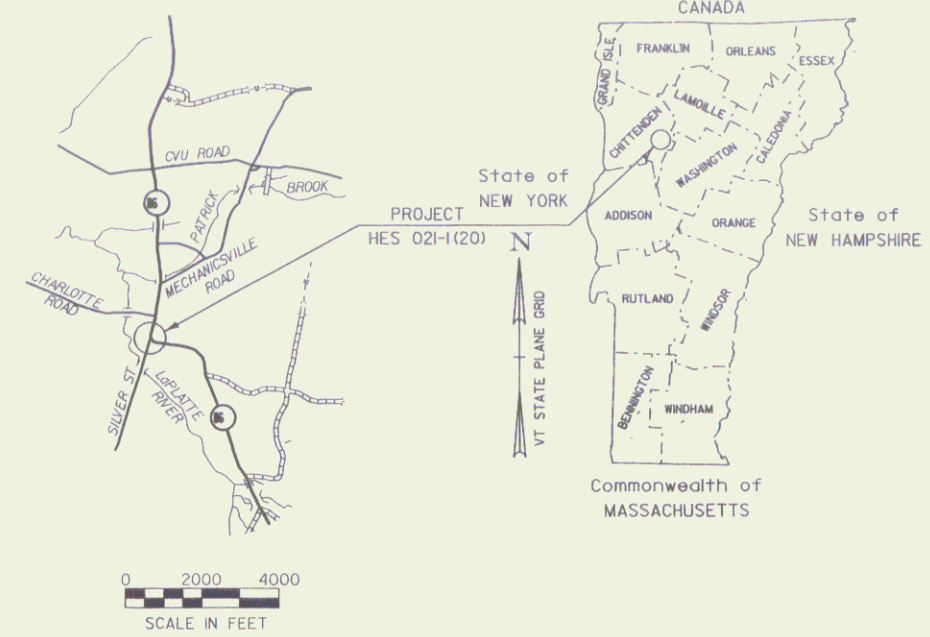


STATE OF VERMONT
AGENCY OF TRANSPORTATION



R.O.W. PLANS

PROPOSED IMPROVEMENT
TOWN OF HINESBURG
COUNTY OF CHITTENDEN
VT 116 (MINOR ARTERIAL)
AT SILVER STREET (MAJOR T.H. COLLECTOR)



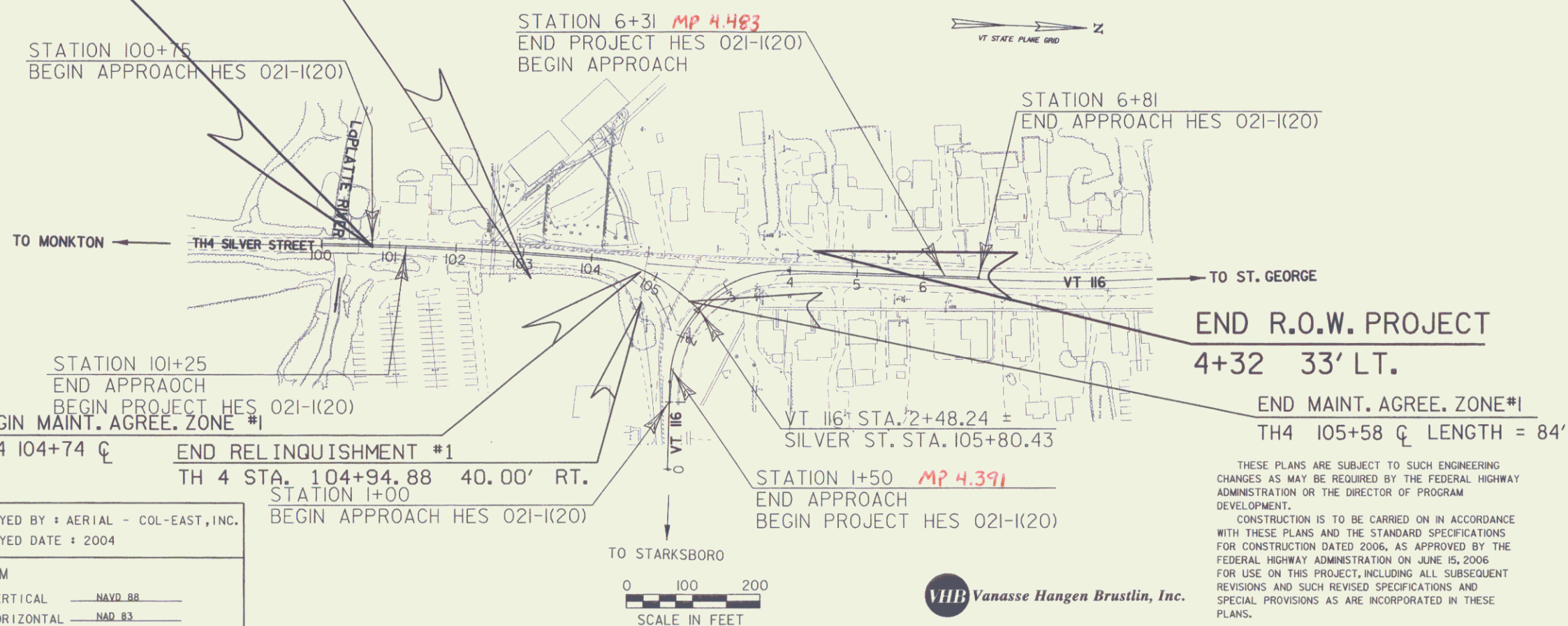
BEGIN RELINQUISHMENT #1
TH 4 STA. 103+12.46 35.27' RT.

BEGIN R.O.W. PROJECT
TH4 100+75 C

BEGINNING ON VT 116 EAST OF THE SILVER ST. INTERSECTION AND EXTENDING NORTHERLY ALONG VT 116 FOR APPROXIMATELY 580 FT.

LENGTH OF ROADWAY: 936 FT = 0.177 MILES
LENGTH OF PROJECT: 936 FT = 0.177 MILES
LENGTH OF R.O.W. PROJECT: 652 FT = 0.12 MILES ±

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES RECONSTRUCTION OF VT 116 AND SILVER ST. WITH NEW PAVEMENT, SUBBASE, CURBING, AND DRAINAGE.



CONVENTIONAL SYMBOLS	
COUNTY LINE	— — — — —
TOWN LINE	— — — — —
LIMITS OF ACCESS	— — — — —
POINT OF ACCESS	X
FENCE LINE	X — X — X — X —
STONE WALL	— — — — —
TRAVELED WAY	— — — — —
RAILROAD	— — — — —
SURVEY LINE	— — — — —
CULVERT	— — — — —
POWER POLE	⊕
TELEPHONE POLE	⊕
TREES	⊕
CONTROL OF ACCESS	— — — — —
PROPERTY LINE	— — — — —
R.O.W. TAKING LINE	— — — — —
SLOPE RIGHTS	— — — — —
TOP OF CUT	— — — — —
TOE OF SLOPE	— — — — —

SURVEYED BY : AERIAL - COL-EAST, INC.
SURVEYED DATE : 2004

DATUM
VERTICAL : NAVD 88
HORIZONTAL : NAD 83

Pin # 04B206

ALL DRIVES AS INDICATED ON PLANS ARE SUBJECT TO PERMITS PURSUANT TO TITLE 19 SECTION III, V.S.A

LINES SHOWN ON THIS PLAN AS EXISTING PROPERTY LINES P/L ARE BELIEVED TO BE ACCURATE BUT SHOULD NOT BE RELIED UPON FOR PURPOSES UNRELATED TO THE STATE OF VERMONT'S ACQUISITION OF LAND AND RIGHTS FOR THIS PROJECT.

PLOT DATE: 11-FEB-2008

APPROVED: *[Signature]* DATE 3-6-08
Director of Program Development

APPROVED: *[Signature]* DATE 2/1/08
Chief of Right of Way

HINESBURG
HES 021-1(20)
R.O.W. SHEET 1 OF 9 SHEETS

END R.O.W. PROJECT
4+32 33' LT.

END MAINT. AGREE. ZONE #1
TH4 105+58 C LENGTH = 84'

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT.

CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.



