

Parcel	Kind of Instrument	Date of Instrument	Grantor	Grantee	Recorded	Remarks
1	Q.C.D.	1-4-33	Charles Tyler	Staff	39 471	1-19-33
2	"	1-8-33	David Yandow	"	39 472	1-19-33
3	"	1-31-33	L.E.A. Rocelleau	"	39 481-401	1-7-33

ALL COLLECTS RETAINED ARE IN GOOD CONDITION, A LARGE PART OF THEM HAVING BEEN CONSTRUCTED AS PART OF T&E C-98-D IN 1932.

ALL COLLECTS RETAINED ARE IN GOOD CONDITION, A LARGE PART OF THEM HAVING BEEN CONSTRUCTED AS PART OF T&E C-98-D IN 1932.

D.M. #1
 30' ELM ST
 STA 149+00
 ELEV 187.96

D.M. #2
 30' ELM ST
 STA 154+00
 ELEV 205.08

D.M. #3
 30' ELM ST
 STA 159+00
 ELEV 224.18

D.M. #4
 30' ELM ST
 STA 164+00
 ELEV 243.28

D.M. #5
 30' ELM ST
 STA 169+00
 ELEV 262.38

D.M. #6
 30' ELM ST
 STA 174+00
 ELEV 281.48

D.M. #7
 30' ELM ST
 STA 179+00
 ELEV 300.58

D.M. #8
 30' ELM ST
 STA 184+00
 ELEV 319.68

D.M. #9
 30' ELM ST
 STA 189+00
 ELEV 338.78

D.M. #10
 30' ELM ST
 STA 194+00
 ELEV 357.88

D.M. #11
 30' ELM ST
 STA 199+00
 ELEV 376.98

D.M. #12
 30' ELM ST
 STA 204+00
 ELEV 396.08

D.M. #13
 30' ELM ST
 STA 209+00
 ELEV 415.18

D.M. #14
 30' ELM ST
 STA 214+00
 ELEV 434.28

D.M. #15
 30' ELM ST
 STA 219+00
 ELEV 453.38

D.M. #16
 30' ELM ST
 STA 224+00
 ELEV 472.48

D.M. #17
 30' ELM ST
 STA 229+00
 ELEV 491.58

D.M. #18
 30' ELM ST
 STA 234+00
 ELEV 510.68

D.M. #19
 30' ELM ST
 STA 239+00
 ELEV 529.78

D.M. #20
 30' ELM ST
 STA 244+00
 ELEV 548.88

D.M. #21
 30' ELM ST
 STA 249+00
 ELEV 567.98

D.M. #22
 30' ELM ST
 STA 254+00
 ELEV 587.08

D.M. #23
 30' ELM ST
 STA 259+00
 ELEV 606.18

D.M. #24
 30' ELM ST
 STA 264+00
 ELEV 625.28

D.M. #25
 30' ELM ST
 STA 269+00
 ELEV 644.38

D.M. #26
 30' ELM ST
 STA 274+00
 ELEV 663.48

D.M. #27
 30' ELM ST
 STA 279+00
 ELEV 682.58

D.M. #28
 30' ELM ST
 STA 284+00
 ELEV 701.68

D.M. #29
 30' ELM ST
 STA 289+00
 ELEV 720.78

D.M. #30
 30' ELM ST
 STA 294+00
 ELEV 739.88

D.M. #31
 30' ELM ST
 STA 299+00
 ELEV 758.98

D.M. #32
 30' ELM ST
 STA 304+00
 ELEV 778.08

D.M. #33
 30' ELM ST
 STA 309+00
 ELEV 797.18

D.M. #34
 30' ELM ST
 STA 314+00
 ELEV 816.28

D.M. #35
 30' ELM ST
 STA 319+00
 ELEV 835.38

D.M. #36
 30' ELM ST
 STA 324+00
 ELEV 854.48

D.M. #37
 30' ELM ST
 STA 329+00
 ELEV 873.58

D.M. #38
 30' ELM ST
 STA 334+00
 ELEV 892.68

D.M. #39
 30' ELM ST
 STA 339+00
 ELEV 911.78

D.M. #40
 30' ELM ST
 STA 344+00
 ELEV 930.88

D.M. #41
 30' ELM ST
 STA 349+00
 ELEV 949.98

D.M. #42
 30' ELM ST
 STA 354+00
 ELEV 969.08

D.M. #43
 30' ELM ST
 STA 359+00
 ELEV 988.18

D.M. #44
 30' ELM ST
 STA 364+00
 ELEV 1007.28

D.M. #45
