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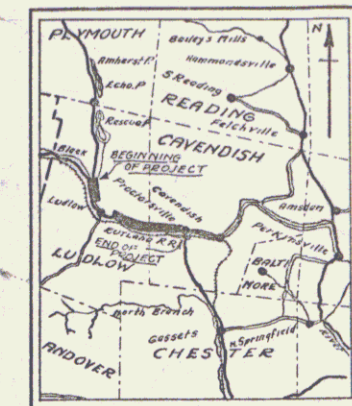
STATE OF VERMONT  
STATE HIGHWAY DEPARTMENT

PLAN AND PROFILE OF PROPOSED  
STATE HIGHWAY  
FEDERAL AID PROJECT

TOWNS OF LUDLOW - CAVENDISH  
RUTLAND-BELLOWS FALLS ROAD

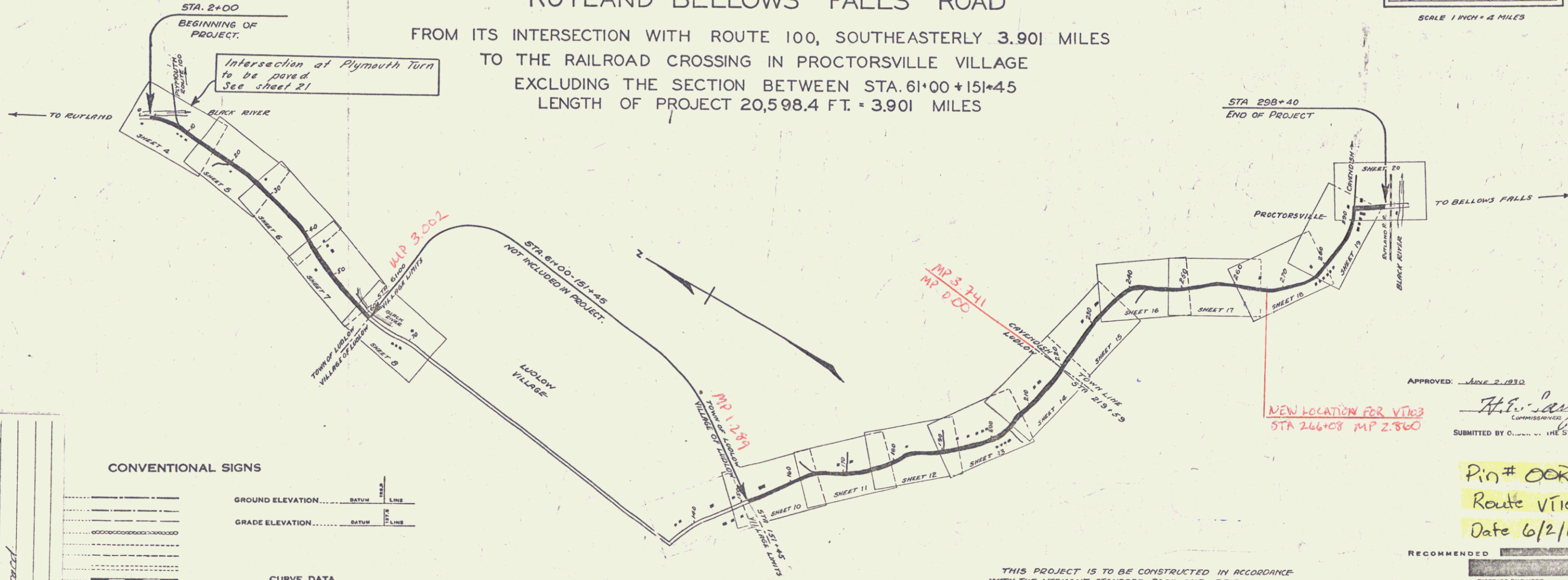
FROM ITS INTERSECTION WITH ROUTE 100, SOUTHEASTERLY 3.901 MILES  
TO THE RAILROAD CROSSING IN PROCTORSVILLE VILLAGE  
EXCLUDING THE SECTION BETWEEN STA. 61+00 + 151+45  
LENGTH OF PROJECT 20,598.4 FT. = 3.901 MILES

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
9	VT.	121-A	1930	1	57



SCALE 1 INCH = 4 MILES

(To Be Returned to R.O.W. Division)  
Town of VT 103  
PERPETUAL  
DOWN FILE  
RIGHT-OF-WAY DIVISION



PROJECT	Ludlow-Cavendish
NUMBER	121-A
TYPE	
CONTRACTOR	See file card
LOCATION	
YEAR	1930

**CONVENTIONAL SIGNS**

GROUND ELEVATION..... BATHY LINE

GRADE ELEVATION..... BATHY LINE

**CURVE DATA**

DEFLECTION ANGLE..... Δ

DEGREE OF CURVE..... D.

RADIUS OF CURVE..... R.

TANGENT DISTANCE..... T.

LENGTH OF CURVE..... L.

EXTERNAL DISTANCE..... E.

POINT OF INTERSECTION..... P. I.

POINT OF CURVE..... P. C.

POINT OF TANGENT..... P. T.

POINT ON TANGENT..... P. O. T.

SCALE: 1 IN. = 1000 FT.

THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE VERMONT STANDARD ROAD AND BRIDGE SPECIFICATIONS FOR 1930.

THESE PLANS ARE SUBJECT TO SUCH REVISIONS AS MAY BE REQUIRED BY THE COMMISSIONER OF HIGHWAYS.

APPROVED: JULY 2, 1930  
H. E. Sargent  
Commissioner of Highways

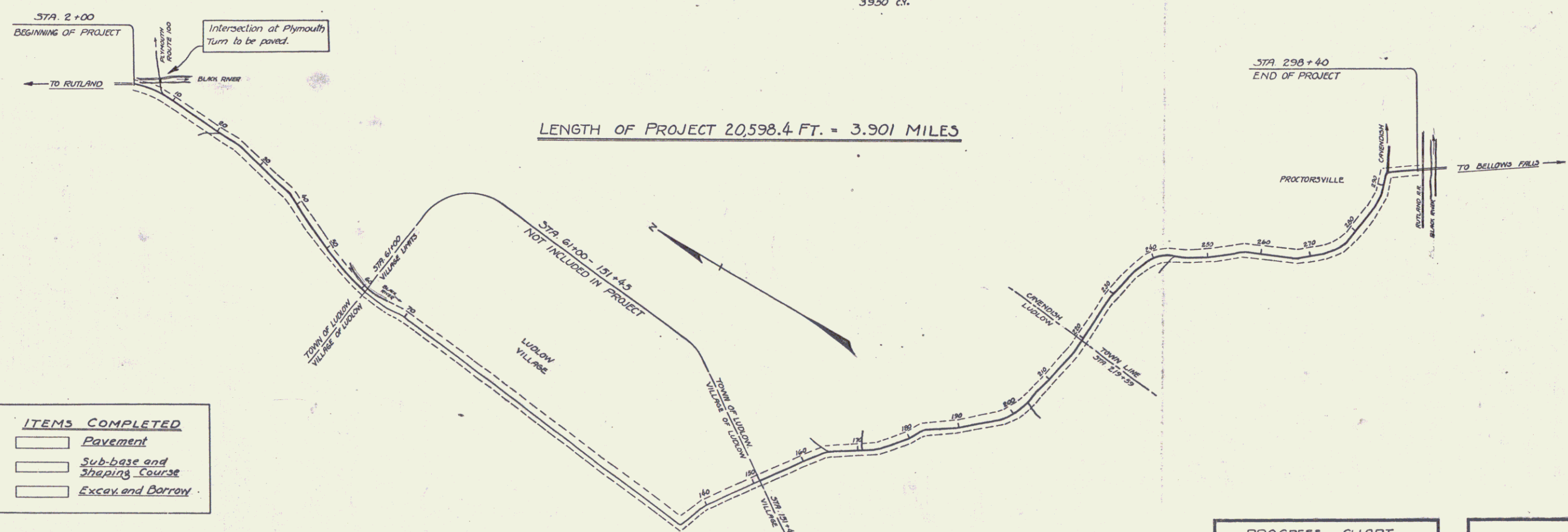
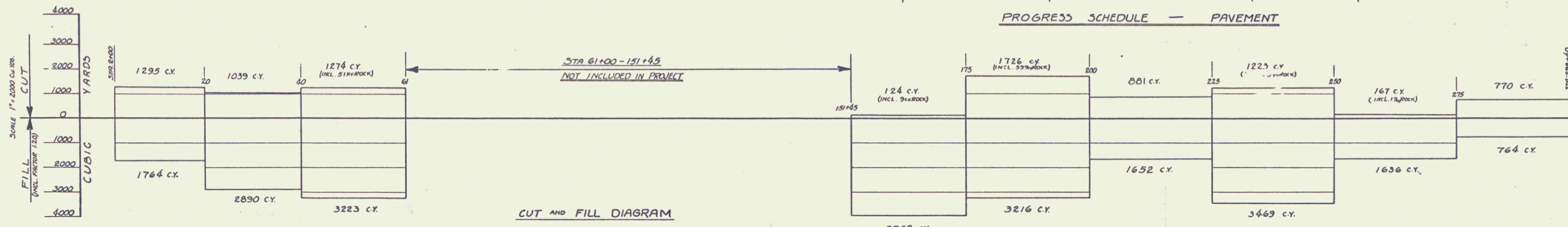
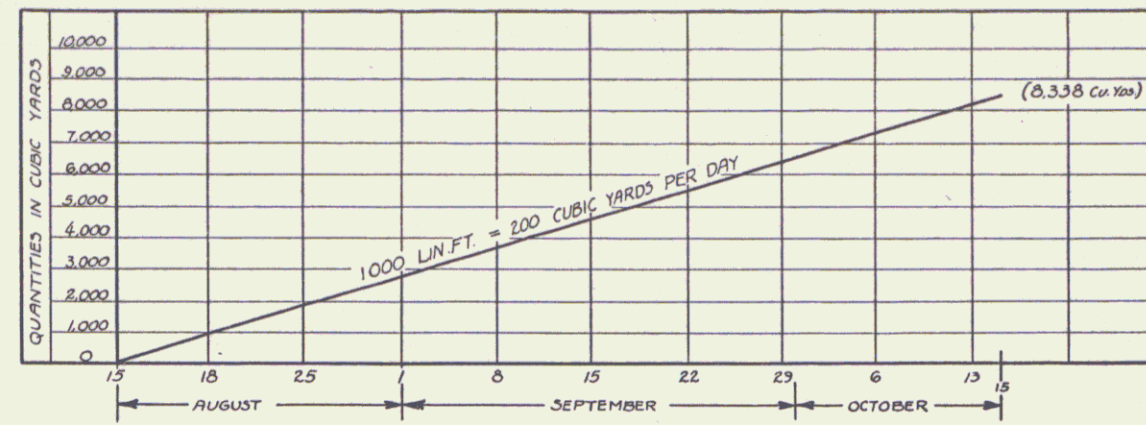
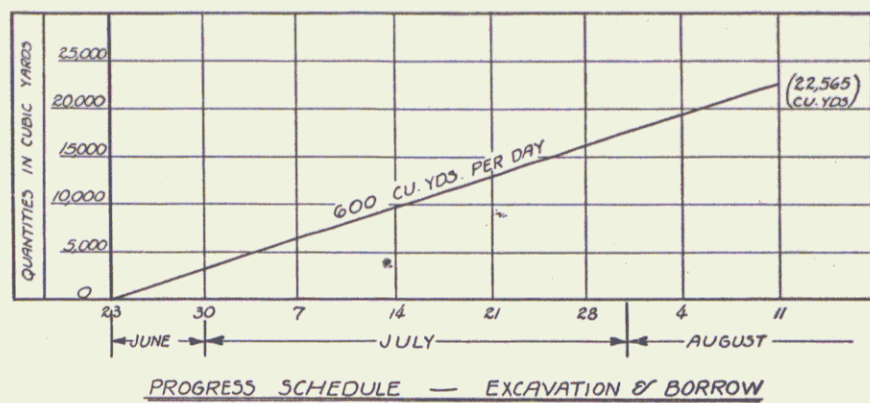
Pin # 00R130  
Route VT 103  
Date 6/2/1930

RECOMMENDED [Signature] DISTRICT ENGINEER BUREAU OF PUBLIC ROADS

RECOMMENDED FOR APPROVAL [Signature] CHIEF ENGINEER BUREAU OF PUBLIC ROADS

APPROVED [Signature] DIRECTOR - BUREAU OF PUBLIC ROADS

SERIES F. NO. 121-A FILED  
SHEET 1 OF 57



- ITEMS COMPLETED**
- Pavement
  - Sub-base and Shaping Course
  - Excav. and Borrow

PLAN

LENGTH OF PROJECT 20,598.4 FT. = 3.901 MILES

PROGRESS CHART  
LUDLOW - CAVENDISH 121-A  
VERMONT DEPT. OF HIGHWAYS  
1930

Surveyed by  
Designed by H.L. TILTON  
Drawn by J.E.F.  
Traced by J.E.F.  
Checked by  
Series No. 1A of 1 Sheets

**WOOD GUARD RAIL REQUIRED**

CULV. STA.	LT.	RT.
5+07	24'	24'

**CABLE GUARD RAIL REQUIRED**

STATIONS	LEFT	RIGHT
5+90 TO 6+70 BLINE 240'	-	-
6+00 BLINE TO 11+50 480'	-	-
10+76 TO 11+26	4-8	-

**WOOD GUIDE POSTS REQUIRED**

CULV. STA.	LT.	RT.
6+92	-	1
8+16	-	1
8+88	-	1

**NEW REINFORCED PIPE CULVERTS REQUIRED**

STATION	DIAM.	LENGTH	CONC.	REIN. STEEL
5+07	15"	36'	2.3 cy	33 lbs
6+08	15"	48'	1.2 cy	174 lbs
8+16	15"	52'	1.5	174 lbs
8+83	15"	36'	1.5	174 lbs
11+00	30"	56'	3.2	58 lbs

