

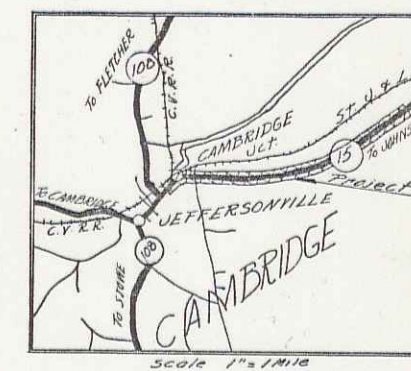
INDEX OF SHEETS

SHEET NO.	TITLE PAGE
1	TITLE PAGE
2	TYPICAL CROSS-SECTION OF IMPROVEMENT PLACE SURFACE COURSE WITH CRUSHED GRAVEL MIXED-IN
3	BANKING AND WIDENING TABLES
4-12	PLAN AND PROFILE STA
13	STANDARD STRUCTURE SHEET 5-28 PIPE CULVERTS
14	" " " " 5-29 BOX CULVERTS
15	" " " " 5-30 GUARD RAIL
16	" " " " 5-31 BOUNDARY MARKERS
17	" " " " 5-35 CONCRETE CRIBBING
18	" " " " 5-40 BARRICADES, LIGHTS, SIGNS
19	" " " " 5-41 ROUNDED SLOPES
20-65	CROSS SECTIONS

STANDARD STRUCTURE SHEETS APPROVED BY THE CHIEF ENGINEER
 5-28 & 29 JULY 11, 1933
 5-30 AUG 15, 1933
 5-31 JULY 25, 1934

STATE OF VERMONT
 STATE HIGHWAY DEPARTMENT
 PLAN AND PROFILE OF PROPOSED
 STATE HIGHWAY
 FEDERAL AID PROJECT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
9	VT.	COLE 2	1940	7	63



TOWN OF CAMBRIDGE Pin # 00R293
 CAMBRIDGE MORRISVILLE ROAD
 BEGINNING AT A POINT 671 FT. EASTERLY FROM ITS INTERSECTION WITH ROUTE 108 TO ENOSBURG & EXTENDING N. EASTERLY 2.337 MILES.

LENGTH OF PROJECT 12341.9 FEET = 2.337 MILES

STA. 3+51 F.A.P. 24 CONT. 2 =
 STA. 18+15.5 F.A.P. 24 CONT. 1
 BEGINNING OF PROJECT
 F.A.P. 24 CONT. 2

MP 580+
 5.25

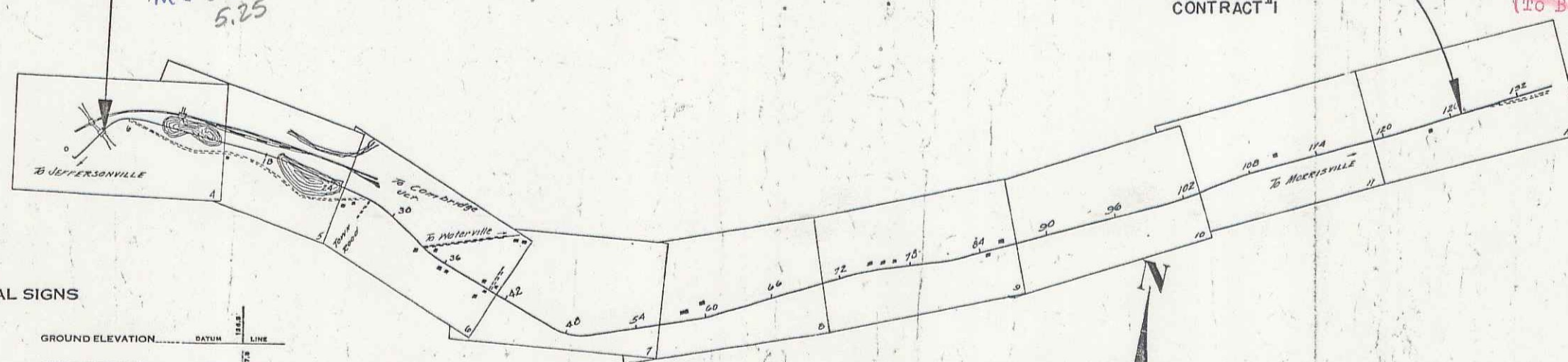
RECORD PLANS
 GOVERNMENT PLANS NOT AVAILABLE

STA. 127+00
 END OF PROJECT
 F.A.P. 24 CONT. 2
 142+64 OF F.A.P. 24
 CONTRACT 1

7.57
 MP 269

RIGHT-OF-WAY DIVISION
 TOWN FILE

PERPETUAL
 Town of U-15
 (To Be Returned To R.O.W. Division)



CONVENTIONAL SIGNS

COUNTY LINE	GROUND ELEVATION DATUM
TOWN LINE	GRADE ELEVATION DATUM
FENCE LINE	
STONE WALL	
UNFENCED PROPERTY	
GUARD RAIL	
TRAVELED WAY	
RAILROAD	
RETAINING WALL	
CENTER LINE	
SURVEY LINE	
CULVERT	
DROP INLET	
TROLLEY POLE	
POWER POLE	
TELEPHONE POLE	
TREES	
HEDGE	

CURVE DATA

DEFLECTION ANGLE	Δ
DEGREE OF CURVE	D
RADIUS OF CURVE	R
TANGENT DISTANCE	T
LENGTH OF CURVE	L
EXTERNAL DISTANCE	E
POINT OF INTERSECTION	P. I.
POINT OF CURVE	P. C.
POINT OF TANGENT	P. T.
POINT ON TANGENT	P. O. T.

These plans are subject to such revisions as may be required by the Bureau of Public Roads or the Commissioner of Highways. Construction is to be carried on in accordance with the plans and standard engineering specifications of 1936 as approved December 23, 1936 by the Bureau of Public Roads including all subsequent approved revisions and such revised specifications and special provisions as are submitted with the plans. Approval of the construction is given on the fact that the State Highway Board reserves the right to extend this project at the same contract price. Such cost of extension not to exceed 25% of the amount of the contract.

* PLAN SET INCLUDES 1926'S SURVEY

SCALES

TITLE	1" = 600'
TYPICAL	1" = 20'
PLAN	1" = 50'
PROFILE	HOR. 1" = 50'
CROSS SECTIONS	1" = 5'

APPROVED: [Signature] DISTRICT ENGINEER
 CORRECT: [Signature] ROAD ENGINEER
 SERIES F.A.P. No. 24 Cont. 2 FILED
 SHEET 1 OF 63

BR

INDEX OF SHEETS

SHEET NO	TITLE PAGE
2	TYPICAL CROSS-SECTION OF IMPROVEMENT
3	PLAN AND PROFILE STA. 0+00 TO STA. 15+00
4	" " " " " " 15+00 " " 31+00
5	" " " " " " 31+00 " " 47+00
6	" " " " " " 47+00 " " 63+00
7	" " " " " " 63+00 " " 79+00
8	" " " " " " 79+00 " " 93+00
9	" " " " " " 93+00 " " 109+00
10	" " " " " " 109+00 " " 124+00
11	" " " " " " 124+00 " " 140+00
12	" " " " " " 140+00 " " 153+00
13	" " " " " " 153+00 " " 169+00
14	" " " " " " 169+00 " " 185+00
15	" " " " " " 185+00 " " 200+00
16	" " " " " " 200+00 " " 213+00
17-18	BRIDGE AT STA. 17+56
19-38	CROSS SECTIONS
39	PLAN AND PROFILE STA. 0+00 TO STA. 11+00 B 0
40	" " " " " " 11+00 B " " 13+35 B 0
41-42	CROSS-SECTIONS. 0

STATE OF VERMONT
STATE HIGHWAY DEPARTMENT

PLAN AND PROFILE OF PROPOSED
STATE HIGHWAY

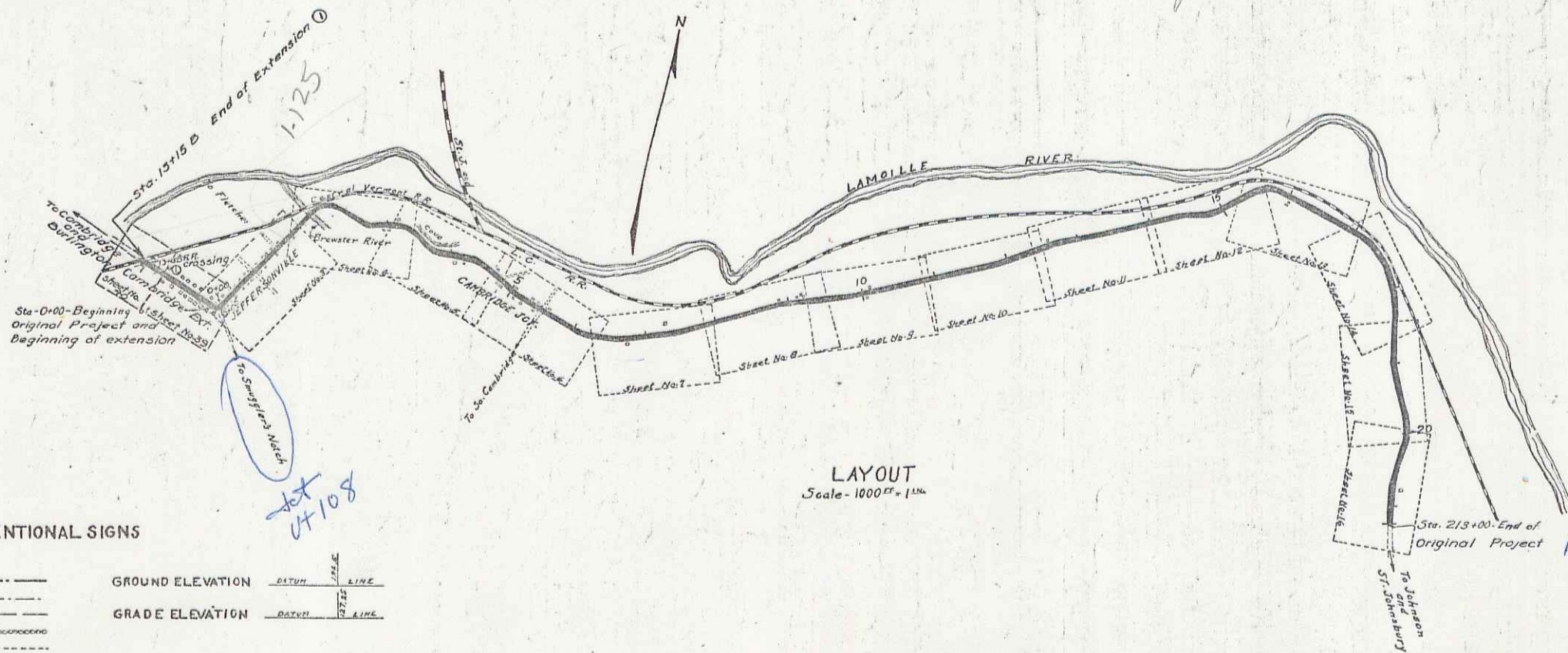
FEDERAL AID PROJECT

TOWN OF CAMBRIDGE
LAMOILLE RIVER ROAD
FROM SMUGGLERS NOTCH ROAD 2130 FT. EASTERLY
EXTENDED 1315 FT. WESTERLY AUG. 1922

Plan: 1" = 50' E
Scale: Profile-Hor. 1" = 50' Vert. 1" = 10' E
Cross-Sections: 1" = 5' E

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
3	V.T.	29	1921	1	42

Maro Wilmes



RIGHT-OF-WAY DIVISION
TOWN FILE
PERPETUAL
Town of UTIS
(To Be Returned To R.O.W. Division)

CONVENTIONAL SIGNS

COUNTY LINE	-----	GROUND ELEVATION	-----
TOWN LINE	-----	GRADE ELEVATION	-----
FENCE LINE	-----		
STONE WALL	-----		
UNFENCED PROPERTY	-----		
GUARD RAIL	-----		
TRAVELCO WAY	-----		
RAILROAD	-----		
RETAINING WALL	-----		
BASE OR SURVEY LINE	-----		
CULVERT	-----		
DROP INLET	-----		
TROLLEY POLE	-----		
POWER POLE	-----		
TELEPHONE POLE	-----		
TREES	-----		
HEDGE	-----		

RECOMMENDED _____
STATE ENGINEER
APPROVED _____
GOVERNOR
SUBMITTED _____
STATE HIGHWAY COMMISSIONER

RECOMMENDED _____
DISTRICT ENGINEER
BUREAU OF PUBLIC ROADS
RECOMMENDED FOR APPROVAL _____
CHIEF ENGINEER - BUREAU OF PUBLIC ROADS
APPROVED _____
DIRECTOR - BUREAU OF PUBLIC ROADS

Series F. No. 29 Filed
Sheet 1 of 42 @

RT 2

RIGHT OF WAY MAP
 FEDERAL AID NO. 24
 PROPERTY OF JAMES BURNS
 Town of Cambridge, Vt.

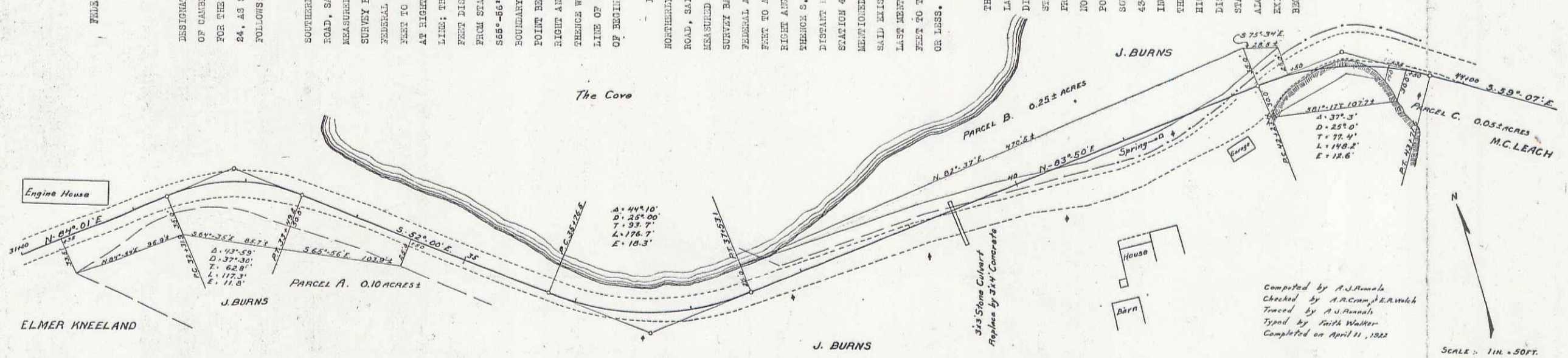
STATE OF VERMONT
 LAND TO BE ACQUIRED FOR THE
 LANOUILLE RIVER ROAD
 FEDERAL AID PROJECT NO. 24 - TOWN OF CAMBRIDGE
 FROM JAMES BURNS, DECEASED OWNER

ALL THESE PIECES OR PARCELS OF LAND HEREAFTER
 DESIGNATED AS PARCELS A AND B AND C, SITUATED IN THE TOWN
 OF CAMBRIDGE, COUNTY OF LANOUILLE, STATE OF VERMONT,
 FOR THE LANOUILLE RIVER ROAD, FEDERAL AID PROJECT NO.
 24, AS SHOWN ON THE ACCOMPANYING MAP AND DESCRIBED AS
 FOLLOWS:-

PARCEL "A".- BEGINNING AT A POINT ON THE
 SOUTHERLY BOUNDARY OF THE EXISTING LANOUILLE RIVER
 ROAD, SAID POINT BEING 26.6 FEET DISTANT SOUTHERLY,
 MEASURED AT RIGHT ANGLES, FROM STATION 37+88 OF THE
 SURVEY BASE LINE OF THE PROPOSED LANOUILLE RIVER ROAD,
 FEDERAL AID PROJECT NO. 24; THENCE N. 64°-54' E. 98.9'
 FEET TO A POINT 26 FEET DISTANT SOUTHERLY, MEASURED
 AT RIGHT ANGLES, FROM STATION 22+21.9 OF SAID BASE
 LINE; THENCE S. 64°-54' E. 88.7' FEET TO A POINT 80
 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES,
 FROM STATION 22+49.2 OF SAID BASE LINE; THENCE
 S. 65°-50' E. 103.9' FEET TO A POINT ON THE SOUTHERLY
 BOUNDARY OF SAID EXISTING HIGHWAY, THE LAST MENTIONED
 POINT BEING 25 FEET DISTANT SOUTHERLY, MEASURED AT
 RIGHT ANGLES, FROM STATION 54+60 OF SAID BASE LINE;
 THENCE WESTERLY ALONG THE LAST MENTIONED BOUNDARY
 LINE OF SAID EXISTING HIGHWAY 504; FEET TO THE POINT
 OF BEGINNING; BEING 0.10 ACRES MORE OR LESS.

PARCEL "B".- BEGINNING AT A POINT ON THE
 NORTHERLY BOUNDARY OF THE EXISTING LANOUILLE RIVER
 ROAD, SAID POINT BEING 26 FEET DISTANT NORTHERLY,
 MEASURED AT RIGHT ANGLES, FROM STATION 27+85.1 OF THE
 SURVEY BASE LINE OF THE PROPOSED LANOUILLE RIVER ROAD,
 FEDERAL AID PROJECT NO. 24; THENCE N. 63°-39' E. 470.5'
 FEET TO A POINT 26 FEET DISTANT NORTHERLY, MEASURED AT
 RIGHT ANGLES, FROM STATION 42+22.4 OF SAID BASE LINE;
 THENCE S. 76°-54' E. 86.5' FEET TO A POINT 26 FEET
 DISTANT NORTHERLY, MEASURED AT RIGHT ANGLES, FROM
 STATION 42+60 OF SAID BASE LINE, THE LAST ABOVE
 MENTIONED POINT BEING IN THE NORTHERLY BOUNDARY OF
 SAID EXISTING HIGHWAY; THENCE WESTERLY FOLLOWING THE
 LAST MENTIONED BOUNDARY OF SAID EXISTING HIGHWAY 504;
 FEET TO THE POINT OF BEGINNING; BEING 0.25 ACRES MORE
 OR LESS.

