

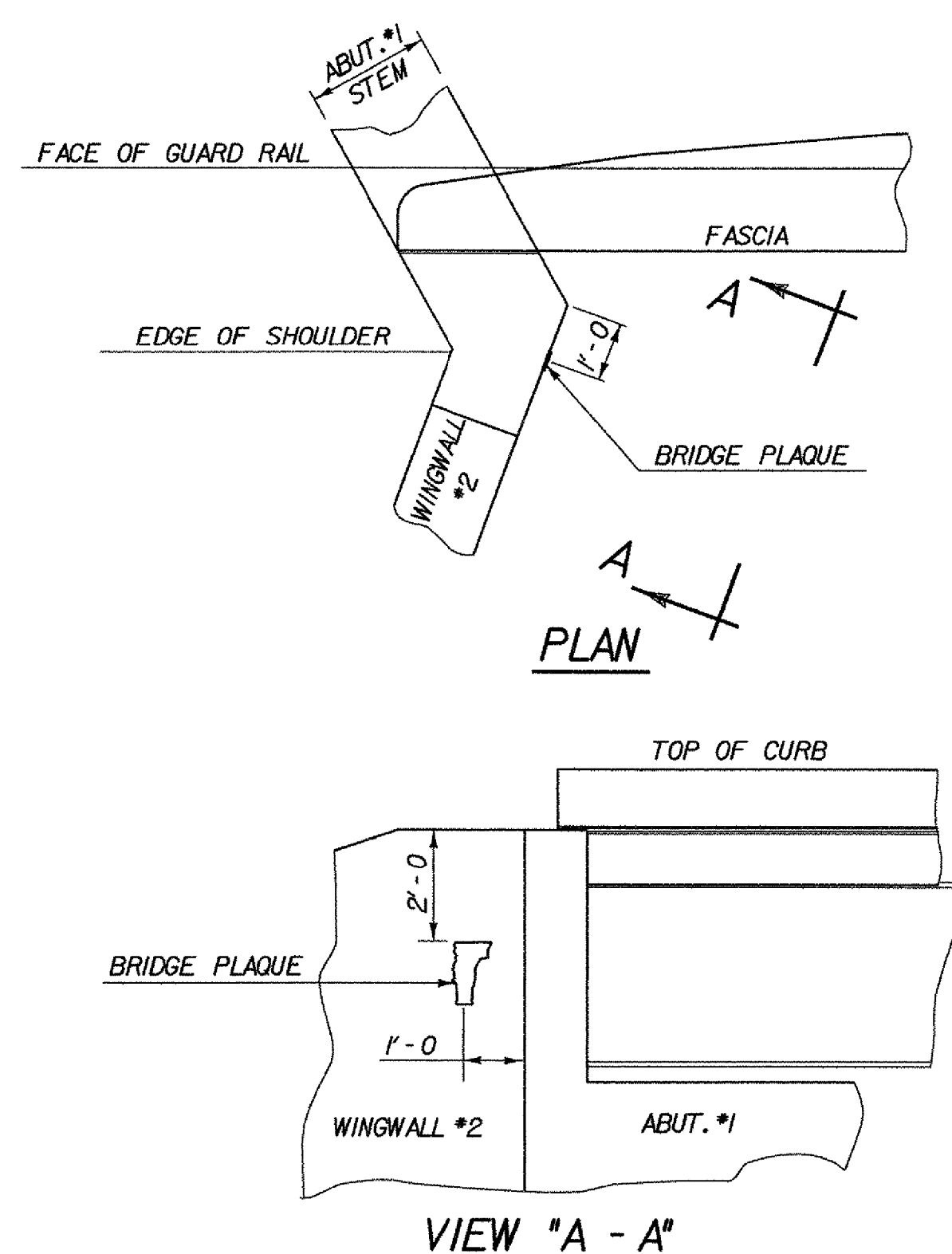
**ASPHALTIC PLUG JOINT NOTES**

**1. INSTALLATION**

- A. LOCATE THE JOINT CENTRALLY OVER THE DECK OVERLAY EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.
- B. EXCAVATE THE JOINT AS SHOWN ON THE PLANS WITH SAWS AND PNEUMATIC HAMMER OR A HAMMER AND CHISEL.
- C. BLAST CLEAN THE JOINT AREA OF DEBRIS AND ASPHALT. THOROUGHLY DRY THE JOINT AREA WITH HOT COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.
- D. REPAIR SPALLED AND DEFECTIVE CONCRETE WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.
- E. PLACE PROPERLY SIZED HEAT RESISTANT BACKER ROD IN THE MOVEMENT GAP ALLOWING FOR 1" +/- OF BINDER ABOVE THE ROD.
- F. HEAT AND PLACE THE BINDER MATERIAL AS RECOMMENDED BY THE MANUFACTURER.
- G. PLACE 1/4" THICK BY 8" WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE THE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRESTAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER. THE STEEL PLATES MAY BE OMITTED WHERE THE APPROACH SLAB IS COVERED WITH A STONE BASE OR BITUMINOUS PAVEMENT AND VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.
