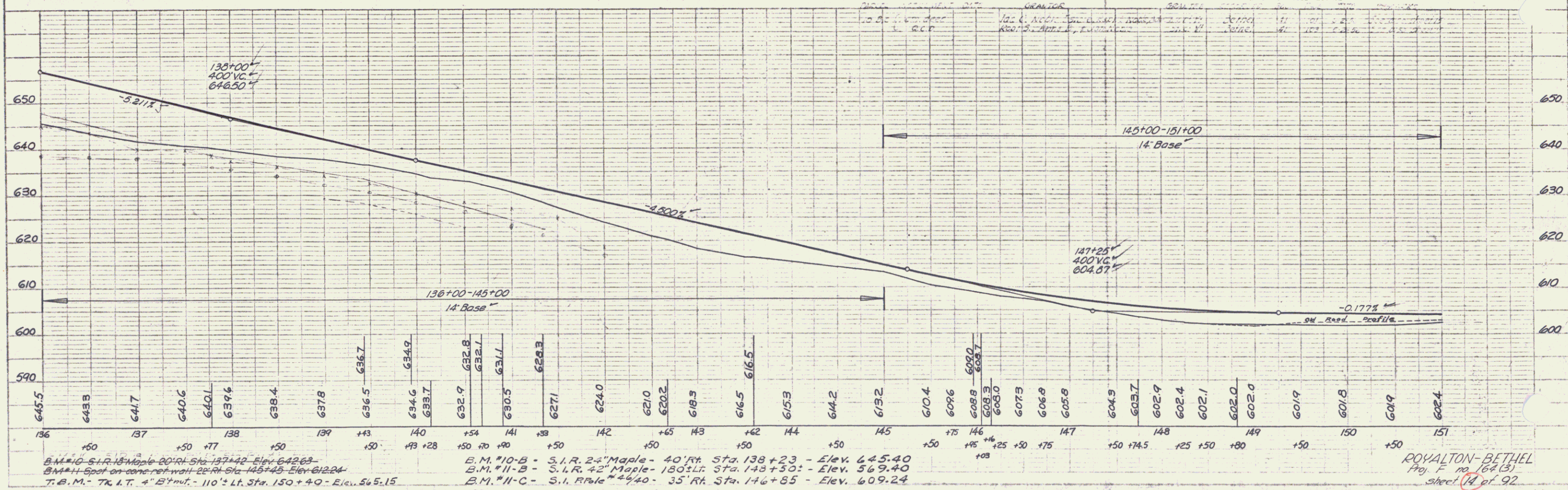
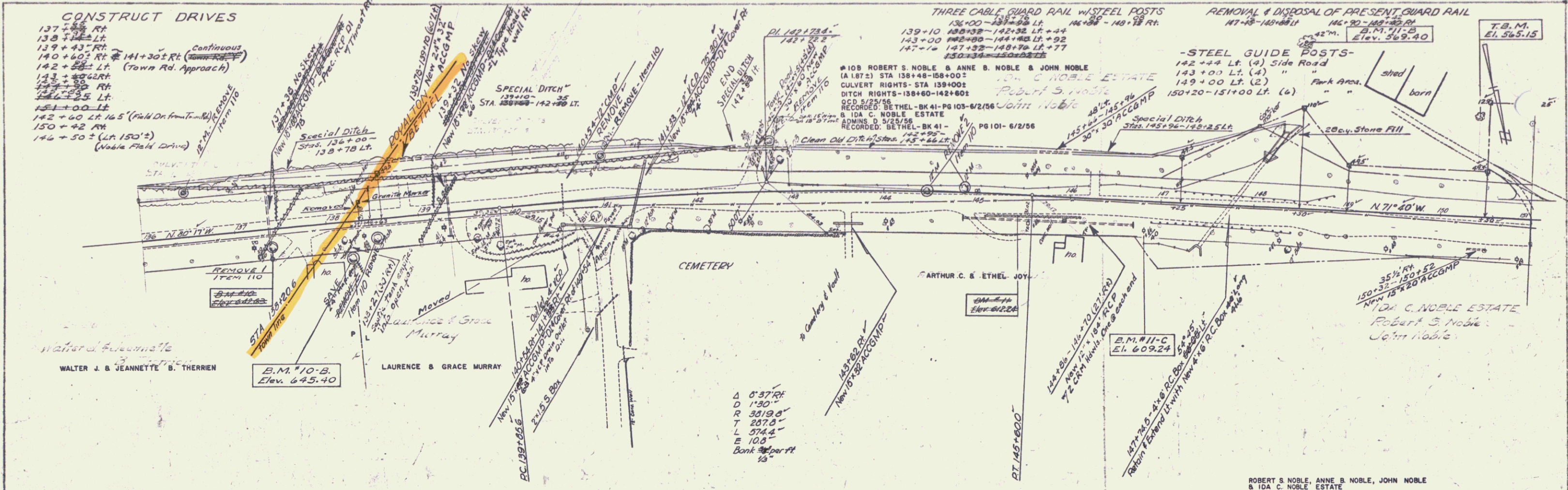


CONSTRUCT DRIVES

- 137+85 RT
- 138+85 LT
- 139+43 RT
- 140+60 RT & 141+30 RT (Continuous)
- 142+50 LT (Town Rd. Approach)
- 143+50 RT
- 144+25 LT
- 145+00 LT
- 142+60 LT 145' (Field Dr. from Town Rd.)
- 150+42 RT
- 146+50 ± (LT 150'±)
- 146+50 ± (Noble Field Drive)

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B.M. #10 - S.I.R. 18" Maple - 20' RT Sta. 137+42 - Elev. 642.65
 B.M. #11 - Spot on conc. ret. wall - 22' RT Sta. 145+45 - Elev. 612.24
 T.B.M. - 7x1.5" - 4" B.M. - 110' ± Lt. Sta. 150+40 - Elev. 565.15

B.M. #10-B - S.I.R. 2" Maple - 40' RT Sta. 138+23 - Elev. 645.40
 B.M. #11-B - S.I.R. 2" Maple - 130' Lt. Sta. 143+50 - Elev. 569.40
 B.M. #11-C - S.I. Pole 4" - 35' RT Sta. 146+85 - Elev. 609.24

ROYALTON-BETHEL
 Proj. F. no. 154(3)
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