1	30	30	6.25					1																				WI-4L		SHS			
		1	18	18	2.25																										WI3-1		SHS		
STA. 174+08 RT VERSHIRE		1	48	24	8.00					2																									
STA. 174+14 RT VERSHIRE		1	48	24	8.00					2																						WI-6		SHS	
STA. 177+29 RT VERSHIRE										1																						WI-6		SHS	
STA. 179+34 LT VERSHIRE		1	24	30	5.00					1																						R2-1		SHS	
STA. 179+37 RT VERSHIRE	 	1	24	30	5.00					1																							R2-1		SHS
		1	6	10	0.42																													E-138	
STA. 181+89 RT VERSHIRE		1	24	30	5.00					1																							VR-017		E-141



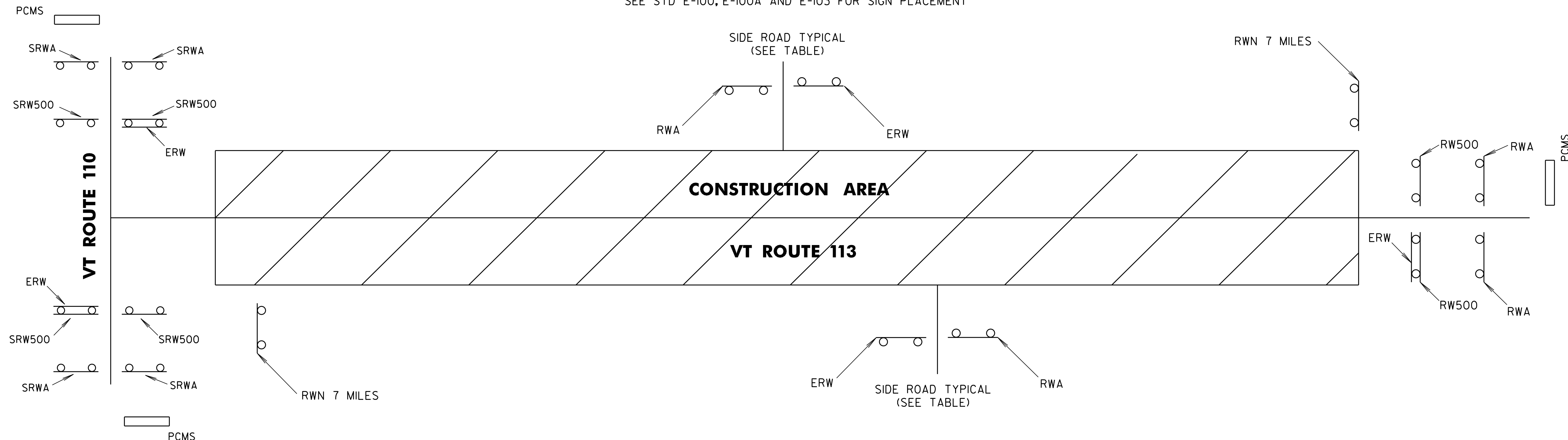
FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE VTRANS STANDARD SHEETS AND "SIGN POST DESIGN GUIDELINE".

TRAFFIC SIGN SUMMARY SHEET 18

PROJECT NAME: CHelsea-VERSHIRE	PLOT DATE: 28-OCT-2011 13:38
PROJECT NUMBER: STP 2331(1)S	DRAWN BY: SJL
FILE NAME: 01c040.dgn	CHECKED BY: EPD
DESIGNED BY: SJL	SHEET 53 OF 55
PLOT FILE: 01c040_53.1	

CONSTRUCTION APPROACH SIGNING

NOT TO SCALE
SEE STD E-100, E-100A AND E-103 FOR SIGN PLACEMENT



NOTES:

- THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION. THE COST OF PREPARING THIS PLAN (AND MAKING CHANGES IF NECESSARY) SHALL NOT BE PAID SEPARATELY BUT SHALL BE INCIDENTAL TO ITEM 641.10, "TRAFFIC CONTROL".
- THE CONTRACTOR SHALL INCLUDE A CONSTRUCTION SIGN APPROACH PACKAGE FOR EXPECTED LANE CLOSURES AND WORK ZONE SPEED REDUCTIONS IN COMPLIANCE WITH THE TRAFFIC CONTROL NOTES AND PART 6 OF THE MUTCD. PAYMENT FOR PROVIDING THIS PACKAGE SHALL BE INCIDENTAL TO ITEM 641.10, "TRAFFIC CONTROL".
- THE BID PRICE FOR "TRAFFIC CONTROL", ITEM 641.10, SHALL INCLUDE ALL OF THE FOLLOWING, AS NEEDED: APPROACH AND ON-PROJECT CONSTRUCTION SIGNING, PORTABLE ARROW BOARDS, BARRIERS, BARRELS, CONES, BARRICADES, TEMPORARY REGULATORY AND WARNING SIGNS, AND POSTS AS DETAILED IN VTRANS STANDARDS. ALL ADJUSTING, RELOCATING, AND REMOVING OF THESE DEVICES AS DIRECTED BY THE RESIDENT ENGINEER SHALL ALSO BE INCLUDED. THE FOLLOWING ITEMS WILL BE PAID FOR SEPARATELY:
646.602, 646.612, 646.682, AND 646.692 - TEMPORARY PAVEMENT MARKINGS
630.15 - FLAGGERS, 630.10 - UNIFORMED TRAFFIC OFFICERS, 646.76 - LINE STRIPING TARGETS
- PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) WILL BE PROVIDED FOR USE ALONG THIS PROJECT. THE PLACEMENT OF THESE UNITS AS WELL AS THE MESSAGE WILL BE APPROVED BY THE RESIDENT ENGINEER. THESE SIGNS WILL BE PAID FOR UNDER ITEM 641.15, "PORTABLE CHANGEABLE MESSAGE SIGN".