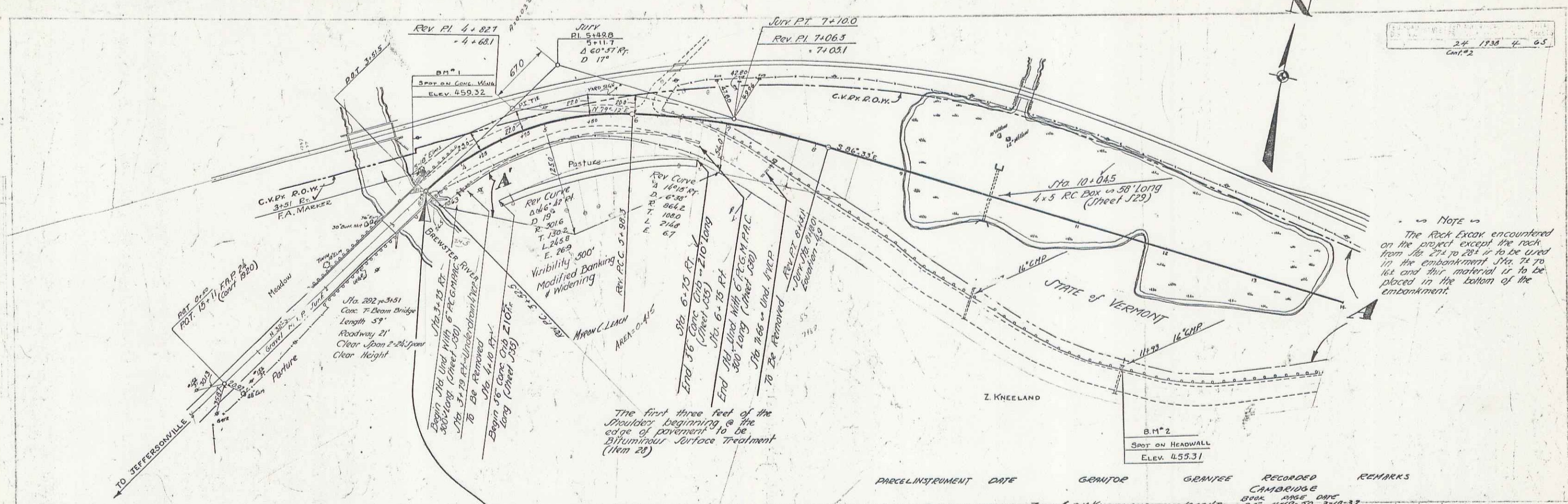
PARCEL "C".- BEGINNING AT A POINT ON
 THE SOUTHERLY BOUNDARY LINE OF THE EXISTING
 LANOUILLE RIVER ROAD, SAID POINT BEING 80 FEET
 DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM
 STATION 42+22.4 OF SAID SURVEY BASE LINE OF THE
 PROPOSED LANOUILLE RIVER ROAD, FEDERAL AID PROJECT
 NO. 24; THENCE S. 61°-17' E. 107.7' FEET TO A
 POINT AT THE FOOT OF THE LEASE 80 FEET DISTANT
 SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION
 42+60 OF SAID BASE LINE; THENCE NORTHERLY FOLLOW-
 ING THE FOOT OF THE LEASE 204 FEET TO A POINT ON
 THE SOUTHERLY BOUNDARY LINE OF SAID EXISTING
 HIGHWAY, THE LAST MENTIONED POINT BEING 14 FEET
 DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES FROM
 STATION 42+60 OF SAID BASE LINE; THENCE WESTERLY
 ALONG THE LAST MENTIONED BOUNDARY LINE OF SAID
 EXISTING HIGHWAY 109; FEET TO THE POINT OF
 BEGINNING; BEING 0.05 ACRES MORE OR LESS.



Computed by A. J. Annals
 Checked by A. C. Annals
 Traced by A. J. Annals
 Typed by Ruth Walker
 Completed on April 11, 1922

SCALE: 1 IN. = 50 FT.

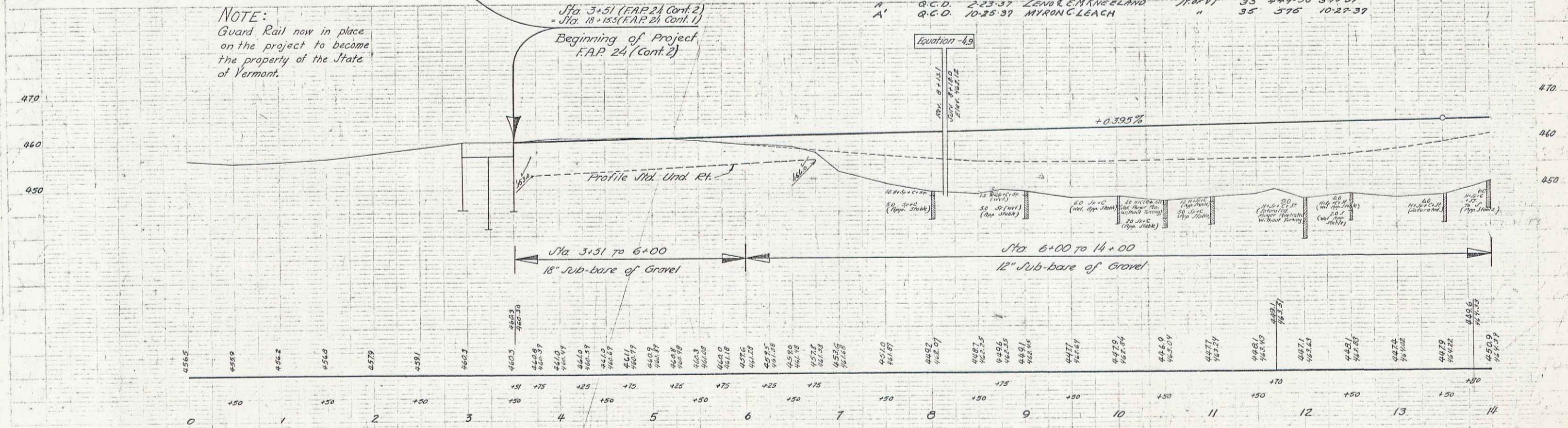
STREET NAME 4A



Note
The Rock Excav encountered on the project except the rock from Sta 274 to 284 is to be stored in the embankment Sta. 7+70 to 116 and this material is to be placed in the bottom of the embankment.

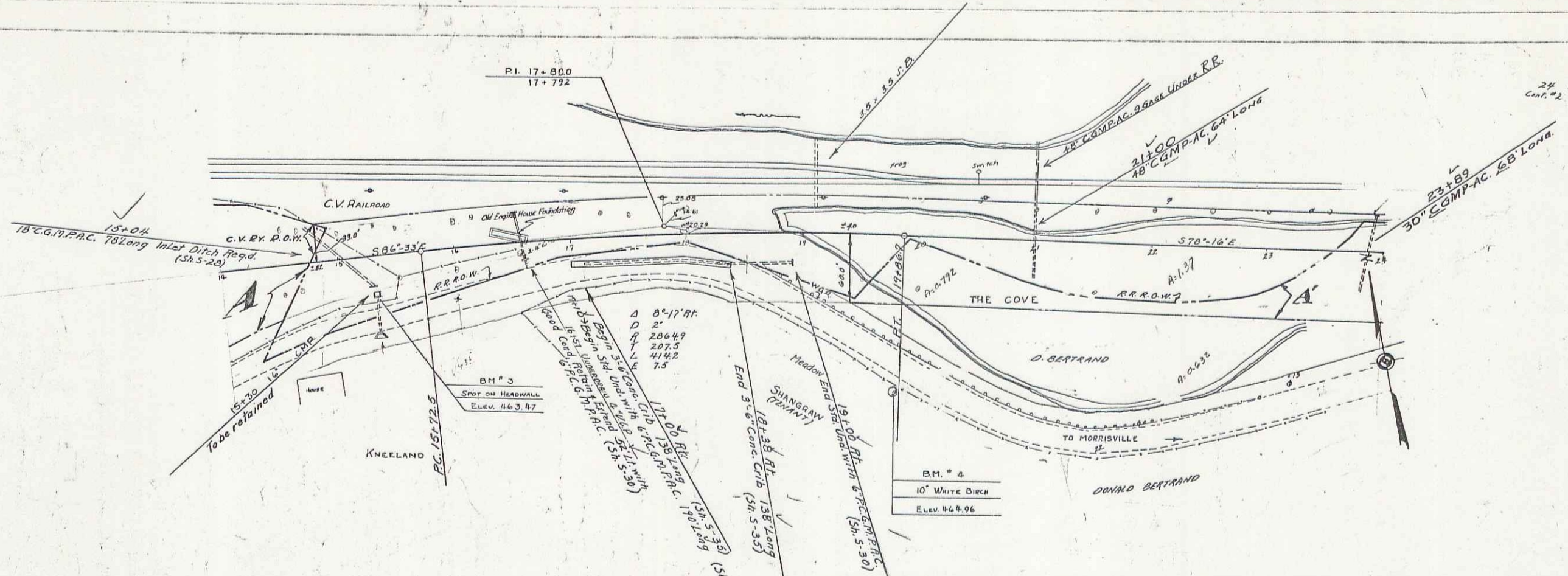
NOTE:
Guard Rail now in place on the project to become the property of the State of Vermont.

PARCEL	INSTRUMENT	DATE	GRANTOR	GRANTEE	RECORDED	REMARKS
A'	Q.C.D.	2-23-37	ZENO & E.M. KNEELAND	STATE	BOOK 2086 PAGE 33	4-19-37
A'	Q.G.D.	10-25-37	MYRON C. LEACH	"	35 576	10-27-37

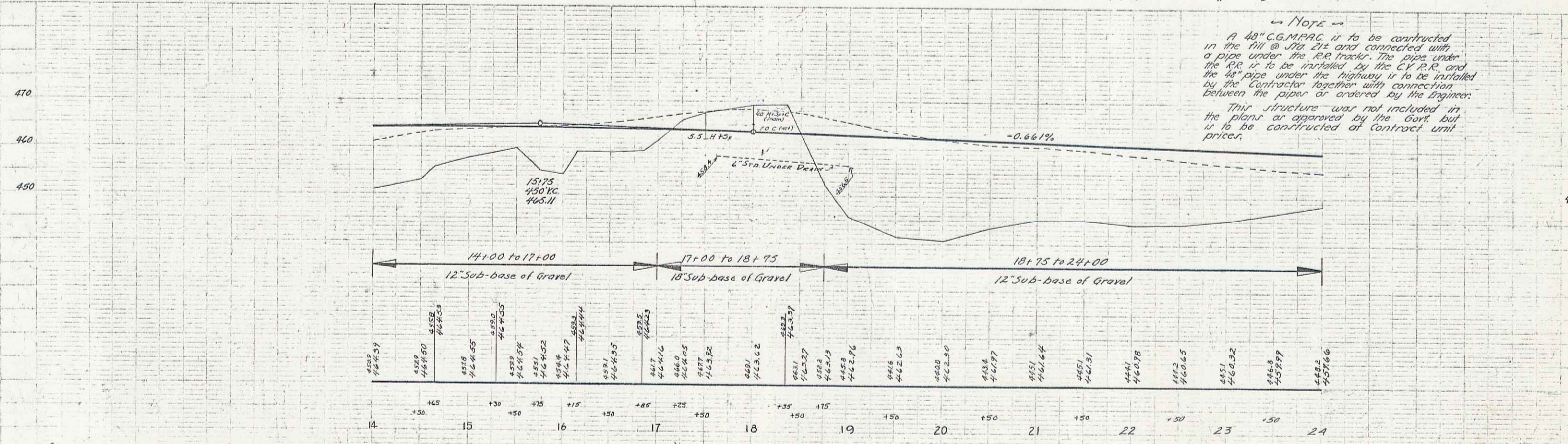


B.M. 1 SPOT ON WING OF CONC. BRIDGE 12' LT. STA 3+50 ELEV. 459.32
B.M. 2 SPOT ON CULVERT HEADWALL 135' RT. STA 11+93 ELEV. 455.31

Series FAP N. 24 (Cont. 2)
Sheet 4 of 65

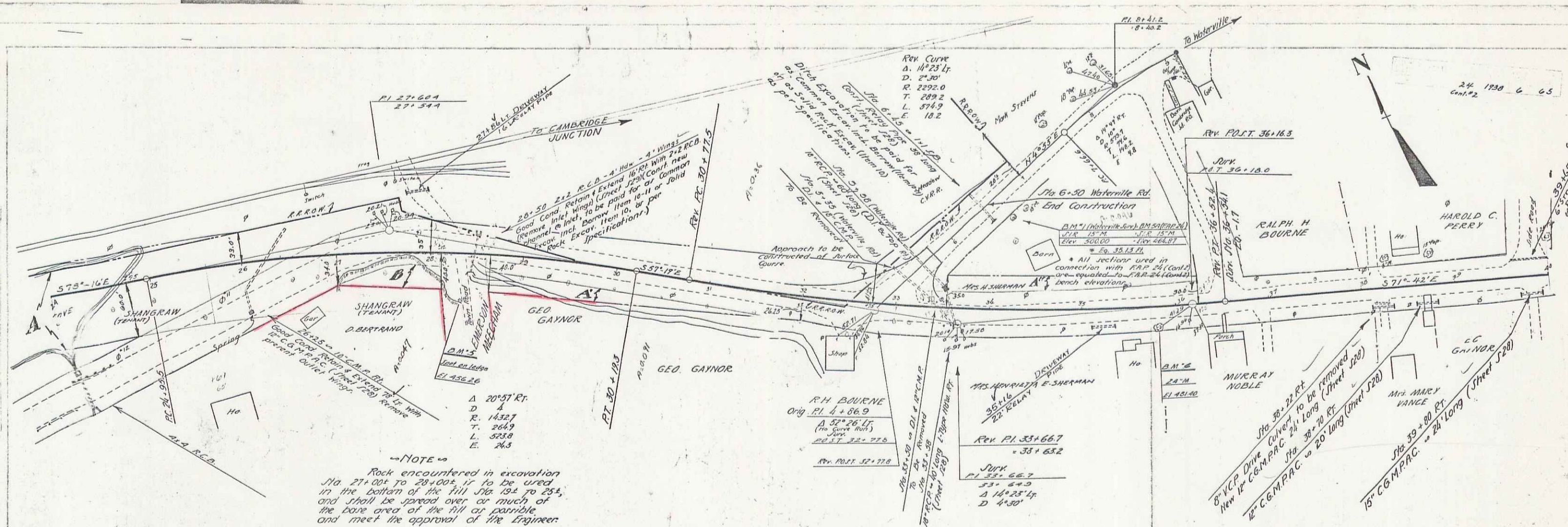


PARCEL	INSTRUMENT	DATE	GRANTOR	GRANTEE	RECORDED	REMARKS
A	G.C.O.	2-23-37	ZENO E. M. KNEELAND	ST. OF VT.	BOOK PAGE DATE	
A'	G.C.O.	10-28-37	DONALD F. BERTRAND	"	35 447-50 3-10-37	
					35 501-2 10-30-37	

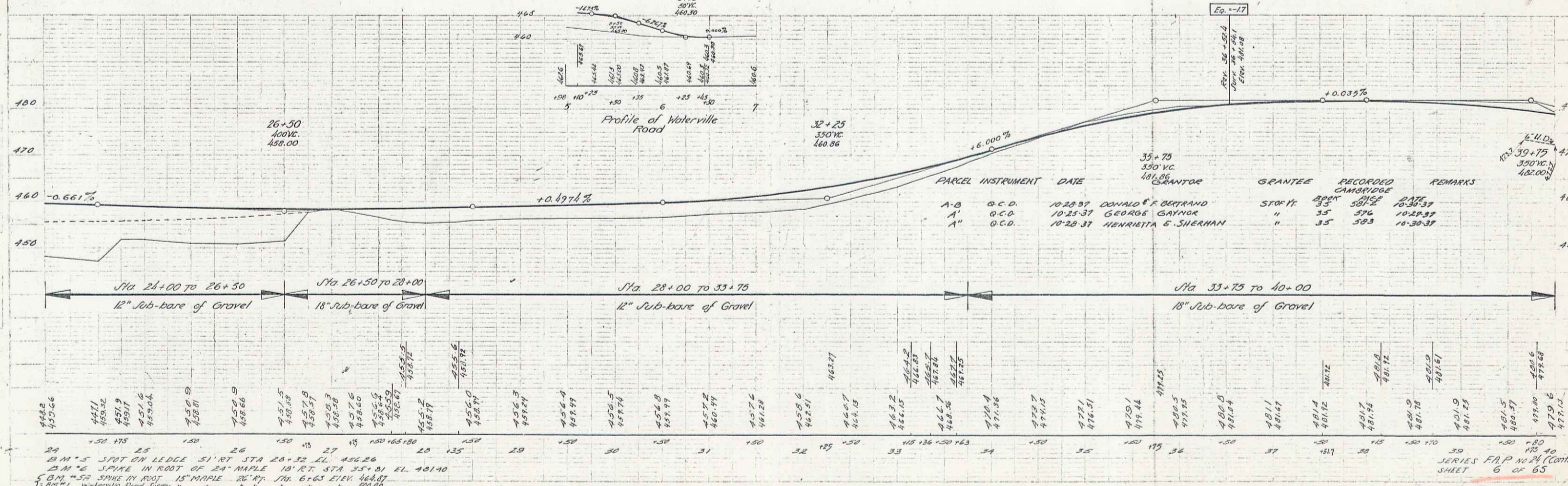


NOTE
A 48" CGMPAC is to be constructed in the fill @ Sta 21+ and connected with a pipe under the RR tracks. The pipe under the RR is to be installed by the CV R.R. and the 48" pipe under the highway is to be installed by the Contractor together with connection between the pipes as ordered by the Engineer.
This structure was not included in the plans as approved by the Govt. but is to be constructed at Contract unit prices.