- H. HEAT AND MIX THE BINDER MATERIAL AND AGGREGATE AS RECOMMENDED BY THE MANUFACTURER.
- I. INSTALLATION OF MATERIAL, COMPACTION, AND TOP COATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- J. IMMEDIATELY AFTER TOP COATING, CAST AN ANTI-SKID MATERIAL OVER THE JOINT TO REDUCE THE RISK OF TRACKING.
- K. PROTECT JOINT FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 51 DEG C (125 DEG F) +/-.

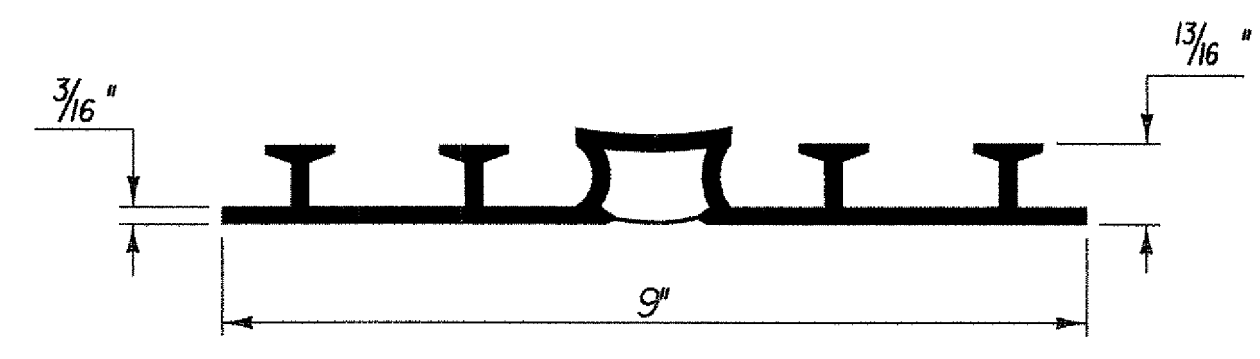
**2. WEATHER LIMITATIONS.** (APPLY BINDER MATERIAL ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL):

- A. THE AMBIENT AIR TEMPERATURE IS AT LEAST 10 DEG C (50 DEG F) AND RISING.
- B. THE ROAD SURFACE IS SUFFICIENTLY DRY.
- C. WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF SATISFACTORY WORK.



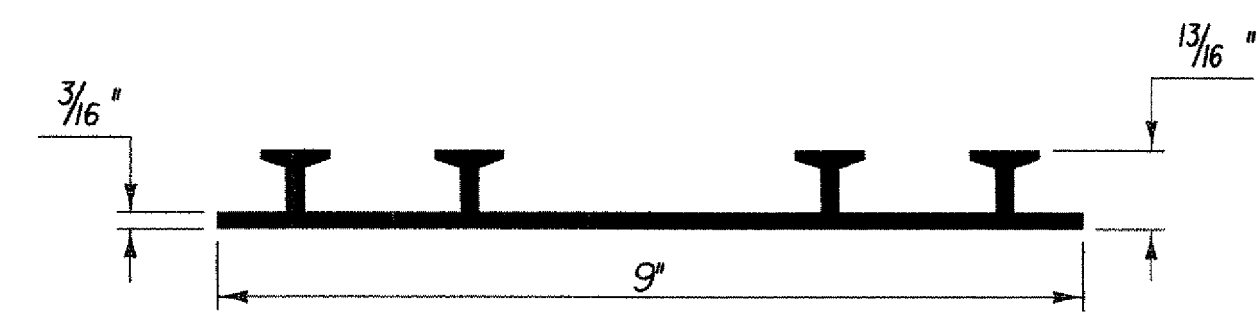
**BRIDGE PLAQUE DETAIL**

THE BRIDGE PLAQUE WILL BE SUPPLIED BY THE AGENCY OF TRANSPORTATION AND SHALL BE INSTALLED BY THE CONTRACTOR AT ABUTMENT #1 ON THE RIGHT SIDE AS SHOWN, OR AS DIRECTED BY THE ENGINEER.  
 PAYMENT FOR INSTALLATION OF THE BRIDGE PLAQUE SHALL BE INCIDENTAL TO THE ADJACENT CONCRETE.