PCMS SHOULD NOT REPLACE ANY OF THE SIGNING DETAILED IN THE MUTCD AND SHOULD NOT BE USED IF STANDARD TRAFFIC CONTROL DEVICES ADEQUATELY PROVIDE THE INFORMATION THE MOTORISTS NEED TO TRAVEL SAFELY.

THE PCMS SHALL CONSIST OF EITHER ONE OR TWO PHASES. TYPICALLY, A PHASE SHALL CONSIST OF UP TO THREE LINES OF EIGHT CHARACTERS PER LINE. THE PCMS SHOULD BE USED AS A SUPPLEMENT AND NOT AS A SUBSTITUTE FOR CONVENTIONAL SIGNS AND PAVEMENT MARKINGS.

THE PCMS SHOULD COMMUNICATE WHAT INFORMATION MOTORISTS NEED TO KNOW. UNNECESSARY INFORMATION SHOULD BE AVOIDED. MESSAGES SHOULD BE UPDATED PERIODICALLY TO DESCRIBE THE WORK ACTIVITY OCCURRING SO THAT THE PCMS CONTINUES TO COMMAND THE ATTENTION OF MOTORISTS.
- THE MUTCD 2003 SHALL BE THE STANDARD FOR ALL TRAFFIC CONTROL DEVICES. EXISTING SIGNS, SIGNALS AND MARKINGS SHALL BE VALID UNTIL SUCH TIME AS THEY ARE REPLACED OR RECONSTRUCTED. WHEN NEW TRAFFIC CONTROL DEVICES ARE ERECTED OR PLACED OR EXISTING TRAFFIC CONTROL DEVICES ARE REPLACED OR REPAIRED THE EQUIPMENT, DESIGN, METHOD OF INSTALLATION, PLACEMENT OR REPAIR SHALL CONFORM WITH SUCH STANDARDS.
- NO CONSTRUCTION SIGNS SHALL BE INSTALLED AS TO INTERFERE OR OBSTRUCT THE VIEW OF EXISTING TRAFFIC CONTROL DEVICES, STOPPING SIGHT DISTANCE, AND CORNER SIGHT DISTANCE FROM DRIVES AND TOWN HIGHWAYS.

SIDE ROAD NAME	ROAD WORK AHEAD	END ROAD WORK	SIDE ROAD WORK AHEAD	SIDE ROAD WORK 500 FT	ROAD WORK 500 FT	ROAD WORK NEXT 7 MILES	PCMS
CHELSEA							
VT 110		2	4	4			2
BEGIN PROJECT (VT 113)						2	
SPEAR STREET							
DENSMORE ROAD	1	1					
BLACKHAWK ROAD	1	1					
LYFORD DRIVE							
ODONELL ROAD	1	1					
SCADIN ROAD							
BURGER ROAD							
VERSHIRE							
MCIVER ROAD	1	1					
GOOSE GREEN ROAD	1	1					
BROWN ROAD	1	1					
RICHARDSON ROAD							
DURGIN HILL ROAD	1	1					
VERSHIRE CENTER ROAD	1	1					
END PROJECT (VT 113)	2	1			2	2	1
TOTALS	10	11	4	4	2	4	3

LEGEND

- RWA = ROAD WORK AHEAD
- RW500 = ROAD WORK 500 FT
- ERW = END ROAD WORK
- RWN = ROAD WORK NEXT 7 MILES
- SRWA = SIDE ROAD WORK AHEAD
- SRW500 = SIDE ROAD WORK 500 FT

CONSTRUCTION APPROACH SIGNING SHEET

PROJECT NAME: CHELSEA-VERSHIRE
PROJECT NUMBER: STP 2331(1)S

FILE NAME: 01c040.dgn
PROJECT LEADER: CDL
DESIGNED BY: BMB
PLOT FILE: 01c040_55.1

PLOT DATE: 28-OCT-2011 13:38
DRAWN BY: BMB
CHECKED BY: EPD
SHEET 55 OF 55