

INDEX OF SHEETS

|             |  |
|-------------|--|
| SHEET NO. 1 | TITLE PAGE                                       |
| 2           | TYPICAL CROSS-SECTION OF IMPROVEMENT (24 GRAVEL) |
| 3           | PLAN AND PROFILE STA. 0+50 TO 4+00               |
| 4           | S.T.B. 20-NO. 55 SPAN SQUARE WITHOUT GRAVEL      |
| 5           | S.A.R. NO. 14 T-BEAM SQ. WITH PILES              |
| 6           | CROSS SECTIONS                                   |
| 7           | PLAN PROFILE STA. 0+00 TO 2+00                   |
| 8           | S.T.B. 20 NO. 30' SPAN SQ. WITH GRAVEL           |
| 9           | S.A.R. NO. 12 HEIGHT SQ.                         |
| 10          | S.B. NO. 2 TYPICAL DETAILS                       |
| 11          | S.B. NO. 4 SOLID RAIL                            |
| 12          | S.N. 19 STANDARD STRUCTURES                      |

*Dist. 5 Br. No. 65*  
*Dist. 5 Br. No. 67*

|                    |       |               |           |              |
|--------------------|-------|---------------|-----------|--------------|
| FED. AID DIST. NO. | STATE | FED. AID YEAR | SHEET NO. | TOTAL SHEETS |
| 9                  | VT.   | 30            | 1         | 12           |

STATE OF VERMONT  
STATE HIGHWAY DEPARTMENT

PLAN AND PROFILE OF PROPOSED  
STATE HIGHWAY

FEDERAL AID PROJECT

**PITTSFIELD**

PITTSFIELD-SHERBURNE ROAD

BRIDGE OVER TWEED RIVER  
LOCATED 1 MILE SOUTH OF PITTSFIELD VILLAGE  
LENGTH 450 FEET 0.085 MILES

ELLENWIRD BRIDGE  
LOCATED 2.2 MILES SOUTH OF PITTSFIELD  
LENGTH 200 FEET 0.037 MILES



Scales  
Plan 1"=20'  
Vertical 1"=20'  
Profile Horizontal 1"=20' No. 67  
Sections 1"=5'  
Layout  
See Sheet No. 3

HIGHWAYS DIVISION  
TOWN FILE  
PERPETUAL  
Town of Ut  
(To Be Returned To R.O.W. Division)

THIS PROJECT TO BE CONSTRUCTED IN ACCORDANCE WITH STANDARD SPECIFICATIONS ON FILE WITH THE U.S. BUREAU OF ROADS. APPROVED 7/1/28

STRUCTURES ON THIS PROJECT TO BE CONSTRUCTED IN ACCORDANCE WITH DETAILS GIVEN ON STANDARD STRUCTURE SHEETS SERIES NOS. 317-48-19.

District No. 3  
Bridge No. 65-67  
Pittsfield

APPROVED: July 2, 1928  
*H. E. Sargent*  
CHIEF ENGINEER  
SUBMITTED BY ORDER OF THE STATE HIGHWAY BOARD  
*A. B. Bates*  
COMMISSIONER OF HIGHWAYS

ROUTE: VT 100  
PROJECT: FR30  
DATE: 01/01/1928  
PIN: 00R658

CONVENTIONAL SIGNS

|                   |                  |
|-------------------|------------------|
| COUNTY LINE       | GROUND ELEVATION |
| TOWN LINE         | GRADE ELEVATION  |
| FENCE LINE        |                  |
| STONE WALL        |                  |
| UNFENCED PROPERTY |                  |
| GUARD RAIL        |                  |
| TRAVELED WAY      |                  |
| RAILROAD          |                  |
| RETAINING WALL    |                  |
| CENTER LINE       |                  |
| SURVEY LINE       |                  |
| CULVERT           |                  |
| DROP INLET        |                  |
| TROLLEY POLE      |                  |
| POWER POLE        |                  |
| TELEPHONE POLE    |                  |
| TREES             |                  |
| HEDGE             |                  |