B.M. #3 SPOT ON CULVERT HEADWALL 73' R.F. STA 15+30 ELEV. 463.47
B.M. #4 SPIKE IN ROOT 10' WHITE BIRCH 130' R.F. STA 19+00 ELEV. 464.98



NOTE
 Rock encountered in excavation
 Sta 21+00 to 28+00, is to be used
 in the bottom of the fill Sta 19+ to 25+
 and shall be spread over or much of
 the base area of the fill as possible
 and meet the approval of the Engineer



PARCEL	INSTRUMENT	DATE	GRANTEE	RECORDED	REMARKS
A-B	O.C.D.	10-25-37	DONALD E. BERTRAND	337	10-30-37
A'	O.C.D.	10-25-37	GEORGE GAYNOR	376	10-29-37
A''	O.C.D.	10-28-37	HENRIETTA E. SHERMAN	383	10-30-37

24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39
 24 M 5 SPOT ON LEDGE 51' RT STA 26+32 EL 456.26
 25 M 5 SPOT IN ROOT OF 24' MAPLE 10' RT STA 35+01 EL 401.40
 5 BM 5.5 SPOT IN ROOT 15' MAPLE 28' RT STA 6+63 ELEV 444.87
 6 BM 41 Waterville Road Survey " " " " " " 300.00

SERIES F&P NO 24 (Cont 2)
 SHEET 6 OF 65

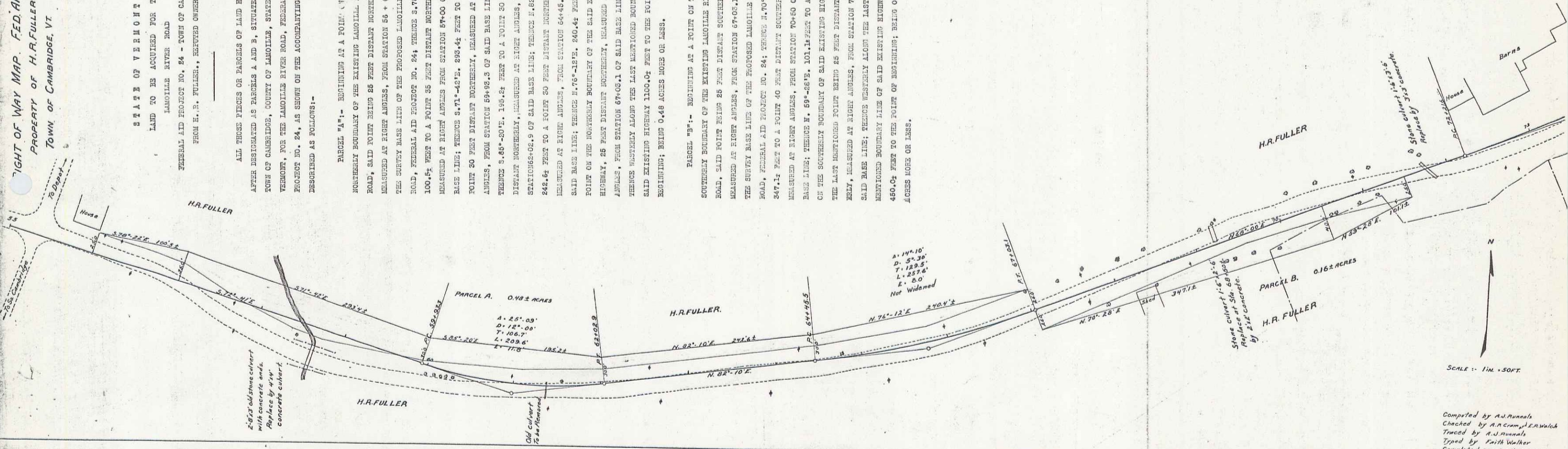
TIGHT OF WAY MAP, FED. AID NO. 24
PROPERTY OF H. R. FULLER
TOWN OF CAMBRIDGE, VT.

STATE OF VERMONT
LAND TO BE ACQUIRED FOR THE
LANOUILLE RIVER ROAD
FEDERAL AID PROJECT NO. 24 - TOWN OF CAMBRIDGE
FROM H. R. FULLER, BEHAVED OWNER,

ALL THOSE PIECES OR PARCELS OF LAND HEREIN-
AFTER DESIGNATED AS PARCELS A AND B, SITUATED IN THE
TOWN OF CAMBRIDGE, COUNTY OF LANOUILLE, STATE OF
VERMONT, FOR THE LANOUILLE RIVER ROAD, FEDERAL AID
PROJECT NO. 24, AS SHOWN ON THE ACCOMPANYING MAP ARE
DESCRIBED AS FOLLOWS:-

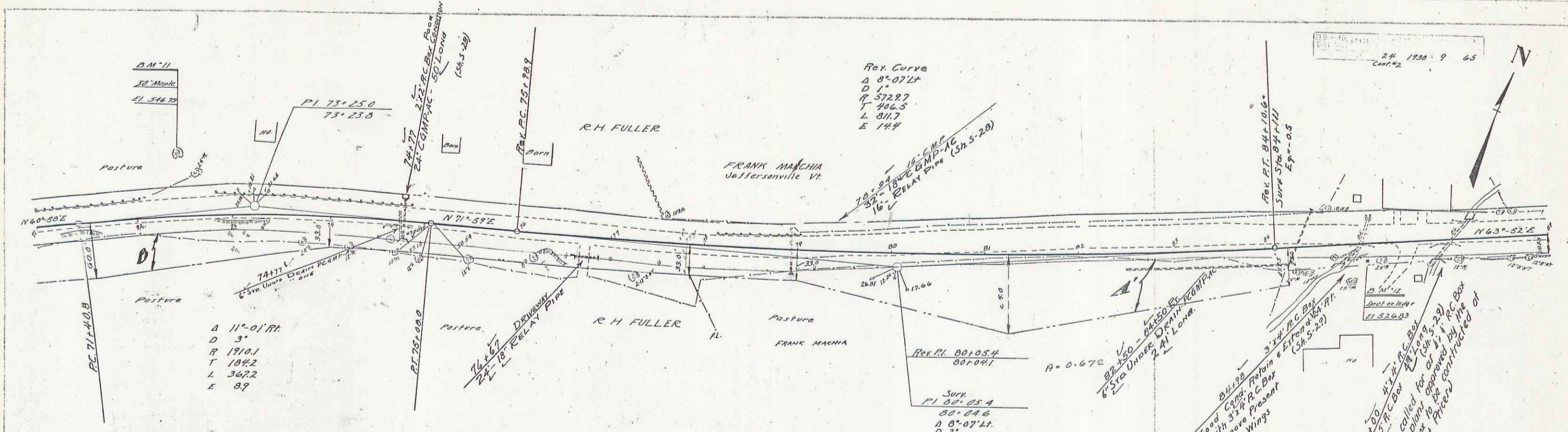
PARCEL "A": BEGINNING AT A POINT ON THE
NORTHERLY BOUNDARY OF THE EXISTING LANOUILLE RIVER
ROAD, SAID POINT BEING 25 FEET DISTANT NORTHERLY,
MEASURED AT RIGHT ANGLES, FROM STATION 66 + 00 OF
THE SURVEY BASE LINE OF THE PROPOSED LANOUILLE RIVER
ROAD, FEDERAL AID PROJECT NO. 24; THENCE S. 70°-28' E,
100.64 FEET TO A POINT 35 FEET DISTANT NORTHERLY,
MEASURED AT RIGHT ANGLES FROM STATION 67+00 OF SAID
BASE LINE; THENCE S. 71°-43' E, 295.44 FEET TO A
POINT 30 FEET DISTANT NORTHERLY, MEASURED AT RIGHT
ANGLES, FROM STATION 68+00.3 OF SAID BASE LINE;
THENCE S. 65°-40' E, 194.42 FEET TO A POINT 30 FEET
DISTANT NORTHERLY, MEASURED AT RIGHT ANGLES, FROM
STATION 68+02.9 OF SAID BASE LINE; THENCE N. 60°-30' E,
242.64 FEET TO A POINT 50 FEET DISTANT NORTHERLY,
MEASURED AT RIGHT ANGLES, FROM STATION 64+45.5 OF
SAID BASE LINE; THENCE N. 75°-18' E, 340.44 FEET TO A
POINT ON THE NORTHERLY BOUNDARY OF THE SAID EXISTING
HIGHWAY, 25 FEET DISTANT NORTHERLY, MEASURED AT RIGHT
ANGLES, FROM STATION 67+02.1 OF SAID BASE LINE;
THENCE WESTERLY ALONG THE LAST MENTIONED BOUNDARY OF
SAID EXISTING HIGHWAY 100.04 FEET TO THE POINT OF
BEGINNING; BEING 0.48 ACRES MORE OR LESS.

PARCEL "B": BEGINNING AT A POINT ON THE
SOUTHERLY BOUNDARY OF THE EXISTING LANOUILLE RIVER
ROAD, SAID POINT BEING 25 FEET DISTANT SOUTHERLY,
MEASURED AT RIGHT ANGLES, FROM STATION 67+02.1 OF
THE SURVEY BASE LINE OF THE PROPOSED LANOUILLE RIVER
ROAD, FEDERAL AID PROJECT NO. 24; THENCE N. 70°-28' E,
100.64 FEET TO A POINT 40 FEET DISTANT SOUTHERLY,
MEASURED AT RIGHT ANGLES, FROM STATION 70+00 OF SAID
BASE LINE; THENCE E. 69°-28' E, 101.12 FEET TO A POINT
ON THE SOUTHERLY BOUNDARY OF SAID EXISTING HIGHWAY,
THE LAST MENTIONED POINT BEING 25 FEET DISTANT SOUTH-
EAST, MEASURED AT RIGHT ANGLES, FROM STATION 71+00 OF
SAID BASE LINE; THENCE WESTERLY ALONG THE LAST
MENTIONED BOUNDARY LINE OF SAID EXISTING HIGHWAY
450.04 FEET TO THE POINT OF BEGINNING; BEING 0.16
ACRES MORE OR LESS.



Computed by A.J. Annals
Checked by A.J. Annals, J.A. Welch
Typed by Faith Walker
Completed on April 11, 1922.

Sheet name 19A

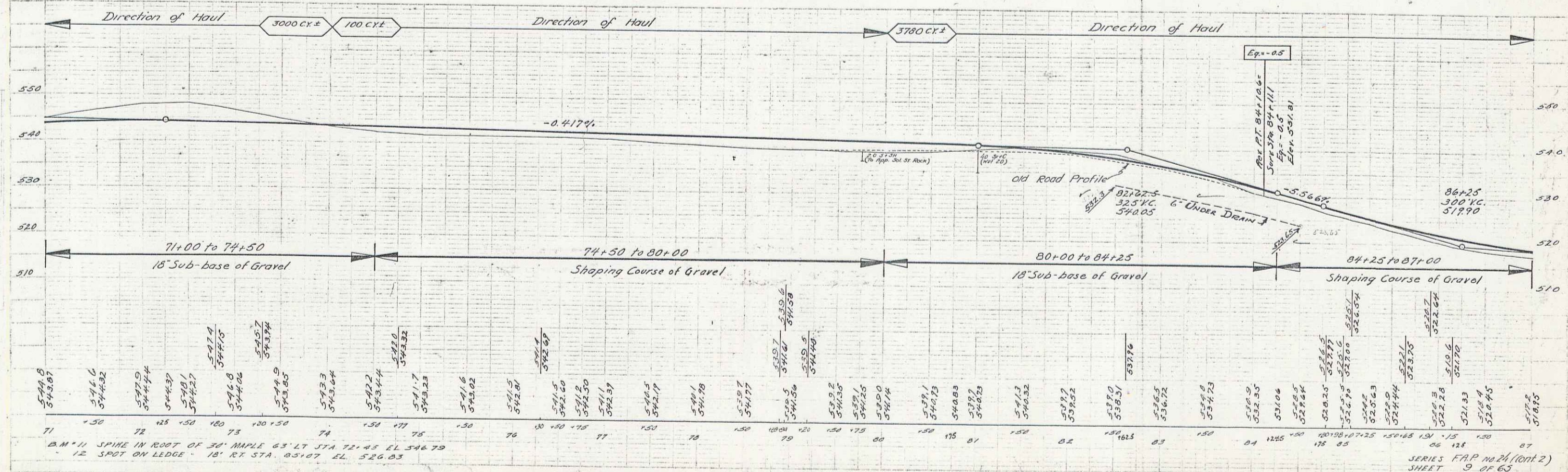


Δ 11°-01' 11"
 D 3'
 R 1910.1
 L 1842
 T 3422
 E 89

Rev. Curve
 Δ 8°-07' 41"
 D 1'
 T 406.3
 L 811.7
 E 144

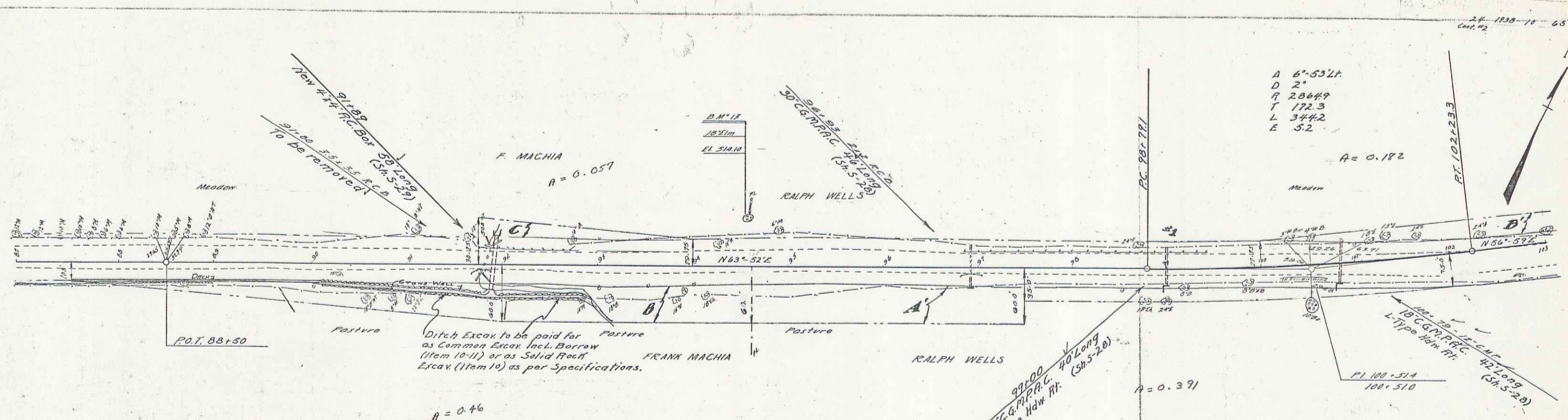
24 1936 9 65
 Cont #2

PARCEL	INSTRUMENT	DATE	GRANTOR	GRANTEE	RECORDED	REMARKS
A-B-C	O.C.D.	10-25-37	R.H. FULLER	ST. OF VT.	35	579-301 10-30-37
A'-B'-C'	O.C.D.	10-25-37	FRANK MARCHA	"	35	574-579 10-25-37



B.M. 11 SPIRE IN ROOT OF 34' MAPLE 63' LT STA 72+45 EL 546.79
 12 SPOT ON LEDGE - 18' RT STA 85+07 EL 526.03

SERIES F.A.P. No 24 (cont 2)
 SHEET 9 OF 65



$A = 6^{\circ} 53' 24''$
 $D = 2'$
 $R = 2864.7$
 $T = 172.3$
 $L = 344.2$
 $E = 5.2$

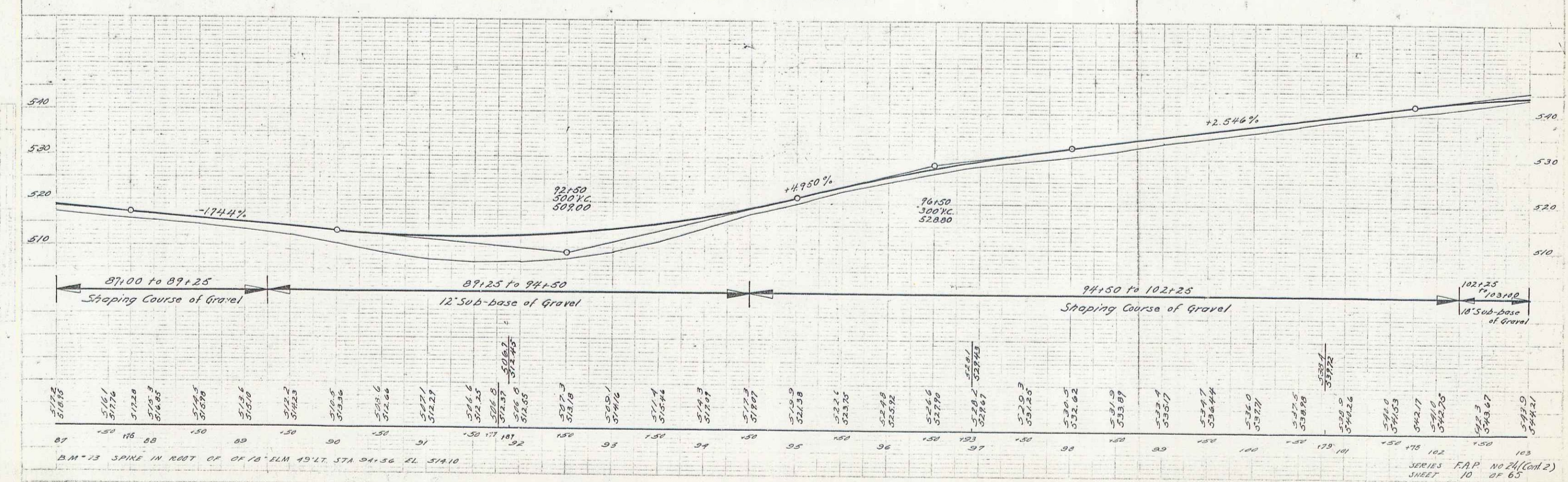
F. MACHIA
 $R = 0.057$

$A = 0.182$

$R = 0.46$

$R = 0.391$

PARCEL	INSTRUMENT	DATE	GRANTOR	GRANTEE	RECORDED	REMARKS
A-B-C	B.C.D.	10-25-37	FRANK MACHIA	ST. OF RT.	8947	10-27-35
A-B-C-D	B.C.D.	11-9-37	RALPH E. WELLS	"	35 594-395	11-12-37 COL. 7@ 99+00