 30' ELM ST
 STA 369+00
 ELEV 1026.38

D.M. #46
 30' ELM ST
 STA 374+00
 ELEV 1045.48

D.M. #47
 30' ELM ST
 STA 379+00
 ELEV 1064.58

D.M. #48
 30' ELM ST
 STA 384+00
 ELEV 1083.68

D.M. #49
 30' ELM ST
 STA 389+00
 ELEV 1102.78

D.M. #50
 30' ELM ST
 STA 394+00
 ELEV 1121.88

D.M. #51
 30' ELM ST
 STA 399+00
 ELEV 1140.98

D.M. #52
 30' ELM ST
 STA 404+00
 ELEV 1160.08

D.M. #53
 30' ELM ST
 STA 409+00
 ELEV 1179.18

D.M. #54
 30' ELM ST
 STA 414+00
 ELEV 1198.28

D.M. #55
 30' ELM ST
 STA 419+00
 ELEV 1217.38

D.M. #56
 30' ELM ST
 STA 424+00
 ELEV 1236.48

D.M. #57
 30' ELM ST
 STA 429+00
 ELEV 1255.58

D.M. #58
 30' ELM ST
 STA 434+00
 ELEV 1274.68

D.M. #59
 30' ELM ST
 STA 439+00
 ELEV 1293.78

D.M. #60
 30' ELM ST
 STA 444+00
 ELEV 1312.88

D.M. #61
 30' ELM ST
 STA 449+00
 ELEV 1331.98

D.M. #62
 30' ELM ST
 STA 454+00
 ELEV 1351.08

D.M. #63
 30' ELM ST
 STA 459+00
 ELEV 1370.18

D.M. #64
 30' ELM ST
 STA 464+00
 ELEV 1389.28

D.M. #65
 30' ELM ST
 STA 469+00
 ELEV 1408.38

D.M. #66
 30' ELM ST
 STA 474+00
 ELEV 1427.48

D.M. #67
 30' ELM ST
 STA 479+00
 ELEV 1446.58

D.M. #68
 30' ELM ST
 STA 484+00
 ELEV 1465.68

D.M. #69
 30' ELM ST
 STA 489+00
 ELEV 1484.78

D.M. #70
 30' ELM ST
 STA 494+00
 ELEV 1503.88

D.M. #71
 30' ELM ST
 STA 499+00
 ELEV 1522.98

D.M. #72
 30' ELM ST
 STA 504+00
 ELEV 1542.08

D.M. #73
 30' ELM ST
 STA 509+00
 ELEV 1561.18

D.M. #74
 30' ELM ST
 STA 514+00
 ELEV 1580.28

D.M. #75
 30' ELM ST
 STA 519+00
 ELEV 1599.38

D.M. #76
 30' ELM ST
 STA 524+00
 ELEV 1618.48

D.M. #77
 30' ELM ST
 STA 529+00
 ELEV 1637.58

D.M. #78
 30' ELM ST
 STA 534+00
 ELEV 1656.68

D.M. #79
 30' ELM ST
 STA 539+00
 ELEV 1675.78

D.M. #80
 30' ELM ST
 STA 544+00
 ELEV 1694.88

D.M. #81
 30' ELM ST
 STA 549+00
 ELEV 1713.98

D.M. #82
 30' ELM ST
 STA 554+00
 ELEV 1733.08

D.M. #83
 30' ELM ST
 STA 559+00
 ELEV 1752.18

D.M. #84
 30' ELM ST
 STA 564+00
 ELEV 1771.28

D.M. #85
 30' ELM ST
 STA 569+00
 ELEV 1790.38

D.M. #86
 30' ELM ST
 STA 574+00
 ELEV 1809.48

D.M. #87
 30' ELM ST
 STA 579+00
 ELEV 1828.58

D.M. #88
 30' ELM ST
 STA 584+00
 ELEV 1847.68

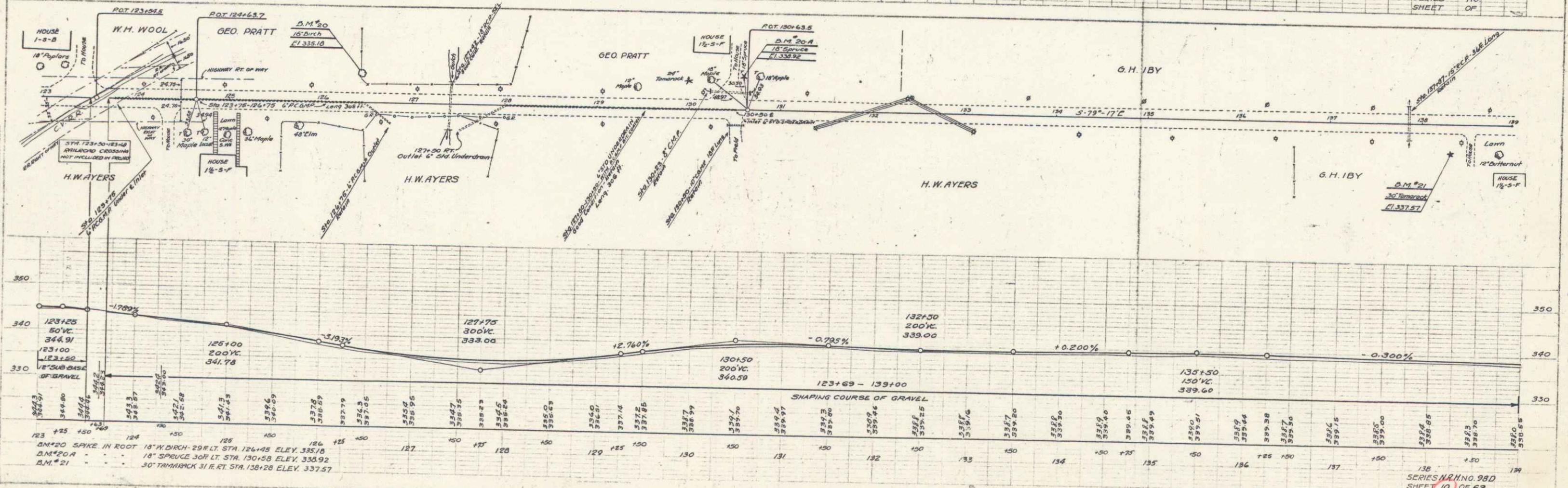
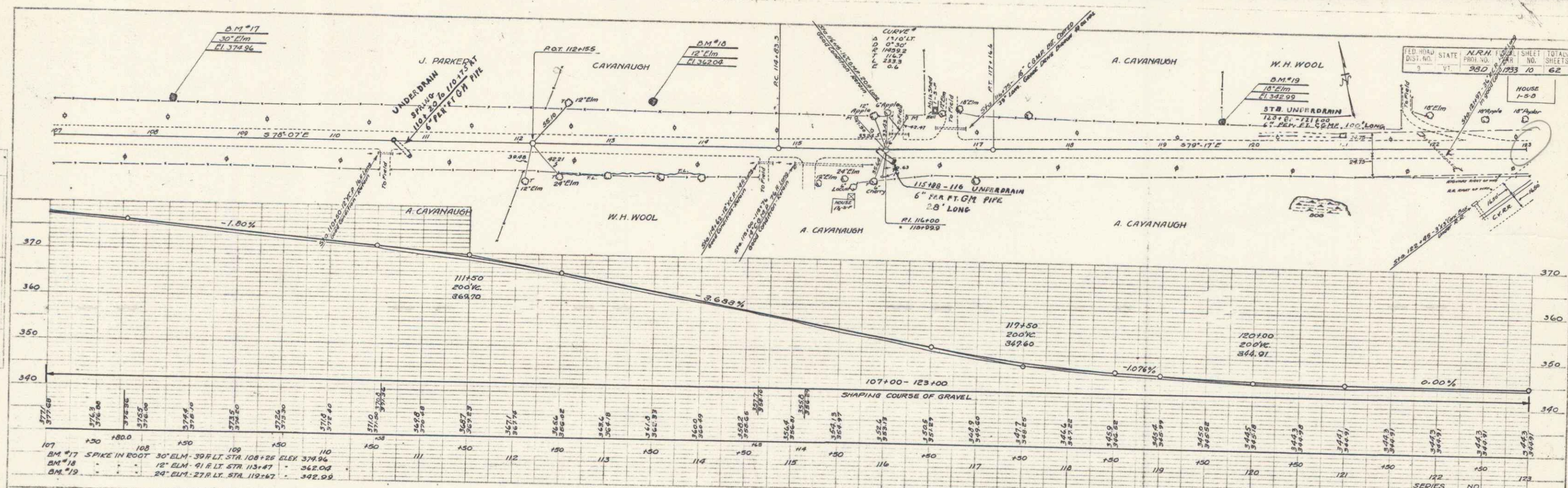
D.M. #89
 30' ELM ST
 STA 589+00
 ELEV 1866.78

D.M. #90
 30' ELM ST
 STA 594+00
 ELEV 1885.88

D.M. #91
 30' ELM ST
 STA 599+00
 ELEV 1904.98

D.M. #92
 30' ELM ST
 STA 604+00
 ELEV 1924.08

D.M. #93
 30' ELM ST
 STA 609+00
 ELEV 1



FED. ROAD DIST. NO.	STATE	N.R.R. PROJ. NO.	SHEET NO.	TOTAL SHEETS
380	VI	380	10	62

PLAN
 1" = 40'
 PROFILE
 1" = 10'