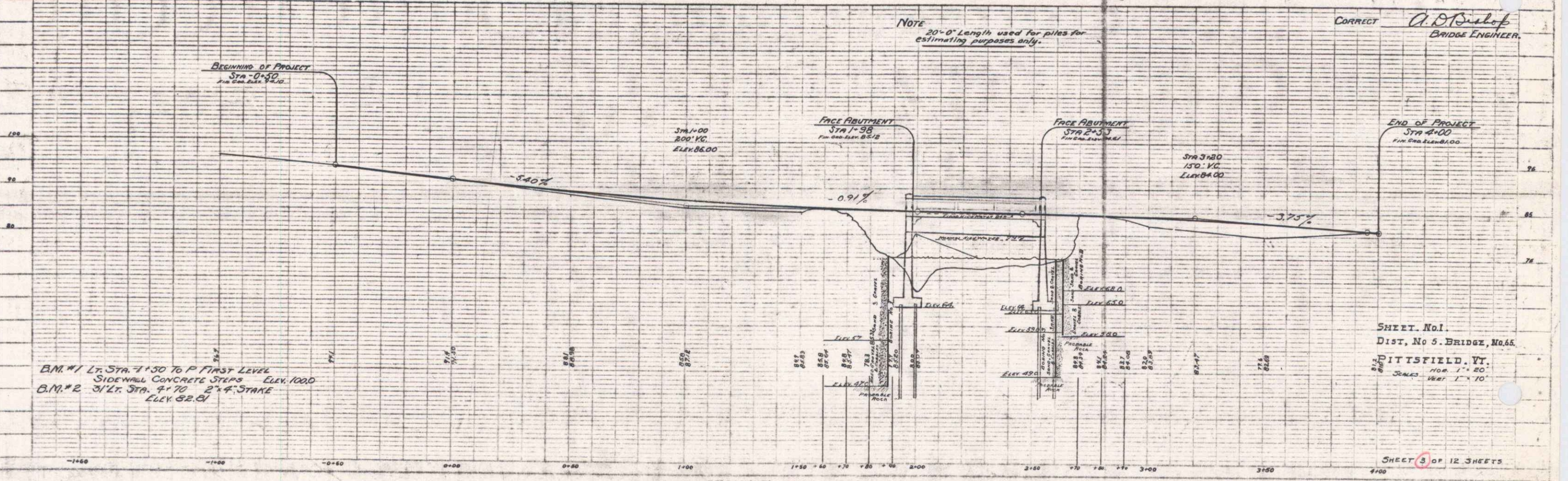
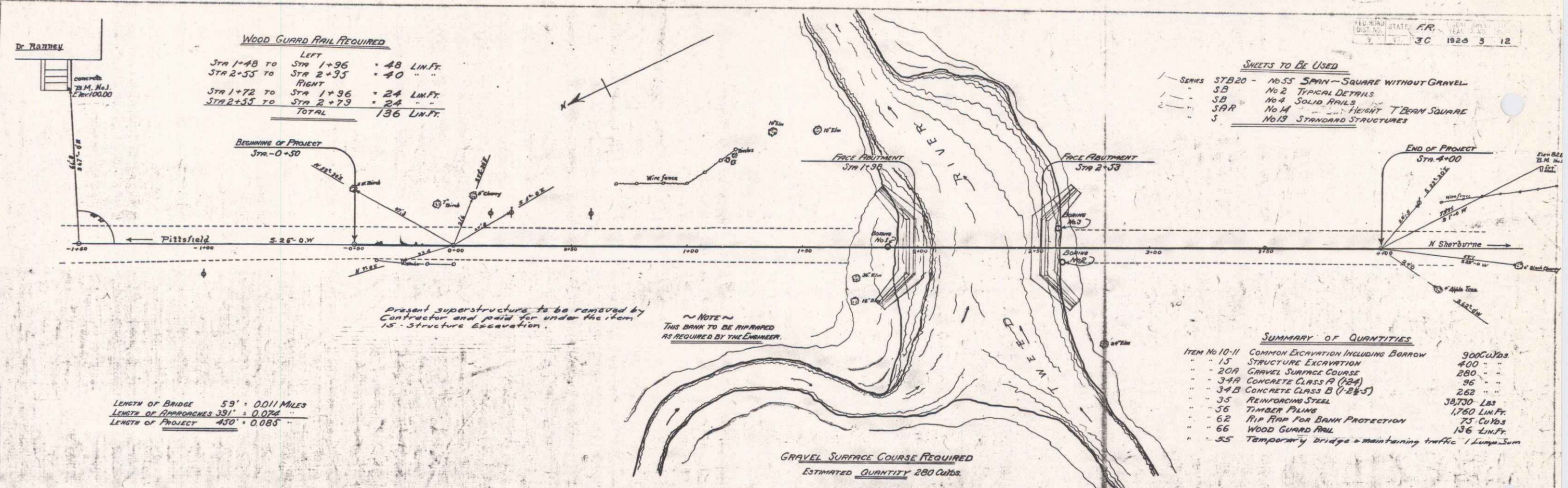
CURVE DATA