SERIES F.A.P. NO 24 (cont. 2)
 SHEET 10 OF 65

R. T. of Way Map Fed. Aid Proj. No. 24.
 PROPERTY OF J. W. GREEN, EST.
 TOWN OF CAMBRIDGE, VT.

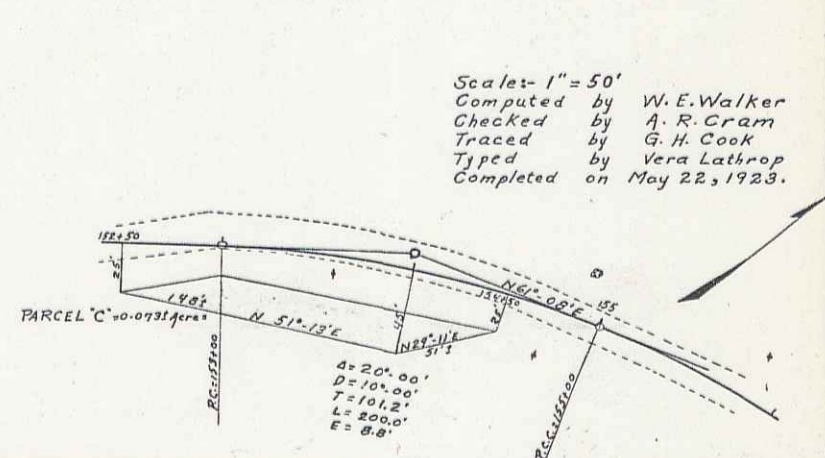
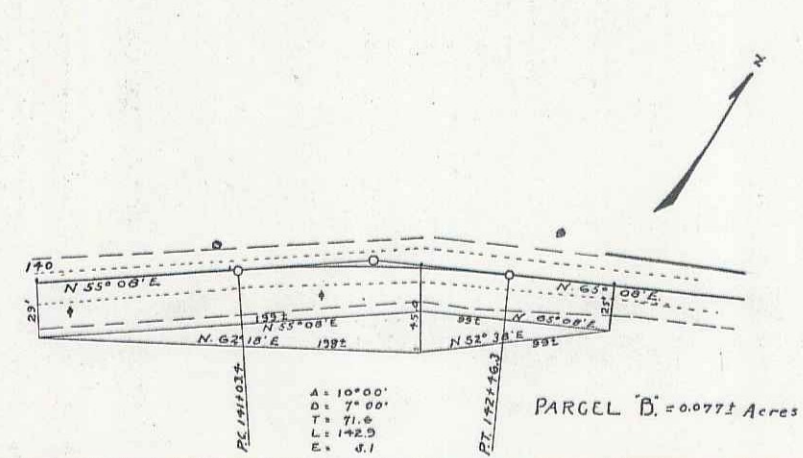
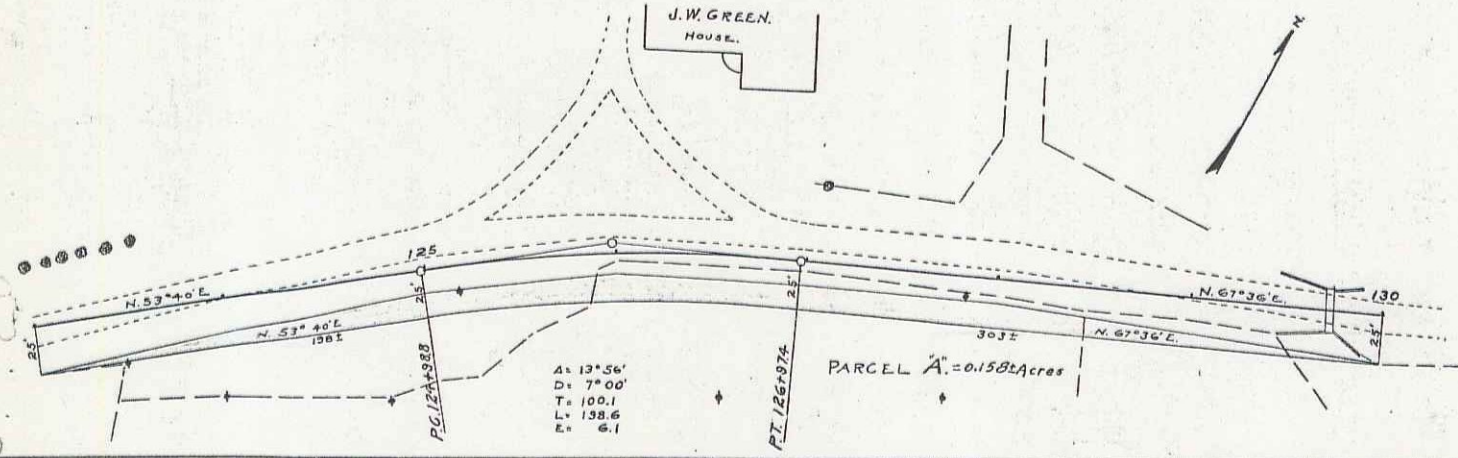
STATE OF VERMONT
 LAND TO BE ACQUIRED FOR THE
 LAMILLE RIVER ROAD
 FEDERAL AID PROJECT NO. 24 - TOWN OF CAMBRIDGE
 FROM J. W. GREEN ESTATE, REVISED OWNER

ALL THOSE PIECES OR PARCELS OF LAND HERINAFTER DESIGNATED AS PARCELS A, B AND C, SITUATED IN THE TOWN OF CAMBRIDGE, COUNTY OF LAMILLE, STATE OF VERMONT, FOR THE LAMILLE RIVER ROAD, FEDERAL AID PROJECT NO. 24, AS SHOWN ON THE ACCOMPANYING MAP AND DESCRIBED AS FOLLOWS:-

PARCEL "A".- BEGINNING AT A POINT ON THE SOUTHERLY BOUNDARY OF THE EXISTING LAMILLE RIVER ROAD, SAID POINT BEING 25.0 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 123+00 OF THE SURVEY BASE LINE OF THE PROPOSED LAMILLE RIVER ROAD, FEDERAL AID PROJECT NO. 24; THENCE N. 53°-40' E. 198.2 FEET TO A POINT 25.0 FEET DISTANT NORTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 124+98.8 OF SAID BASE LINE; THENCE ON A CURVE TO THE RIGHT, HAVING A RADIUS OF 794.02 FEET, TO A POINT 25.0 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 126+97.4 OF SAID BASE LINE; THENCE N. 67°-58' E. 302.2 FEET TO A POINT 25.0 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 130+00 OF SAID BASE LINE; THENCE WESTERLY ALONG THE SOUTHERLY BOUNDARY LINE OF SAID EXISTING HIGHWAY 700-02 FEET TO THE POINT OF BEGINNING; BEING 0.158 ACRES MORE OR LESS.

PARCEL "B".- BEGINNING AT A POINT ON THE SOUTHERLY BOUNDARY OF THE EXISTING LAMILLE RIVER ROAD, SAID POINT BEING 25.0 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 140+00 OF THE SURVEY BASE LINE OF THE PROPOSED LAMILLE RIVER ROAD, FEDERAL AID PROJECT NO. 24; THENCE N. 62°-18' E. 192.1 FEET TO A POINT 45.0 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 142+00 OF SAID BASE LINE; THENCE N. 62°-58' E. 99.2 FEET TO A POINT 25.0 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 143+00 OF SAID BASE LINE; THENCE WESTERLY ALONG THE SOUTHERLY BOUNDARY LINE OF SAID EXISTING HIGHWAY 284-01 FEET TO THE POINT OF BEGINNING; BEING 0.077 ACRES MORE OR LESS.

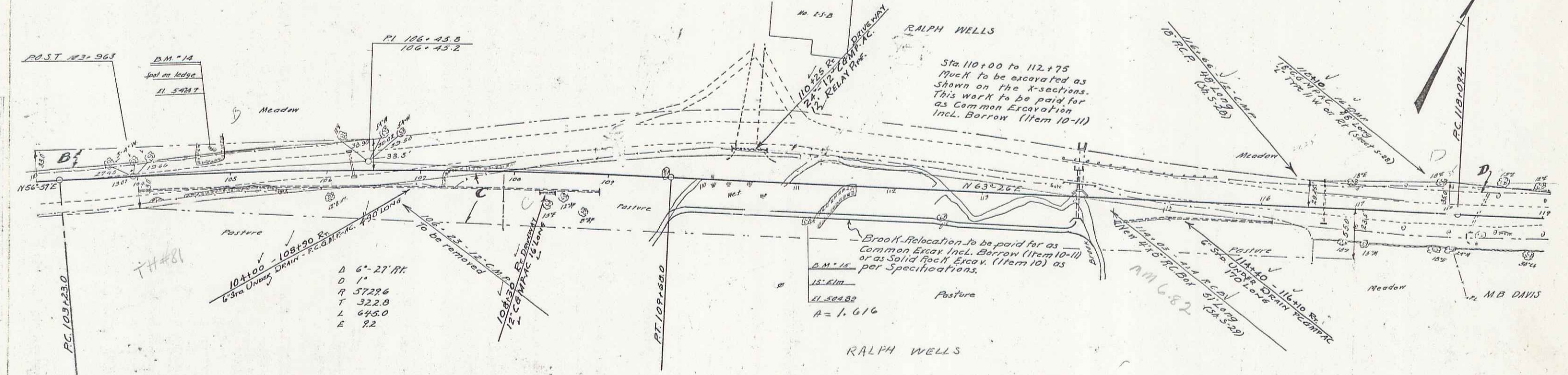
PARCEL "C".- BEGINNING AT A POINT ON THE SOUTHERLY BOUNDARY OF THE EXISTING LAMILLE RIVER ROAD, SAID POINT BEING 25.0 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 152+50 OF THE SURVEY BASE LINE OF THE PROPOSED LAMILLE RIVER ROAD, FEDERAL AID PROJECT NO. 24; THENCE N. 51°-13' E. 142.1 FEET TO A POINT 45.0 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 154+50 OF SAID BASE LINE; THENCE N. 29°-11' E. 51.1 FEET TO A POINT 25.0 FEET DISTANT SOUTHERLY, MEASURED AT RIGHT ANGLES, FROM STATION 154+50 OF SAID BASE LINE; THENCE WESTERLY ALONG THE SOUTHERLY BOUNDARY LINE OF SAID EXISTING HIGHWAY 106-01 FEET TO THE POINT OF BEGINNING; BEING 0.073 ACRES MORE OR LESS.



Scales- 1" = 50'
 Computed by W. E. Walker
 Checked by A. R. Gram
 Traced by G. H. Cook
 Typed by Vera Lathrop
 Completed on May 22, 1923.

SHEET NAME 11A

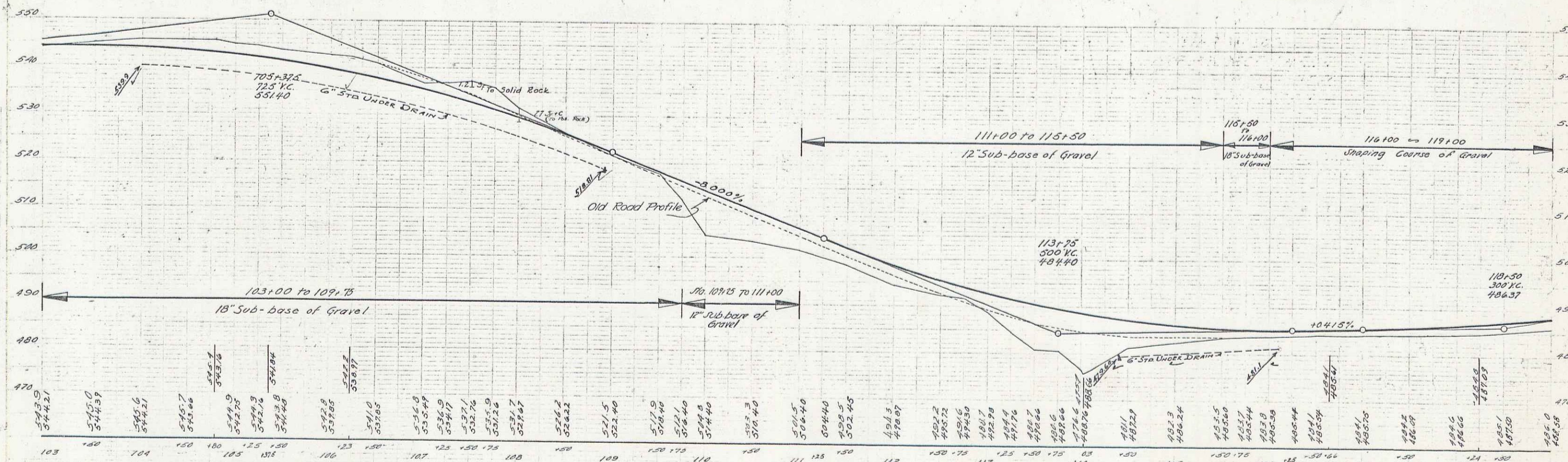
24 1938 11 65
 CAP 2



104+100 - 108+90 P.C.
 6" Sta Under Drain P.C.B.M.P.C.
 A 6'-27" RT.
 D 1'
 P 5722.6
 T 322.8
 L 645.0
 E 22

Brook Relocation to be paid for as
 Common Excav Incl. Borrow (Item 10-11)
 or as Solid Rock Excav. (Item 10) as
 per Specifications.
 15' R/W
 EL 502.80
 R = 1.616

PARCEL	INSTRUMENT	DATE	GRANTOR	GRANTEE	RECORDED	REMARKS
A-B-C-D	G.C.O.	11-9-37	RALPH E. WELLS	STOF V.T.	BOOK PAGE DATE 35 548-549 11/2/37	Sta 110+00



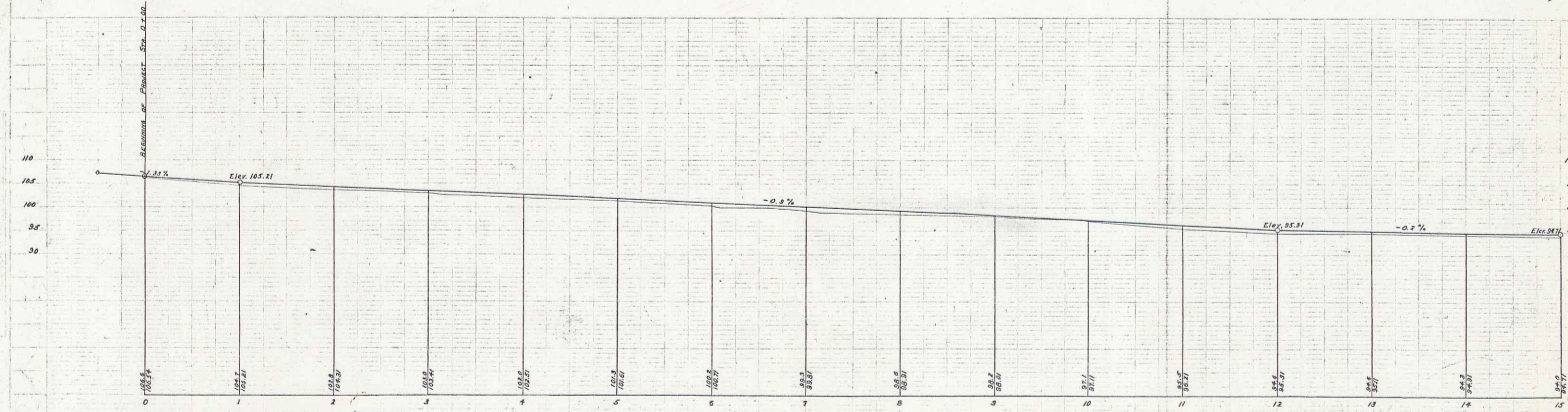
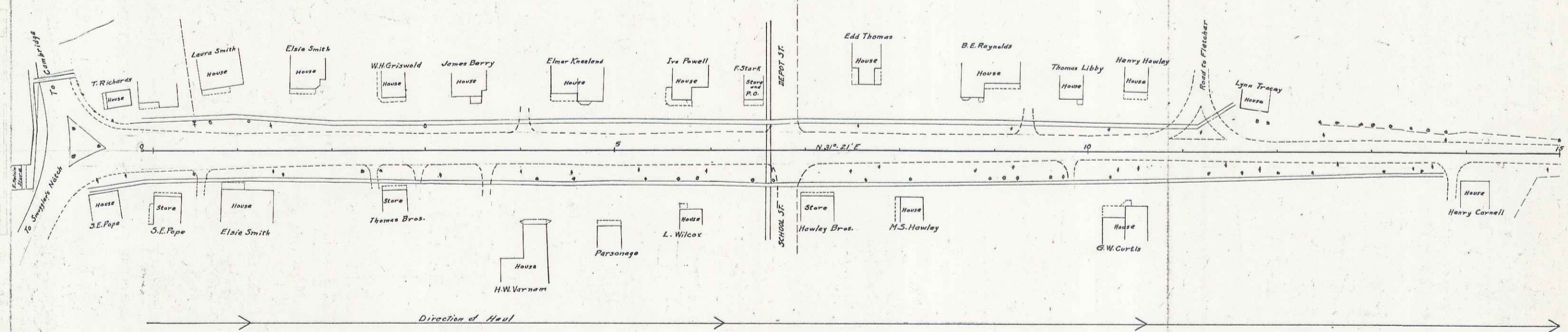
Station	Elevation	Station	Elevation	Station	Elevation	Station	Elevation
103	543.9	107	521.5	111	504.4	115	487.3
104	544.21	108	522.40	112	504.40	116	486.24
105	545.0	109	521.9	113	502.85	117	485.75
106	546.6	110	517.9	114	502.66	118	485.0
107	547.1	111	516.40	115	481.6	119	484.58
108	547.3	112	514.40	116	480.6		
109	547.6	113	512.3	117	479.6		
110	548.0	114	510.40	118	478.6		
111	548.5	115	508.5	119	477.6		
112	549.0	116	506.6				
113	549.5	117	504.7				
114	550.0	118	502.8				
115	550.5	119	500.9				
116	551.0						
117	551.5						
118	552.0						
119	552.5						