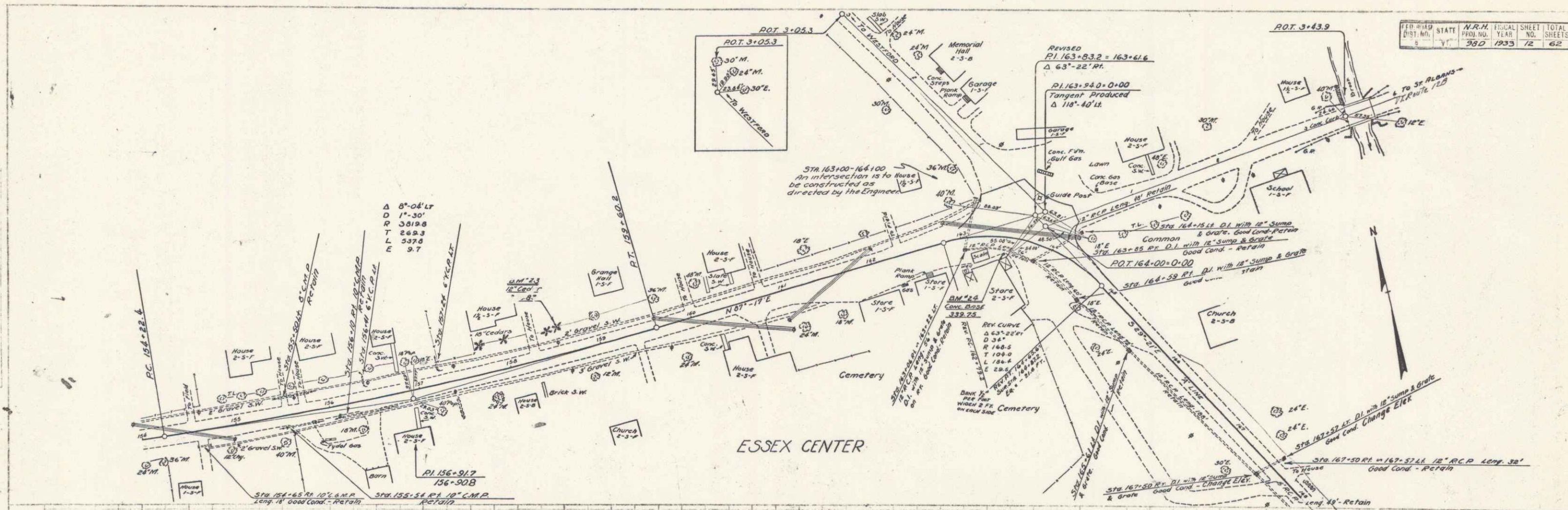
RETURNS
 1" = 40'

BM #17 SPIKE IN ROOT 30" ELM. 39' LT. STA. 108+26 ELEV. 374.94
 BM #18 12" ELM. 41' LT. STA. 113+47 362.00
 BM #19 24" ELM. 27' LT. STA. 119+87 342.99

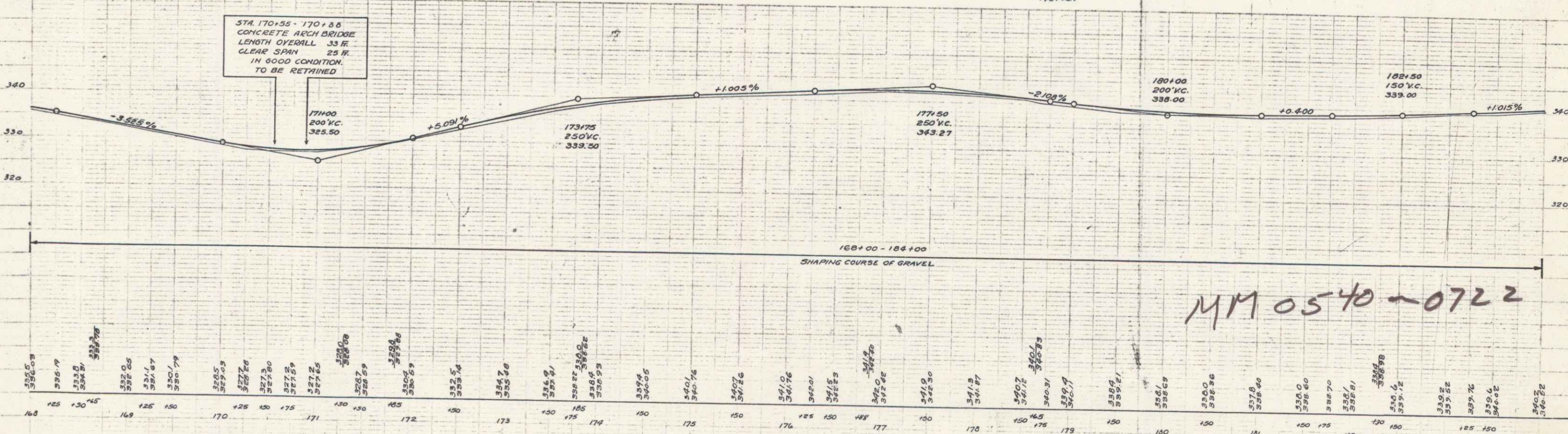
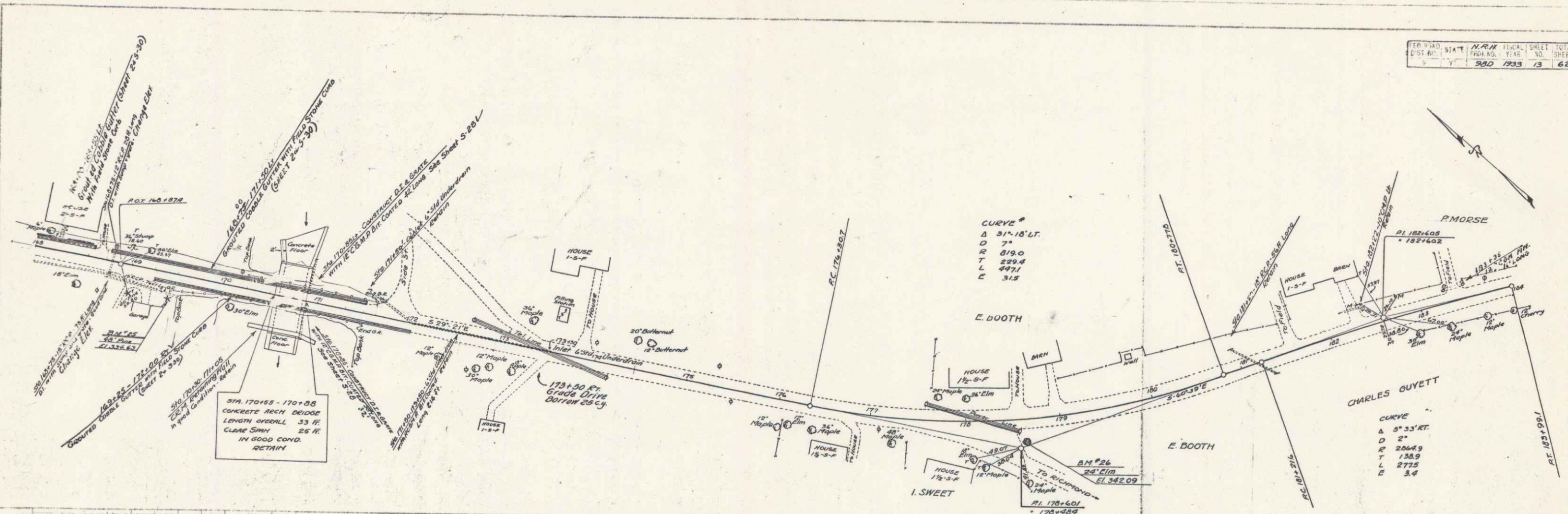
BM #20 SPIKE IN ROOT 18" W. DICH. 29' LT. STA. 124+45 ELEV. 335.18
 BM #20A 18" SPURCE 30' LT. STA. 130+58 ELEV. 335.92
 BM #21 30" TRAMPACK 31' RT. STA. 138+28 ELEV. 337.57