|                       |        |
|-----------------------|--------|
| DEFLECTION ANGLE      | Δ      |
| DEGREE OF CURVE       | D      |
| RADIUS OF CURVE       | R      |
| TANGENT DISTANCE      | T      |
| LENGTH OF CURVE       | L      |
| EXTERNAL DISTANCE     | E      |
| POINT OF INTERSECTION | P.I.   |
| POINT OF CURVE        | P.C.   |
| POINT OF TANGENT      | P.T.   |
| POINT ON TANGENT      | P.O.T. |

RECOMMENDED  
DISTRICT ENGINEER BUREAU OF PUBLIC ROADS  
RECOMMENDED FOR APPROVAL  
CHIEF ENGINEER BUREAU OF PUBLIC ROADS  
APPROVED  
DIRECTOR - BUREAU OF PUBLIC ROADS

SERIES F.R. No. 30 FILED  
SHEET 1 OF 12

1 DIST. # 3 PLANS



CORRECT *A. D. Sneloff*  
 BRIDGE ENGINEER.

**WOOD GUARD RAIL REQUIRED**

|                        |              |
|------------------------|--------------|
| Lt.                    | 24 Lin. Ft.  |
| Sta. 0+64 to Sta. 0+88 | 24 " "       |
| Sta. 1+22 to Sta. 1+46 | 24 " "       |
| At.                    | 70 " "       |
| Sta. 0+64 to Sta. 0+88 | 70 " "       |
| Sta. 1+22 to Sta. 2+00 | 130 Lin. Ft. |

COMMON EXCAVATION INCLUDING BORROW  
ESTIMATED QUANTITY 125 Cu. Yds.

Note:  
Present superstructure to be removed  
by the contractor and paid for under item  
No. 15 Structure Excavation.

GRAVEL SURFACE COURSE REQUIRED  
ESTIMATED QUANTITY 124 Cu. Yds.