103 104 105 106 107 108 109 110 111 112 113 114 115 116 117 118 119
 B.M. 14 SPOT ON LEDGE 26' LT STA 104+70 EL 544.47
 B.M. 15 SPIKE IN ROOT OF 15' ELM 37' RT STA 111+15 EL 504.89

SERIES F.A.P. No 24 (cont 2)
 SHEET 11 OF 65

STONE BASE REQUIRED
Sta 0+00 to Sta 11+00 Length 1100'

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
9	VT.	24	1920	3	30



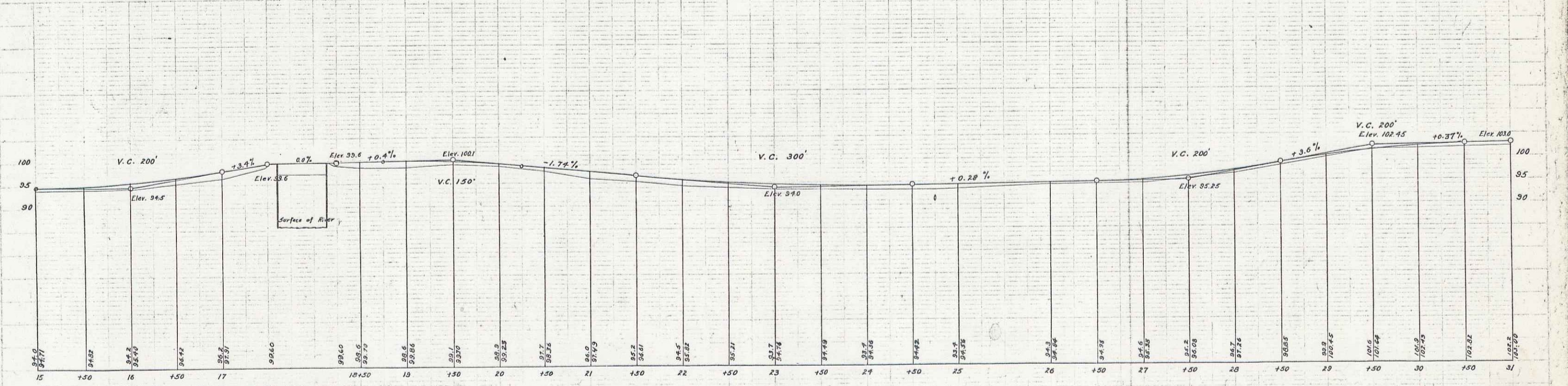
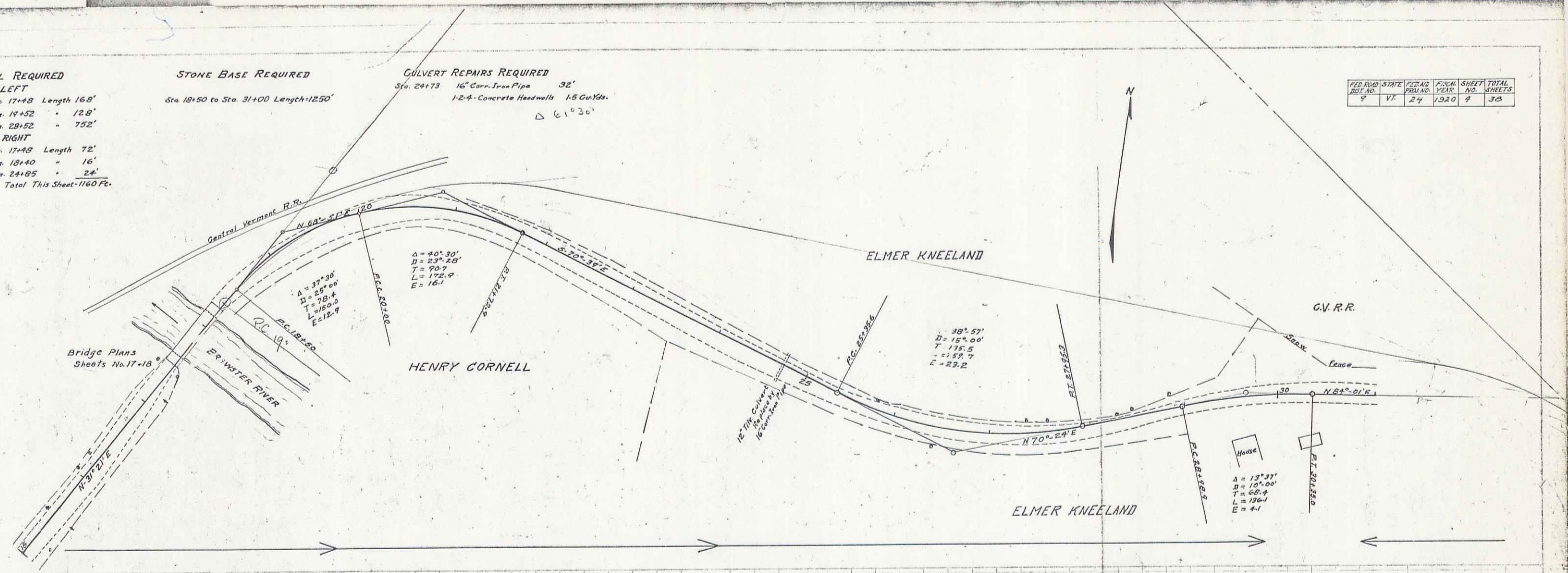
D.M. N.W. corner bottom step S.E. Pope's store Elev. 107.09
D.M. N.W. corner concrete step Howley Bros. store Elev. 100.73

GUARD RAIL REQUIRED
ON LEFT
 Sta. 15+80 to Sta. 17+48 Length 168'
 Sta. 18+24 - Sta. 19+52 - 128'
 Sta. 21+00 - Sta. 23+52 - 252'
ON RIGHT
 Sta. 16+76 to Sta. 17+48 Length 72'
 Sta. 18+24 - Sta. 18+40 - 16'
 Sta. 24+61 - Sta. 24+85 - 24'
 Total This Sheet - 1160 Ft.

STONE BASE REQUIRED
 Sta. 18+50 to Sta. 31+00 Length 1250'

CULVERT REPAIRS REQUIRED
 Sta. 24+73 16" Cor. Iron Pipe 32'
 1-2-4 Concrete Headwalls 15 Cu Yds.
 Δ 61°30'

FED. ROAD DIST. NO.	STATE	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	Vt.	24	1920	7



B.M. Top of ball inside of top girder S.E. corner R.R. bridge. Elev. 100.
 B.M. Spike in elm tree at Sta. 20+35 Elev. 98.99

Series P. No. 24. 11/10
 Sheet 4 of 8

16

GUARD RAIL REQUIRED ON LEFT
 Sta. 33+50 to Sta. 38+54 Length 504'
 Sta. 41+25 to Sta. 42+53 " 128'
 Sta. 43+88 to Sta. 44+12 " 24'
ON RIGHT
 Sta. 38+33 to Sta. 38+57 " 24'
 Sta. 43+10 to Sta. 44+12 " 70'
 Total This Sheet 752'

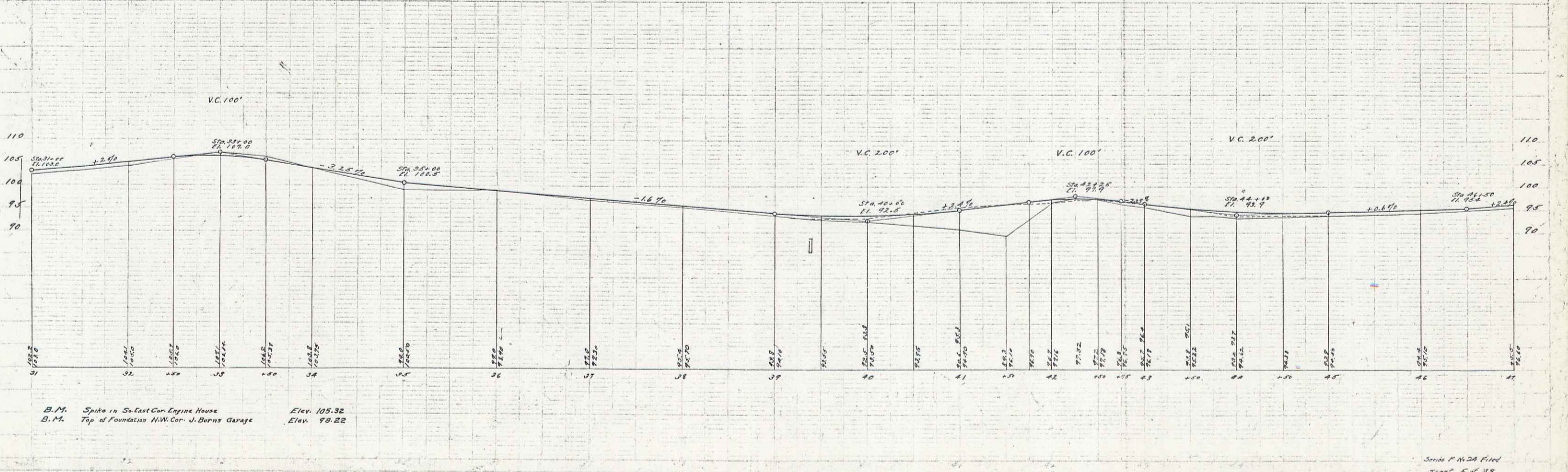
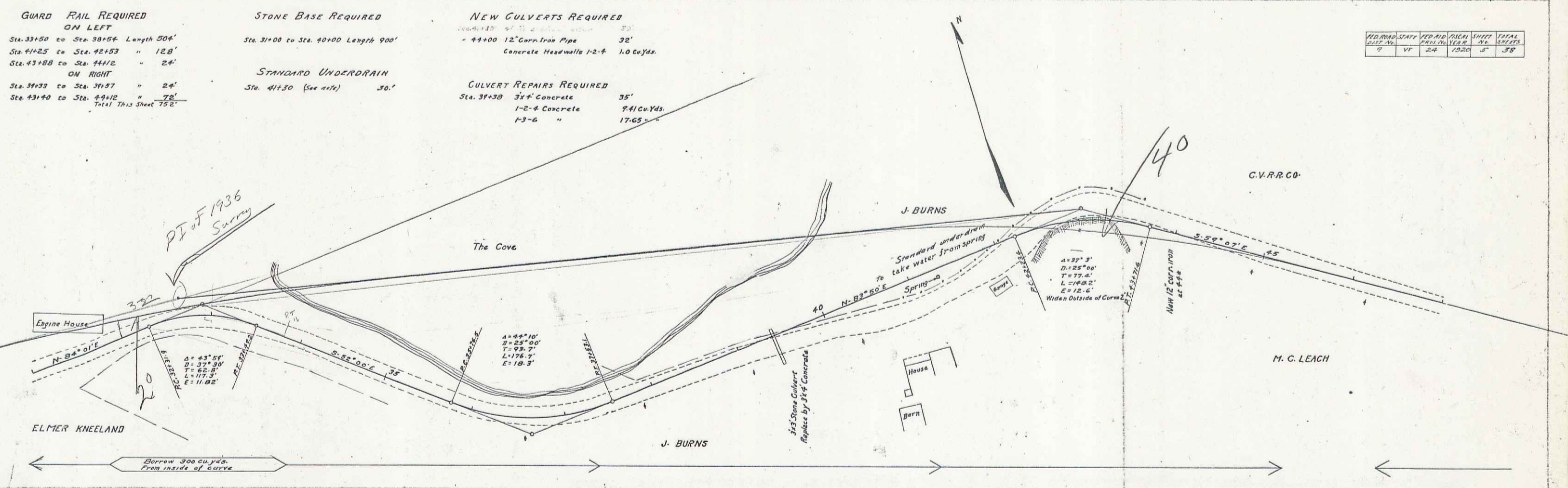
STONE BASE REQUIRED
 Sta. 31+00 to Sta. 40+00 Length 900'

STANDARD UNDERDRAIN
 Sta. 41+30 (see note) 30'

NEW CULVERTS REQUIRED
 44+00 12" Cor'n Iron Pipe 32'
 Concrete Headwalls 1-2-4 1.0 Cu Yds.

CULVERT REPAIRS REQUIRED
 Sta. 38+38 3/4" Concrete 35'
 1-2-4 Concrete 7.41 Cu Yds.
 1-3-6 " 17.65 "

NO.	DATE	BY	CHKD.	SCALE	SHEET	TOTAL SHEETS
9	VT	24	1020	5'	38	



E. R. Welch
A. A. Pittman
A. H. Brinkley

E. R. Welch
A. A. Pittman
A. H. Brinkley

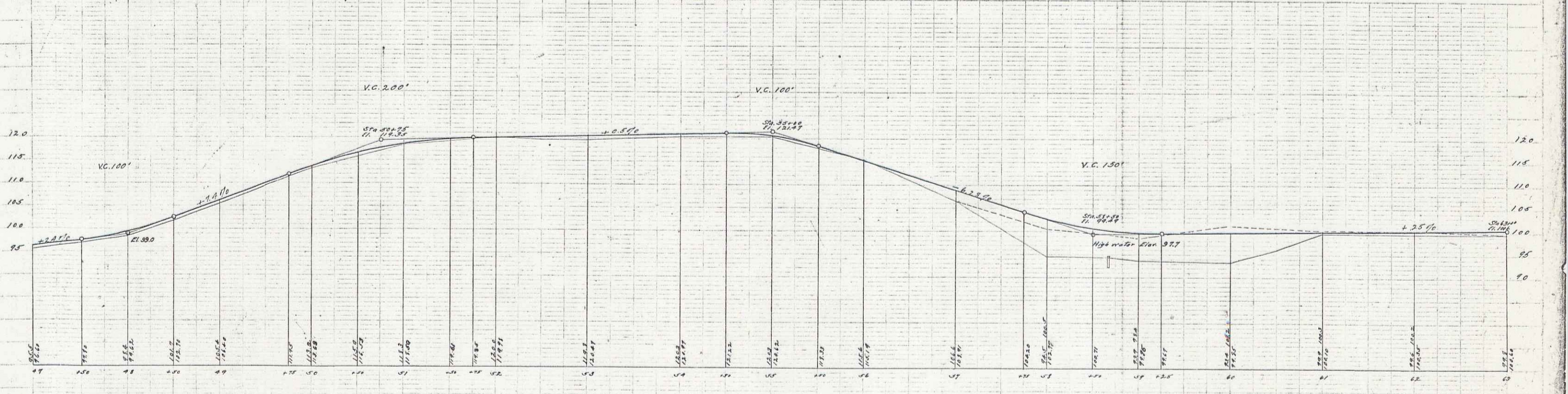
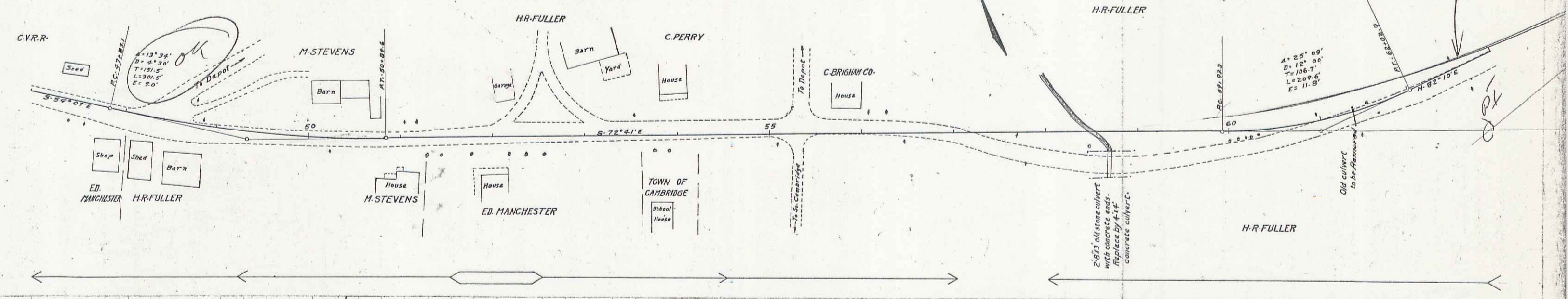
GUARD RAIL REQUIRED
ON LEFT
 Sta. 56+52 to Sta. 63+00 Length 698'
ON RIGHT
 Sta. 56+50 to Sta. 59+70 " 320'
 Total This Sheet... 928'

STONE BASE REQUIRED
 Sta. 62+00 to Sta. 63+00 Length 100'

GULVERT REPAIRS REQUIRED
 Sta. 58+67 4x4' Concrete 32'
 1-2-4 Concrete 11.09 Cu.Yds.
 1-3-6 " 16.98 "

UNDERDRAIN REQUIRED
 Sta. 57+83 to Sta. 56+00 Length 817'

DATE	BY	REVISION	NO.	TOTAL SHEETS
7	VT	24	1320	6
				38



B. 17. Nail in El. Light Pole Near No. W. Cor. E. Manchester's Shop Elev. 98.37
 B. 17. Bottom Cor. Base S.W. Cor. House Owned by C. Brigham Co. No. of Sta. 55+60 Elev. 118.80
 B. 17. No. W. Cor. of No. End Wall on Culvert at Sta. 58+67 Elev. 98.71

Scale 1" = 20' H'd
 Sheet 6 of 38

GUARD RAIL REQUIRED ON LEFT

Sta. 63+00 to Sta. 66+74	Length 374'
Sta. 68+38 to Sta. 68+62	" 24'
Sta. 71+63 to Sta. 71+87	" 24'
Sta. 73+00 to Sta. 74+28	" 128'