MATERIAL TOLERANCES	
MATERIAL ITEM	THICKNESS TOLERANCE
PAVEMENT (TOTAL DEPTH)	± 1/8"
SUBBASE	± 1/8"
SAND	± 1/8"

TYPICAL SECTIONS

1/2" BITUMINOUS CONCRETE PAVEMENT - TYPE III (PG 58-28)
 2" BITUMINOUS CONCRETE PAVEMENT - TYPE II (PG 58-28)
 2 1/2" BITUMINOUS CONCRETE PAVEMENT - TYPE I (PG 58-28)
 18" SUBBASE OF DENSE GRADED CRUSHED STONE
 15" OF SAND BORROW (SILVER ST. WHERE SHOWN)

SEEDING FORMULA URBAN AREAS

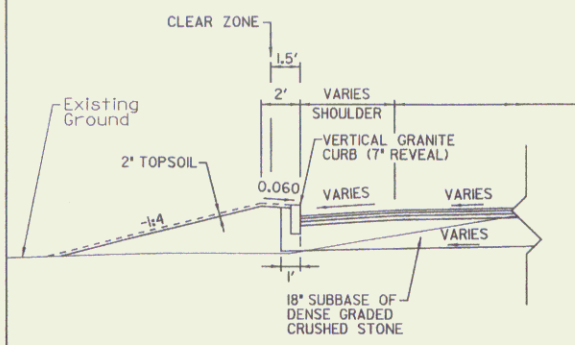
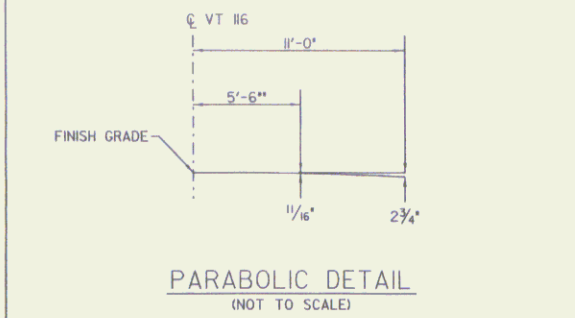
% WT.	LBS./A.	NAME	PUR %	GERM %
42.5	34.0	CREeping RED FESCUE	98	85
10.0	8.0	PERENNIAL RYE GRASS	95	90
42.5	34.0	KENTUCKY BLUE GRASS	85	85
5.0	4.0	ANNUAL RYE GRASS	95	85
100.00	80.0			

GENERAL NOTES

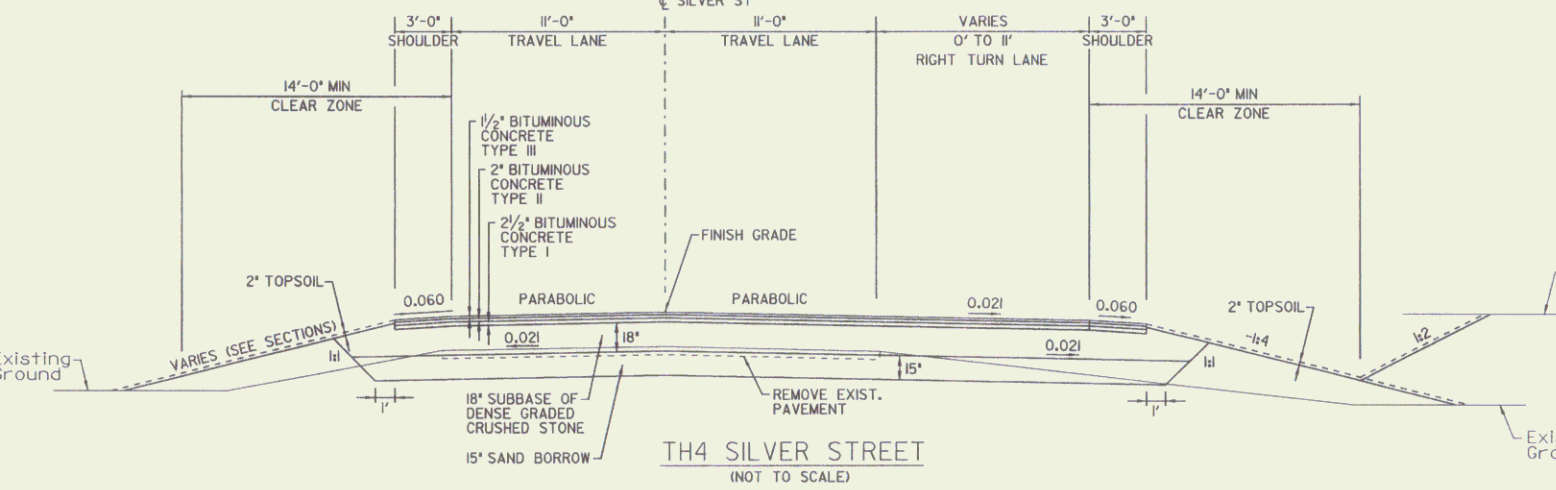
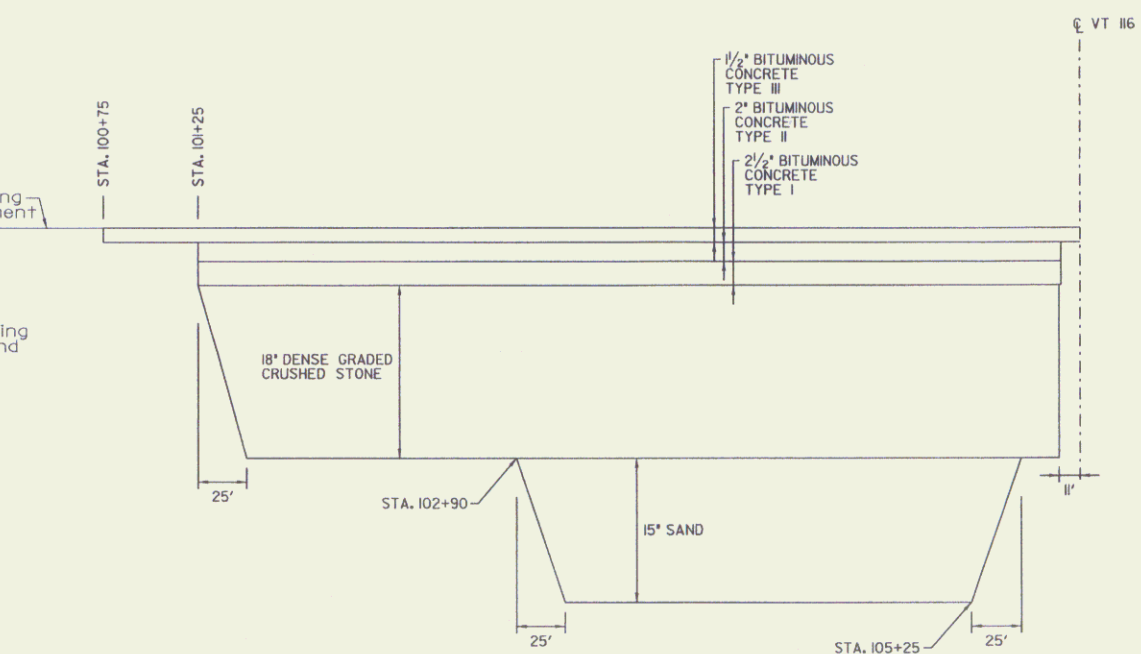
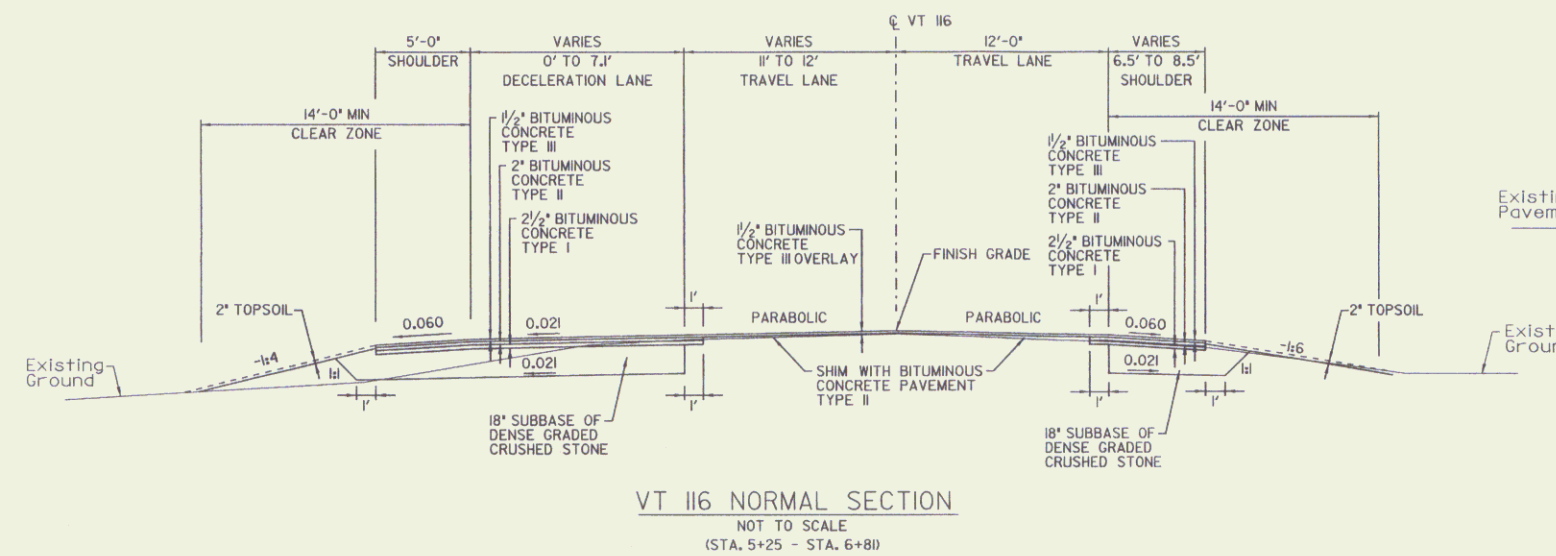
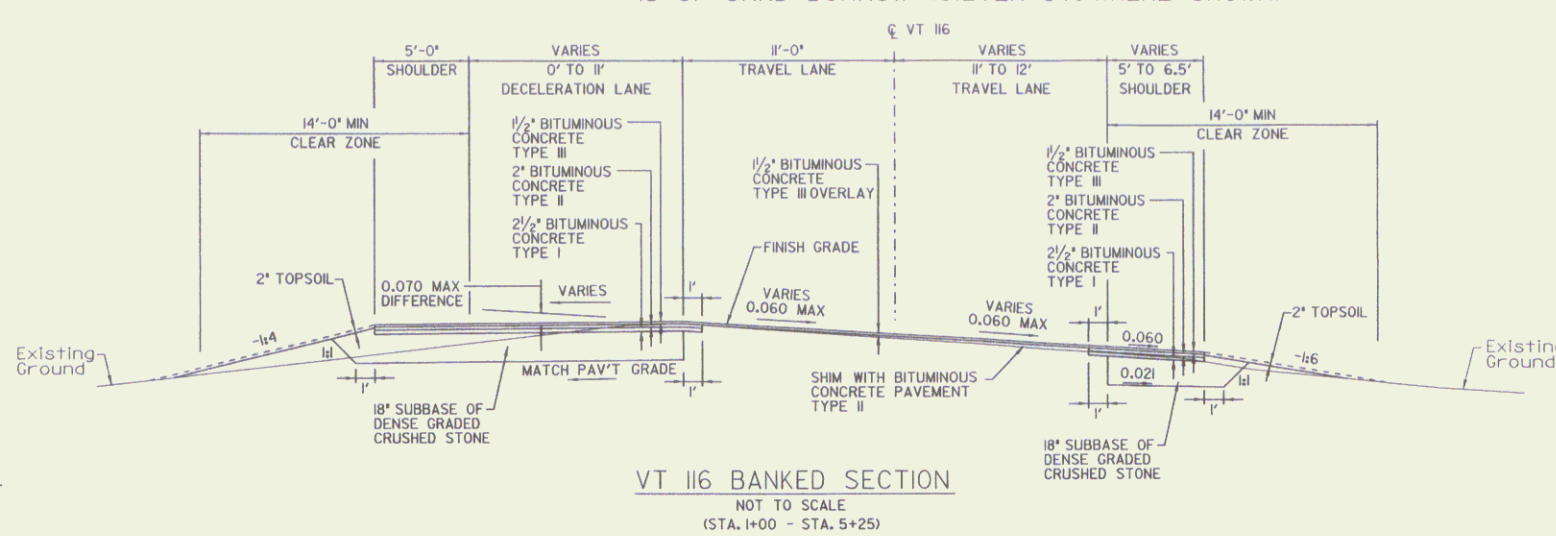
- SEED MIXTURE SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS SEED.
- SEEDS TO BE APPLIED PER SEEDING FORMULAS OR AS DIRECTED BY THE ENGINEER.
- FERTILIZER: FORMULA 10-20-10, TO BE USED WITH SEED, APPLIED AT THE RATE OF 500 LBS./ACRE. HYDRO SEEDERS MAY USE 19-19-19 FORMULA.
- AGRICULTURAL LIMESTONE: TO BE APPLIED AT THE RATE OF 2 TONS/ACRE, OR AS DIRECTED BY THE ENGINEER.
- HAY MULCH: TO BE PLACED ON EARTH SLOPES AT THE RATE OF 2 TONS/ACRE, OR AS DIRECTED BY THE ENGINEER.
- TOPSOIL: TO BE USED WITH SEED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.