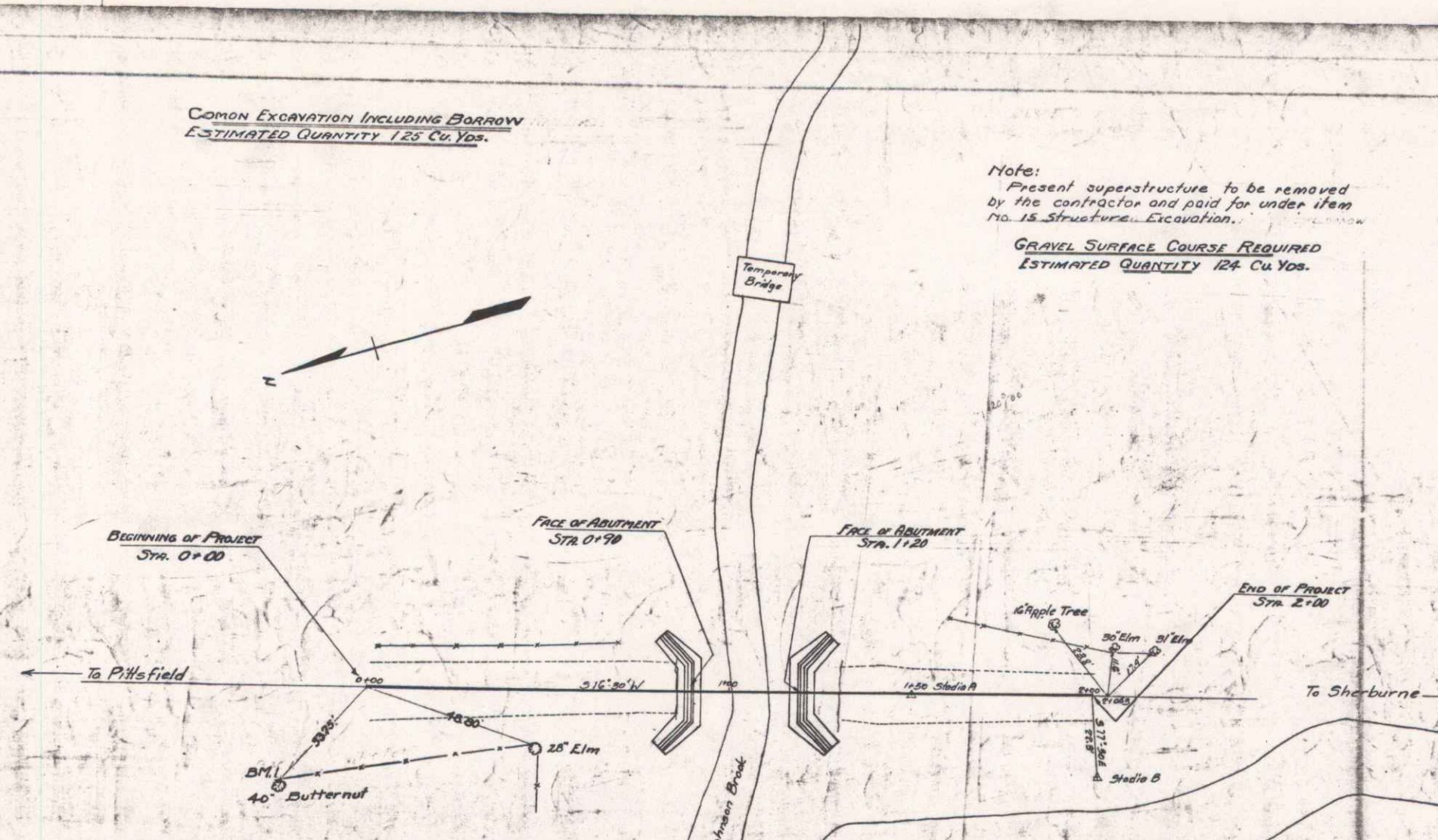
**INDEX OF SHEETS**

|                  |       |                         |              |
|------------------|-------|-------------------------|--------------|
| Series S.T.B. 20 | No 30 | Span Square with Gravel | 320 - -      |
| Series S.B.      | No 2  | Typical Details         | 41 - -       |
| Series S.A.      | No 9  | Soil's Rail             | 102 - -      |
| Series S.A.R.    | No 12 | Height Square           | 20,530 lbs.  |
| Series S.        | No 19 | Standard Structures     | 150 Lin. Ft. |