**NEW REINFORCED DRAINAGE PIPE CULVERTS REQUIRED**

STATION	DIAM.	LENGTH	CONC.	REIN. STEEL
12+75 R.	12"	24'	2.3 cy	-
9+00 "	12"	36'	1.2 cy	-

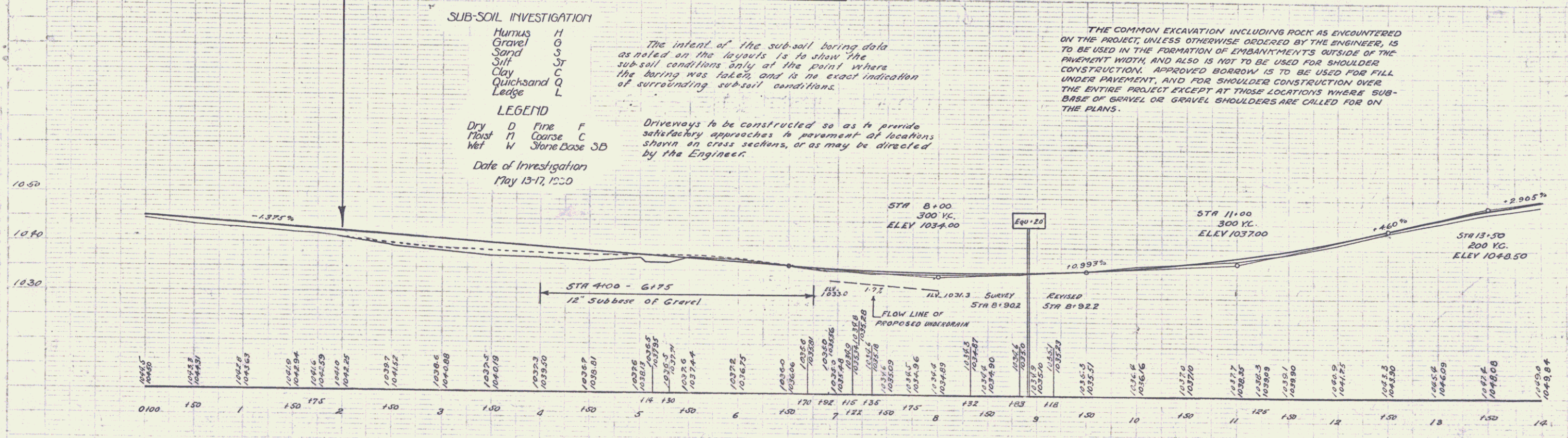
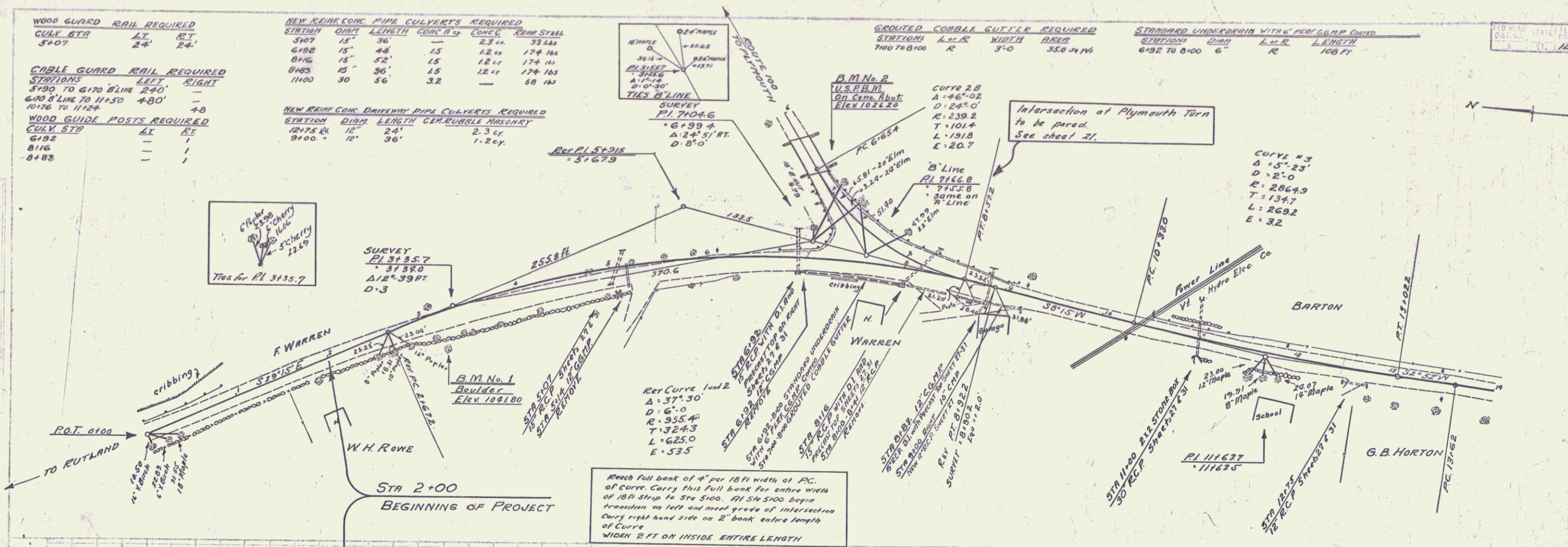
**GRADED COBBLE CUTTER REQUIRED**

STATIONS	L. R.	WIDTH	BASE
7+00 TO 8+00	R	3'-0"	35.0 in. W

**STANDARD UNDERDRYIN WITH 6" PER' G.M.P. COVERED**

STATIONS	DIAM.	L. R.	LENGTH
6+92 TO 8+00	6"	R	108 FT

100' SCALE  
 12/1/1930  
 57



**SUB-SOIL INVESTIGATION**

- Mudus H
- Gravel G
- Sand S
- Silt SJ
- Clay C
- Quicksand Q
- ledge L

**LEGEND**

- Dry D
- Moist M
- Wet W
- Fine F
- Coarse C
- Stone Dose SD

Date of Investigation  
 May 13-17, 1930

The intent of the sub-soil boring data as noted on the layouts is to show the sub-soil conditions only at the point where the boring was taken, and is no exact indication of surrounding sub-soil conditions.

Driveways to be constructed so as to provide satisfactory approaches to pavement at locations shown on cross sections, or as may be directed by the Engineer.

THE COMMON EXCAVATION INCLUDING ROCK AS ENCOUNTERED ON THE PROJECT, UNLESS OTHERWISE ORDERED BY THE ENGINEER, IS TO BE USED IN THE FORMATION OF EMBANKMENTS OUTSIDE OF THE PAVEMENT WIDTH AND ALSO IS NOT TO BE USED FOR SHOULDER CONSTRUCTION. APPROVED BOARDING IS TO BE USED FOR FILL UNDER PAVEMENT, AND FOR SHOULDER CONSTRUCTION OVER THE ENTIRE PROJECT EXCEPT AT THOSE LOCATIONS WHERE SUB-BASE OF GRAVEL OR GRAVEL SHOULDERS ARE CALLED FOR ON THE PLANS.

B.M. No. 1 - Point on boulder, 25' RT. of Sta. 3+18. Elev. 1041.80  
 B.M. No. 2 - U.S. B.M. on south-westerly wing at bridge. Elev. 1026.20

**WOOD GUARD RAIL REQUIRED**

CONV. STA	LEFT	RIGHT
16+25	24'	24'
20+90	24'	24'

**CABLE GUARD RAIL REQUIRED**

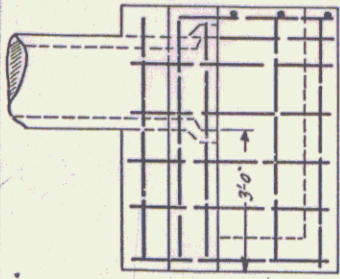
STATIONS	LEFT	RIGHT
22+22 TO 23+50	120'	—

**WOOD GUIDE POSTS REQUIRED**

CONV. STA	LEFT	RIGHT
23+36	—	1
20+90	—	1

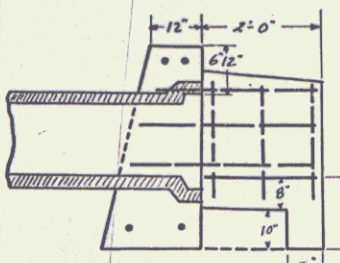
**NEW REIN. CONC. PIPE CULVERTS REQUIRED**

STATION	DIAM.	LENGTH	CONC. II	CONC. I	REIN. STEEL
16+12	15"	44'	13 cy	12 cy	92 lbs
23+36	15"	40'	13 cy	12 cy	174 "
20+90	15"	36'	13 cy	12 cy	174 "

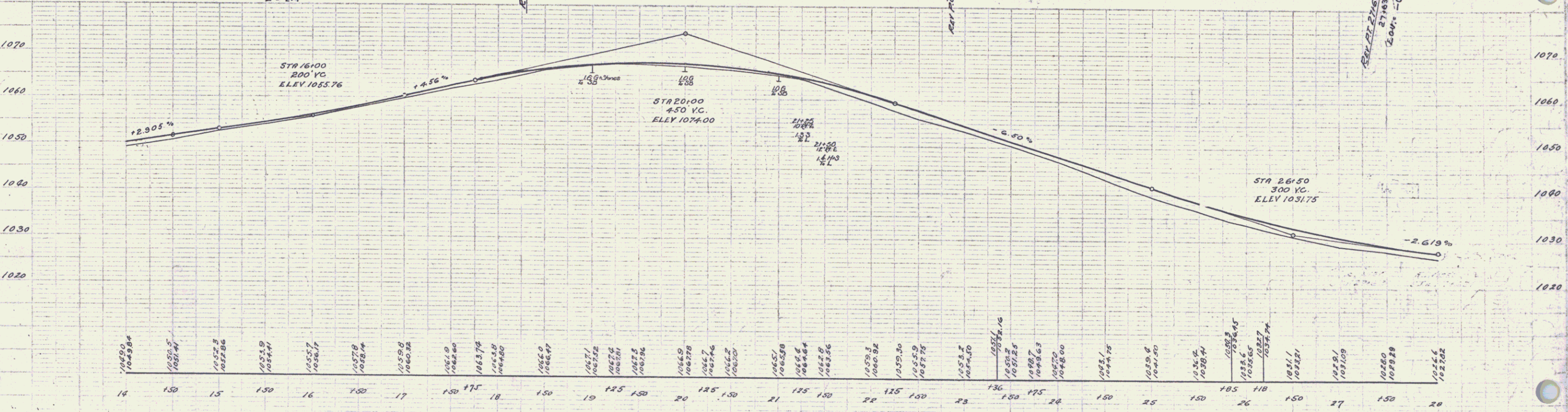
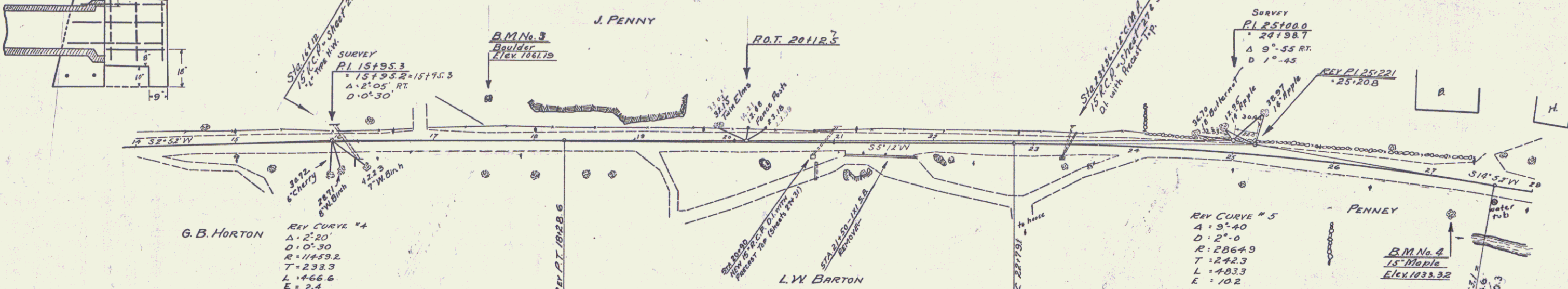


**DETAIL OF L-TYPE HEADWALL**  
Concrete Class B

All Rein. Steel to be  $\frac{3}{8}$ " dia. bars, 12%  
Quantities for 18" Pipe  
Conc. Class B, 1.9 cy  
Rein. Steel 75 lbs.