NOTES

- PROPOSED DRIVE APRONS SHALL INCLUDE 1/2" COLD PLANED PAVEMENT.
- TACK COAT: EMULSIFIED ASPHALT IS TO BE APPLIED AT THE RATE OF 0.025 GAL/SY BETWEEN SUCCESSIVE COURSES OF PAVEMENT AS DIRECTED BY THE ENGINEER.
- SLOPE ROUNDING: ALL CUT SLOPES TO BE ROUNDED IN ACCORDANCE WITH STANDARD SHEET B - 5.
- MARKER POSTS: TO BE PLACED AS INDICATED OR AS DIRECTED BY THE ENGINEER.



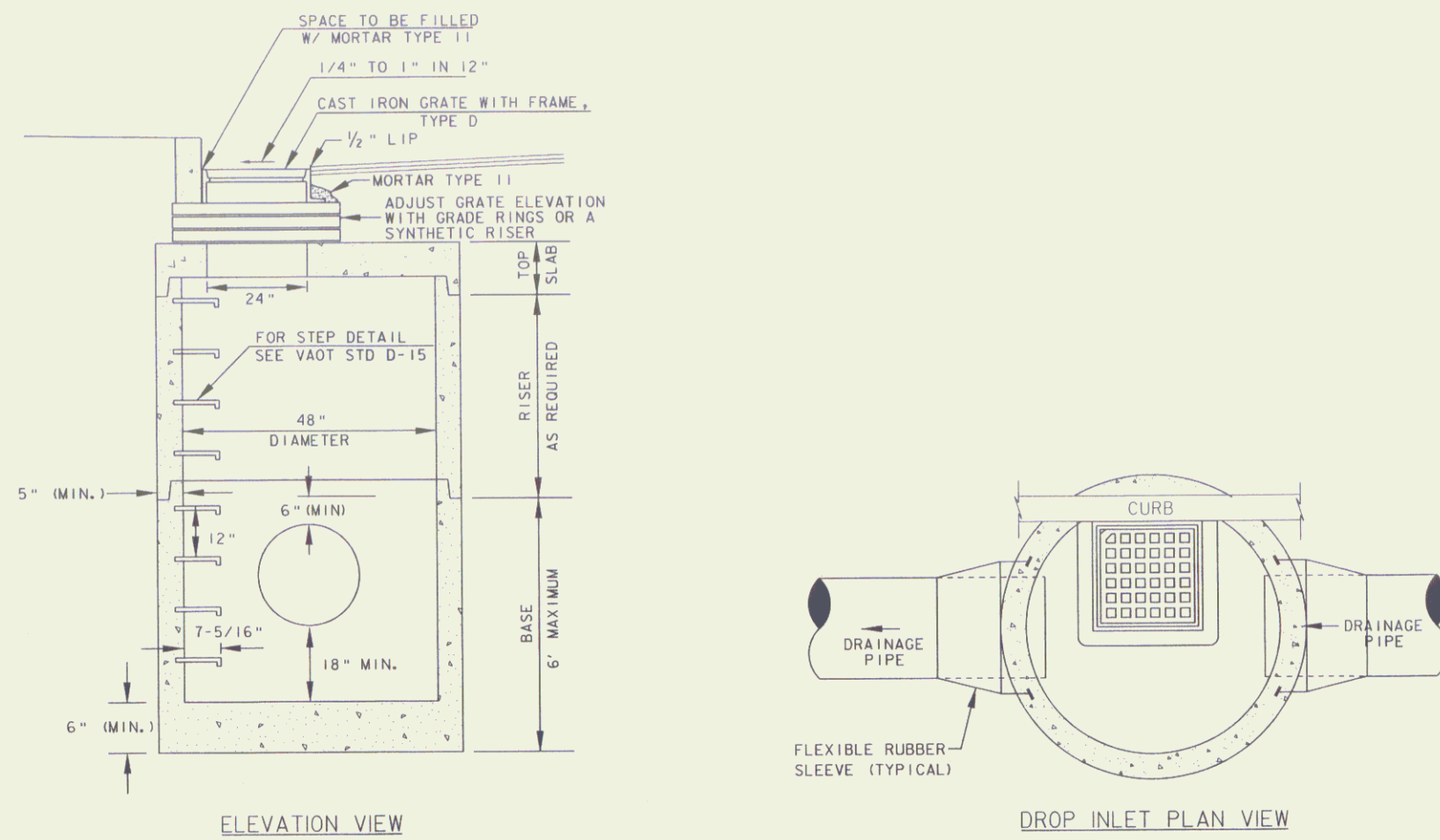
VERTICAL GRANITE CURB INSTALLATION
 STA. 1+50, LT. - 105+08.4, RT.
 STA. 105+16, LT. - 5+35.2, RT.
 STA. 3+78.2 - 4+27.8, LT.
 STA. 4+86.2 - 6+16.0, LT.
 (NOT TO SCALE)