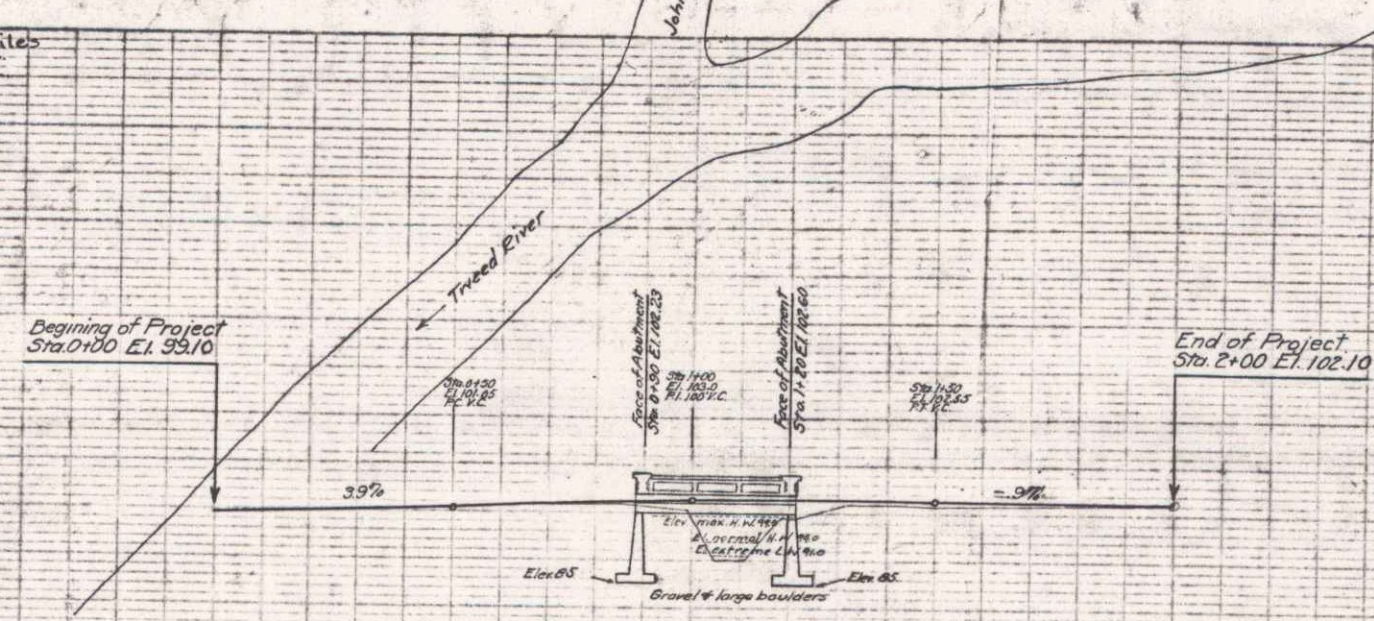
**SUMMARY OF QUANTITIES**

|               |  |              |
|---------------|--|--------------|
| Item No 10-11 | Common Excavation Including Borrow     | 125 Cu. Yds. |
| Item No 15    | Structure Excavation                   | 320 - -      |
| Item No 20A   | Gravel Surface Course                  | 124 - -      |
| Item No 34A   | Concrete Class A 1'-2'-4               | 41 - -       |
| Item No 34B   | Concrete Class B 1'-2'-5               | 102 - -      |
| Item No 35    | Reinforcing Steel                      | 20,530 lbs.  |
| Item No 66    | Wood Guard Rail                        | 150 Lin. Ft. |
| Item No 55    | Temporary Bridge - maintaining traffic | 1 Lump Sum   |

NEW ROAD STATE F.R.  
J.C. 1929 T 12



|                  |     |        |       |
|------------------|-----|--------|-------|
| Length of bridge | 35  | 10.064 | tiles |
| approaches       | 167 | 0.052  |       |
| Project          | 200 | 0.050  |       |



B.M. #1 40' A trunk butternut. Elev. 100.00  
El. of Sta. 0+28

|       |       |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |
|-------|-------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| Sta.  | 0+00  | 0+10   | 0+20   | 0+30   | 0+40   | 0+50   | 0+60   | 0+70   | 0+80   | 0+90   | 1+00   | 1+10   | 1+20   | 1+30   | 1+40   | 1+50   | 1+60   | 1+70   | 1+80   | 1+90   | 2+00   |        |
| Elev. | 99.10 | 100.00 | 100.90 | 101.80 | 102.70 | 103.60 | 104.50 | 105.40 | 106.30 | 107.20 | 108.10 | 109.00 | 109.90 | 110.80 | 111.70 | 112.60 | 113.50 | 114.40 | 115.30 | 116.20 | 117.10 | 118.00 |

Correct  
A. B. Schup  
Bridge Engineer

PITTSFIELD  
DISTRICT No. 5  
BRIDGE No. 67  
Scale: 1" = 20'  
Vertical: 1" = 20'