SERIES N.A. NO. 98D
 SHEET 10 OF 62

FED. ROAD DIST. NO.	STAT.	N.R.H. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
1	VT	380	1933	12	62

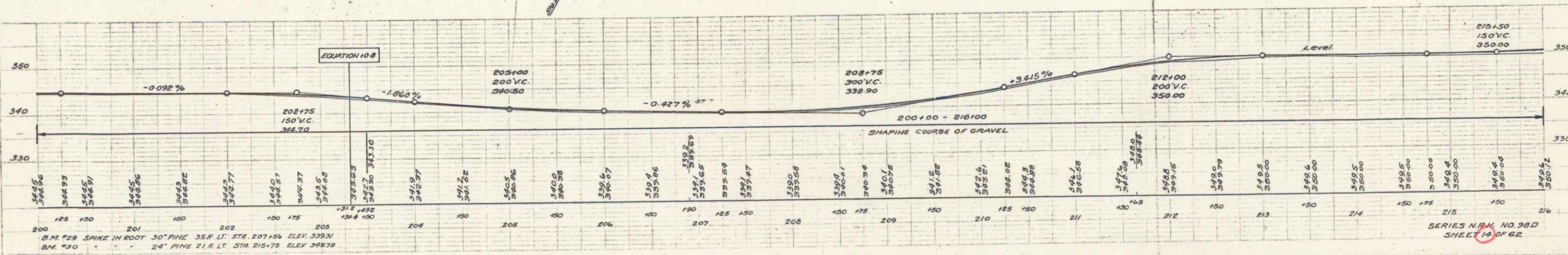
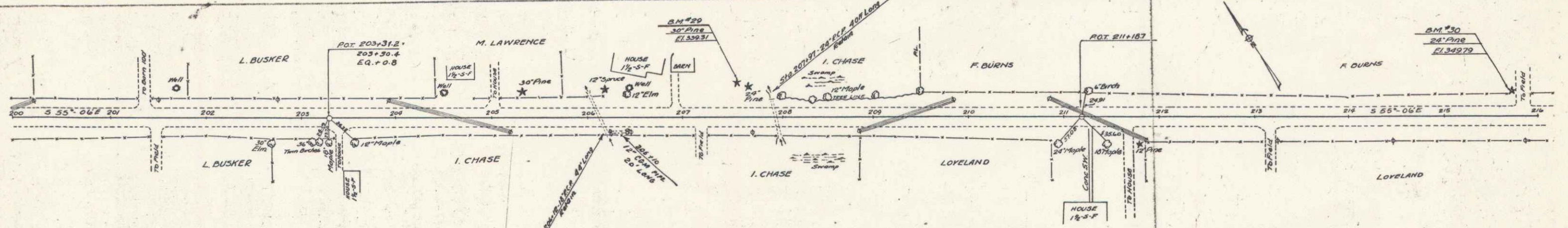
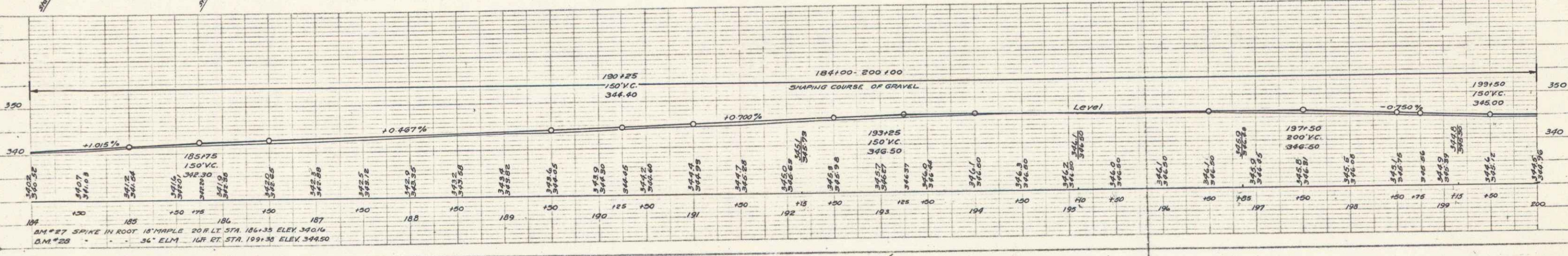
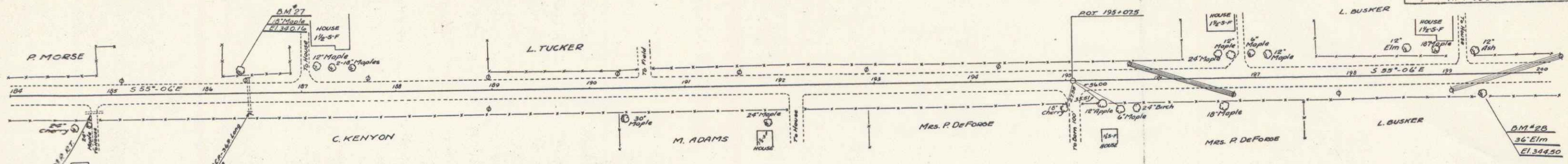