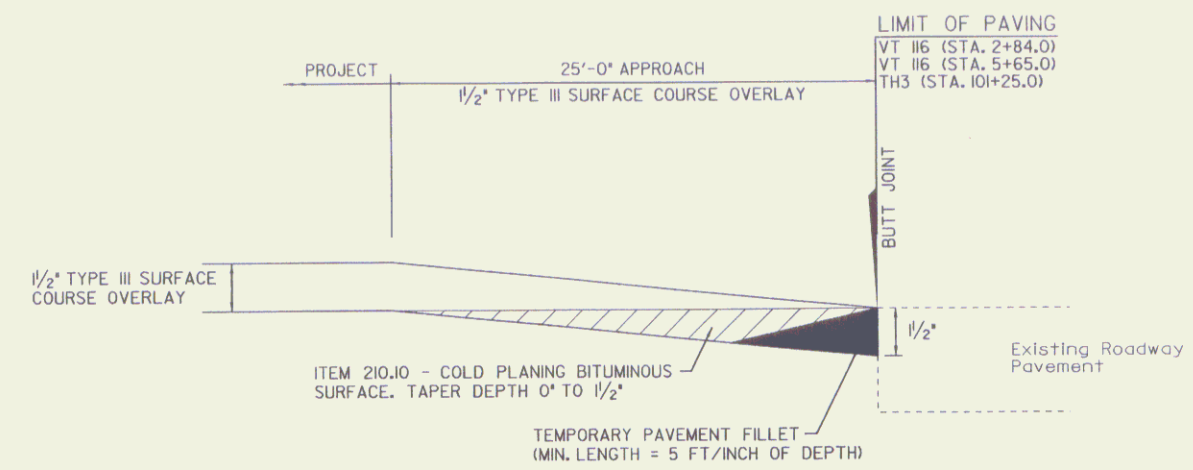
TYPICAL SECTIONS

PROJECT NAME: HINESBURG	PLOT DATE: ***DATE***
PROJECT NUMBER: HES 021-1(20)	DRAWN BY: D. PECK
FILE NAME: ***FILENAME***	CHECKED BY: G. BAKOS
PROJECT LEADER: G. BAKOS	DESIGNED BY: D. PECK
DESIGNED BY: D. PECK	ROW SHEET 2 OF 9





TYPICAL PRECAST DROP INLET INSTALLED IN ROADWAY
NOT TO SCALE



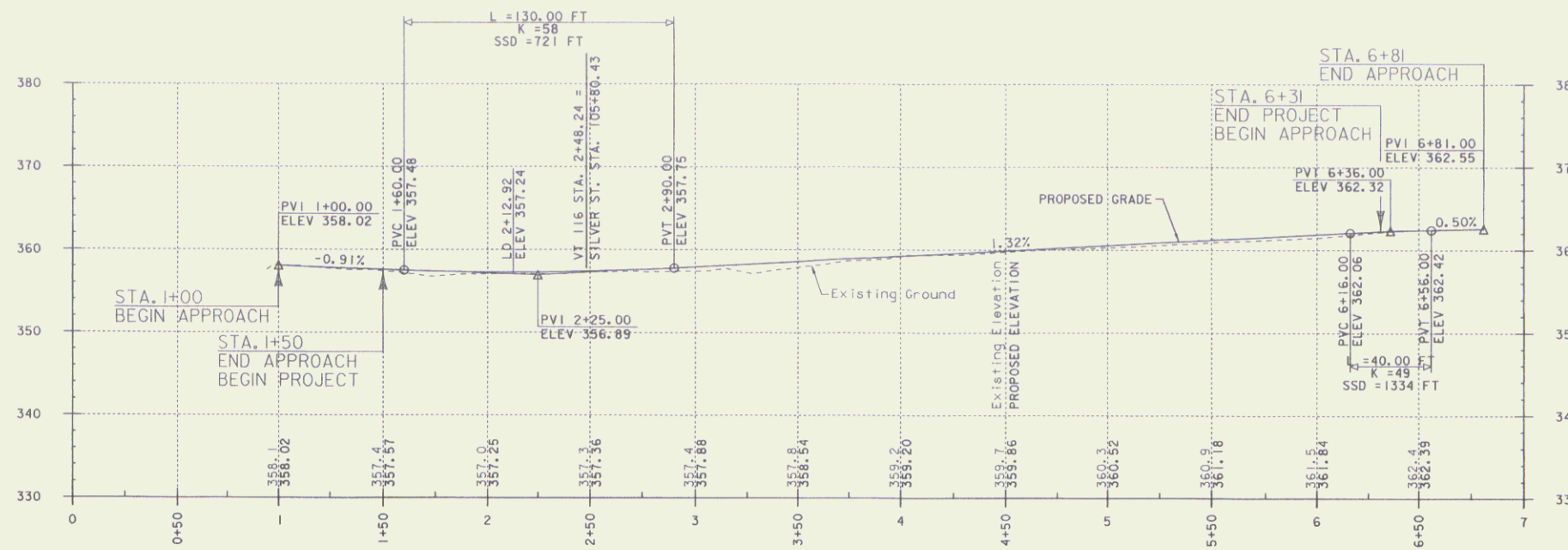
OVERLAY PAVEMENT MATCH TRANSITION DETAIL
NOT TO SCALE

PRECAST CONCRETE DROP INLET AND MANHOLE NOTES:

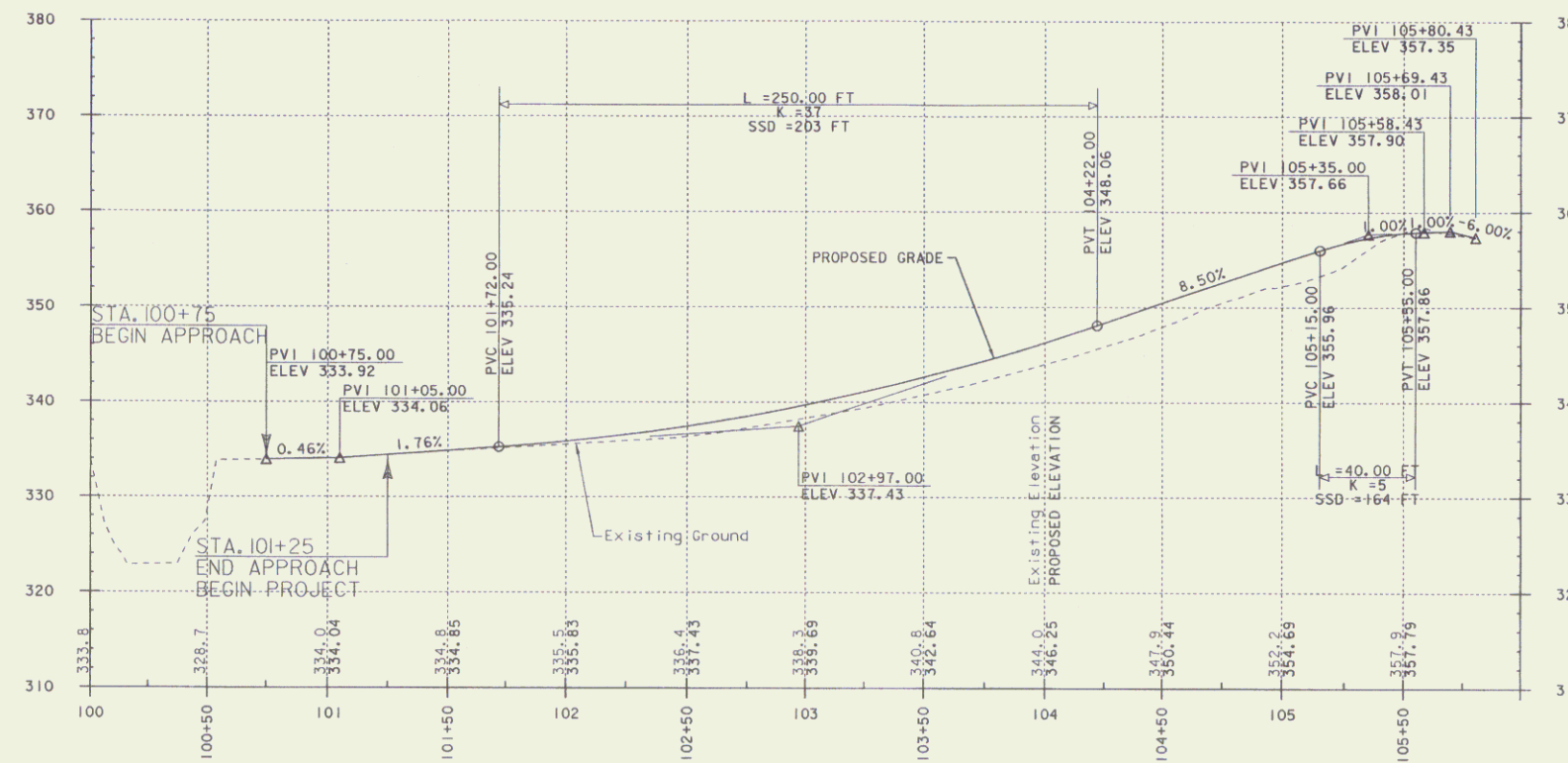
1. PRECAST CONCRETE SECTIONS SHALL CONFORM TO THE STANDARD SPECIFICATIONS AND ASTM C-478.
2. MINIMUM CONCRETE COMPRESSIVE STRENGTH: 4,000 PSI AT 28-DAYS
3. STEEL REINFORCING SHALL CONFORM TO ASTM A185 OR A82 FOR HS25 LOADING.
4. MANHOLE STEPS SHALL BE 14" WIDE STEEL REINFORCED COPOLYMER POLYPROPYLENE PLASTIC CONFORMING TO ASTM C-478 AND SHALL BE CAST INTO MANHOLE SECTIONS BY THE PRECAST CONCRETE MANUFACTURER.
5. FACE OF PIPE SHALL NOT PROJECT MORE THAN 2" OR LESS THAN 1" FROM INSIDE WALL OF STRUCTURE.
6. ALL STRUCTURES WITH MULTIPLE PIPES SHALL HAVE A MINIMUM OF 12" OF OUTSIDE SURFACE BETWEEN HOLES, NO MORE THAN 75% OF A HORIZONTAL CROSS-SECTION SHALL BE HOLES, AND THERE SHALL BE NO HOLES CLOSER THAN 3" TO JOINTS.
7. FITTING FRAME TO FINAL GRADE MAY BE DONE WITH A SYNTHETIC RISER OR WITH PRECAST CONCRETE GRADE RINGS OF APPROPRIATE THICKNESS (3 COURSES MAX).
8. ALL PIPE INVERTS AND PENETRATION ANGLES SHALL BE FIELD VERIFIED PRIOR TO PRECASTING.
9. PRECAST SECTIONS SHALL HAVE A TONGUE AND GROOVE JOINT AND BE ASSEMBLED USING A BUTYL RUBBER OR APPROVED EQUAL SEALANT.
10. PROVIDE FLEXIBLE RUBBER SLEEVES CONFORMING TO ASTM C-923, RESILIENT, OF SIZE REQUIRED, FOR EACH PIPE CONNECTING TO STRUCTURE. SLEEVES SHALL BE CAST INTO PRECAST STRUCTURE BY THE MANUFACTURER FOR ALL PIPE PENETRATIONS.
11. DROP INLET GRATE ORIENTATION SHALL BE IN ACCORDANCE WITH STANDARD DRAWING D-15 FOR TYPE D GRATES.
12. INSTALLATION OF DROP INLETS OVER EXISTING PIPES SHALL INCLUDE CLEAN CUTTING OF EXISTING PIPES, PROVIDING AN EXTENSION PIPE OF SIMILAR MATERIAL AND SIZE AS THE EXISTING PIPE, COUPLINGS REQUIRED FOR THE CONNECTION BETWEEN THE EXTENSION PIPE AND THE EXISTING PIPE, AND INSTALLING FLEXIBLE RUBBER SLEEVES AS SHOWN IN DETAILS PROVIDED ON THIS SHEET.
13. PAYMENT FOR INSTALLATION OF THE DROP INLETS SHALL BE MADE UNDER THE PRECAST REINFORCED CONC. DROP INLET WITH CAST IRON GRATE (ITEM 604.18).

VHB Vanasse Hangen Brustlin, Inc.

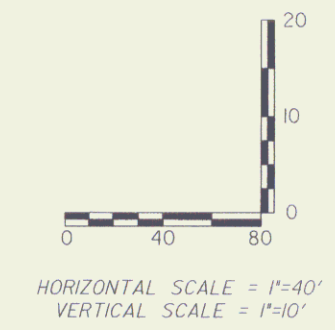
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PROJECT NUMBER:	HES 021-1(20)	DRAWN BY:	D. PECK
FILE NAME: ***FILENAME***		CHECKED BY:	G. BAKOS
PROJECT LEADER:	G. BAKOS	ROW SHEET:	3 OF 9
DESIGNED BY:	D. PECK		
DETAILS SHEET			



PROFILE VT ROUTE 116



PROFILE SILVER ST



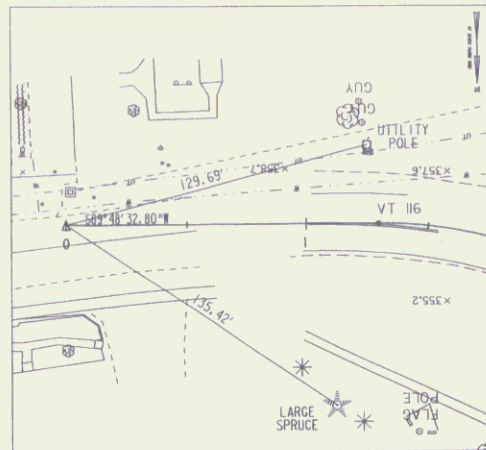
AERIAL SURVEY BY: COL-EAST, INC.	
SURVEYED DATE: 2004	
DATUM	
VERTICAL	NAVD 88
HORIZONTAL	NAD 83

PROFILES	
PROJECT NAME:	HINESBURG
PROJECT NUMBER:	HES 021-1(20)
FILE NAME: ***FILENAME***	PLOT DATE: ***DATE***
PROJECT LEADER: G. BAKOS	DRAWN BY: D. PECK
DESIGNED BY: D. PECK	CHECKED BY: G. BAKOS
	ROW SHEET 4 OF 9