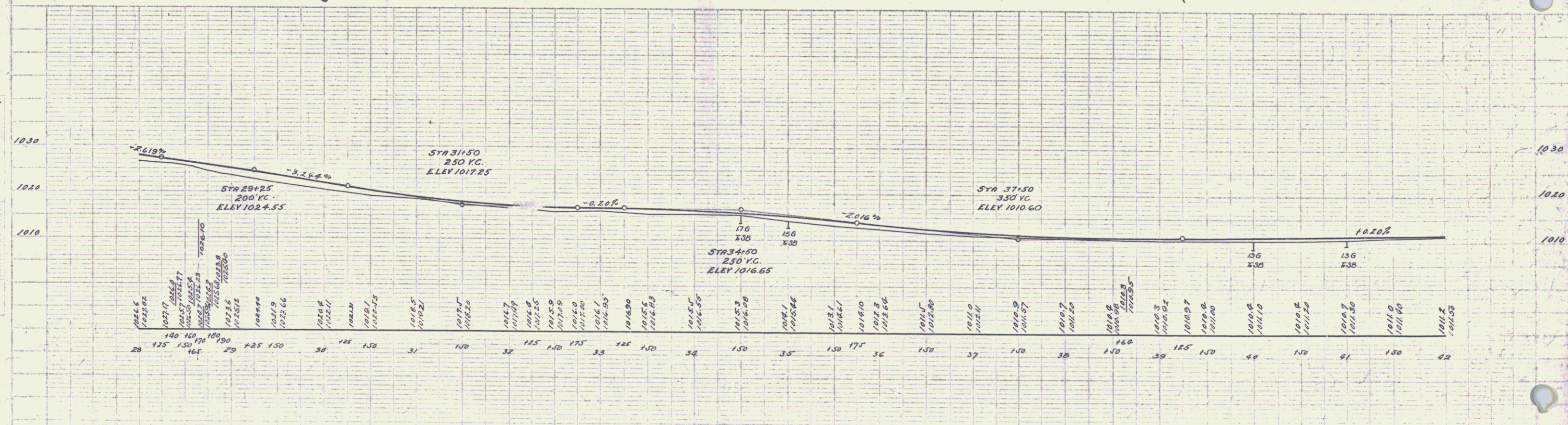
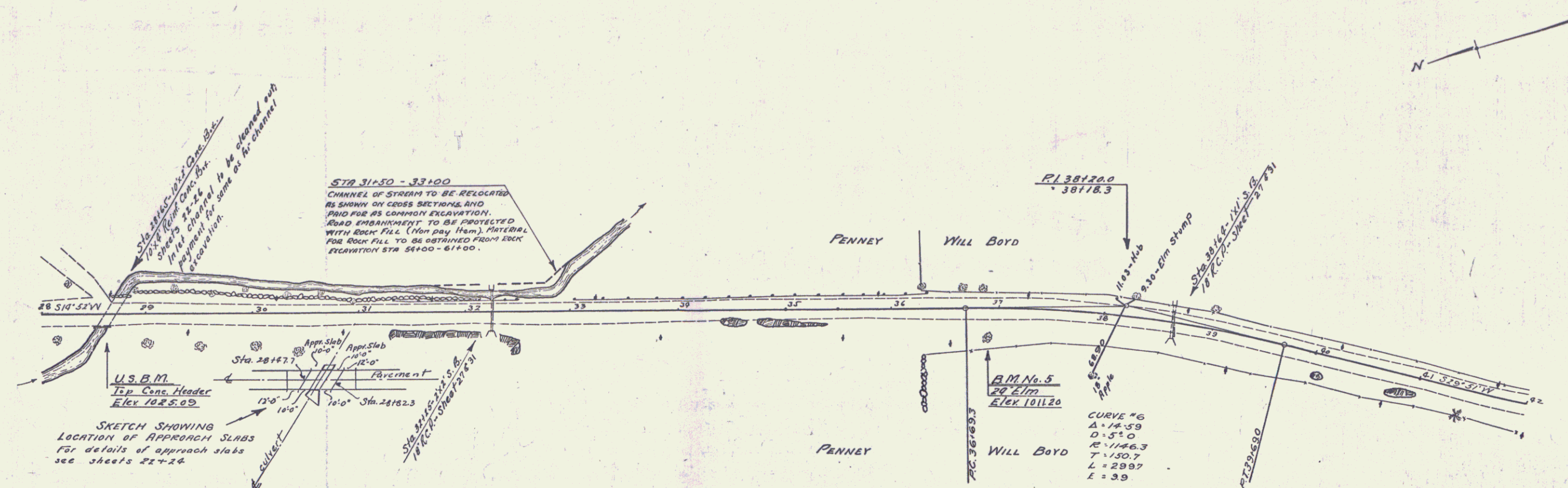
C.E. Plummer  
Apr 1930  
Apr 1930



B.M. No. 3 - Boulder, 40' left of Sta 17+54. Elev 1061.19  
B.M. No. 4 - Spike in root of 15" Maple, 32' right of Sta 27+20. Elev 1033.32

WOOD GUARD RAIL REQUIRED			CABLE GUARD RAIL REQUIRED			NEW REINFORCED CONCRETE PIPE CULVERTS REQUIRED					
CULY STA	LEFT	RIGHT	STATIONS	LEFT	RIGHT	STATION	DIAM	LENGTH	CODE	CODE C	RAINF. STEEL
32+25	—	24'	28+17 to 28+09	44'	48'	32+25	18"	35'	18 1/2"	—	5# 10s
38+64	24'	24'	31+00 to 32+04	184'	—	38+64	18"	32'	18 1/2"	—	5# 10s

FED. ROAD DIST. NO.	STATE	FED. AID DIST. NO.	SHEET NO.	TOTAL SHEETS
8	IA	1910	6	57



U.S.B.M. - On conc. header, 11' right of Sta. 29+25. Elev 1025.09  
 B.M. No. 5 - Spike in root of 24' Elm, 24' right of Sta. 36+90. Elev 1011.20

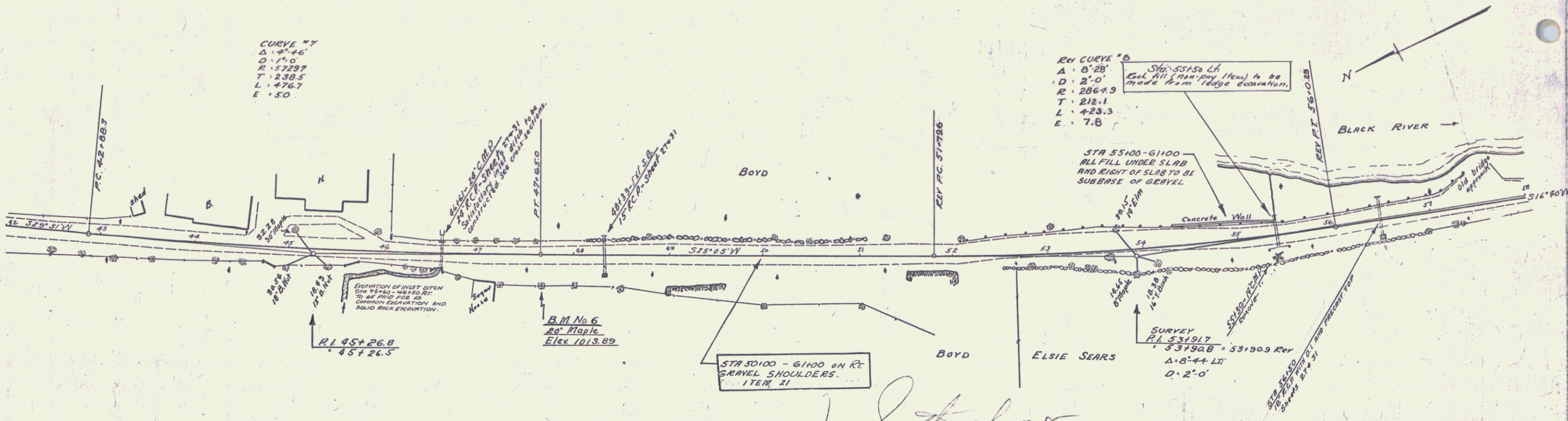
5

WOOD GUARD RAIL REQUIRED			CABLE GUARD RAIL REQUIRED			WOOD GUIDE POSTS REQUIRED			NEW REIN CONC PIPE CULVERTS REQUIRED				
CULV STA	LEFT	RIGHT	STATIONS	LEFT	RIGHT	CULV STA	LEFT	RIGHT	CULV STA	DIAM	LEN	CONC	REIN. STEEL
46161	24'	24'	48120 TO 57100	800'	—	40133	—	1	46161	24"	30'	2.4	54.161
			57460 TO 58100	20'	—	56150	—	1	48123	18"	40'	15.12	174.161
									56150	18"	40'	15.12	174.161

FED. ROAD DIST. NO. 121A 1930 7 57

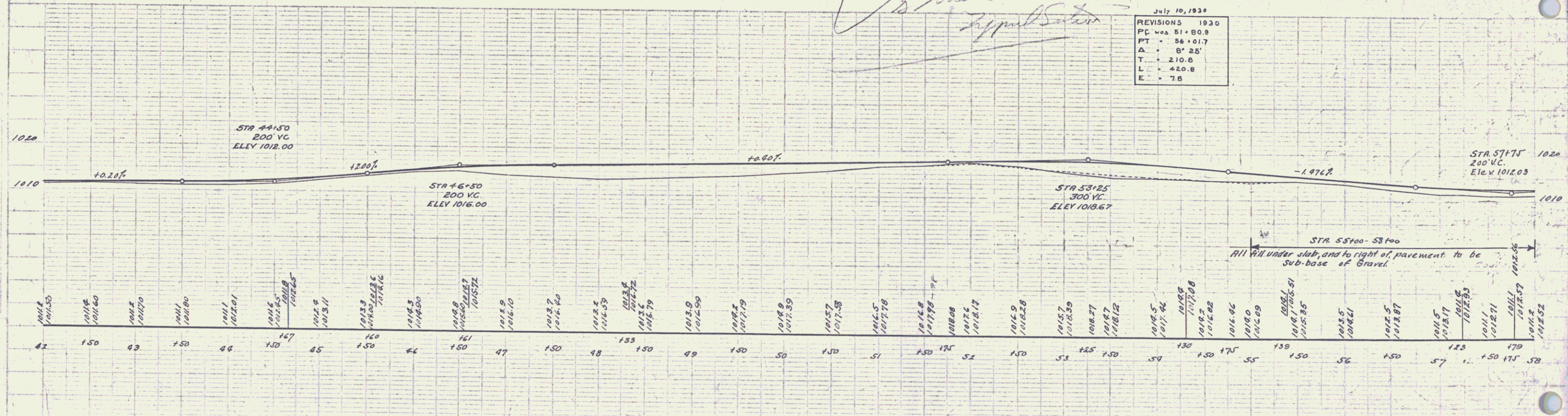
CURVE #7  
 Δ = 4° 46'  
 D = 150'  
 R = 5729.7'  
 T = 238.5'  
 L = 476.7'  
 E = 50'

REV CURVE #8  
 Δ = 6° 28'  
 D = 212.1'  
 R = 2864.9'  
 T = 212.1'  
 L = 423.3'  
 E = 7.8'



*J.S. the show or support station*

July 10, 1930  
 REVISIONS 1930  
 PC = 51+80.8  
 PT = 56+01.7  
 Δ = 8° 25'  
 T = 210.6'  
 L = 450.8'  
 E = 7.8'