SERIES N.R.H. No. 380
 SHEET 12 OF 62



B.M. #25 SPIKE IN ROOT 48 INCH PINE 24 FT STA 169+42 ELEV 336.63
 B.M. #26 24 INCH ELM 25 FT STA 170+60 ELEV 342.09

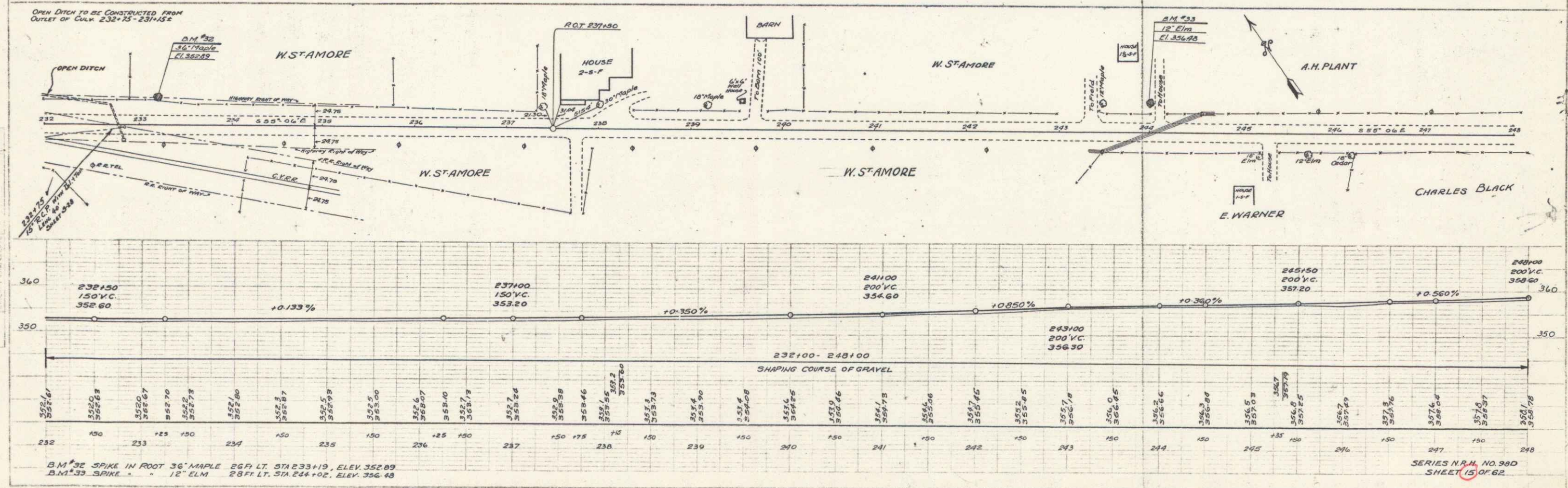
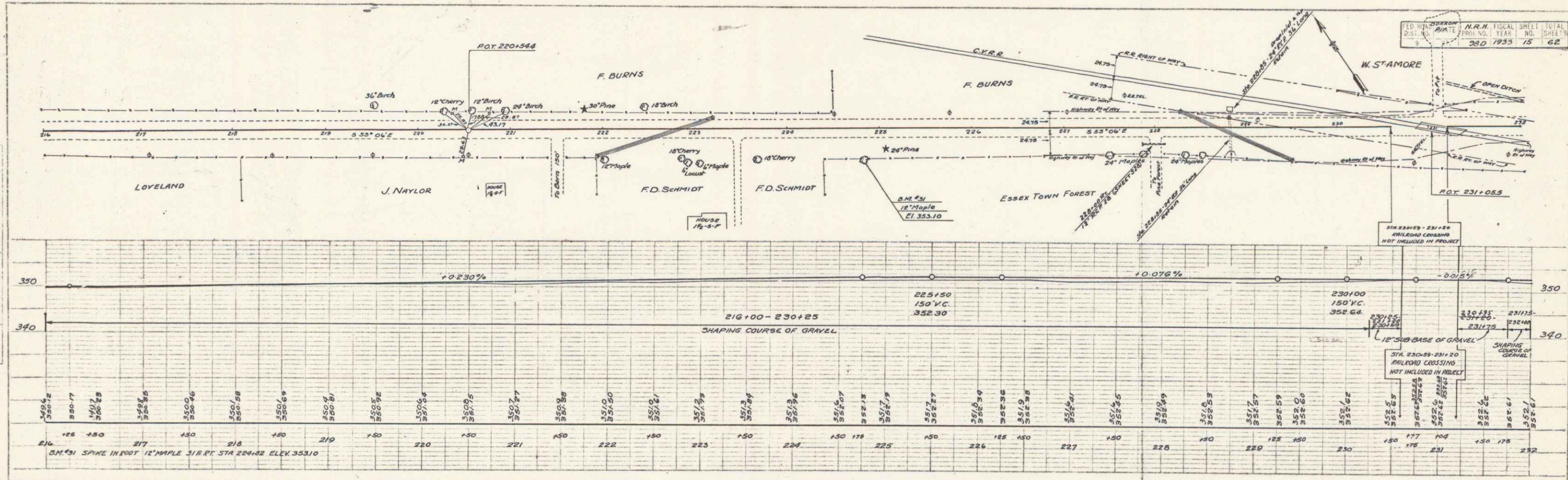
MM 0540-0722