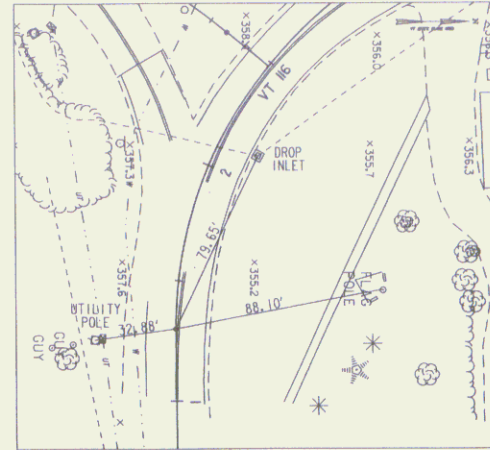


VT ROUTE 116

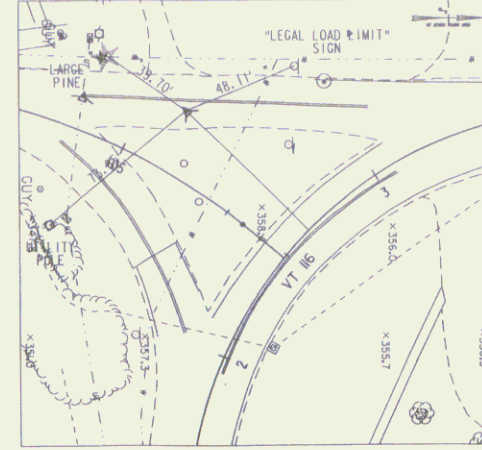
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E = 1481059.9985



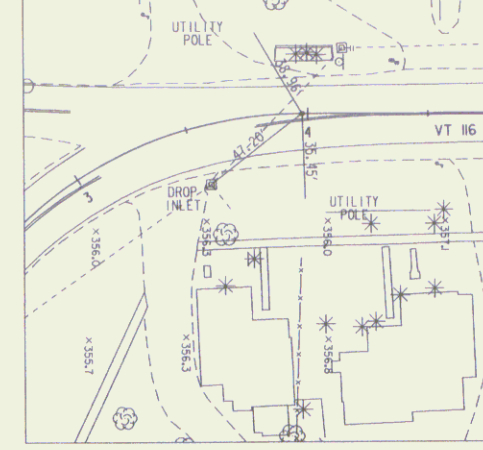
PC 1+30.00
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E = 1480929.9992



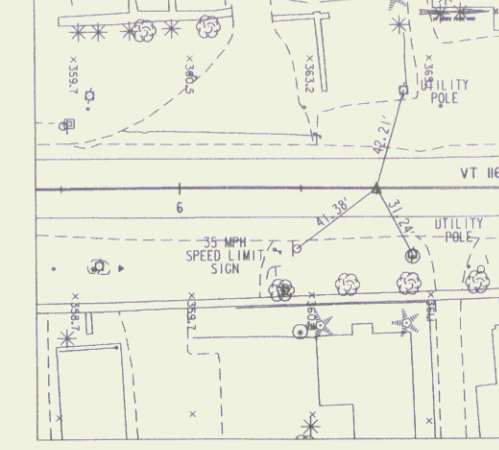
PI 3+00.72
N = 666353.2897
E = 1480759.2754



PT 3+97.76
N = 666524.0143
E = 1480759.4329

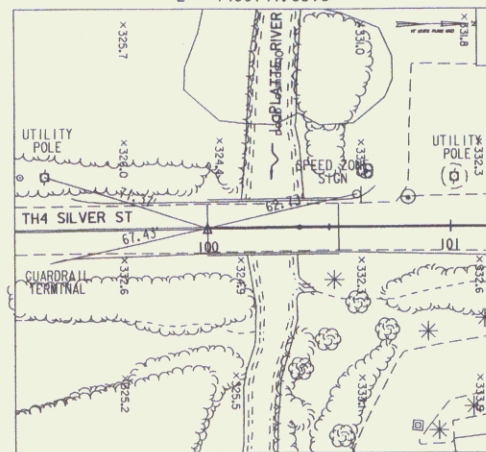


POE 6+80.99
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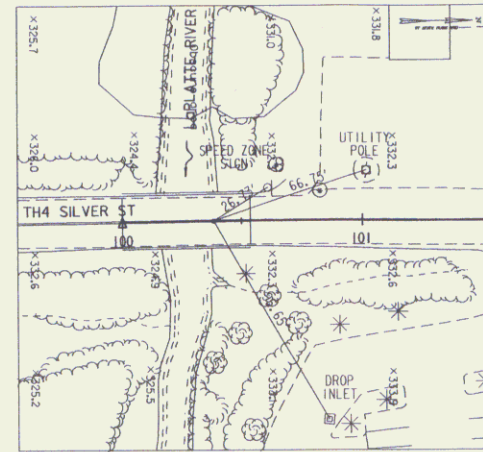


TH4 SILVER STREET

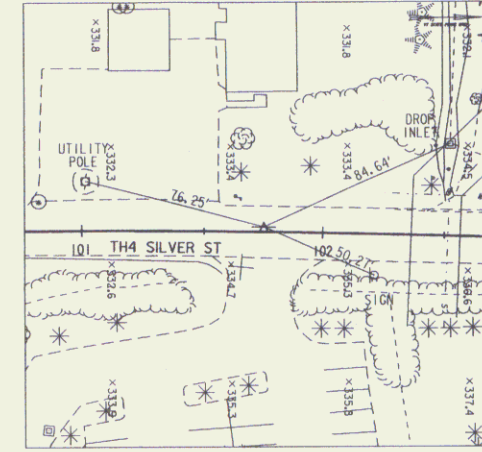
POB 100+00.00
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E = 1480741.6373



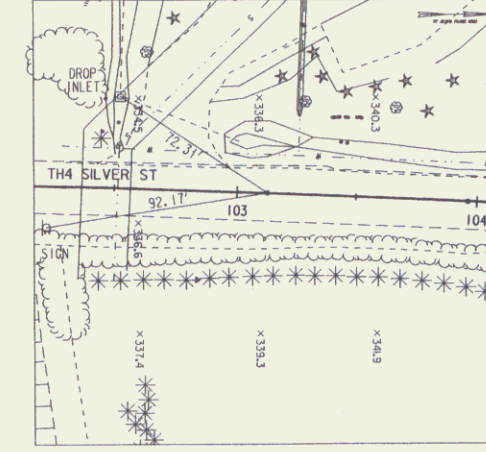
PC 100+38.20
N = 665869.5383
E = 1480741.1446



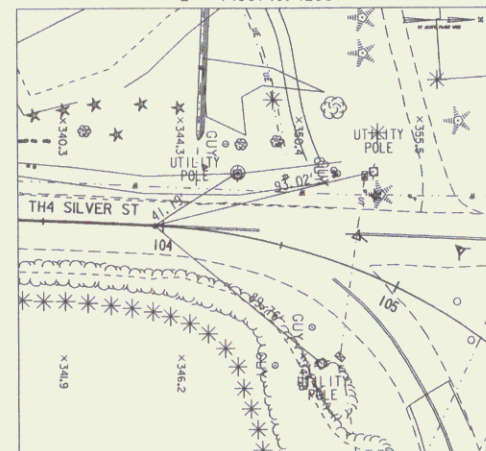
PI 101+75.37
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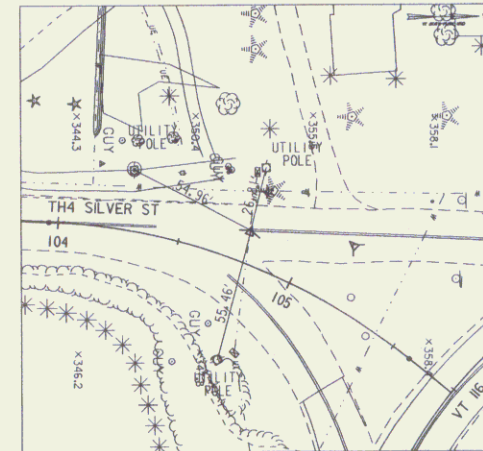
PT 103+12.46
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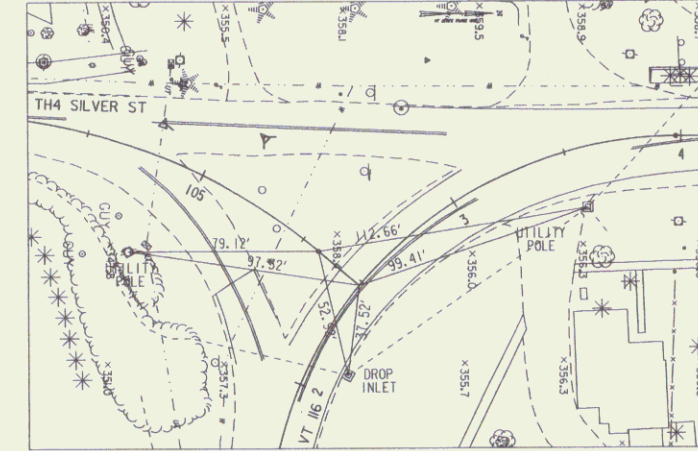
PC 103+96.45
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E = 1480749.4253