B.M. No 6 - Spike in 20' Maple, 30' right of Sta. 47163. Elev 1013.80

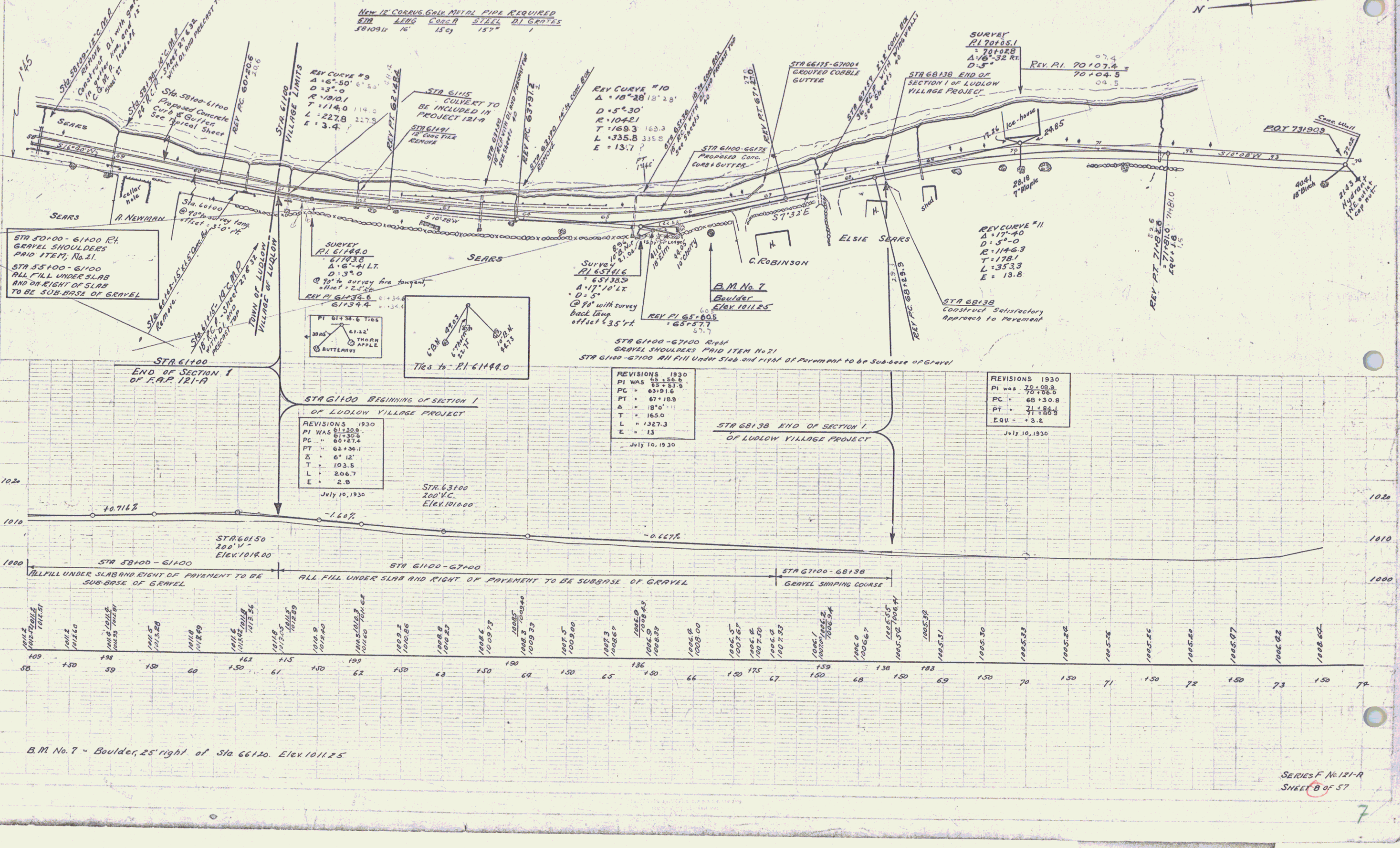
SERIES F No. 121-A SHEET 7 OF 57

S.E. Blinn  
 Surveyor

Apr 17, 30  
 S.E. Blinn  
 Surveyor

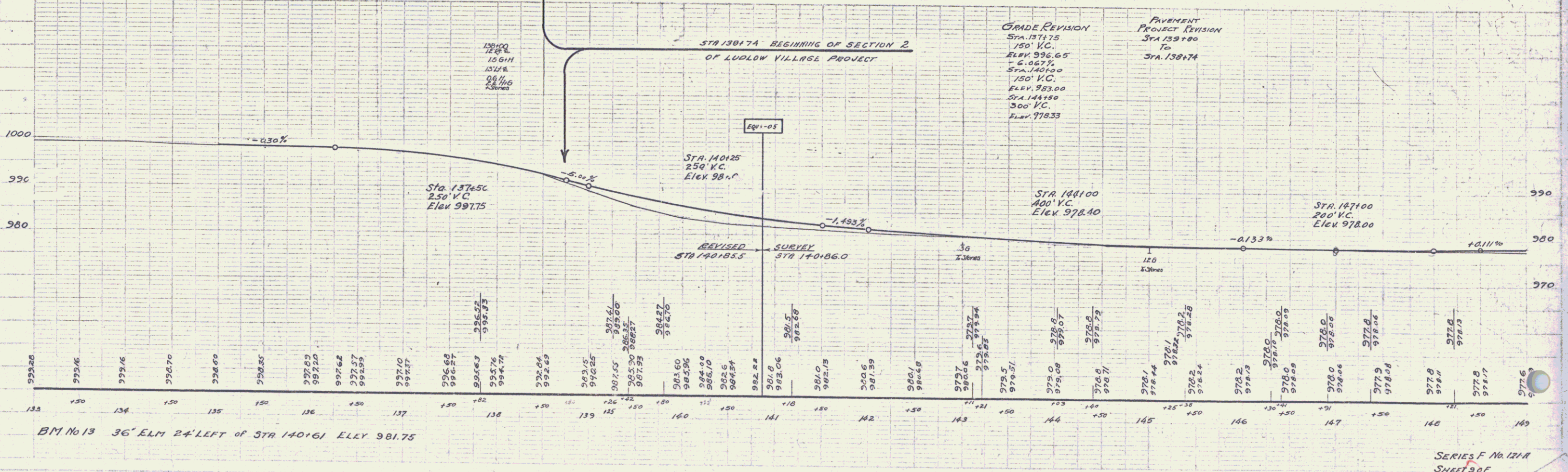
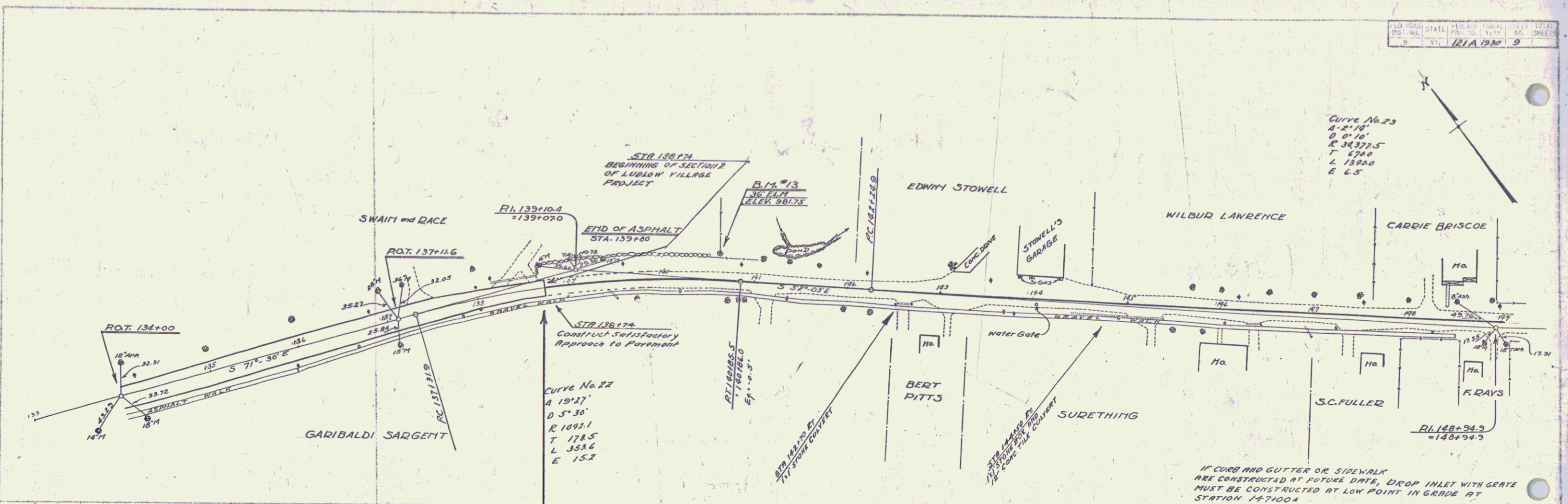
6

CABLE GUARD RAIL REQUIRED		WOOD GUIDE POSTS REQUIRED		REV. CORRUGATED PIPE CULVERTS REQUIRED					NEW CONC CURB AND GUTTER REQUIRED			GROUTED CORNELL GUTTER RUNOFF						
STATIONS	LEFT	RIGHT	STATION	LEFT	RIGHT	COLL. STA.	DIAM.	LENG.	CONC. II	CONC.	REIN. STEEL	STATIONS	L	E	LENG.	CONC. Q	REIN. STEEL	
58100-61100	324	-	58198	-	-	58198	24"	36'	1.8 cy	-	157 lb	58100-61100	L	300	19.1cy	550"	-	
61100-66100	360	-	61115	-	-	61115	18"	32'	1.8 cy	-	157 lb	61000-66175	L	575'	36.6cy	1033"	-	
			63130	-	-	63130	18"	40'	1.8 cy	-	157 lb							
			65136	-	-	65136	18"	44'	1.8 cy	-	157 lb							
			67157	24"	24"	67157	36"	36'	4.0 cy	-	58 lb							



STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION																						
58	109	59	109	60	109	61	109	62	109	63	109	64	109	65	109	66	109	67	109	68	109	69	109	70	109	71	109	72	109	73	109	74	109	75	109	76	109	77	109	78	109	79	109

B.M. No. 7 - Boulder, 25' right of Sta 66120. Elev 1011.25



C. E. PUTNAM  
 N. B. HAMBALL

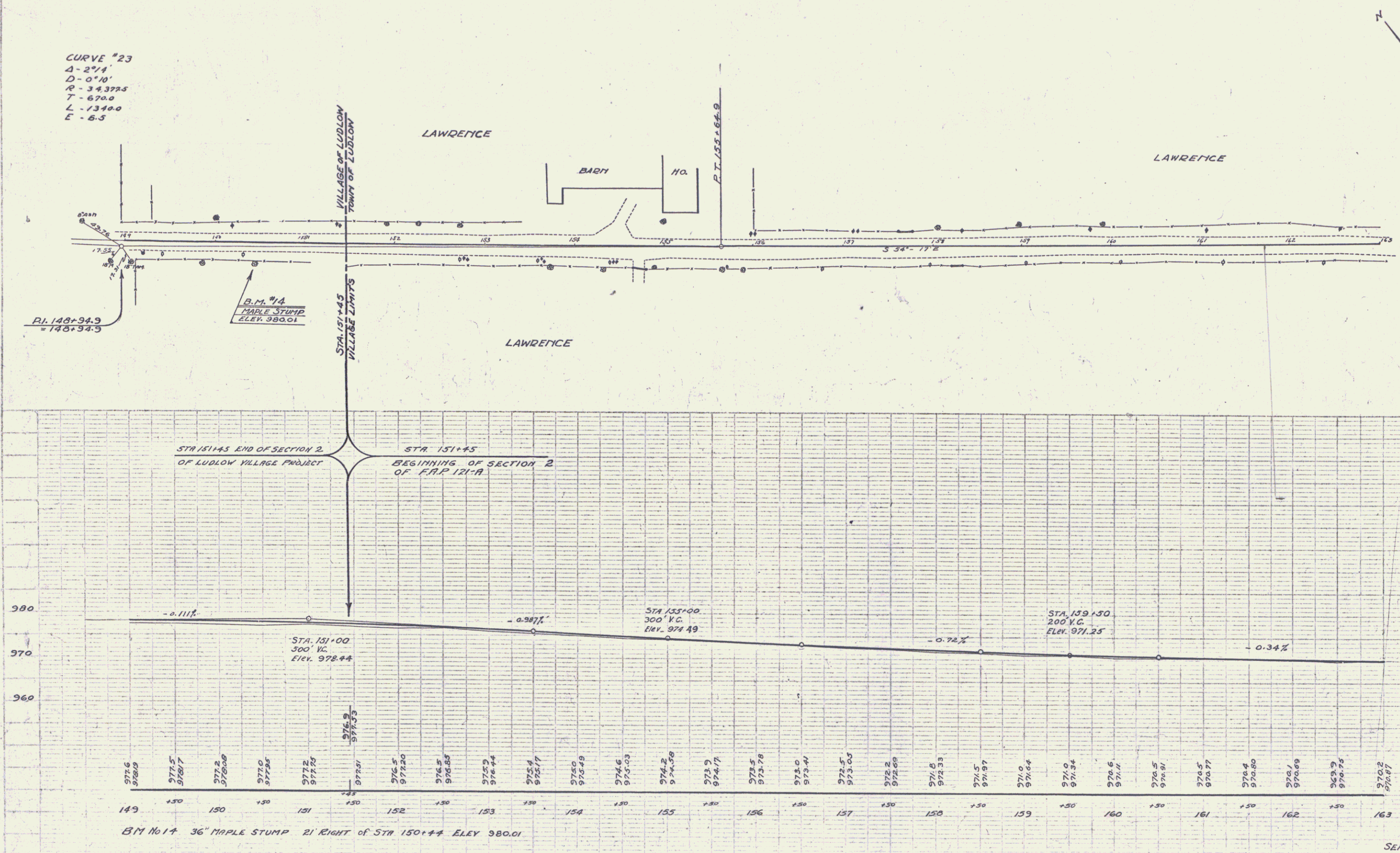
C. E. PUTNAM  
 A. J. GIBBY

BM No 13 36' ELM 24' LEFT of STA 140+61 ELEV 981.75

CURVE #23  
 Δ - 2°14'  
 D - 0°10'  
 R - 34.3725  
 T - 670.0  
 L - 1340.0  
 E - 6.5

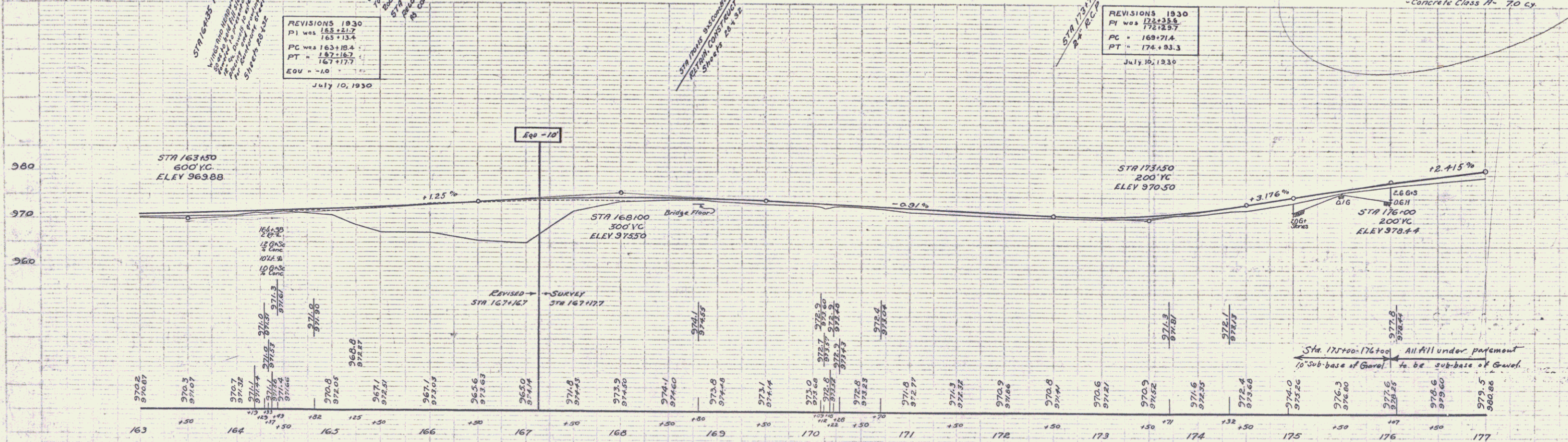
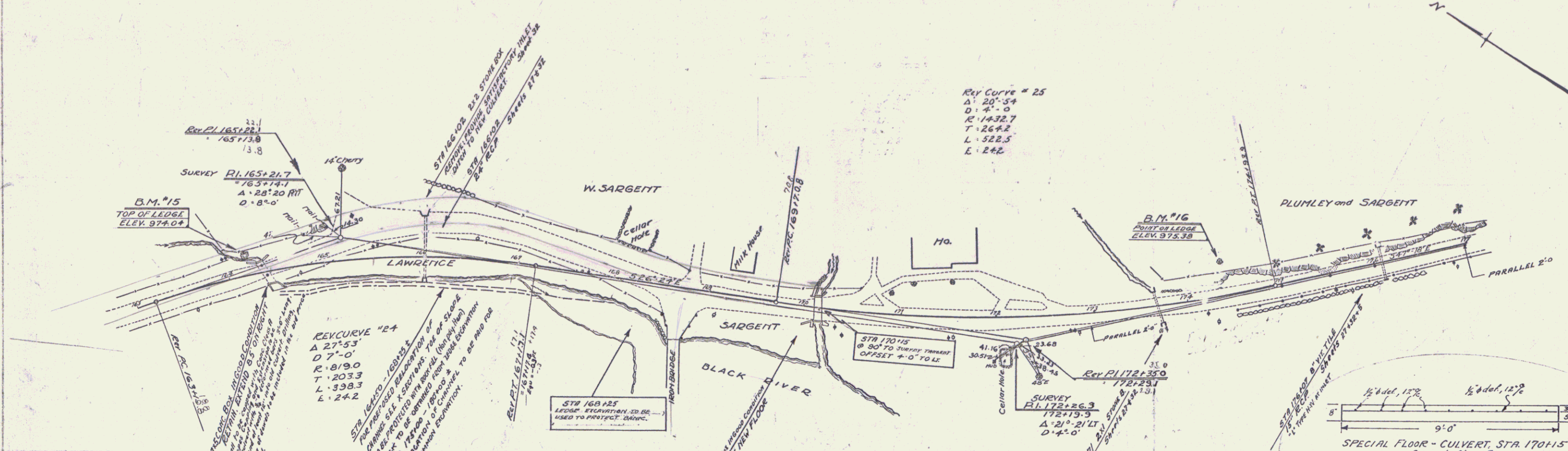
S.E. Putnam  
 Surveyor  
 No. 121A  
 1930

