

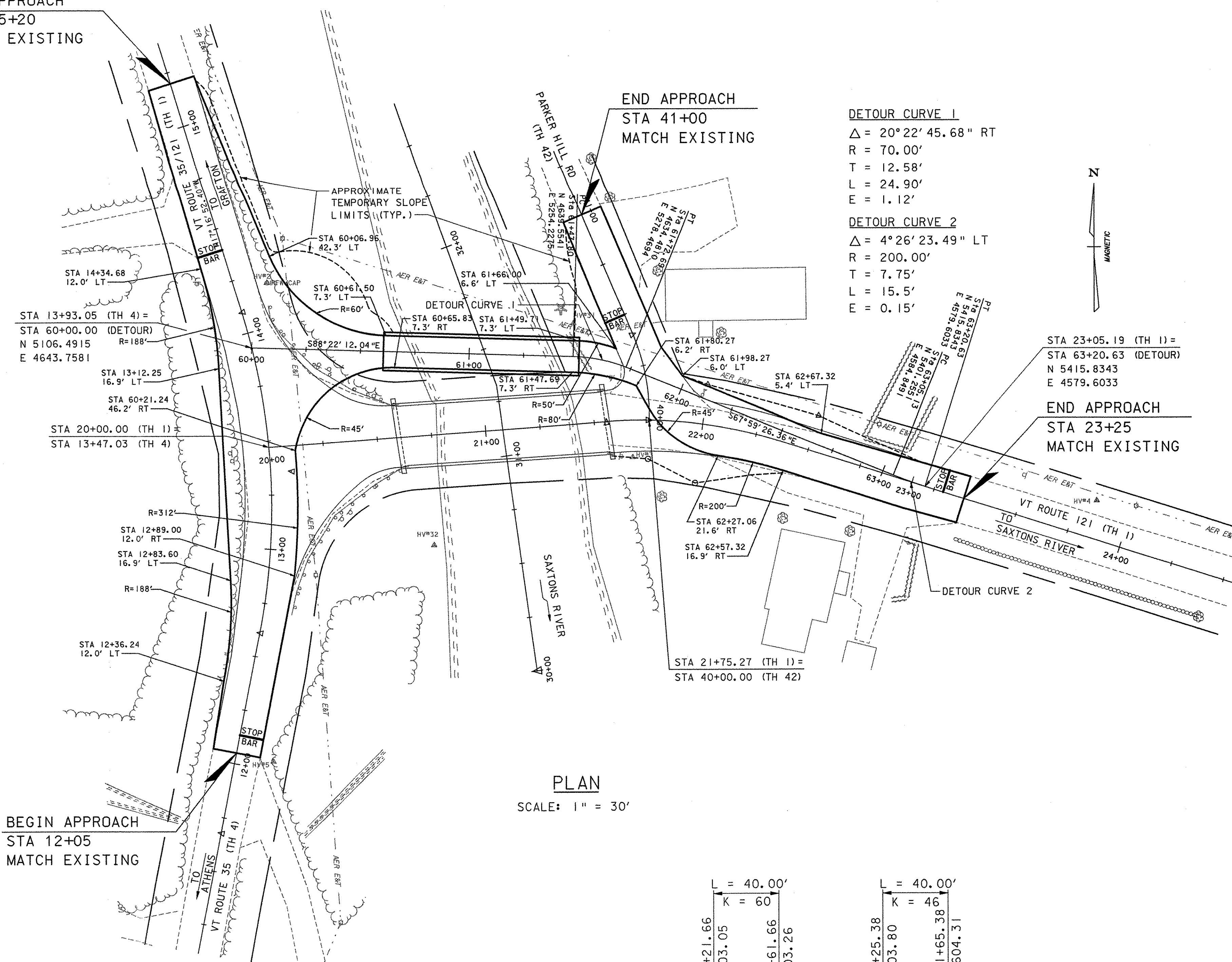
END APPROACH
STA 15+20
MATCH EXISTING

END APPROACH
STA 41+00
MATCH EXISTING

DETOUR CURVE 1
 $\Delta = 20^\circ 22' 45.68''$ RT
R = 70.00'
T = 12.58'
L = 24.90'
E = 1.12'

DETOUR CURVE 2
 $\Delta = 4^\circ 26' 23.49''$ LT
R = 200.00'
T = 7.75'
L = 15.5'
E = 0.15'