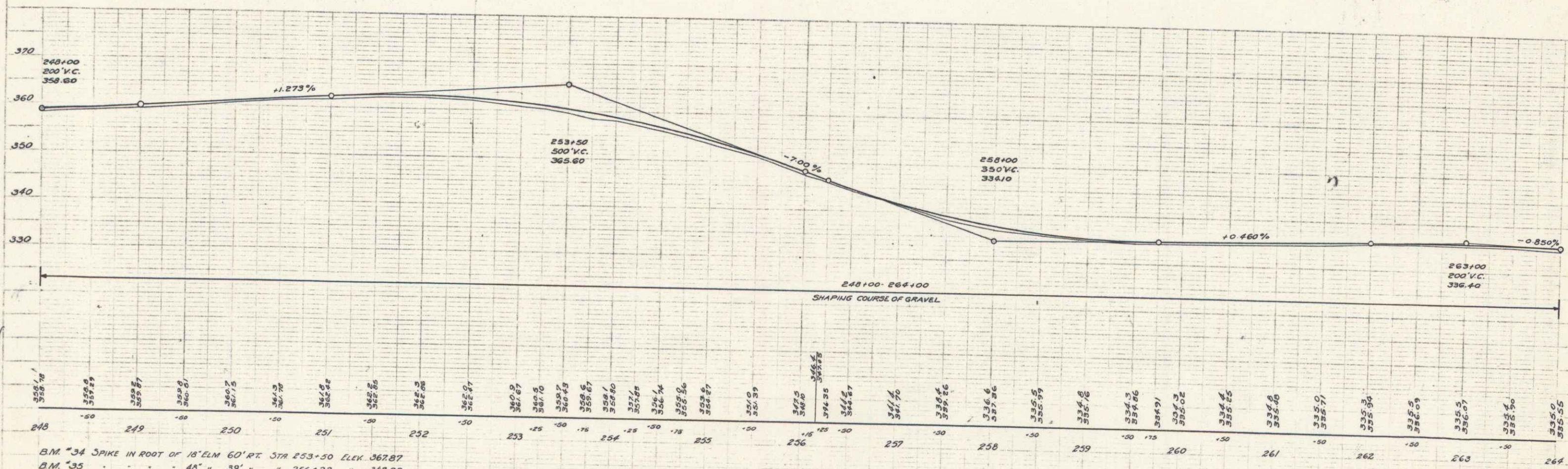
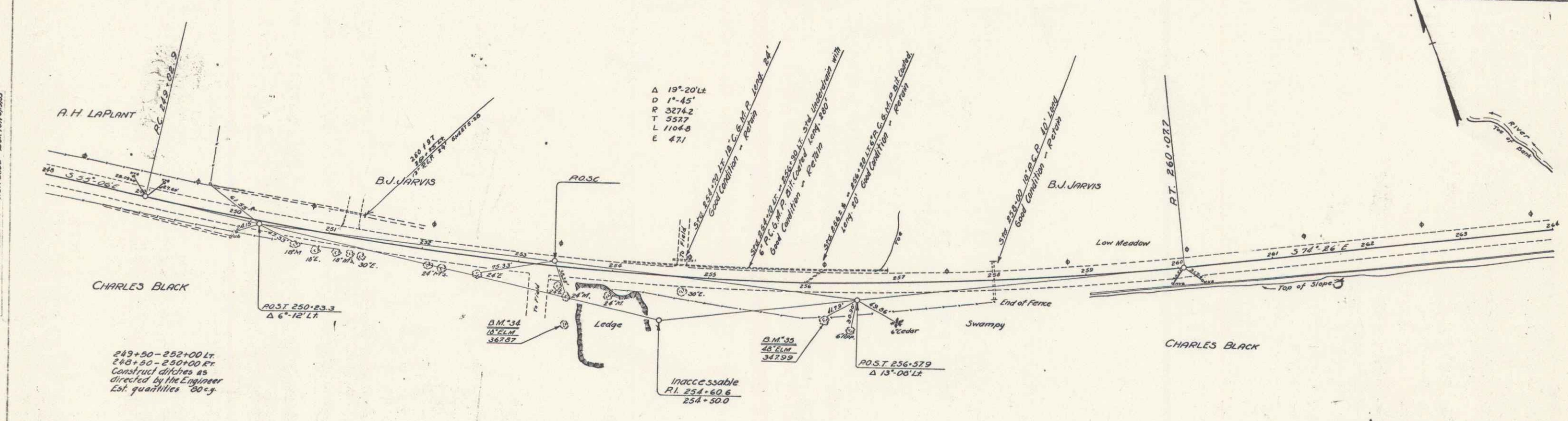
B.M. #27 SPIKE IN ROOT 18' MAPLE 208 LT. STA. 184+35 ELEV. 340.14
 B.M. #28 36' ELM. 147 RT. STA. 191+30 ELEV. 344.50
 B.M. #29 SPIKE IN ROOT 30' PINE 35 R. LT. STA. 207+56 ELEV. 339.31
 B.M. #30 24' PINE 21 R. LT. STA. 215+75 ELEV. 349.79

PLAN
 1/4" = 100'
 1/4" = 10'

PROFILES
 1/4" = 10'

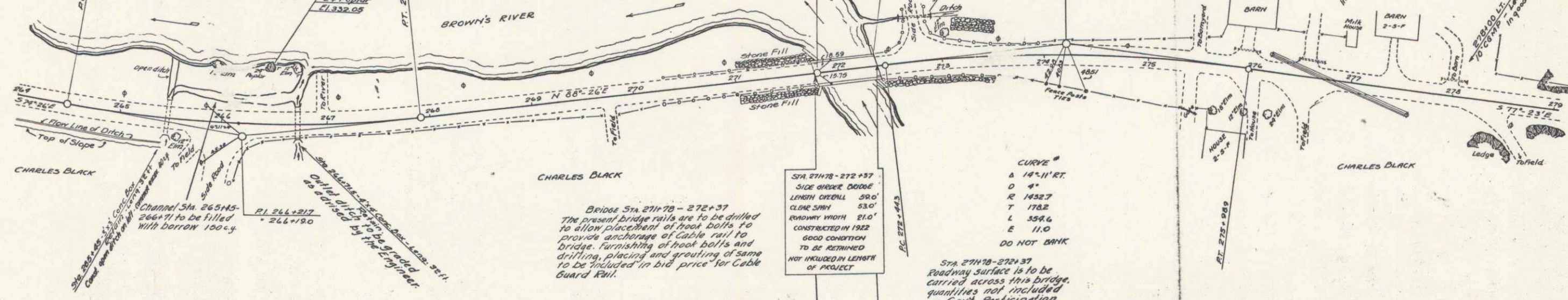


B.M. #32 SPIKE IN ROOT 36' MAPLE 26 FT. LT. STA 233+19, ELEV. 352.89
 B.M. #33 SPIKE - 12" ELM 28 FT. LT. STA 244+02, ELEV. 356.48



B.M. 34 SPIKE IN ROOT OF 18' ELM 60' RT STA 253+50 ELEK 362.87
 B.M. 35 " " " 45' " 39' " " 256+22 " 347.99

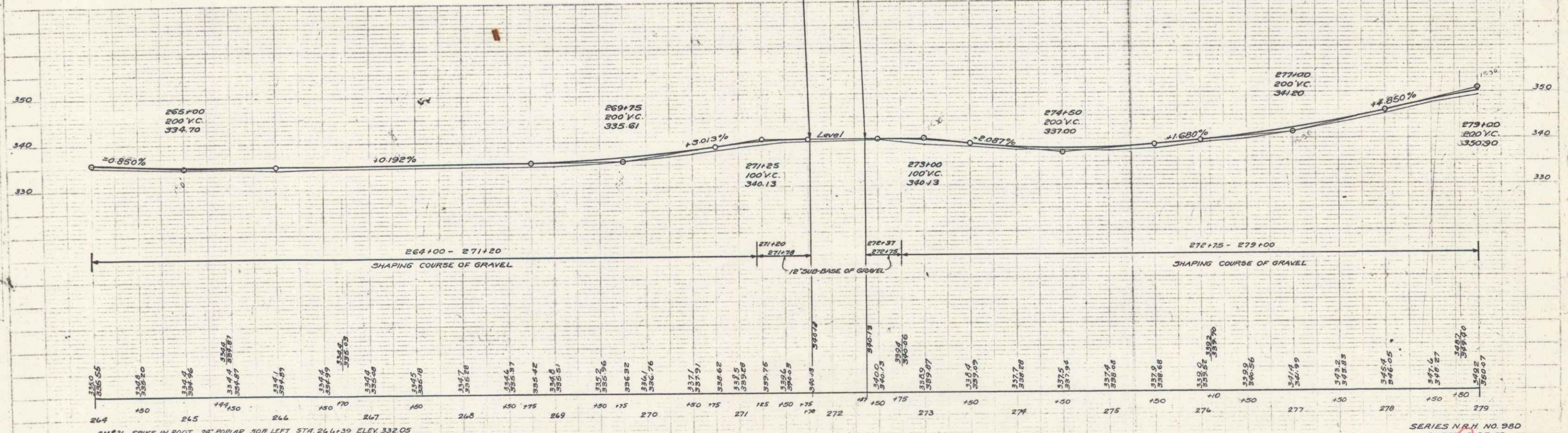
CURVE
 Δ 17° 00' L.T.
 D 5°
 R 1146.3
 T 172.7
 L 342.7
 E 129



Bridge Sta. 271+70 - 272+37
 The present bridge rails are to be drilled to allow placement of hook bolts to provide anchorage of cable rail to bridge. Furnishing of hook bolts and drilling, placing and grouting of same to be included in bid price for Cable Guard Rail.