S.E. Putnam  
 Surveyor  
 No. 121A  
 1930



WOOD GUARD RAIL REQUIRED				CAST IRON GUARD RAIL REQUIRED				NEW REINF CONC PIPE COLVERTS REQUIRED				COLVERT REPAIRS REQUIRED			
CULVERT	LEFT	RIGHT		STA	LEFT	RIGHT		COLVERT	DIM	LENGTH	CONC	REINF STEEL	COLVERT	CONC	REINF STEEL
166102	2'			164+23	168+14	2'		166102	24"	6'	24"	54 lbs	164+35	12.5'	513 lbs
173+71	2'	2'		170+23	170+23		+40'	173+71	24"	40'	24"	54 lbs	170+15	9.0'	500 lbs
176+07	2'			173+25	177+00		175'	176+07	15"	36'	1.94	12"	92 lbs		

STATE ROAD DISTRICT NO. 1  
 DIVISION OF HIGHWAYS  
 PROJECT NO. 12/A 1930 11 57



BM #15 TOP OF LEDGE AT BRIDGE 55' LEFT OF STA 164+17 ELEV 974.08  
 BM #16 POINT ON LEDGE 36' LEFT OF STA 174+40 ELEV 975.38

SERIES F No. 121-A  
 SHEET 11 OF 57

WOOD GUARD RAIL REQUIRED	
STATION	RIGHT
183+00	2'

CABLE GUARD RAIL REQUIRED	
STATION	RIGHT
177+00 - 178+53	153
179+50 - 183+26	376

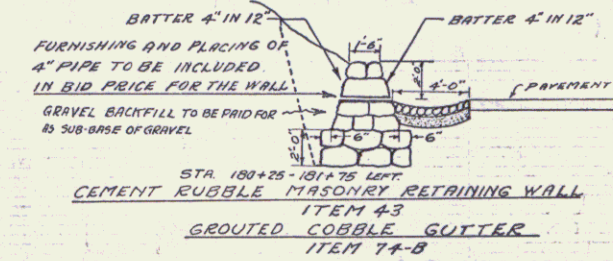
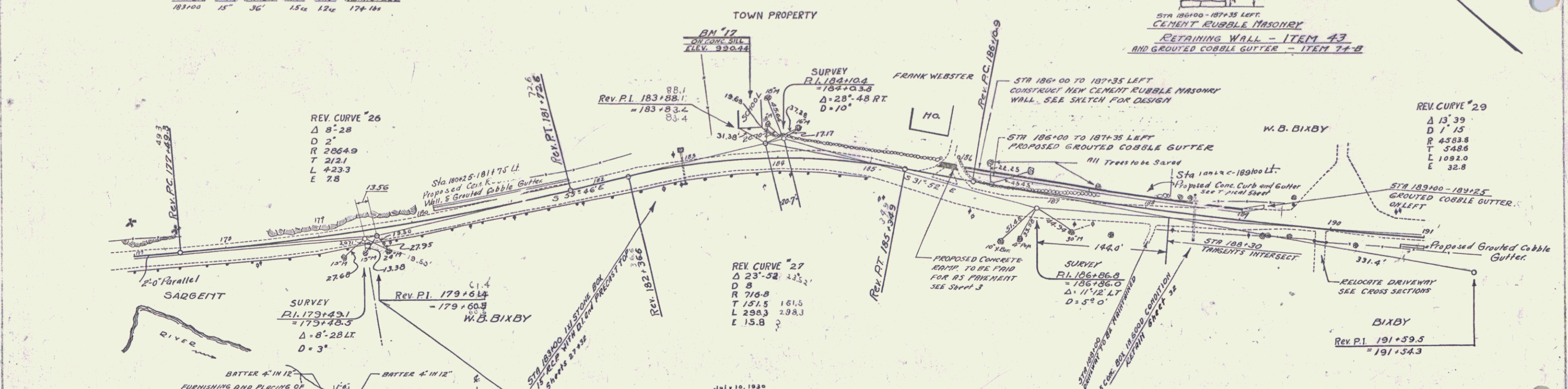
GROUTED COBBLE GUTTER REQUIRED	
STATION	RIGHT
180+00 - 180+00	200'
188+31 - 188+79	48'

CEMENT RUBBLE MASONRY RETAINING WALL	
STATION	RIGHT
186+00 - 187+35	125'
187+42 - 187+72	150'

WOOD GUIDE POSTS REQUIRED	
STATION	RIGHT
183+00	1

NEW REIN. CONC. PIPE CULVERTS REQUIRED	
STATION	RIGHT
183+00	15" 36' 15x12x12

REVISED STATE ROAD AND TOLLAGE DISTRICT NO. YEAR NO. SHEETS  
 121 A 1930 12 57

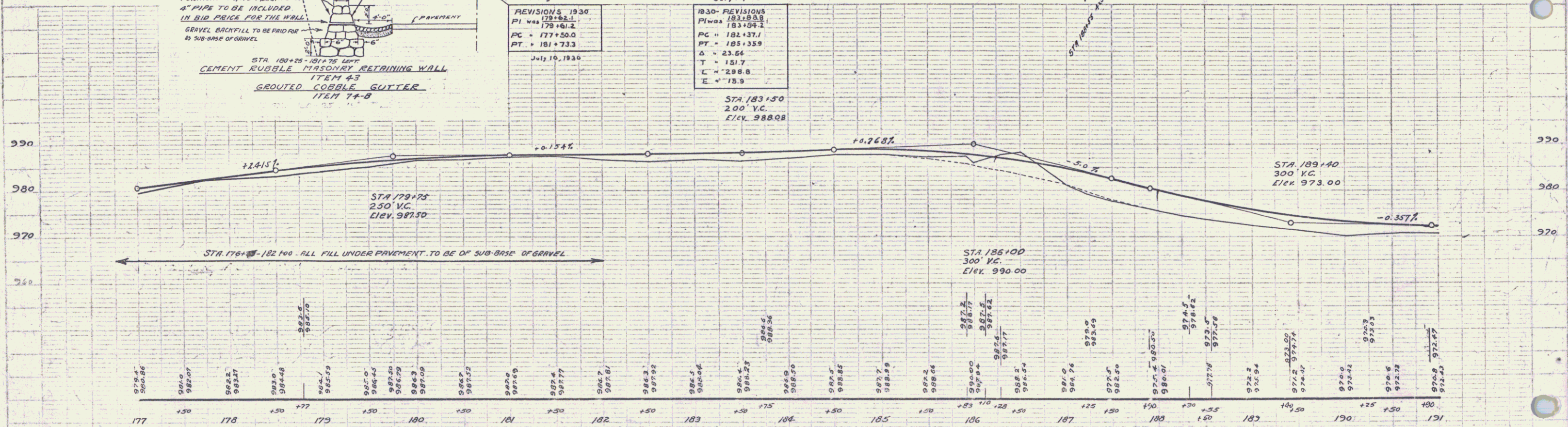


July 10, 1930

1830- REVISIONS  
 PI was 179+60.9  
 PC = 177+50.0  
 PT = 181+73.3

July 10, 1930

1830- REVISIONS  
 PI was 183+83.8  
 PC = 181+37.1  
 PT = 185+35.9  
 Δ = 23.64  
 T = 151.7  
 L = 298.8  
 E = 15.9



B.M. No 17 SPOT IN CONC. WINDOW SILL AT SCHOOL 31.5' LEFT STA 183+73 ELEV 990.44

SERIES F No. 121-A  
 SHEET 12 OF 57



WOOD GUIDE POSTS REQUIRED

QUAL. STA.	LEFT	RIGHT
210+15	1	1

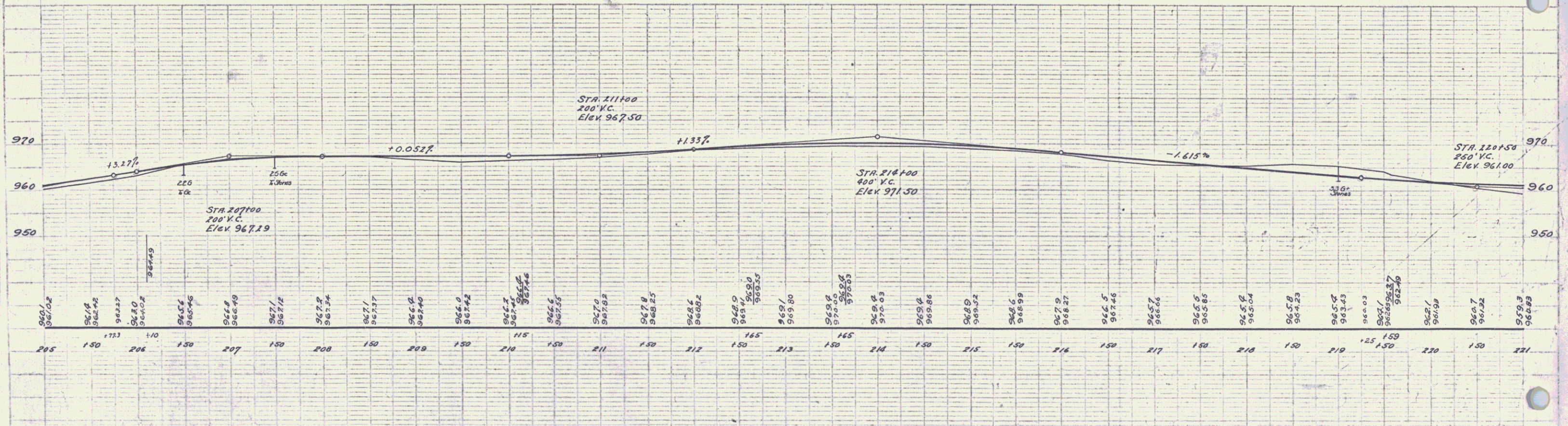
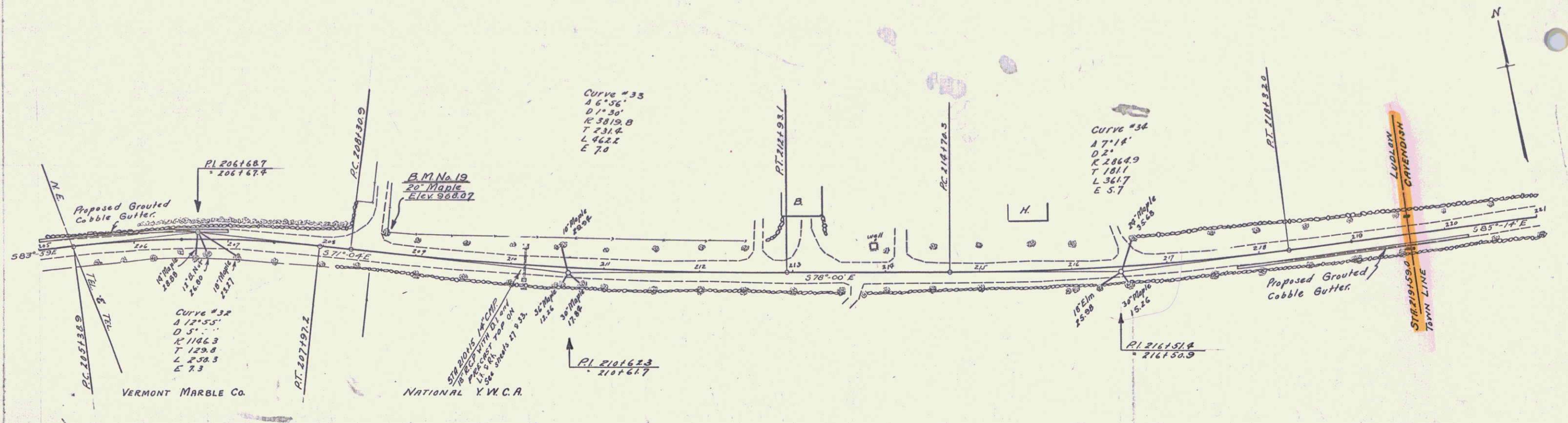
KEINING COM. PIPE CULVERTS REQUIRED

