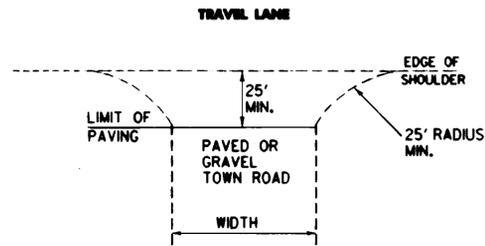
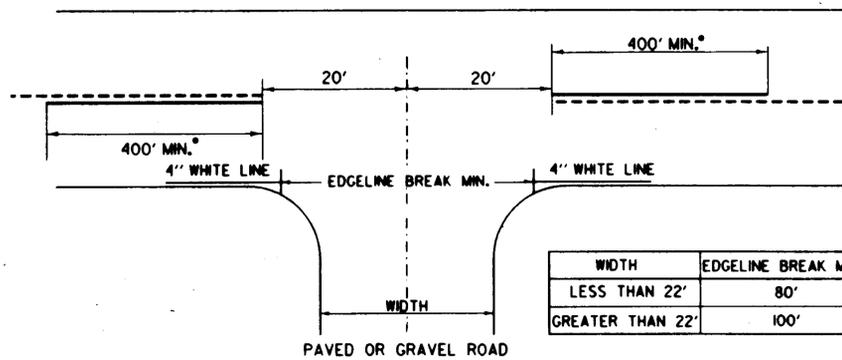


PAVEMENT MARKING PLACEMENT DETAIL
TYPICAL

DETAIL I



PAVING LIMITS FOR TOWN ROADS
NOT TO SCALE

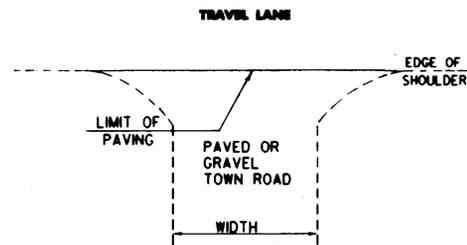


A SOLID LINE IN THE DIRECTION OF TRAVEL IS BEGUN AT A LOCATION 400 FEET IN ADVANCE OF THE INTERSECTION. THE DISTANCE IS MEASURED FROM A POINT 20 FEET IN ADVANCE OF THE CENTERLINE OF THE INTERSECTING ROADWAY.

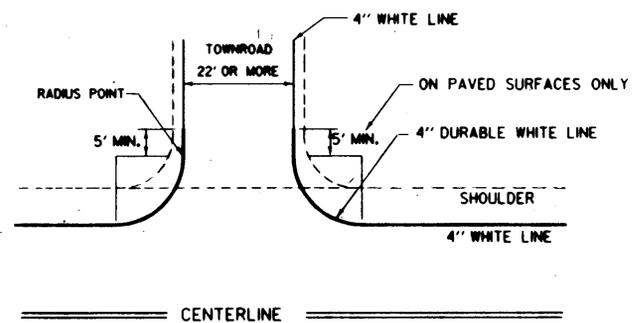
• CENTERLINE TREATMENT SHALL CONSIST OF A MINIMUM OF 400 FEET OF SOLID LINE IN ADVANCE OF THE INTERSECTION AND SHALL BE PAIRED WITH EITHER A SOLID OR DASHED LINE DEPENDING ON SIGHT DISTANCE AVAILABILITY IN THE OPPOSING LANE.

APPROACH TO A SIDE ROAD INTERSECTION
TYPICAL

DETAIL II

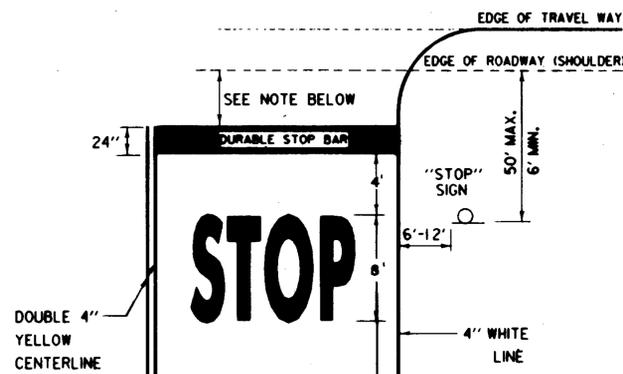


PAVING LIMITS FOR TOWN ROADS
NOT TO SCALE



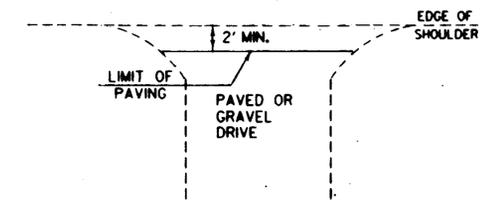
NOTE: EDGELINE RADI WILL BE 35' UNLESS OTHERWISE NOTED ON THE LAYOUTS.

DURABLE EDGELINE RADIUS LAYOUT TYPICAL FOR TOWN ROADS



THE "DESIRED STOPPING POINT" IS THE LOCATION BASED ON SITE CONDITIONS THAT BEST ALLOWS THE STOPPED VEHICLE TO VIEW THE APPROACHING TRAFFIC, IN NO CASE MORE THAN 30 FEET OR LESS THAN 4 FEET FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.

STOP BAR DETAIL



PAVING LIMITS FOR DRIVES
NOT TO SCALE

**PAVEMENT MARKING
DETAIL SHEET**

SURVEYED BY _____ DATE _____
 DRAWN BY BRYANT ASSOC. DATE 11/92
 TRACED BY _____ DATE _____
 U.S. ROUTE 2 - MARSHFIELD
 PROJ. NO. FE6C F028 - 3(28)
 SHEET 102 OF _____