Sta. 271+70 - 272+37
 SIDE GRADE DRAIN
 LENGTH GENERAL 59.0'
 CLEAR SWAY 53.0'
 ROADWAY WIDTH 21.0'
 CONSTRUCTED IN 1922
 GOOD CONDITION
 TO BE RETAINED
 NOT INCLUDED IN LENGTH OF PROJECT

CURVE
 Δ 14° 11' RT.
 D 41'
 R 1432.7
 T 178.2
 L 354.6
 E 11.0
 DO NOT BANK
 Sta. 271+70 - 272+37
 Roadway surface is to be carried across this bridge. quantities not included in Govt. Participation

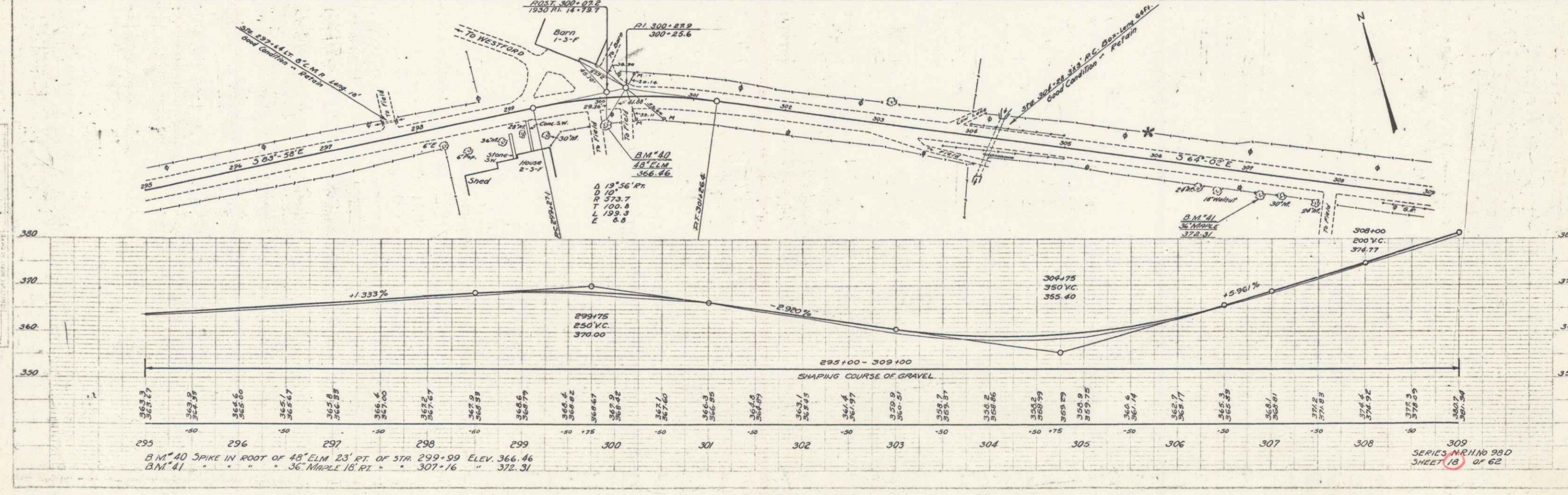
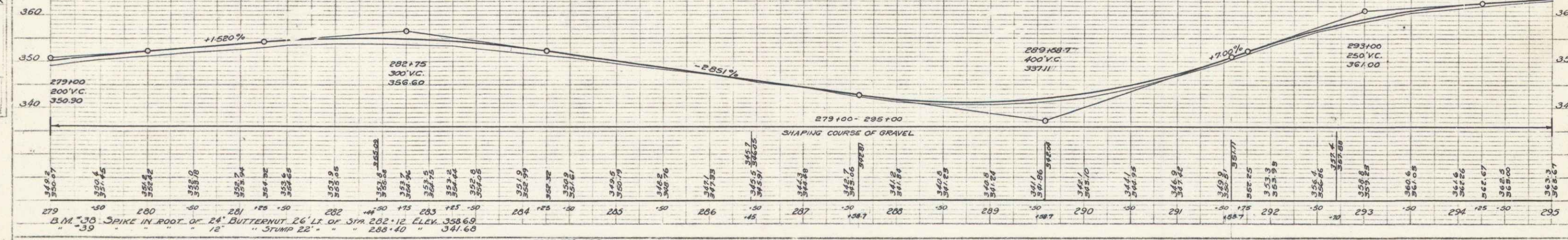
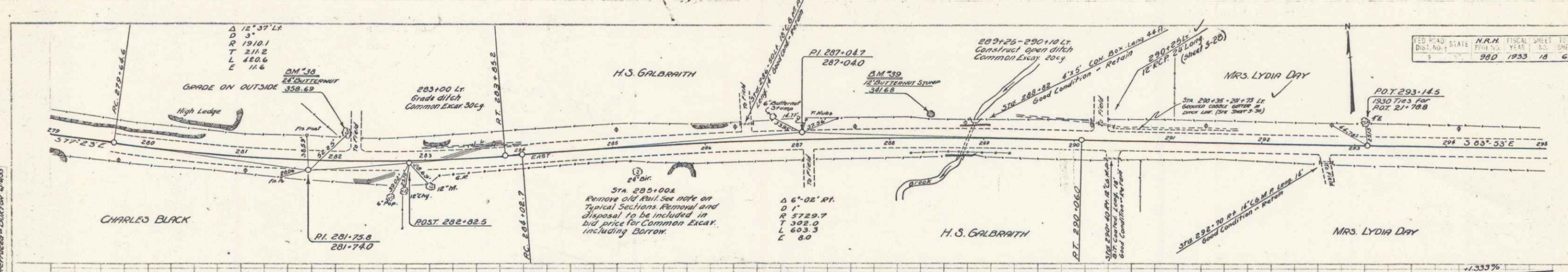


Prepared by
 Checked by
 Approved by
 Date

Station
 Elevation
 Curve

BM #36 SPIKE IN ROOT 28' POPLAR 30ft LEFT STA. 264+39 ELEV. 332.05
 BM #37 TOP OF CONCRETE BRIDGE RAIL 10.5 ft LT. STA. 272+32 ELEV. 344.31