CULV. STA.	DIAM.	LENGTH	CONCR. CONC.	KEINING STALL
210+15	10"	44'	3.0%	15
				231

GRADED COBBLE GUTTER REQUIRED

STATIONS	LOC.	LENGTH	WIDTH	GRADE
205+00-207+00	L	200'	3'-0"	6.7% H
217+25-220+25	R	250'	3'-0"	6.4% H

STATE	1930	YEAR	74	SHEET	107A
NO.	121A	1930	74	37	

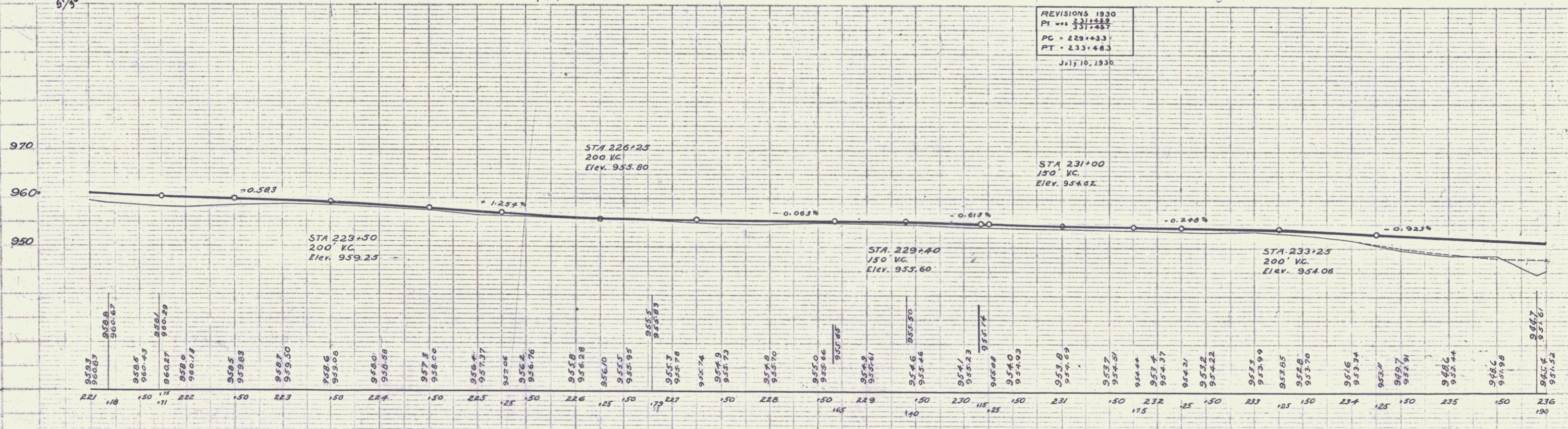
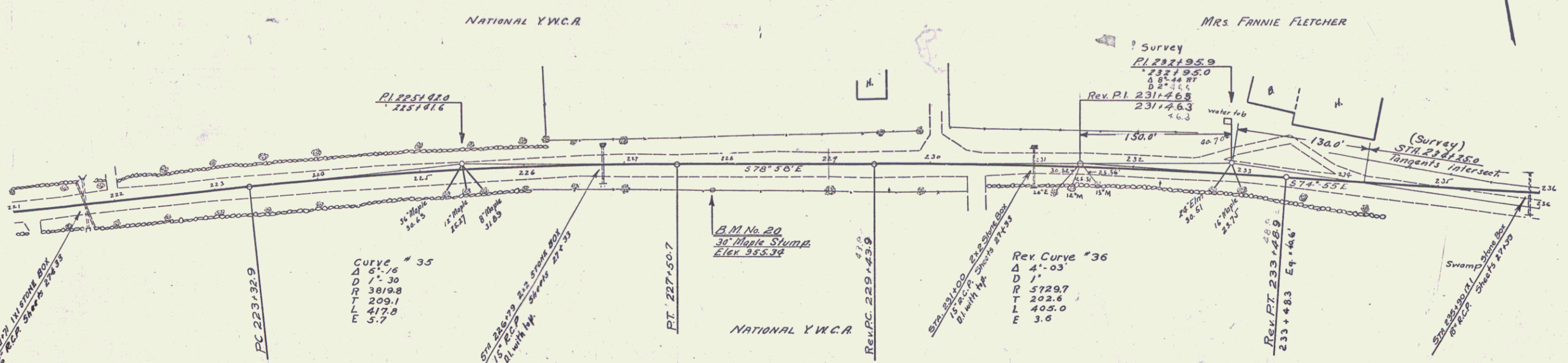


B.M. No. 19 SPINE IN ROOT OF 20' MAPLE AT SIDE ROAD 19' LEFT OF STA 200+62. ELEV. 968.07

SERIES F No. 121-A  
SHEET 17 OF 57

WOOD GUARD RAIL REQUIRED			CABLE GUARD RAIL REQUIRED			NEW REINFORCED CONCRETE PIPE CULVERTS REQUIRED				
CULV. STA.	LEFT	RIGHT	STALLIONS	LEFT	RIGHT	CULV. STA.	DIAM.	LENGTH	CONC.	REINFC. STEEL
226+79	—	24'	—	—	—	221+73	36"	48'	4.0%	33 lbs
231+00	—	24'	—	—	—	226+79	15"	38'	1.5%	174 lbs
235+90	24'	24'	4-0'	4-0'	—	231+00	15"	40'	1.5%	174 lbs
						235+90	18"	40'	3.0%	33 lbs

WOOD GUIDE POSTS REQUIRED		
CULV. STA.	LEFT	RIGHT
226+79	—	—
231+00	1	—



REVISIONS 1930  
 PI = 231+25.9  
 PC = 229+43.3  
 PT = 233+48.3  
 July 10, 1930

B.M. No. 20 SPIKE IN 30' MAPLE STUMP 23' RIGHT OF STA 227+85 ELEV 955.34

**WOOD GUARD RAIL REQUIRED**

CULV. STA.	LEFT	RIGHT
237+34	24'	24'
245+87	24'	24'
247+55	24'	24'

**CABLE GUARD RAIL REQUIRED**

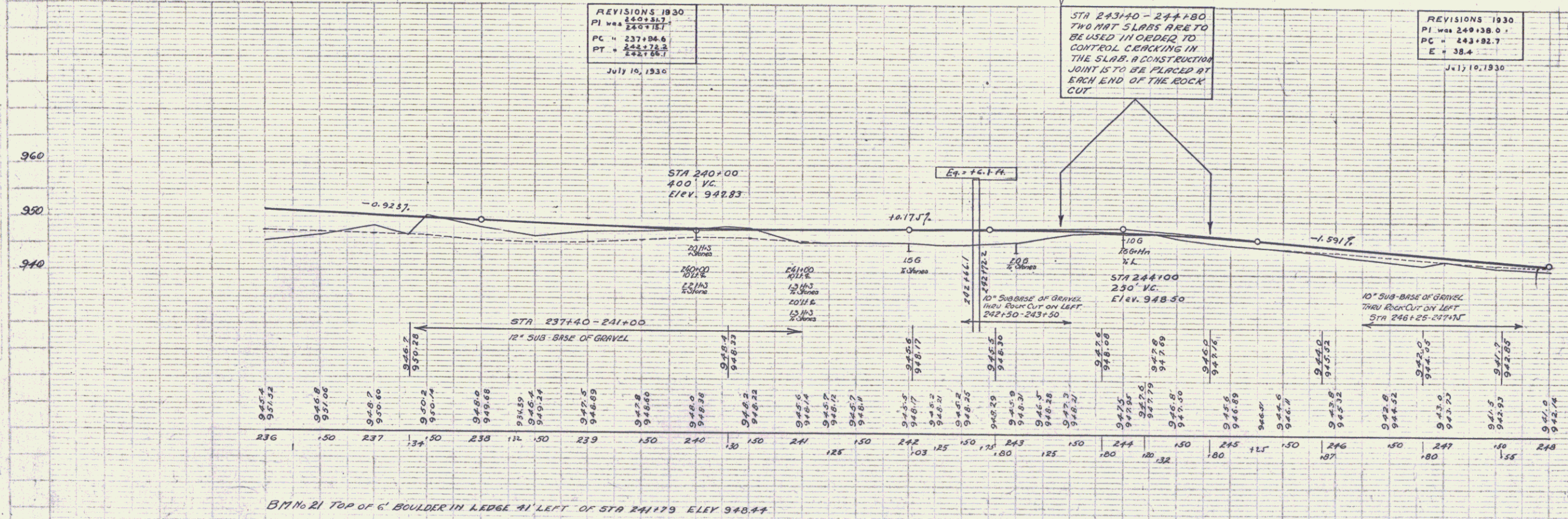
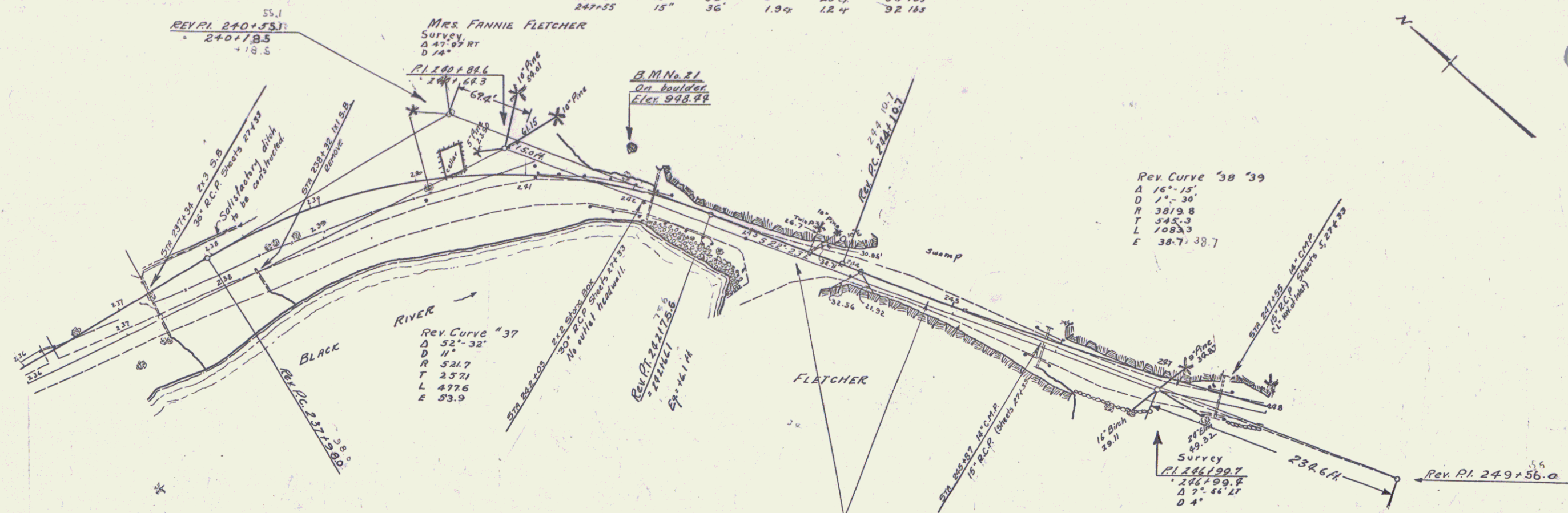
STATIONS	LEFT	RIGHT
24118-24218	160'	136'

**NEW REINFORCED CONCRETE PIPE CULVERTS REQUIRED**

CULV. STA.	DIAM.	LENGTH	CONC. C	REIN. STEEL
237+34	36"	40'	4.0cy	58 lbs
242+03	30"	52'	1.8cy	29 lbs
245+87	15"	36'	2.3cy	33 lbs
247+55	15"	36'	1.9cy	32 lbs