ON RIGHT

Sta. 68+38 to Sta. 68+62	" 24'
Sta. 71+63 to Sta. 71+87	" 24'
Total This Sheet 600'	

STONE BASE REQUIRED

Sta. 63+00 to Sta. 71+75	Length 875'
--------------------------	-------------

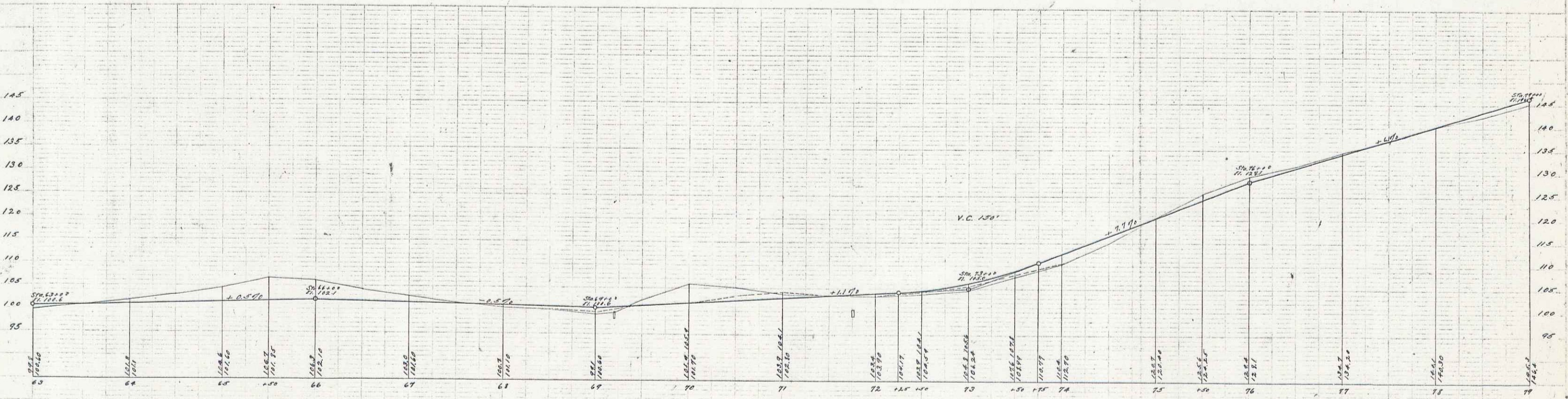
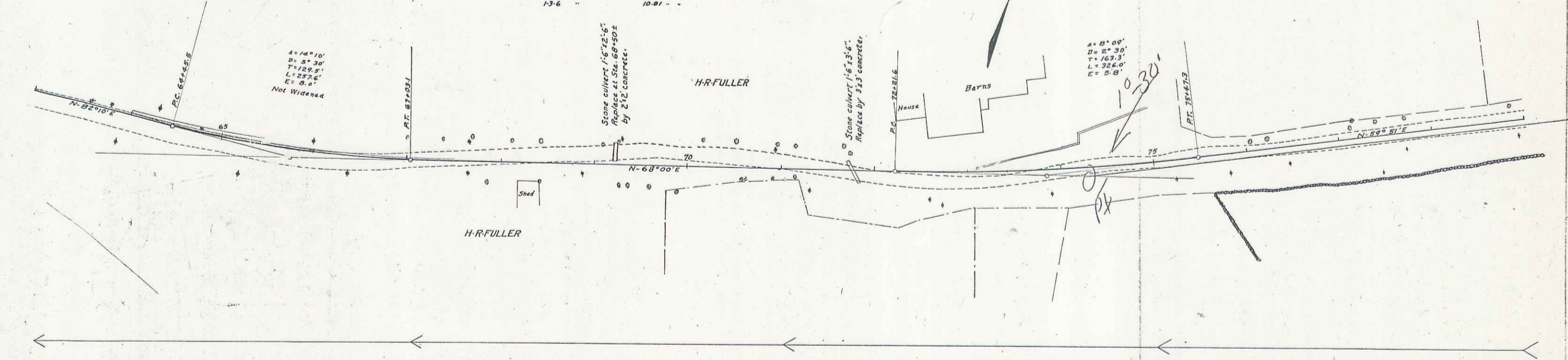
NEW CULVERTS REQUIRED

Sta. 68+50	2'x2' Concrete	32'
	1-2-4 Concrete	6.78 Cu Yds.
	1-3-6 "	6.81 "

CULVERT REPAIRS REQUIRED

Sta. 71+75	3'x3' Concrete	26'
	1-2-4 Concrete	7.06 Cu Yds.
	1-3-6 "	10.81 "

STATION	DATE	NO. OF SHEETS
9	VT	24
10	VT	24
11	VT	24
12	VT	24
13	VT	24
14	VT	24
15	VT	24
16	VT	24
17	VT	24
18	VT	24
19	VT	24
20	VT	24
21	VT	24
22	VT	24
23	VT	24
24	VT	24
25	VT	24
26	VT	24
27	VT	24
28	VT	24
29	VT	24
30	VT	24
31	VT	24
32	VT	24
33	VT	24
34	VT	24
35	VT	24
36	VT	24
37	VT	24
38	VT	24
39	VT	24
40	VT	24
41	VT	24
42	VT	24
43	VT	24
44	VT	24
45	VT	24
46	VT	24
47	VT	24
48	VT	24
49	VT	24
50	VT	24
51	VT	24
52	VT	24
53	VT	24
54	VT	24
55	VT	24
56	VT	24
57	VT	24
58	VT	24
59	VT	24
60	VT	24
61	VT	24
62	VT	24
63	VT	24
64	VT	24
65	VT	24
66	VT	24
67	VT	24
68	VT	24
69	VT	24
70	VT	24
71	VT	24
72	VT	24
73	VT	24
74	VT	24
75	VT	24
76	VT	24
77	VT	24
78	VT	24
79	VT	24
80	VT	24
81	VT	24
82	VT	24
83	VT	24
84	VT	24
85	VT	24
86	VT	24
87	VT	24
88	VT	24
89	VT	24
90	VT	24
91	VT	24
92	VT	24
93	VT	24
94	VT	24
95	VT	24
96	VT	24
97	VT	24
98	VT	24
99	VT	24
100	VT	24



B.M. Top of Post North of Sta. 64+80 Elev. 103.89
 B.M. Spike in 7 1/2" Pole South of Sta. 72+75 Elev. 108.09

Sheet 7 of 33

GUARD RAIL REQUIRED

ON LEFT

Sta. 80+38 to Sta. 80+62	Length	24'
Sta. 83+54 to Sta. 84+74	"	120'
Sta. 85+38 to Sta. 87+78	"	240'
Sta. 88+50 to Sta. 91+54	"	304'

ON RIGHT

Sta. 80+38 to Sta. 80+62	"	24'
Sta. 83+54 to Sta. 83+78	"	24'
Sta. 85+38 to Sta. 85+62	"	24'
Sta. 90+21 to Sta. 90+45	"	24'

Total This Sheet 784'

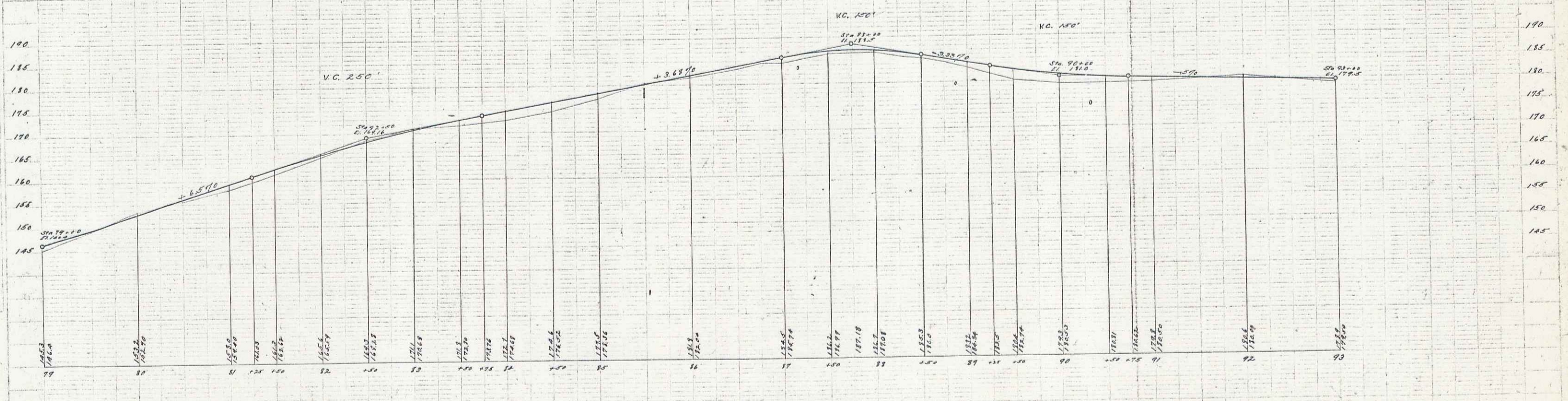
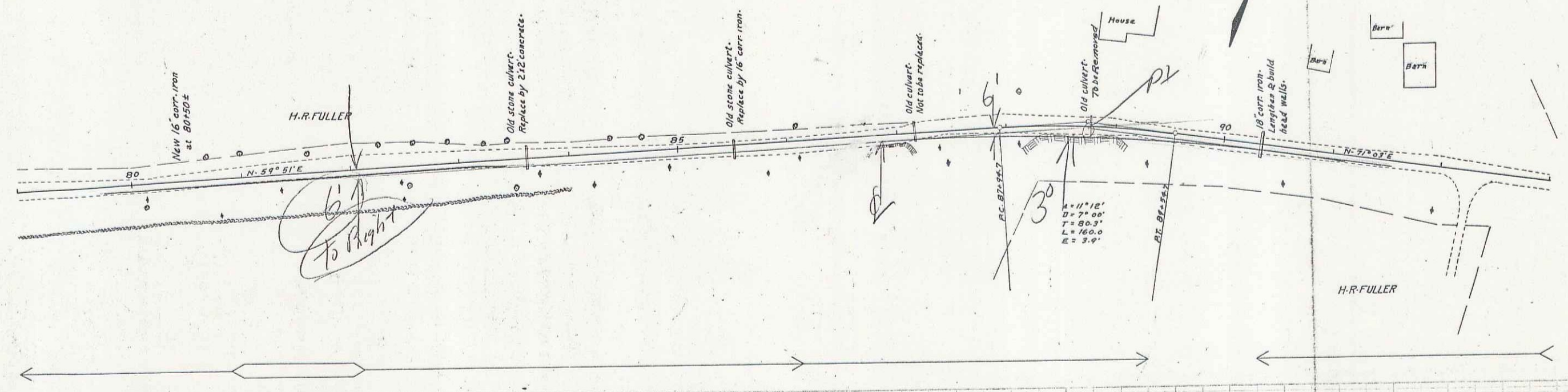
NEW CULVERTS REQUIRED

Sta. 80+50	16" Corr. Iron with B.I.	26'
	Concrete Headwalls 1-2-4	20 Cu Yds.

CULVERT REPAIRS REQUIRED

Sta. 83+66	2x2' Concrete	2.8'
	1-2-4 Concrete	5.65 Cu Yds.
	1-3-6 "	6.79 "
Sta. 85+50	16" Corr. Iron Pipe	26'
	Concrete Headwalls 1-2-4	1.5 Cu Yds.
Sta. 90+33	18" Corr. Iron Pipe	6'
	" " " "	8'
	Concrete Headwalls 1-2-4	1.5 Cu Yds.

FIELD STATE	NO. OF	FIELD SHEETS	TOTAL
VT	20	8	37



B.P. On Maple Tree No. of Sta. 88+20 Elev. 187.97

Series F. N. 24 P. 17
Sheet 8 of 38

20

GUARD RAIL REQUIRED

ON LEFT		
Sta. 93+60	to Sta. 95+60	Length 200'
Sta. 100+46	to Sta. 100+70	24'
Sta. 101+52	to Sta. 101+76	24'
Sta. 107+18	to Sta. 109+00	182'
ON RIGHT		
Sta. 95+36	to Sta. 95+60	24'
Sta. 100+46	to Sta. 100+70	24'
Sta. 101+52	to Sta. 101+76	24'
Sta. 107+18	to Sta. 107+66	48'
Total This Sheet 563'		

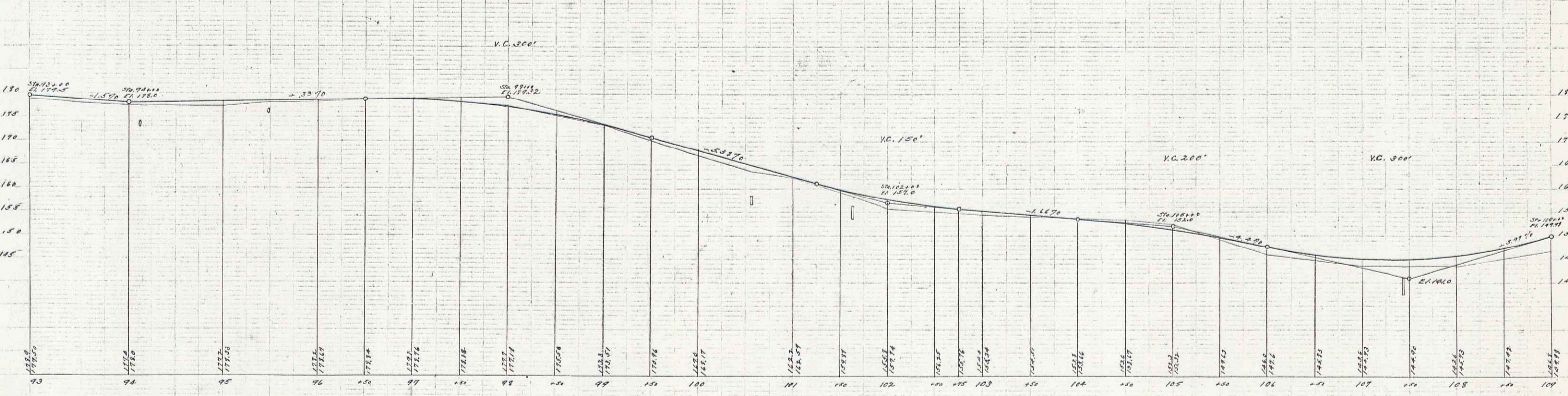
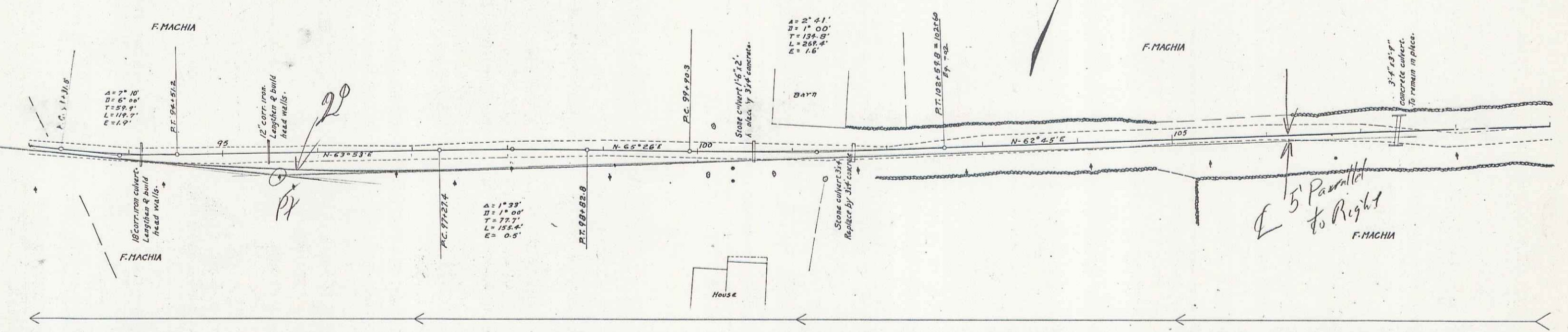
STONE BASE REQUIRED

Sta. 101+50	to Sta. 107+42	Length 592'
-------------	----------------	-------------

CULVERT REPAIRS REQUIRED

Sta. 94+12	18" Corr. Iron Pipe	6'
	Concrete Headwalls 1-2-4	1.5 Cu Yds.
- 95+48	12" Corr. Iron Pipe	4'
	Concrete Headwalls 1-2-4	1.5 -
- 100+58	3x4' Concrete	32'
	1-2-4 Concrete	0.63 Cu Yds.
	1-3-6	16.65 -
- 101+64	3x4' Concrete	26'
	1-2-4 Concrete	7.06 Cu Yds.
	1-3-6	10.81 -

NO.	DATE	BY	REVISION
1	1920		
2	1920		
3	1920		
4	1920		



B. M. Spike in Tel. Pole Sta. of Sta. 94+38 Elev. 178.28
 B. M. On Rock Sta. of Sta. 100+65 Elev. 165.92
 B. M. Top of W. End No. Headwall of Culvert Sta. 107+42 Elev. 145.08