SERIES N.R.N. NO. 980
 SHEET 17 OF 62



PLAN
 C.C. PLUMMER
 S.C. PLUMMER
 NOTES RE: SEE SHEET 18
 REVISION - BURTON 4/1933

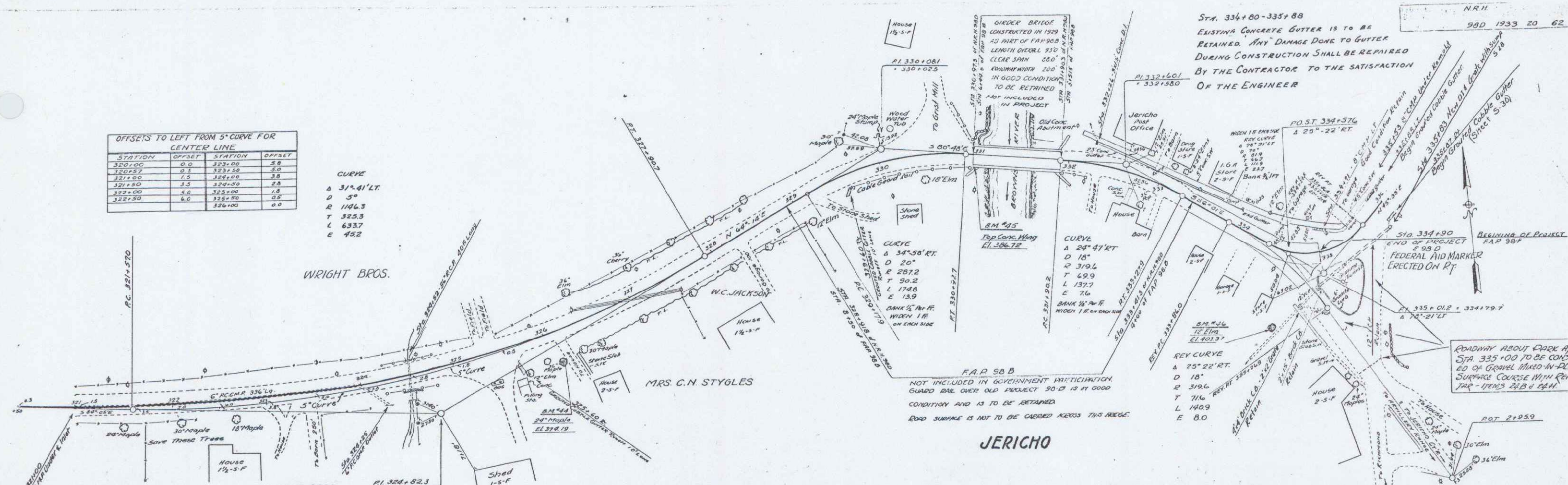
295+00
 S.C. BURTON
 1/1933

B.M. 40 SPIKE IN ROOT OF 48' ELM 23' RT. OF STR. 299+99 ELEV. 366.46
 B.M. 41 " " " 36' MAPLE 16' RT. " 307+16 " 372.31

309
 SERIES MARKING 38 D
 SHEET 18 OF 62

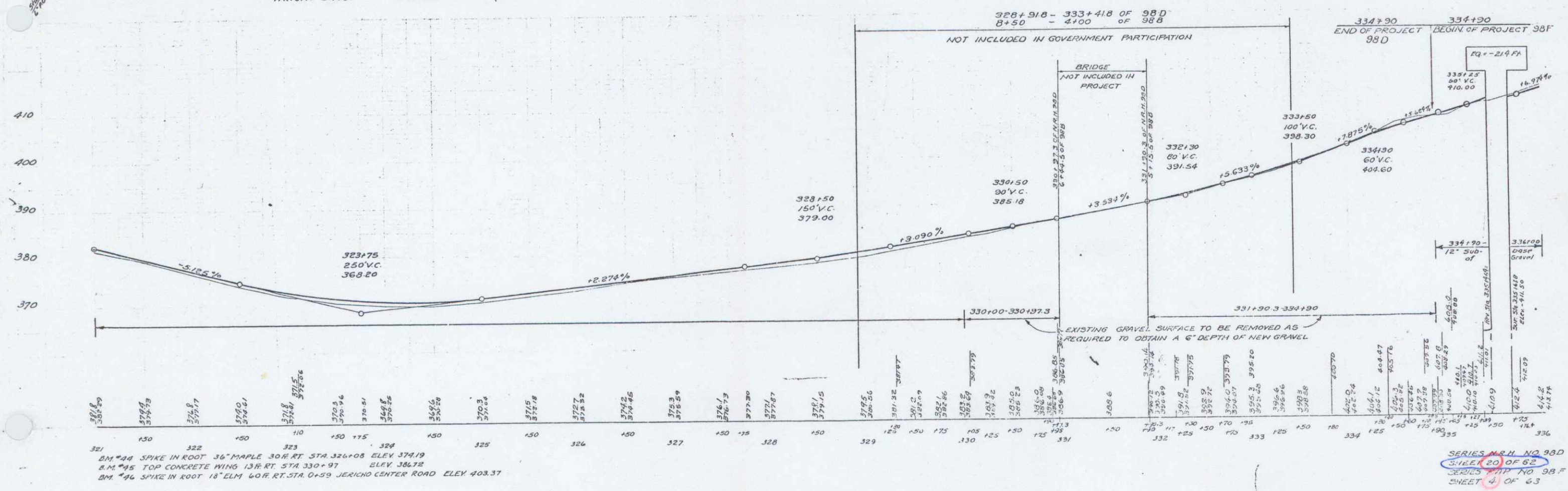
STATION	OFFSET	STATION	OFFSET
320+00	0.0	323+50	0.0
320+25	0.5	323+25	0.5
321+00	1.5	322+50	1.5
321+50	2.5	322+00	2.5
322+00	3.0	321+50	3.0
322+50	4.0	321+00	4.0

CURVE
 A 31'-4 1/2" LT
 D 3°
 E 194.3
 T 325.3
 L 633.7
 E 452



STA. 334+00-335+00
 EXISTING CONCRETE GUTTER IS TO BE
 RETAINED. ANY DAMAGE DONE TO GUTTER
 DURING CONSTRUCTION SHALL BE REPAIRED
 BY THE CONTRACTOR TO THE SATISFACTION
 OF THE ENGINEER

F.A.P. 98 B
 NOT INCLUDED IN GOVERNMENT PARTICIPATION
 GUARD RAIL OVER OLD PROJECT 98-B IS IN GOOD
 CONDITION AND IS TO BE RETAINED.
 ROAD SURFACE IS NOT TO BE CARRIED ACROSS THIS RIDGE.



BM #4 SPIKE IN ROOT 36" MAPLE 30R RT STA. 326+08 ELEV. 374.19
 B.M. #5 TOP CONCRETE WING 13R RT STA. 330+97 ELEV. 386.72
 B.M. #6 SPIKE IN ROOT 18" ELM 60R RT STA. 0+59 JERICHO CENTER ROAD ELEV. 403.37

SERIES N.R.H. NO 98D
 SHEET 20 OF 62
 SERIES 1933-20 62
 SHEET 4 OF 63