STATE: ILL. FISCAL YEAR: 1930 SHEET NO.: 16 OF 57

S. E. P. ...



B.M. No. 21 TOP OF 6' BOULDER IN LEDGE 41' LEFT OF STA 241+79 ELEV. 948.44

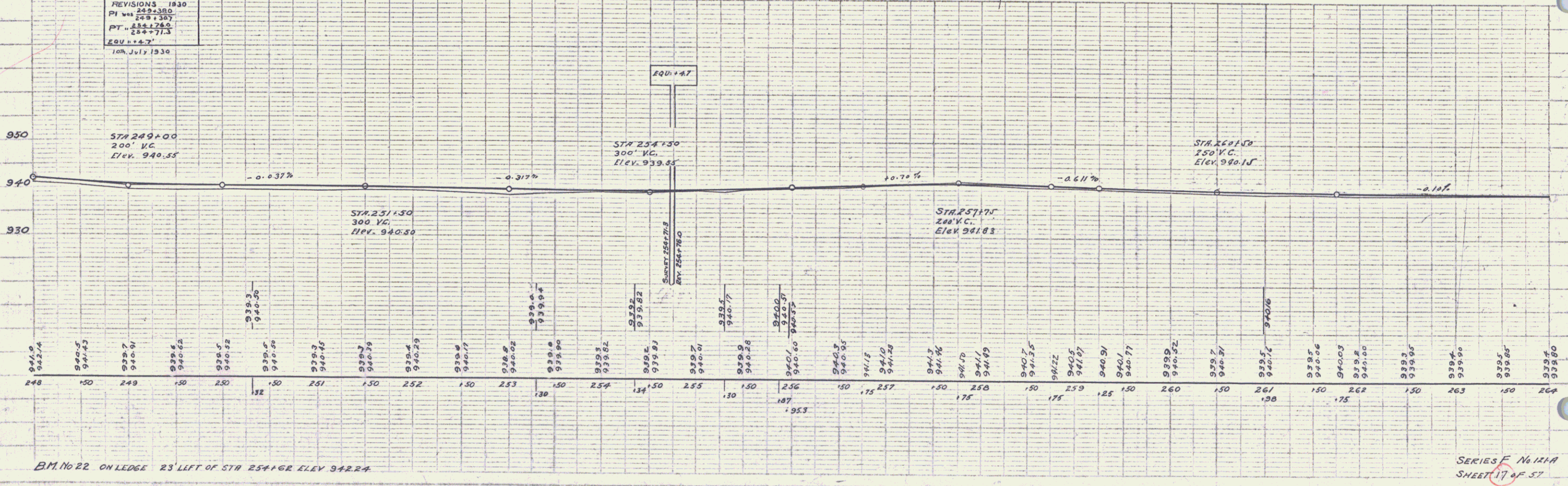
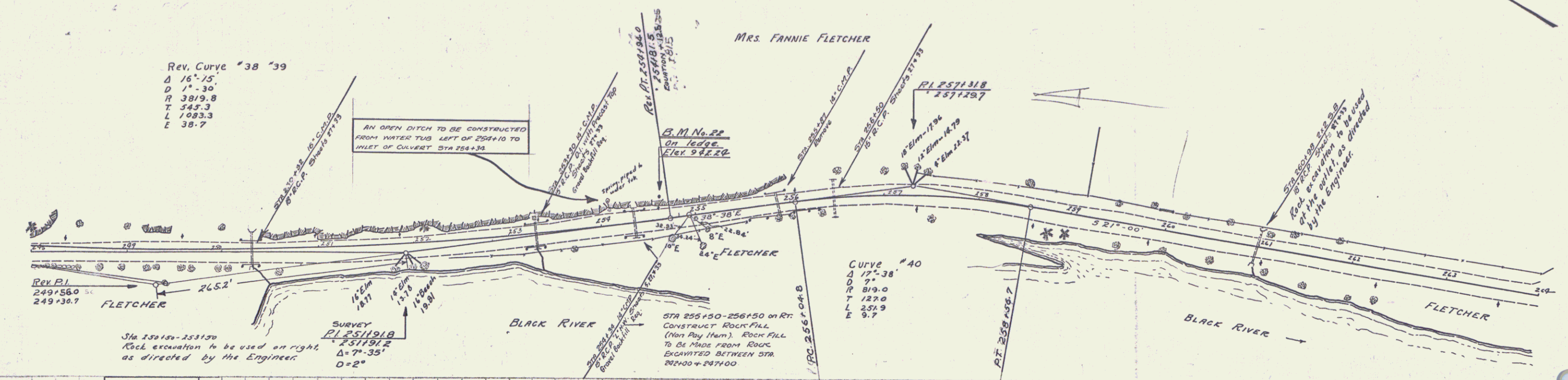
SERIES F No. 121A  
 SHEET 16 OF 57

WOOD GUARD RAIL REQUIRED		
CULV. STA.	LEFT	RIGHT
250+32	24'	24'
253+30	24'	24'
254+34	24'	24'
256+50	24'	—
260+38	24'	24'

CABLE GUARD RAIL REQUIRED		
STATIONS	LEFT	RIGHT
255+24-257+00	—	176'

NEW REIN. CONC. PIPE CULVERTS REQUIRED					
CULV. STA.	DIA.	LENGTH	CONC.	REIN. STEEL	
250+32	18"	36'	1.0 cu	—	54 lbs
253+30	15"	32'	1.2 cu	1.2 cu	174 lbs
254+34	15"	32'	1.9 cu	1.2 cu	92 lbs
256+50	15"	40'	1.0 cu	1.2 cu	44 lbs
260+38	18"	36'	1.8	—	54 lbs

DES. NO. 127 A 1930 17 57

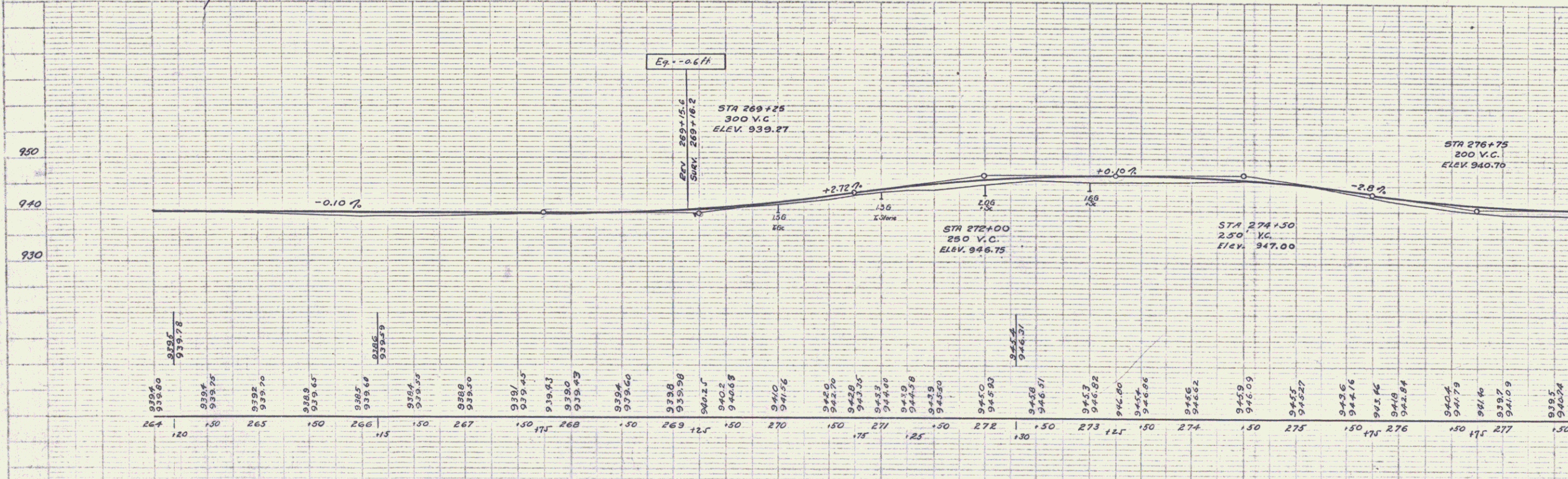
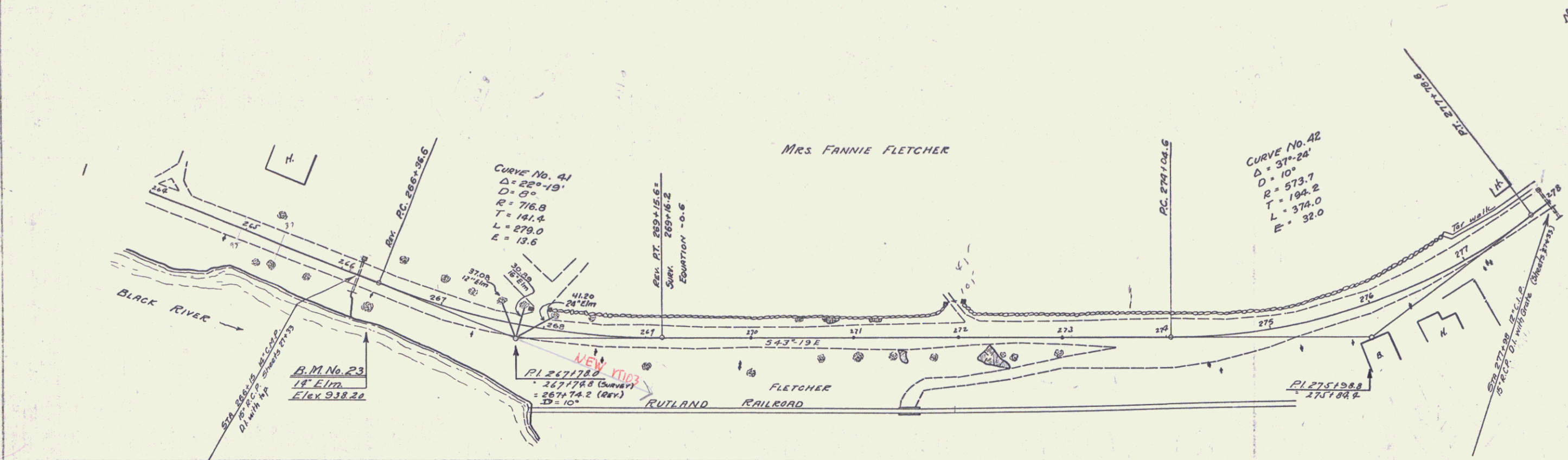


BM. No. 22 ON LEDGE 23' LEFT OF STA 254+6R ELEV 942.24

SERIES F No. 127A SHEET 17 OF 57

WOOD GUARD RAIL REQUIRED			CABLE GUARD RAIL REQUIRED			WOOD GUIDE POSTS REQUIRED			NEW REIN CONC PIPE CULVERTS REQUIRED						
CULV. STA	LEFT	RIGHT	STATIONS	LEFT	RIGHT	CULV. STA	LEFT	RIGHT	CULV. STA	DIAM	LENGTH	CONCR	COSE C	REIN STEEL	GRATE
266+15	-	24'	269+26-269+74	-	48'	278+00	1	-	278+00	15"	32'	1.4%	1.2%	116.165	1
278+00	-	24'				266+15	1	-	278+00	15"	32'	1.4%	1.2%	116.165	1

DATE	11/10/20	SCALE	1"=40'
PROJECT	NO. 121-R	YEAR	1930
SHEET	18	TOTAL SHEETS	57



B.M. No. 23 SPIKE IN ROOT OF 14" ELY NERE R.R. BRIDGE 20' RIGHT OF STA 266+34 ELEV 938.20

**NEW CONCRETE GRAVITY CURB REQUIRED**

STATIONS	LEN.	WIDTH	CONC. CLASS
279+40-279+90	50'	4'-3"	A

**NEW CONC CURB AND GUTTER REQUIRED**

STATIONS	WIDTH	CURB HEIGHT	CONC. CLASS	REINF. STEEL
279+90-290+50	2'-0"	0'-6"	A	67+cy 800#

**NEW DROP INLETS REQUIRED**

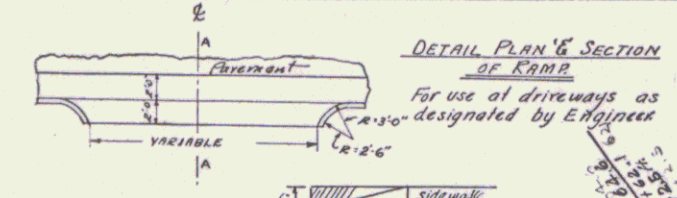
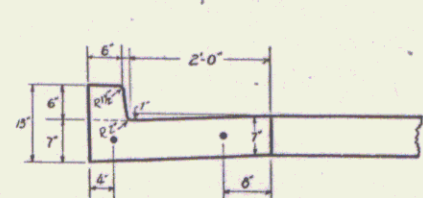
STATION	L or R	CONC. CLASS	REINF. STEEL	DIAMETER
287+50	L	2.8	19.8	14"
287+50	R	2.8	19.8	14"
290+75	R	2.4	9.9	14"

**REINF. CONC. PIPE REQUIRED**

Station	Diamp	Length
287+50	15"	15'

**VITRIFIED CLAY PIPE REQUIRED**

Stations	Diamp	Length
290+75-291+00	8"	126'



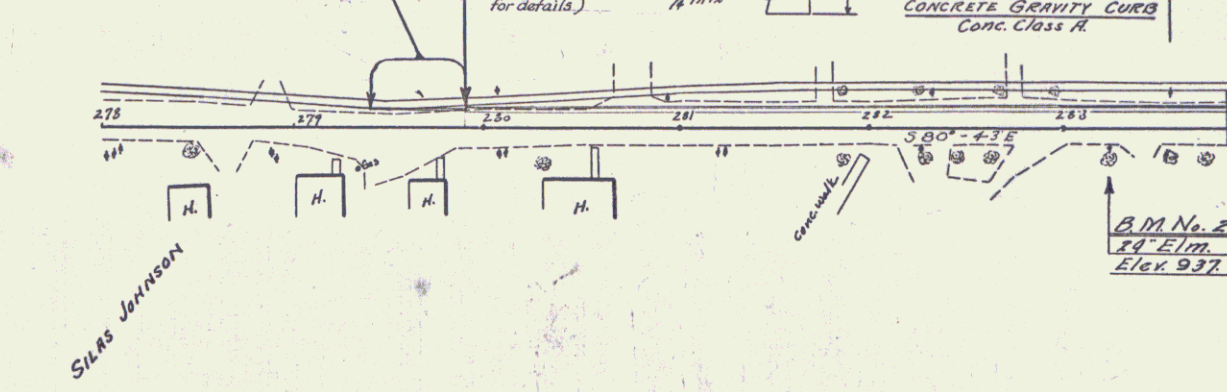
JOINTS IN CURB AND GUTTER NOT TO EXCEED 6"00" UNLESS PREPROVIDED BITUMINOUS EXPANSION JOINT FILLED, 1/2" THICK, TO BE PLACED AFTER EVERY 5TH SECTION. REMAINING JOINTS TO BE EITHER BUTT JOINTS PAINTED WITH BITUMINOUS PAINT OR CUT PLATE JOINTS.