PI 104+80.23
N = 666311.3254
E = 1480753.2325



PT 105+58.14
N = 666375.8263
E = 1480806.7071



POE 105+80.43
N = 666392.9848
E = 1480820.9324

TIE SHEET

PROJECT NAME: HINESBURG
PROJECT NUMBER: HES 021-1(20)

FILE NAME: ***FILENAME***
PROJECT LEADER: G. BAKOS
DESIGNED BY: D. PECK

PLOT DATE: ***DATE***
DRAWN BY: D. PECK
CHECKED BY: G. BAKOS
ROW SHEET 5 OF 9

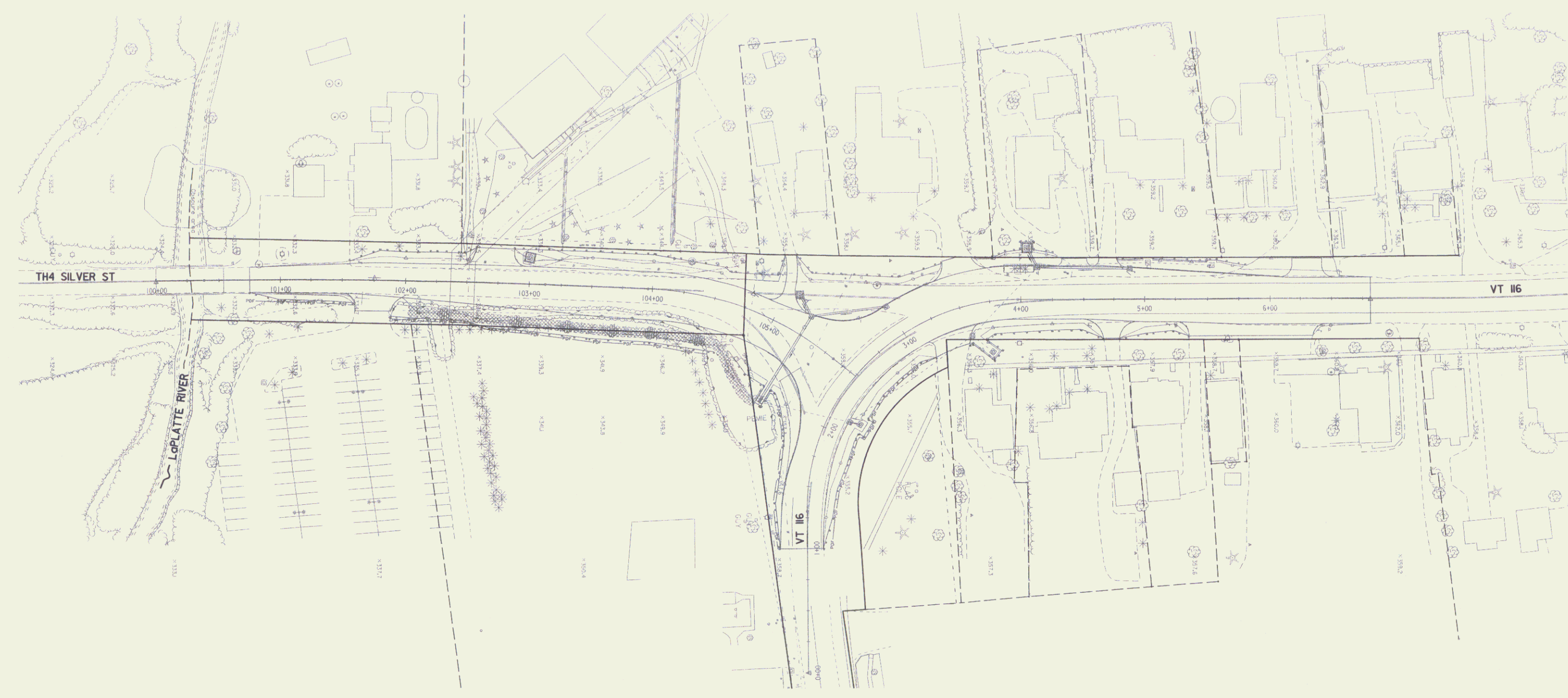
AERIAL SURVEY BY: COL-EAST, INC.
SURVEYED DATE: 2004

DATUM

VERTICAL: NAVD 88
HORIZONTAL: NAD 83

0 40 80
SCALE IN FEET

VHB Vanasse Hangen Brustlin, Inc.



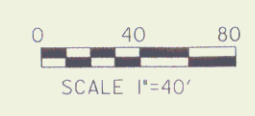
AERIAL SURVEY BY: COL-EAST, INC.
 SURVEYED DATE: 2004

DATUM

VERTICAL NAVD_88

HORIZONTAL NAD_83

LEGEND	
	TOE OF FILL SLOPE
	TOP OF CUT SLOPE
	SILT FENCE
	PROJECT DEMARCATION FENCE DEFINING LIMITS OF DISTURBANCE
	TEMPORARY STONE CHECK DAM
	INLET PROTECTION DEVICE
	TEMPORARY EROSION MATTING



EPSC PLAN	
PROJECT NAME:	HINESBURG
PROJECT NUMBER:	HES 021-1(20)
FILE NAME: ***FILENAME***	PLOT DATE: ***DATE***
PROJECT LEADER: G. BAKOS	DRAWN BY: D. PECK
DESIGNED BY: D. PECK	CHECKED BY: G. BAKOS
	ROW SHEET 6 OF 9

CONSTRUCTION NOTES

EXCAVATION OF SURFACES AND PAVEMENTS

STATION 1+50, LT. - 105+00, RT.
STATION 104+50, LT. - 3+40, LT.

COLD PLANING - BITUMINOUS PAVEMENT

STATION 100+75 - 101+25
STATION 1+00 - 1+50
STATION 6+31 - 6+81

RELOCATE MAILBOX - SINGLE SUPPORT

STATION 101+63, LT.
STATION 104+90, LT.
STATION 4+35, LT.
STATION 4+54, RT.
STATION 4+90, LT.
STATION 5+18, RT.
STATION 6+33, RT.
STATION 6+55, LT.

RELOCATE MAILBOX - MULTIPLE SUPPORT

STATION 3+87, LT.

VERTICAL GRANITE CURB

STATION 1+50, LT. - 105+08.4, RT.
STATION 105+16, LT. - 3+35.2, LT.
STATION 3+78.2 - 4+27.8, LT.
STATION 4+86.2 - 6+07.4, LT.

CONSTRUCT DRIVES - 1 1/2" BCP, TYPE III

STATION 101+75, RT. - 28' WIDE
STATION 102+73, LT. - 30' WIDE
STATION 105+01, LT. - 10' WIDE
STATION 3+35, RT. - 29' WIDE (COMM.)
STATION 3+57, LT. - 23' WIDE
STATION 4+43, LT. - 20' WIDE (COMM.)
STATION 4+70, LT. - 13.5' WIDE
STATION 4+73, RT. - 14.5' WIDE
STATION 5+85, RT. - 99' WIDE
STATION 6+30, LT. - 24' WIDE

TRANSPLANTING SHRUBS

STATION 103+00 - 104+15, RT. (14 SHRUBS)

REMOVE & RESET SIGN (COMM. - LIGHTED)

STATION 4+00, LT. (3 SIGNS)