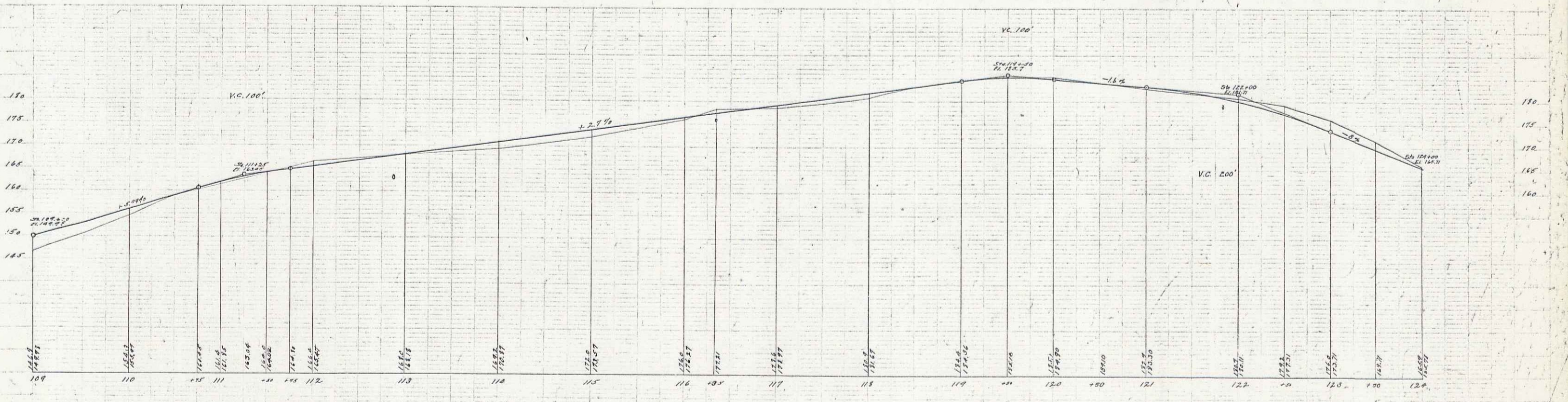
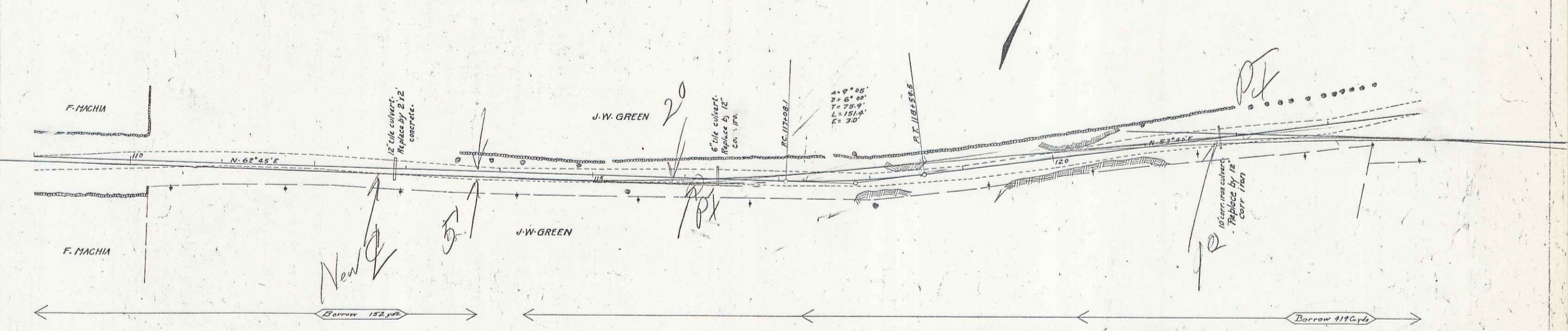
GUARD RAIL REQUIRED ON LEFT

Sta. 109+00	to Sta. 109+50	Length	50'
Sta. 112+64	to Sta. 113+12	"	48'
Sta. 116+23	to Sta. 116+47	"	24'
Sta. 121+70	to Sta. 121+94	"	24'
ON RIGHT			
Sta. 112+76	to Sta. 113+00	"	24'
Sta. 116+23	to Sta. 116+47	"	24'
Sta. 121+70	to Sta. 121+94	"	24'
Total This Sheet			268'

CULVERT REPAIRS REQUIRED

Sta. 112+88	2x2' Concrete	31'
1-2-4	"	8.24 Cu.Yd.
1-3-6	"	8.06 "
116+35	12" Corr. Iron Pipe	28'
Concrete Headwalls 1-2-4		1.0 Cu.Yd.
121+82	12" Corr. Iron Pipe	28'
Concrete Headwalls 1-2-4		1.0 Cu.Yd.

VEG. REMOVAL	TOTAL	AREA	SHIFT	TOTAL	
SQ. YD.	YD.	SQ. YD.	FO	SHIFT	
0	17	24	120	10	37



B.P. On Rock So. of Sta. 113+85 Elev. 171.48
 B.P. Nail in Willow Tree No. of Sta. 122+20 Elev. 161.47

GUARD RAIL REQUIRED

ON LEFT

Sta. 129+54	to Sta. 130+50	Length 96'
Sta. 132+18	to Sta. 132+42	24'
Sta. 133+00	to Sta. 134+04	24'
Sta. 137+57	to Sta. 139+49	192'

ON RIGHT

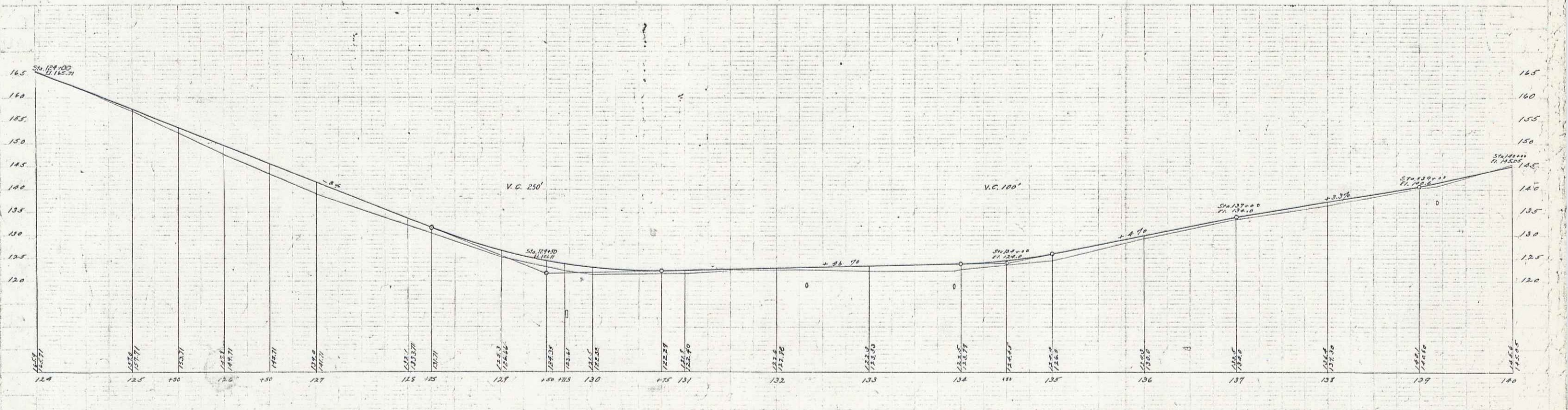
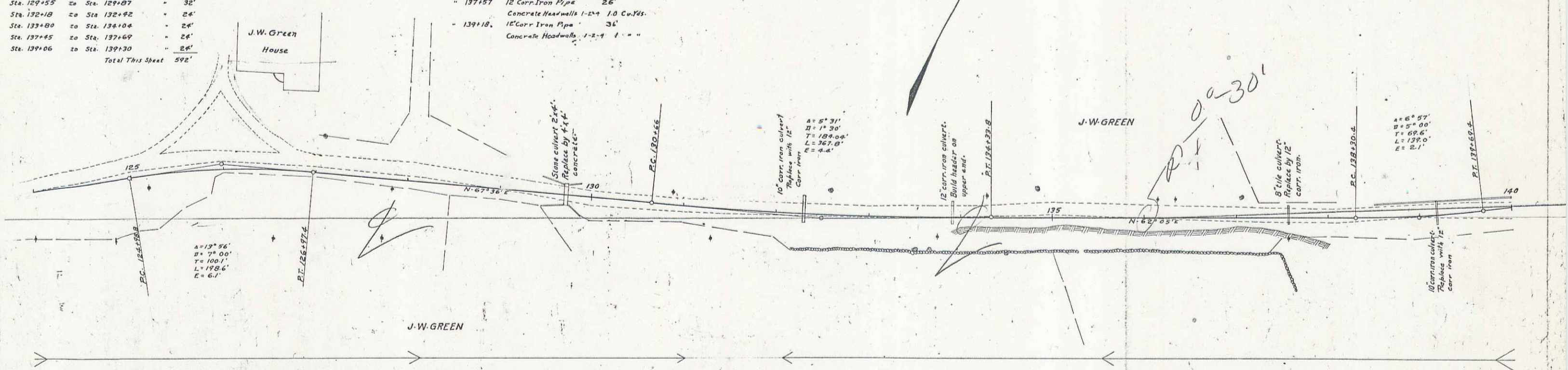
Sta. 125+75	to Sta. 127+03	128'
Sta. 129+55	to Sta. 129+87	32'
Sta. 132+18	to Sta. 132+42	24'
Sta. 133+00	to Sta. 134+04	24'
Sta. 137+45	to Sta. 137+69	24'
Sta. 139+06	to Sta. 139+30	24'

Total This Sheet 592'

CULVERTS REPAIRS REQUIRED

Sta. 129+71.5	4'x4' Concrete	41'
	1-2'x Concrete	13.99 Cu. Yds.
	1-3'x6 "	19.94 "
132+30	12" Corv. Iron Pipe	38'
	Concrete Headwalk 1-2-4	1 - "
133+92	12" Corv. Iron Pipe	6'
	Concrete Headwalk on Right 1-2-4 0.5 - "	
137+57	12" Corv. Iron Pipe	26'
	Concrete Headwalk 1-2-4 1.0 Cu. Yds.	
139+18	12" Corv. Iron Pipe	36'
	Concrete Headwalk 1-2-4 1 - "	

FORM NO.	REV.	DATE	BY	CHK.
4	1	2-24	1926	JA



B.M. On South End of Stake Sta. 129+71.5 Elev. 120.84
 B.M. On Peak South of Sta. 139+30 Elev. 142.91

Series F-24 P-100
 Sheet 11 of 38

23

GUARD RAIL REQUIRED

ON LEFT

Sta. 142+40 to Sta. 151+00	Length 752'
Sta. 151+25 to Sta. 152+21	96'
ON RIGHT	
Sta. 143+75 to Sta. 143+99	24'
Sta. 148+23 to Sta. 148+47	24'
Sta. 149+73 to Sta. 149+97	24'
Sta. 151+39 to Sta. 151+63	24'
Total This Sheet 944'	

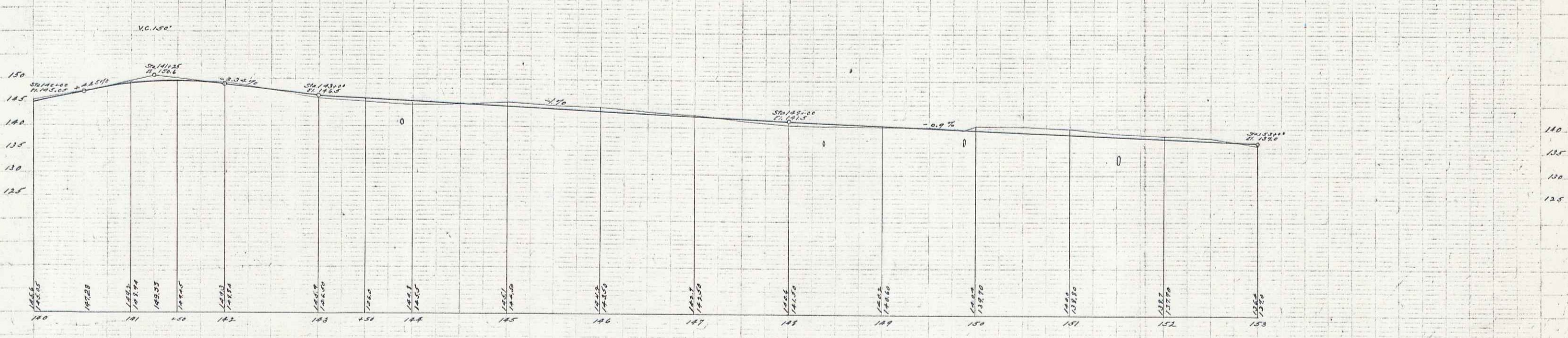
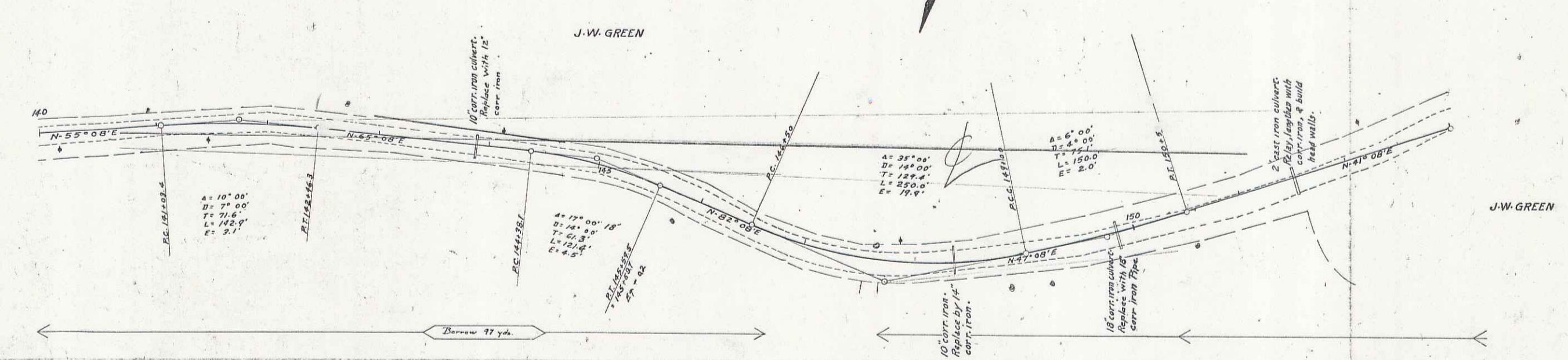
CULVERT REPAIRS REQUIRED

Sta. 143+07	12" Cor. Iron Pipe	34'
	Concrete Headwalls 1-2-4	1.0 Cu. Yd.
Sta. 148+35	14" Cor. Iron Pipe	36'
	Concrete Headwalls 1-2-4	1.0 "
Sta. 149+85	18" Cor. Iron Pipe	38'
	Concrete Headwalls 1-2-4	1.5 "
Sta. 151+51	24" Cor. Iron Pipe	12'
	Concrete Headwalls 1-2-4	2.0 "

RELAYING PIPE CULVERTS

Sta. 151+51	27'
-------------	-----

NO. OF SHEETS	NO. OF SHEETS	TOTAL SHEETS
9	26	35



B.M. On Rock North of Sta. 141+90 Elev. 150.30
 B.M. Nail in Maple Tree S. of Sta. 148+12 Elev. 144.11

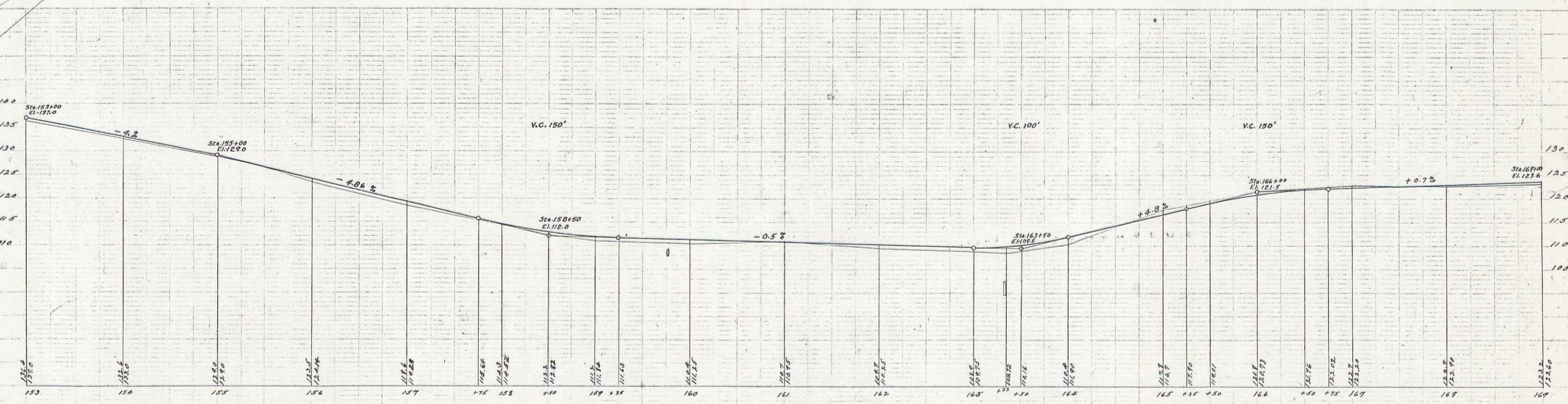
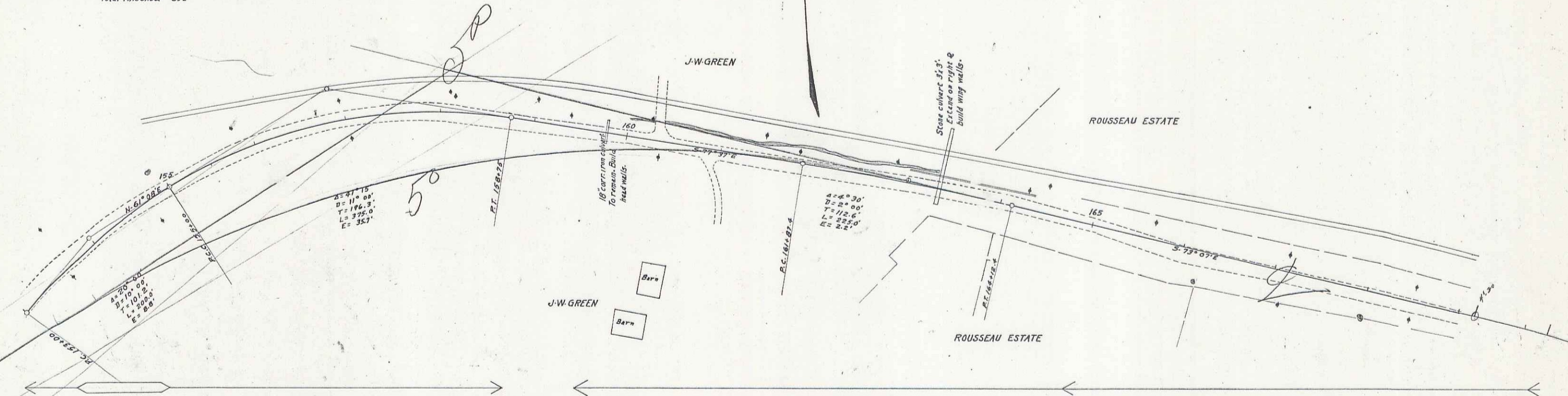
Series F 24 P 104
 Sheet 12 of 39

