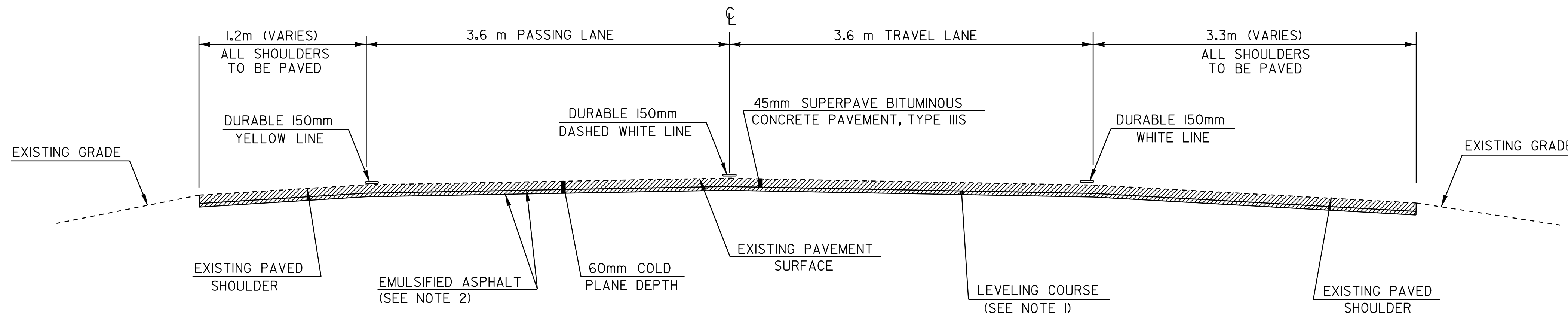


NOTES

- THE PAVEMENT WEARING COURSE SHALL BE TYPE IIIS. THE LEVELING COURSE SHALL BE TYPE IVS UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. LEVELING HAS BEEN INCLUDED TO RESHAPE THE ROADWAY AFTER COLD PLANING AND PRIOR TO PAVING THE TOP COURSE. AN ESTIMATED THICKNESS OF 15mm OF ITEM 490.30 HAS BEEN INCLUDED THROUGHOUT THE PROJECT LIMITS TO COVER THIS PROVISION. ALL ASPHALT CEMENT USED IN THE SUPERPAVE BITUMINOUS CONCRETE PAVEMENT SHALL BE PG 58-34.
- EMULSIFIED ASPHALT SHALL BE APPLIED ON EXISTING PAVEMENT SURFACES, BETWEEN ALL COURSES OF PAVEMENT AND ON COLD PLANED SURFACES AT THE RATE OF 0.12 L/SM OR AS DIRECTED BY THE RESIDENT ENGINEER.
- SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = +/- 5 mm (TOTAL THICKNESS).
- COLD PLANING TO BE COMPLETED ACCORDING TO THE TYPICAL OR AS NOTED OTHERWISE ON THE PLANS. A FULL DEPTH BUTT JOINT SHALL BE CONSTRUCTED AT THE PROJECT BEGINNING/END AND AT ALL RAMP APPROACHES AS DENOTED ON THE PROJECT PLANS OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
- ITEMS 604.40 AND 604.412 SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER. ALL D.I.'S TO RECEIVE WORK SHALL BE RAISED OR REHABILITATED SUCH THAT THE NEW GRATE ELEVATION IS LEVEL WITH THE SURROUNDING TERRAIN.
- AREAS ADJACENT TO THE SHOULDER, WHERE EXISTING GUARD RAIL IS BEING RETAINED, THAT HAVE BUILT UP EXCESS MATERIAL ARE TO BE GRADED IN ORDER TO ALLOW THE SHOULDER TO DRAIN, PAYMENT IS UNDER ITEM 203.99, SHOULDER BERM REMOVAL.
- GRASS GROWING ADJACENT TO PAVEMENT, OR THROUGH CRACKS IN THE PAVEMENT, WHICH MAY HAMPER THE PLACEMENT OF NEW BITUMINOUS CONCRETE, SHALL BE REMOVED BY THE CONTRACTOR, AS DIRECTED BY THE ENGINEER. PAYMENT FOR THIS WORK WILL NOT BE MADE DIRECTLY, BUT WILL BE CONSIDERED SUBSIDIARY TO ITEM 490.30, SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (PG 58-34).
- ALL EDGES OF PAVEMENT SHALL BE BACKED UP FULL HEIGHT WITH COLD PLANE GRINDINGS AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.12, AGGREGATE SHOULDER (MOD.).
- THERE ARE DISCONTINUED D.I.'S ON THIS PROJECT THAT WERE COVERED DURING THE LAST CONSTRUCTION PROJECT. THE CONTRACTOR SHALL LOCATE AND/OR COORDINATE WITH A PROJECT ENGINEER ON THEIR LOCATIONS.
- ESTIMATED QUANTITIES OF ITEMS 601.995 AND 601.996, CLEANING CULVERT IN PLACE (0-600 mm) AND 601.996 CLEANING CULVERT PIPE IN PLACE (GREATER THAN 600 mm) SHALL BE USED TO CLEAN CULVERTS AT LOCATIONS DETERMINED BY THE RESIDENT ENGINEER. ALL EXISTING IN-DITCH DROP INLETS SHALL ALSO BE CLEANED OUT. THE COSTS ASSOCIATED WITH THIS WORK SHALL BE CONSIDERED SUBSIDIARY TO THE APPROPRIATE CULVERT CLEANING ITEM.
- POWER BROOM RENTAL, TYPE II SHALL BE USED TO CLEAN THE MATERIAL FROM IN FRONT OF EXISTING CURB BOARD.