DRAINAGE NOTES

- ① STA. 2+15.1, RT. 24.8 - STA. 2+12.8, RT. 16.8
NEW PRECAST DI, TYPE A
RIM ELEV. = 355.15
NEW 12" x 4.5 LF PCPSP
INV. OUT = 351.95
- ② STA. 2+12.8, RT. 16.8
ADJUST DI
REMOVE DI GRATE & FRAME
REPLACE WITH MH COVER & FRAME
RIM ELEV. = 356.08
- ③ STA. 3+71.0, RT. 41.0
REMOVE EXIST DI
NEW PRECAST DI, TYPE D
CONST. OVER EXIST. 6" PVC
RIM ELEV. = 356.00
- ④ STA. 3+54.1, RT. 24.6
ADJUST DI
REMOVE DI GRATE & FRAME
REPLACE WITH MH COVER & FRAME
RIM ELEV. = 356.47

- ⑤ STA. 4+88.2, LT. 24.8 - STA. 4+13.1, LT. 26.0
NEW PRECAST DI, TYPE D
RIM ELEV. = 359.69
NEW 15" x 7.2 LF PCPSP
15" INV. OUT (S) = 354.25
- ⑥ STA. 4+05.0, LT. 41.6 - STA. 4+13.0, LT. 26.0
NEW PRECAST DI, TYPE A
RIM ELEV. = 357.64
NEW 12" x 13.1 LF PCPSP
INV. OUT = 354.43
- ⑦ STA. 4+13.1, LT. 26.0
NEW PRECAST DI, TYPE D
RIM ELEV. = 359.27
15" INV. OUT (E) = 353.3 (Exist.)
15" INV. IN (N) = 353.55
12" INV. IN (W) = 353.65
REMOVE EXIST. DI
- ⑧ STA. 3+00, LT. 26.0 - STA. 105+25.0, LT. 17.7
NEW PRECAST DI, TYPE D
RIM ELEV. = 358.10
NEW 12" x 55.5 LF PCPSP
INV. OUT = 352.76
- ⑨ STA. 105+09.5, LT. 28.1 - STA. 105+25.0, LT. 17.7
NEW PRECAST DI, TYPE A
RIM ELEV. = 353.00
NEW 12" x 16.0 LF PCPSP
12" INV. OUT = 349.83

- ⑩ STA. 105+25.0, LT. 17.7 - STA. 105+25.0, RT. 59.1
NEW PRECAST DI, TYPE D
RIM ELEV. = 355.05
NEW 18" x 74.9 LF RCP
18" INV. OUT = 349.75
12" INV. IN (S) = 349.75
12" INV. IN (N) = 350.43
INV. AT OUTLET = 349.37
CONST. TYPE II STONE PAD (2' x 5' x 2' DEEP)
- ⑪ STA. 103+00.0, LT. 21.3
NEW PRECAST DI, TYPE A
RIM ELEV. = 337.50
INV. OUT = 331.0 (Exist.)
- ⑫ STA. 5+02.8, LT. 29.4 - STA. 5+53.4, LT. 27.2
REMOVE 47 LF X 12" RCP
REMOVE DI, +53.4, LT. 27.2
- ⑬ STA. 4+14.2, LT. 27.3 - STA. 5+02.8, LT. 29.4
REMOVE 83 LF X 12" RCP
REMOVE DI, +02.8, LT. 29.4

NOTES

- 1. THE DRAINAGE STRUCTURE RIM AND INVERT ELEVATIONS ARE BASED ON SURVEY INFORMATION FROM AERIAL SURVEY. THE CONTRACTOR SHALL SET ACTUAL RIM ELEVATIONS ONE INCH BELOW FINISHED PAVEMENT ELEVATIONS. THE CONTRACTOR SHALL FIELD CHECK PROPOSED PIPE INVERTS BEFORE ORDERING DRAINAGE STRUCTURES.
- 2. THE USE OF BRICK AND MORTAR TO ADJUST THE ELEVATION OF DRAINAGE STRUCTURES IS PROHIBITED. ALL ELEVATION ADJUSTMENTS SHALL BE MADE USING EITHER GRADE RINGS OR A SYNTHETIC RISER.
- 3. ALL CONNECTIONS BETWEEN PRECAST DRAINAGE STRUCTURES AND DRAINAGE PIPES SHALL BE A BOOTED CONNECTION.

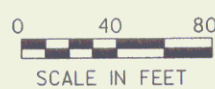
04B206

PROJECT NAME: HINESBURG
PROJECT NUMBER: HES 021-K20

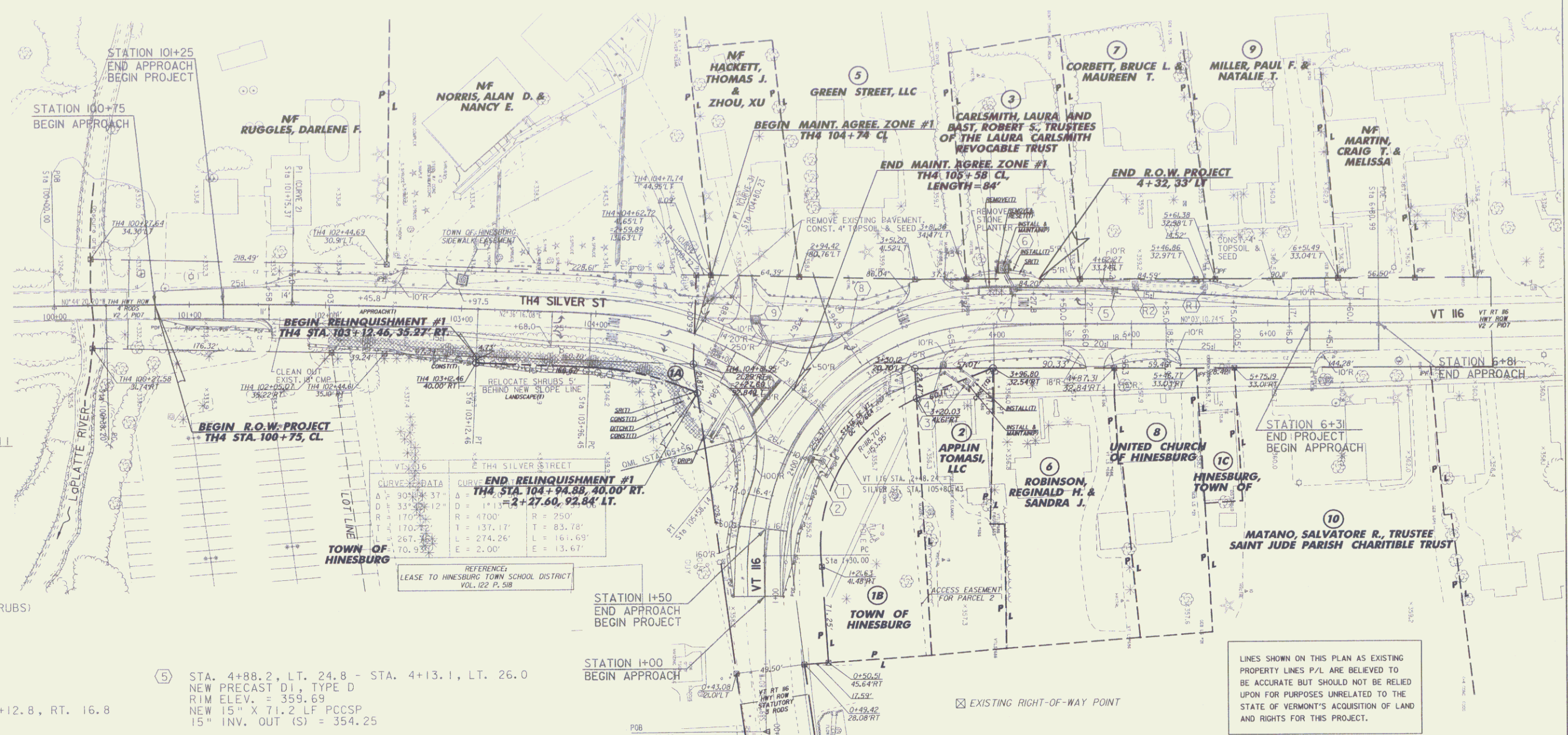
FILE NAME: ***FILENAME***
PROJECT LEADER: G. BAKOS
DESIGNED BY: D. PECK

PLOT DATE: 05-MAR-2008
DRAWN BY: D. PECK
CHECKED BY: G. BAKOS
R. O. W. SHEET 9 OF 9

AERIAL SURVEY BY: COL-EAST, INC.	
SURVEYED DATE: 2004	
DATUM	
VERTICAL	NAVD 88
HORIZONTAL	NAD 83



Vanasse Hangen Brustlin, Inc.



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