**CONCRETE CURB & GUTTER**  
Sta 279+90-290+50 LH  
Conc. Class A  
Section Area 170 sq. ft.

Joints in Gravity Curb not to exceed 6'0" apart. Preprovided bituminous expansion joint filler, 1/2" thick, to be placed after every third section. Remaining joints to be either butt joints painted with bituminous material or cut plate joints.

**STA 279+40-279+90 LEFT**  
Proposed Gravity Curb.  
(See Sketch for Details)  
CONSTRUCT 10 FT. PAVEMENT ON LEFT BETWEEN THESE STATIONS.

**STA 279+90 LEFT**  
BEGINNING OF PROPOSED CONCRETE CURB AND GUTTER.  
(See Sketch for details)



REV. CURVE No. 43  
D = 35°-33'  
R = 955.4  
T = 306.3  
L = 456  
E = 456  
CURVE NOT BANNED OR WIDENED

DETAIL PLAN & SECTION OF RAMP  
For use of driveways as designated by Engineer

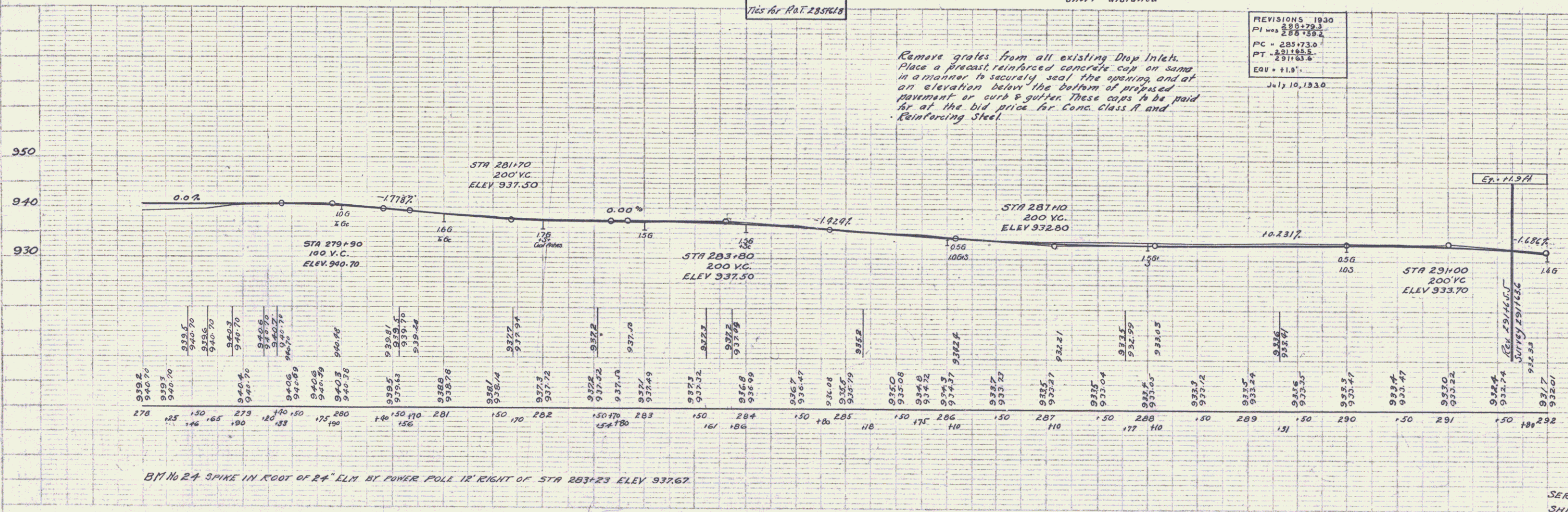
STA 290+50 LEFT  
END OF PROPOSED CONCRETE CURB AND GUTTER.  
(See Sketch for details)

Inaccessible  
PI 288163.3  
288+44.2  
Δ = 35°-02'  
D = 6'

Sta 287+50  
Min. grade of pipe left last three feet to be constructed at a steeper grade to avoid the water setting back in culvert for more than a short distance.

Remove grates from all existing Drop Inlets. Place a precast reinforced concrete cap on same in a manner to securely seal the opening and at an elevation below the bottom of proposed pavement or curb & gutter. These caps to be paid for at the bid price for Conc. Class A and Reinforcing Steel.

REVISIONS 1930  
PI was 288+29.3  
PC = 285+73.0  
PT = 291+65.5  
EOL = 11.8'  
July 10, 1930



BM No 24 SPIKE IN FOOT OF 24" ELM BY POWER POLE 12' RIGHT OF STA 283+23 ELEV 937.67

SERIES F No. 121A  
SHEET 10 OF 57

**DEAD INLETS WITH GATE REQUIRED**

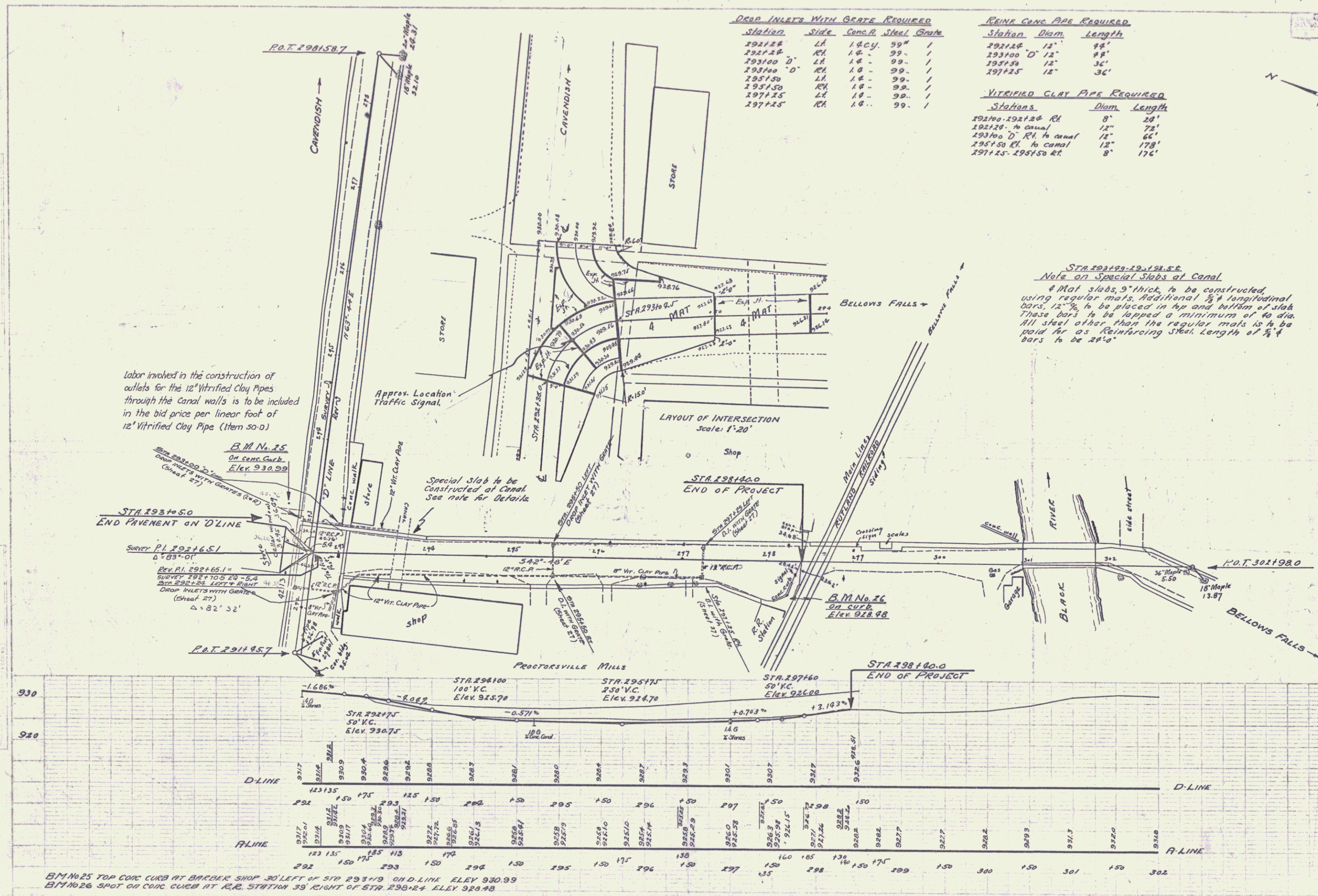
Station	Side	Concr.	Steel	Gate
292124	LH	1.00	99"	1
292124	RH	1.00	99"	1
293100	D	1.00	99"	1
293100	D	1.00	99"	1
295150	LH	1.00	99"	1
295150	RH	1.00	99"	1
297125	LH	1.00	99"	1
297125	RH	1.00	99"	1

**REINFC CONC PIPE REQUIRED**

Station	Diam.	Length
292124	12"	48'
293100	12"	48'
295150	12"	36'
297125	12"	36'

**VITRIFIED CLAY PIPE REQUIRED**

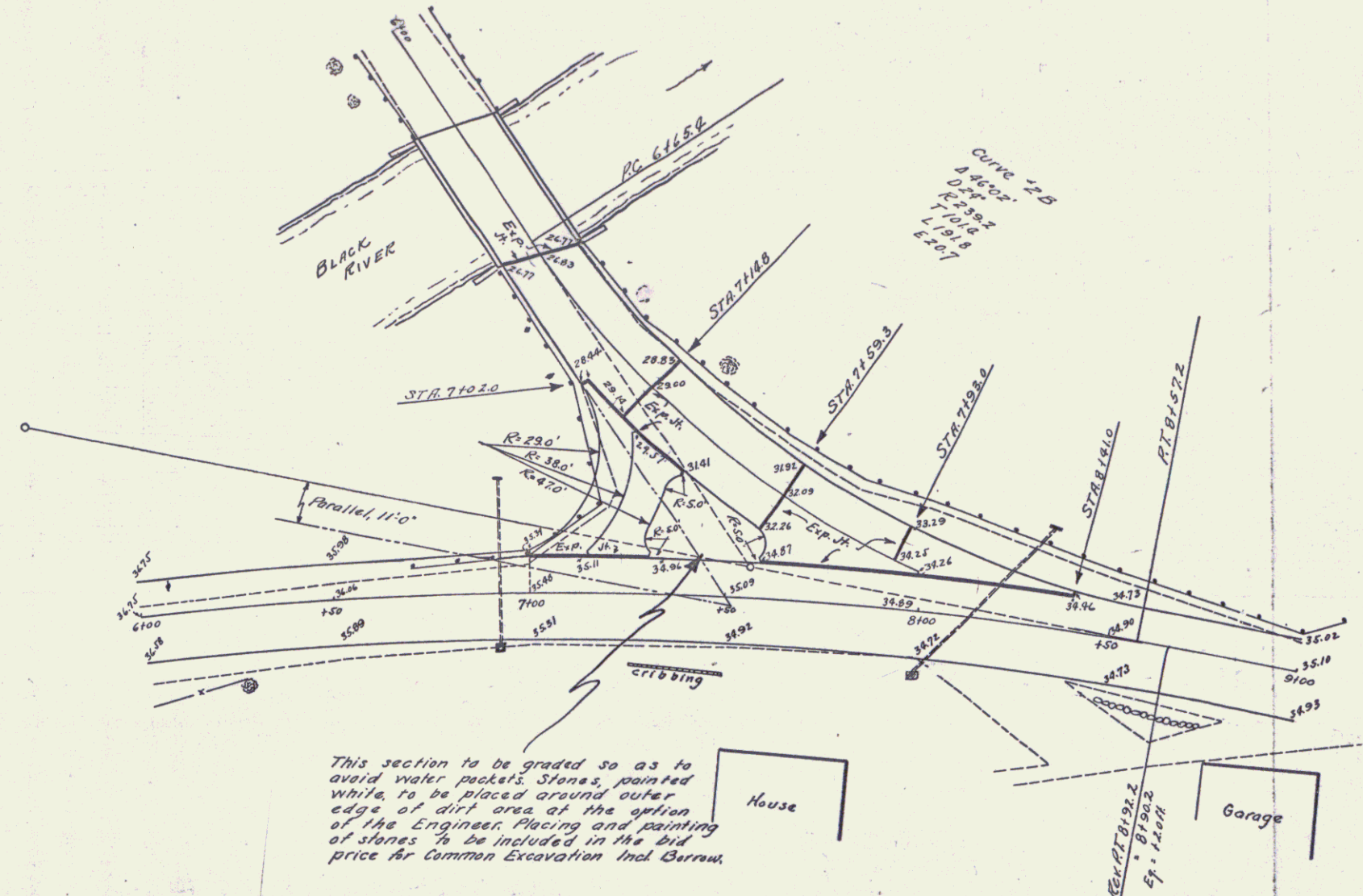
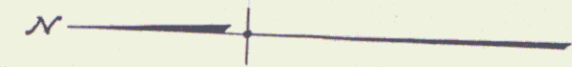
Stations	Diam.	Length
192100-192124 RH	8"	24'
192124 to canal	12"	72'
293100 D RH to canal	12"	66'
295150 RH to canal	12"	178'
297125-295150 RH	8"	176'



S.E. Palmer  
 Engineer  
 No. 121A  
 1930

S.E. Palmer  
 Engineer  
 No. 121A  
 1930

S.E. Putnam  
 Engineer  
 1930



INTERSECTION AT PLYMOUTH TURN  
 - Scale: 1"=20' -  
 Drainage structures shown are those proposed.

This section to be graded so as to avoid water pockets. Stones, painted white, to be placed around outer edge of dirt area at the option of the Engineer. Placing and painting of stones to be included in the bid price for Common Excavation that carries.