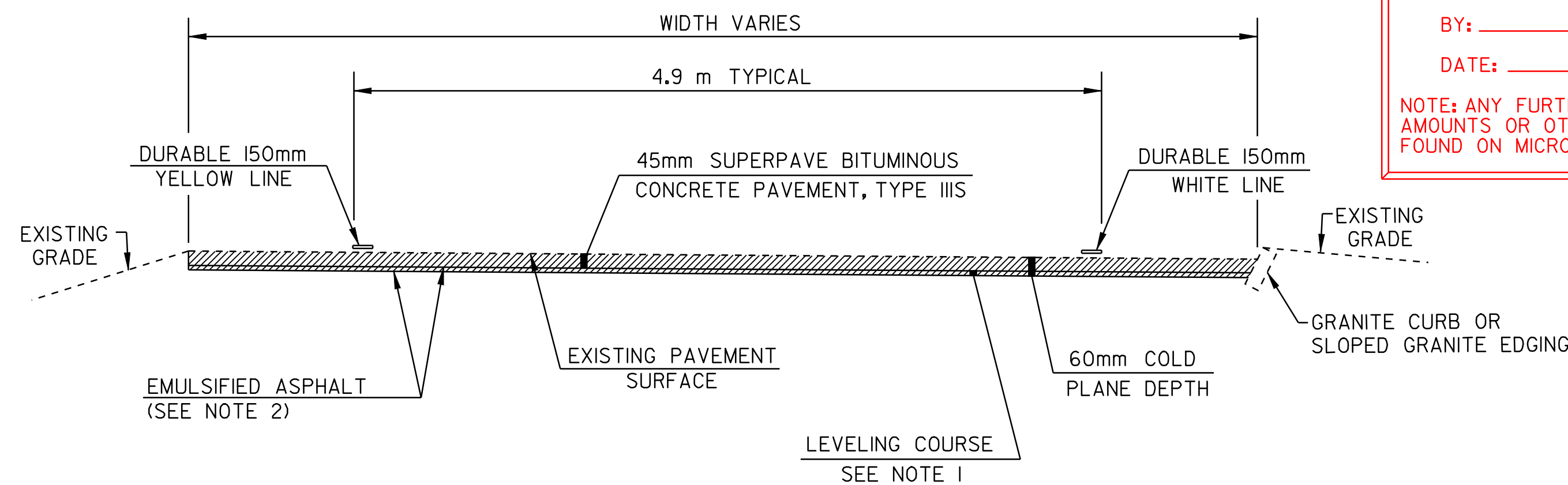
**MAINLINE TYPICAL SECTION
NORTHBOUND**

STA 220+719.9 TO 251+138.1

RECORD PLANS

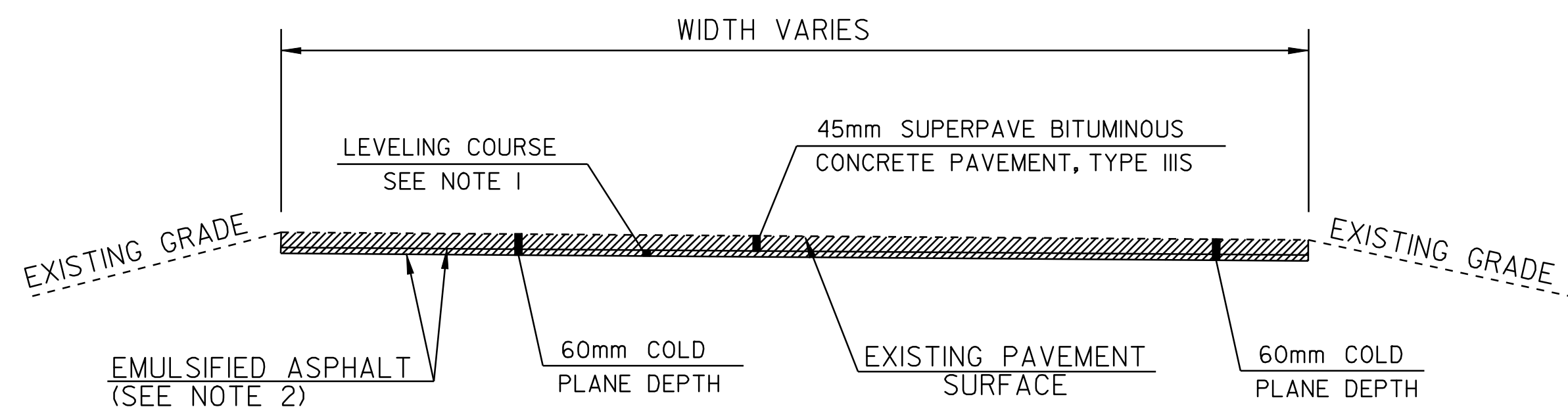
CONTRACTOR: MILLER CONSTRUCTION, INC. WINDSOR, VERMONT
 RESIDENT ENGINEER: IAN JENIKE
 CONSTRUCTION BEGAN: FEBRUARY 22, 1993
 CONSTRUCTION COMPLETED: OCTOBER 15, 1993
 RECORD PLANS BY: CONSTRUCTION CADD
 I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.
 BY: _____, RESIDENT ENGINEER
 DATE: _____

NOTE: ANY FURTHER INFORMATION CONCERNING FINAL QUANTITIES, AMOUNTS OR OTHER DETAILS RELATIVE TO THIS PROJECT MAY BE FOUND ON MICROFILM IN CENTRAL FILES.