24

GUARD RAIL REQUIRED
 ON LEFT
 Sta. 158+00 to Sta. 164+48 Length 648'
 ON RIGHT
 Sta. 159+65 to Sta. 159+89 Length 24'
 Sta. 162+75 to Sta. 164+35 " 160'
 Total This Sheet 832'

CULVERT REPAIRS REQUIRED
 Sta. 159+77 18" Cor. Iron Pipe
 Concrete Heavealls 141.5 Cu Yds
 - 163+33 3'x3' Stone 6.0'
 Concrete Wingwalls 2-4 1.87' -
 " " 1-2-6 4.28' -

PROJ. NO.	STATE	PROJ. NO.	PROJ. DATE	ENGR.	DATE
9	VT	28	1920	13	57



B.M. Nail in Tel. Pole Sta. of Sta. 153+60 Elev. 137.67
 B.M. Base of Rail No. of Sta. 158+80 Elev. 113.03

Series P. 1620 P. 100
 3200' 13 of 39

25

GUARD RAIL REQUIRED ON LEFT

Sta. 174+22 to Sta. 174+70	Length 48'
Sta. 176+88 to Sta. 177+12	" 24'
Sta. 180+00 to Sta. 185+09	" 509'

ON RIGHT

Sta. 174+30 to Sta. 174+54	" 24'
Sta. 176+08 to Sta. 177+12	" 24'
Sta. 181+92 to Sta. 182+16	" 24'
Sta. 183+25 to Sta. 183+49	" 24'
Total This Sheet 668'	

NEW CULVERTS REQUIRED

Sta. 177+00	16" Cor. Iron Pipe With B.I. 30'
	1-2-4 Concrete 1.0 Cu Yds.
	1-3-6 " 1.0 - "

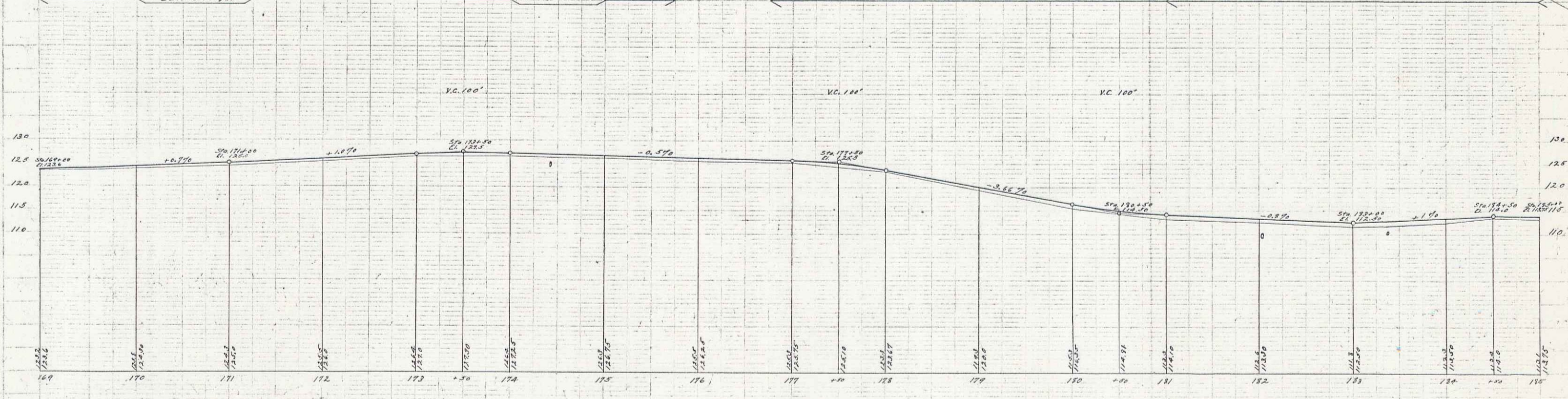
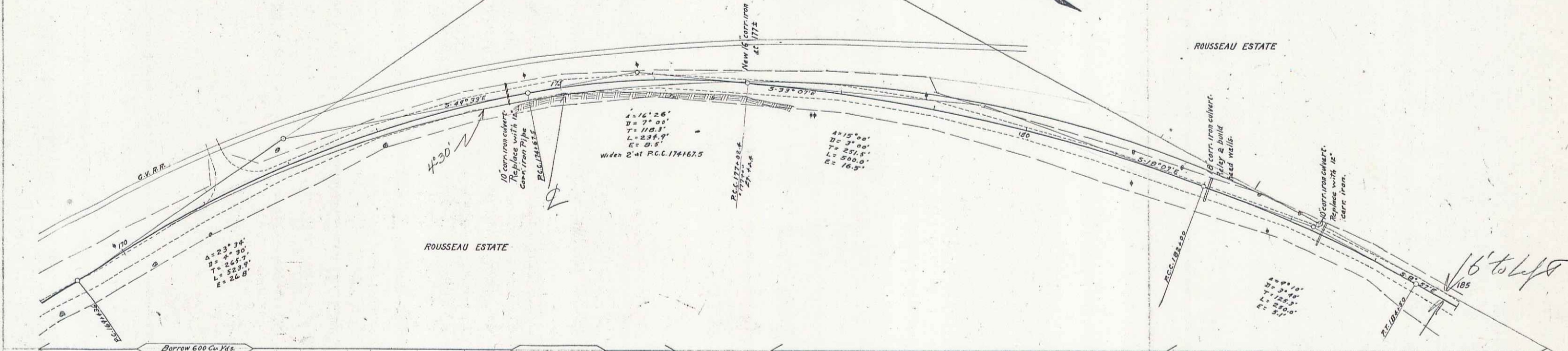
RELAYING PIPE CULVERTS

Sta. 182+04	" 25'
-------------	-------

GULVERT REPAIRS REQUIRED

Sta. 174+42	12" Cor. Iron Pipe 30'
	Concrete Headwalls 1-2-4 1.0 Cu Yds.
182+04	18" Cor. Iron Pipe 0.0'
	Concrete Headwalls 1-2-4 1.5 - "
183+37	12" Cor. Iron Pipe 35'
	Concrete Headwalls 1-2-4 1.0 - "

NO. ROAD	STRT	END	FOOTING	SWEEP	TOTAL
NO. ST.	NO.	NO.	NO.	NO.	NO.
7	VF	24	1820	24	1820



B.M. Nail in T.S. Pile No. of Sta. 169+95 Elev. 124.87
 B.M. Nail in T.S. Pile No. of Sta. 175+85 Elev. 123.61
 B.M. On Elm Tree No. of Sta. 181+15 Elev. 112.81

Series 37128 Filed
 Sept. 14, 1911

GUARD RAIL REQUIRED

ON LEFT

Sta. 185+00 to Sta. 187+00	Length 200'
Sta. 189+54 to Sta. 189+78	24'
Sta. 199+92 to Sta. 197+00	200'
Sta. 199+28 to Sta. 199+52	24'

ON RIGHT

Sta. 186+78 to Sta. 187+00	22'
Sta. 189+34 to Sta. 189+78	24'
Sta. 194+93 to Sta. 195+17	24'
Sta. 199+28 to Sta. 199+52	24'

Total This Sheet 556'

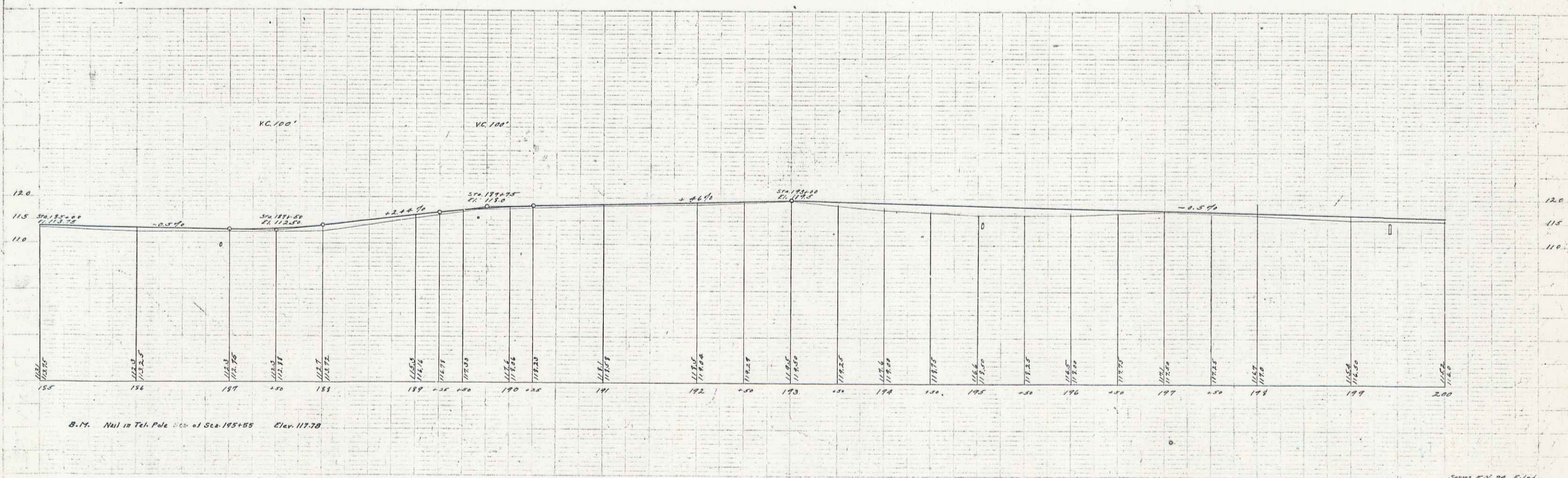
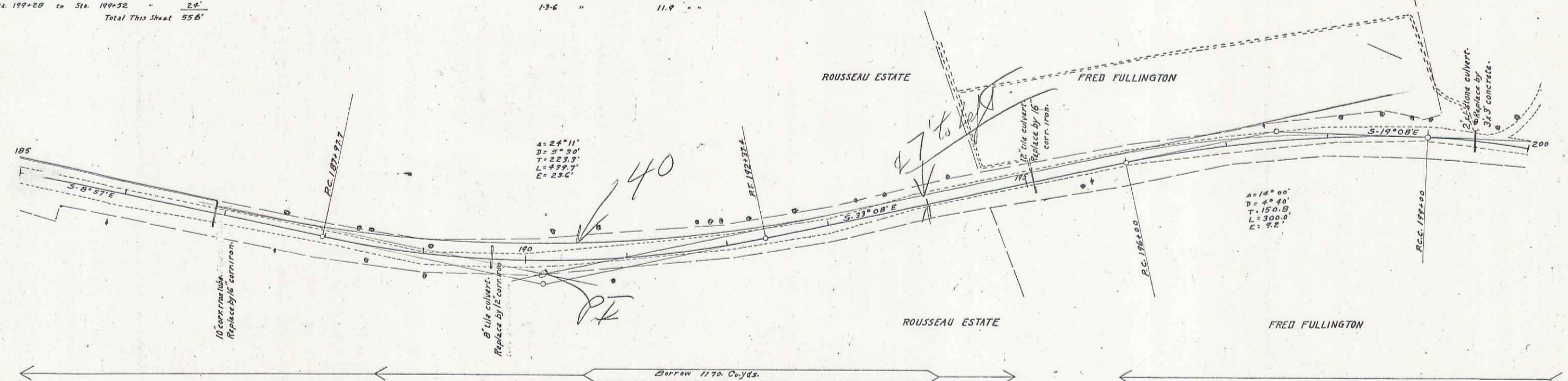
STONE BASE REQUIRED

Sta. 198+00 to Sta. 200+00	Length 200'
----------------------------	-------------

GULVERT REPAIRS REQUIRED

Sta. 186+90	16" Corr. Iron Pipe	32'
	Concrete Headwalls 1-2-4	15 Cu. Yds.
189+66	12" Corr. Iron Pipe	30'
	Concrete Headwalls 1-2-4	1.0 "
195+05	16" Corr. Iron Pipe With D.I.	30'
	1-E-4 Concrete	1.0 "
	1-3-6 "	2.5 "
199+40	3'x3' Concrete With D.I.	29'
	1-E-4 Concrete	7.85 Cu. Yds.
	1-3-6 "	11.9 "

SECTION	STAKE	TOP OF	SIGNAL	HEIGHT	TOTAL
NO.	VT.	24	1920	15	39



Series N. 24. Filed
Sheet 18 of 38

27

GUARD RAIL REQUIRED ON LEFT

Sta. 200+52	to Sta. 201+48	Length 96'
Sta. 205+13	to Sta. 205+37	" 24'
Sta. 207+57	to Sta. 207+83	" 26'
Sta. 208+50	to Sta. 212+50	" 400'

ON RIGHT

Sta. 205+13	to Sta. 205+37	" 24'
Sta. 207+57	to Sta. 207+83	" 26'
Sta. 212+20	to Sta. 212+44	" 24'

Total This Sheet 616'

STONE BASE REQUIRED

Sta. 200+00 to Sta. 207+65 Length 765'

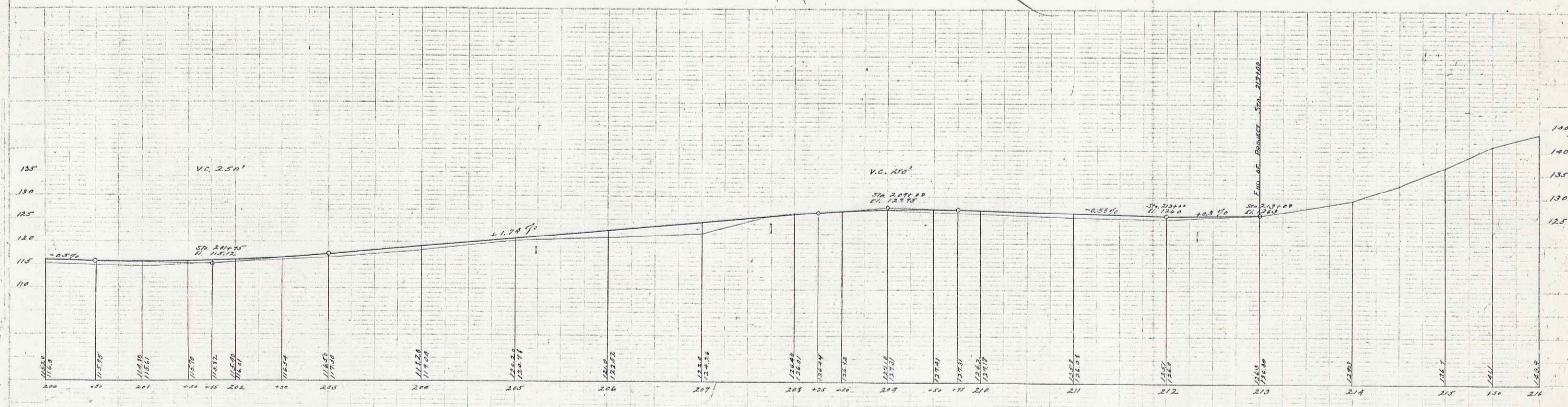
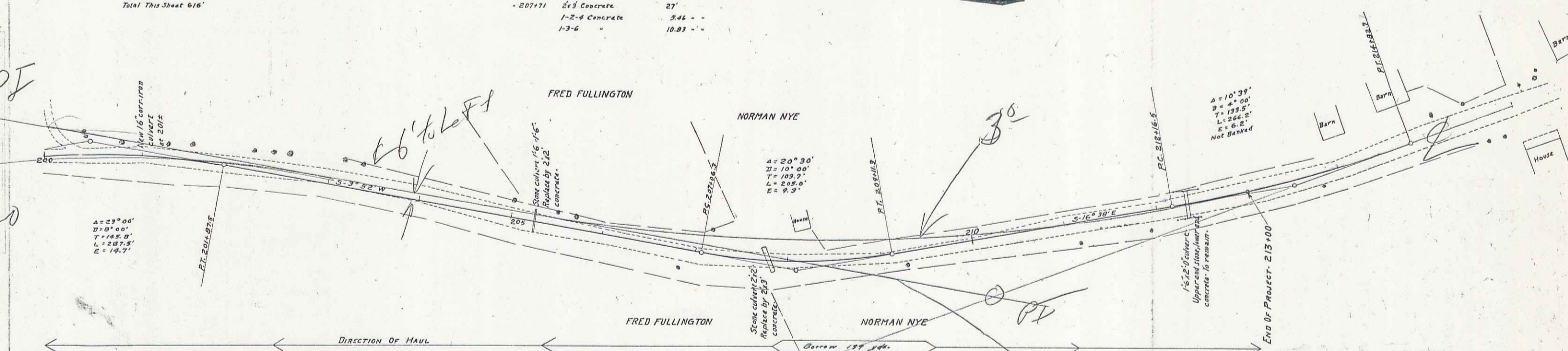
NEW CULVERTS REQUIRED

Sta. 201+00	16" Carr. Iron With D.I.	22'
	1-2-4 Concrete	10 Cu Yds.
	1-3-6 "	2.5 "

CULVERT REPAIRS REQUIRED

Sta. 205+23	24" Concrete	26'
	1-2-4 Concrete	527 Cu Yds.
	1-3-6 "	7.59 "
	2-3 Concrete	27 "
- 207+71	1-2-4 Concrete	5.44 "
	1-3-6 "	10.83 "

FOR ROAD DIST. STA.	FOR A.D. YEAR	FOR FISCAL YEAR	TOTAL SHEETS
9	VE	24	16 37



B.M. On Rock S. of Sta. 200+06 Elev. 118.52
 B.M. Set. Cor. Board N.W. Cor. Horse Barn on Nye Farm Elev. 145.92

Series P-16 24 Piped
 50207 16 of 38