END APPROACH
STA 23+25
MATCH EXISTING



BEGIN APPROACH
STA 12+05
MATCH EXISTING

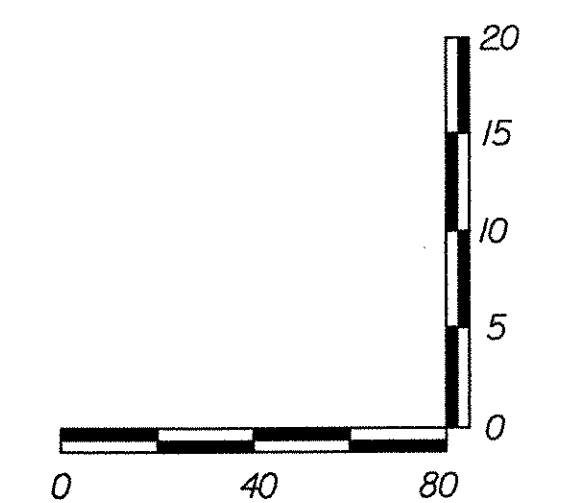
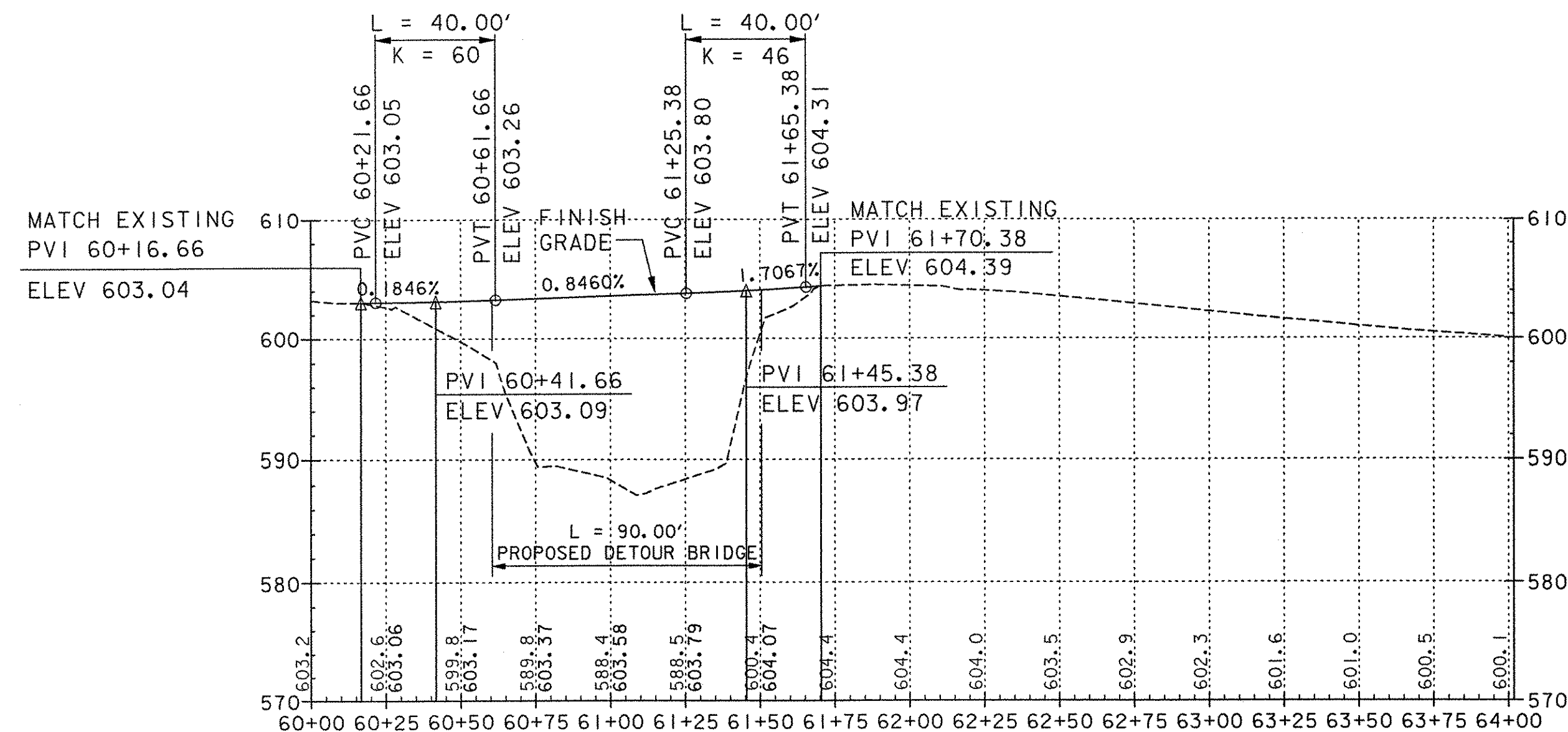
GENERAL NOTES:

- TRAFFIC SHALL BE MAINTAINED DURING CONSTRUCTION WITH A DETOUR AND TEMPORARY BRIDGE. THE DETOUR AND BRIDGE SHALL BE IN ACCORDANCE WITH AND PAID AS ITEM 528.10, ONE-WAY TEMPORARY BRIDGE. THE CONTRACTOR SHALL SUBMIT THE DETOUR SIGNAGE AND ALIGNMENT PACKAGE TO THE ENGINEER FOR APPROVAL PRIOR TO COMMENCEMENT OF WORK.
- THE PURPOSE OF THE DETOUR PLAN AND PROFILE IS TO DEFINE APPROXIMATE SLOPE LIMITS. APPROXIMATE SLOPE LIMITS ARE BASED ON A 14 FOOT WIDE TRAVELED WAY WITH 1:2 SIDESLOPES. THE CONTRACTOR MAY SUBMIT AN ALTERNATIVE ALIGNMENT FOR THE ENGINEER'S APPROVAL. THE DETOUR SHALL MEET THE REQUIREMENTS OF AASHTO POLICY ON GEOMETRIC DESIGN, THE MUTCD, VERMONT SPECIFICATION 528 AND STANDARD E-107M. ADDITIONAL PERMITS OR RIGHT-OF-WAY REQUIRED FOR AN ALTERNATIVE DETOUR SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- DETOUR DESIGN SPEED IS 35 MPH AND POSTED SPEED IS 20 MPH.
- THE TEMPORARY DETOUR SHALL BE PAVED TO A DEPTH OF 2 INCHES. THE PAVEMENT SHALL BE PAID UNDER ITEM 406.25, BITUMINOUS CONCRETE PAVEMENT. SAMPLING AND MIX DESIGN REQUIREMENTS DO NOT APPLY TO TEMPORARY PAVEMENT. ALL OTHER COSTS ASSOCIATED WITH THE TEMPORARY BRIDGE, DETOUR SHALL BE PAID FOR UNDER ITEM 528.10, ONE WAY TEMPORARY BRIDGE. SEE PLAN SHEET 7 FOR FINAL LAYOUT.
- THE DETOUR SHALL BE REMOVED AS PER SPECIFICATION 528.
- PERMIT REQUIREMENTS:
ALL TEMPORARY FILLS WILL BE PLACED ON A GEOTEXTILE MAT. UPON COMPLETION OF THE NEW STRUCTURE, THE TEMPORARY BRIDGE AND FILLS FOR THE DETOUR WILL BE REMOVED DOWN TO THE ORIGINAL GRADE. ALL AREAS OF DISTURBANCE WILL BE IMMEDIATELY SEEDED AND MULCHED UPON COMPLETION OF THE FINAL GRADE TO PREVENT SOIL EROSION.

TEMPORARY ABUTMENTS SHALL BE NO CLOSER TO THE CHANNEL THAN THE TOP OF BANK. (ELEV. 598' WEST ABUT., ELEV. 602' EAST ABUT.).

- SEE SHEET 13 FOR TRAFFIC CONTROL PLAN.
- SEQUENCE OF WORK:
A. PLACE ALL REQUIRED PAVEMENT FOR THE DETOUR WORK.
B. COMPLETE NEW BRIDGE AND PAVE APPROACHES.
C. REMOVE ANY TEMPORARY PAVEMENT THAT IS NOT TO BE INCORPORATED INTO THE FINAL PROJECT.

SAW CUTS SHALL BE USED BETWEEN PAVEMENT TO BE REMOVED AND PAVEMENT TO BE RETAINED.



TOWN OF GRAFTON
CAMBRIDGEPORT BRIDGE

Town Of	GRAFTON	Bridge No.	16G
Highway No.	TH 1	Log Sta.	
TH 1 (CAMBRIDGEPORT BRIDGE) OVER THE SAXTONS RIVER		Surv. Sta.	
DETOUR PLAN & PROFILE			
Designed By	C.L. CILLEY	Drawn By	B.J. MASSE
Checked By	Date	Bridge Design Supervisor	Date
M.A. COLGAN	6/05	M.A. COLGAN	Date 6/05
PROJECT	GRAFTON	PROJECT NO.	TH2-0104
I.G.C. Info.		File No. 51335DETPLN	
		Sheet 14 of 42	

NO.	DESCRIPTION	BY	DATE
1	ADDITIONAL ALIGNMENT TIES AND SURVEY COORDINATES	BJM	6/05

VHB Vanasse Hangen Brustlin, Inc.