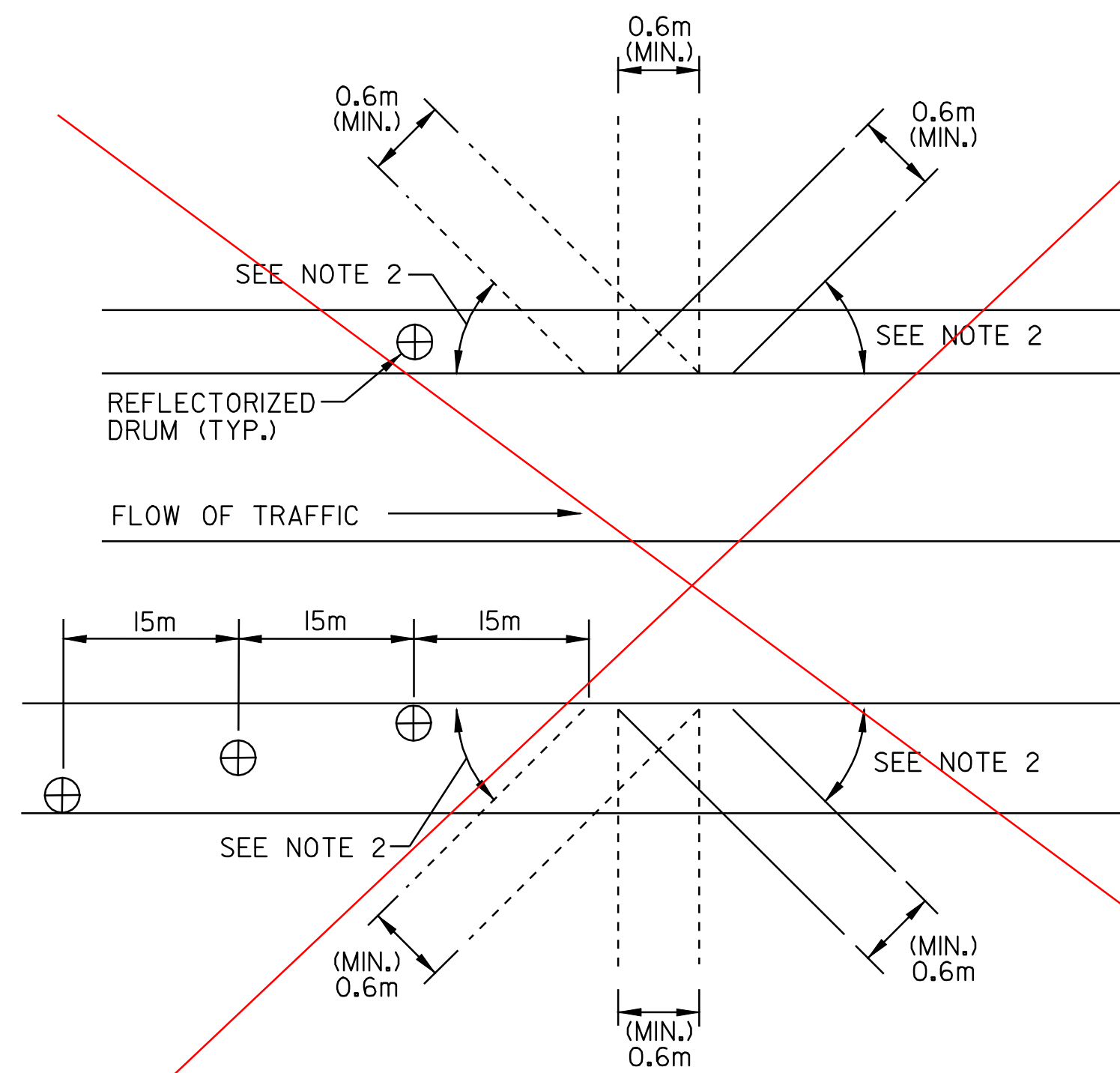
**RAMP PAVEMENT TYPICAL
LOCATION**

- INTERCHANGE 23 ON RAMP - SEE DETAILS, SHEET 8
- INTERCHANGE 24 ON & OFF RAMP - SEE DETAILS, SHEETS 9 & 10
- INTERCHANGE 25 OFF RAMP - SEE DETAILS, SHEET 11



**U-TURN PAVEMENT TYPICAL
LOCATION**

- ✓ STA 221+969
- ✓ STA 225+018
- ✓ STA 226+163
- ✓ STA 230+683
- ✓ STA 234+656
- ✓ STA 238+585
- ✓ STA 242+544
- ✓ STA 242+657
- ✓ STA 245+344
- ✓ STA 246+905
- ✓ STA 250+406
- ✓ STA 250+462



BLEEDER DETAIL

NOT NEEDED

NOTES:

- BLEEDERS ARE TO BE CUT WHILE COLD PLANING, AT LOCATIONS SPECIFIED BY THE RESIDENT ENGINEER.
- ANGLE BLEEDERS TO BEST ACCEPT WATER FLOW.
- CUT ALL BLEEDERS TO THE DEPTH OF COLD PLANING AND GRADE TO DRAIN. PAYMENT SUBSIDIARY TO ITEM 210.10.
- BLEEDERS ARE TO BE FILLED DURING PAVING WITH ITEM 490.30 OR AS DESIGNATED BY THE RESIDENT ENGINEER.
- REFLECTORIZED DRUMS MUST BE PLACED AS SHOWN TO WARN MOTORISTS OF THE GAP IN THE SHOULDER.
- BLEEDERS ARE NOT REQUIRED IF THE COLD PLANED AREAS ARE PAVED BACK THE SAME DAY.

**ARCHIVED
IN DPR**

Metric

**PROJECT
TYPICAL
SHEET #1**

DESIGNED BY BCE/PJM DATE 9-00
 DRAWN BY C.E.A., INC. DATE 9-00
 DESIGN FILE NO. /pave/99a204/pa204.dgn
 PRF FILE pa204pt1.i DATE PLOTTED 03-APR-2008 15:02
 PROJ. NAME LYNDON - BARTON
 PROJ. NO. IM 091-3(10)
 SHEET 2 OF 17 SHEETS