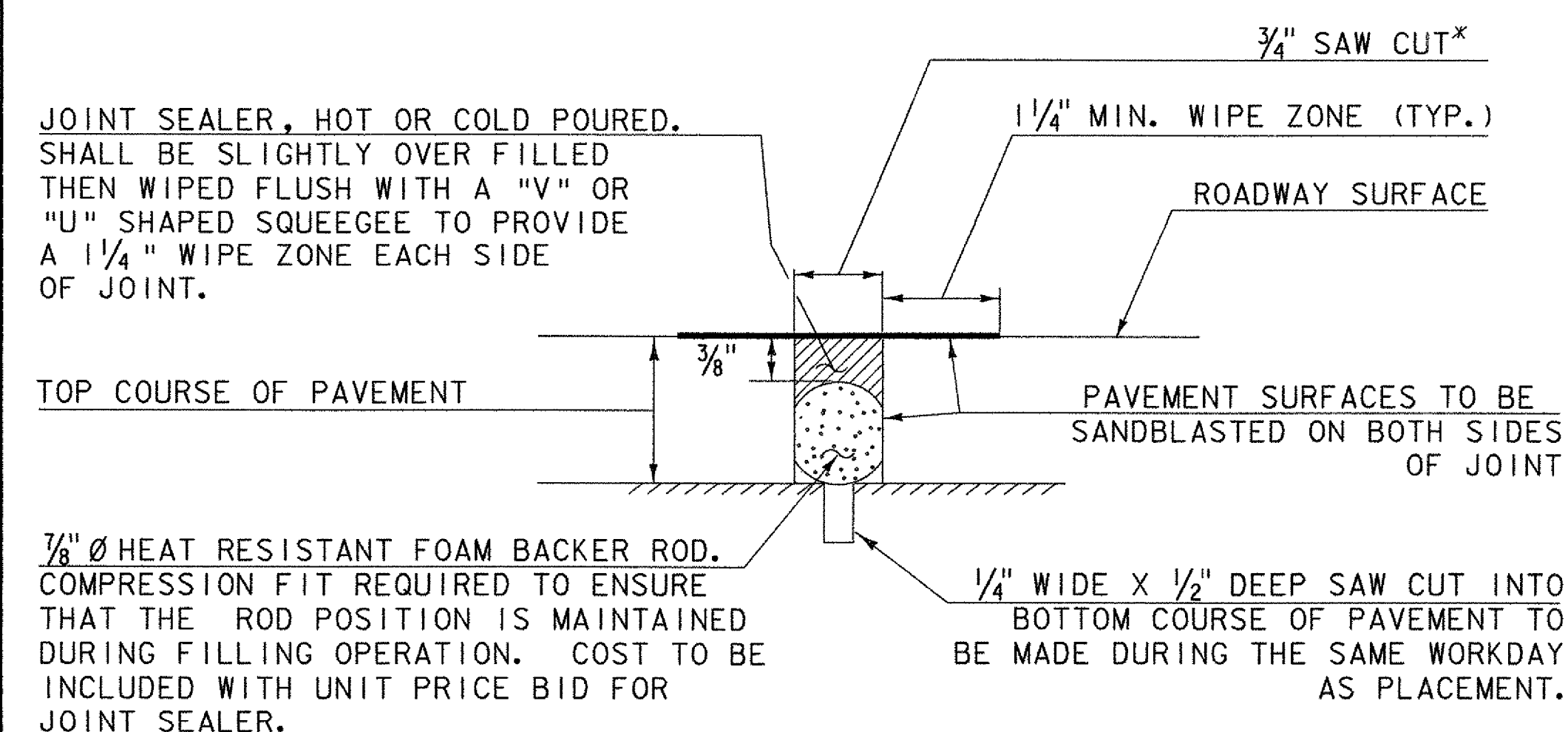
**P.V.C. WATERSTOP FOR EXPANSION JOINTS**

PAYMENT FOR THE P.V.C. WATERSTOP SHALL BE INCIDENTAL TO THE UNIT BID PRICE FOR THE ADJACENT CONCRETE.  
 OTHER CONFIGURATIONS OF WATERSTOP MAY BE USED UPON APPROVAL OF THE ENGINEER.



**P.V.C. WATERSTOP FOR CONSTRUCTION JOINTS**

PAYMENT FOR THE P.V.C. WATERSTOP SHALL BE INCIDENTAL TO THE UNIT BID PRICE FOR THE ADJACENT CONCRETE.  
 OTHER CONFIGURATIONS OF WATERSTOP MAY BE USED UPON APPROVAL OF THE ENGINEER.

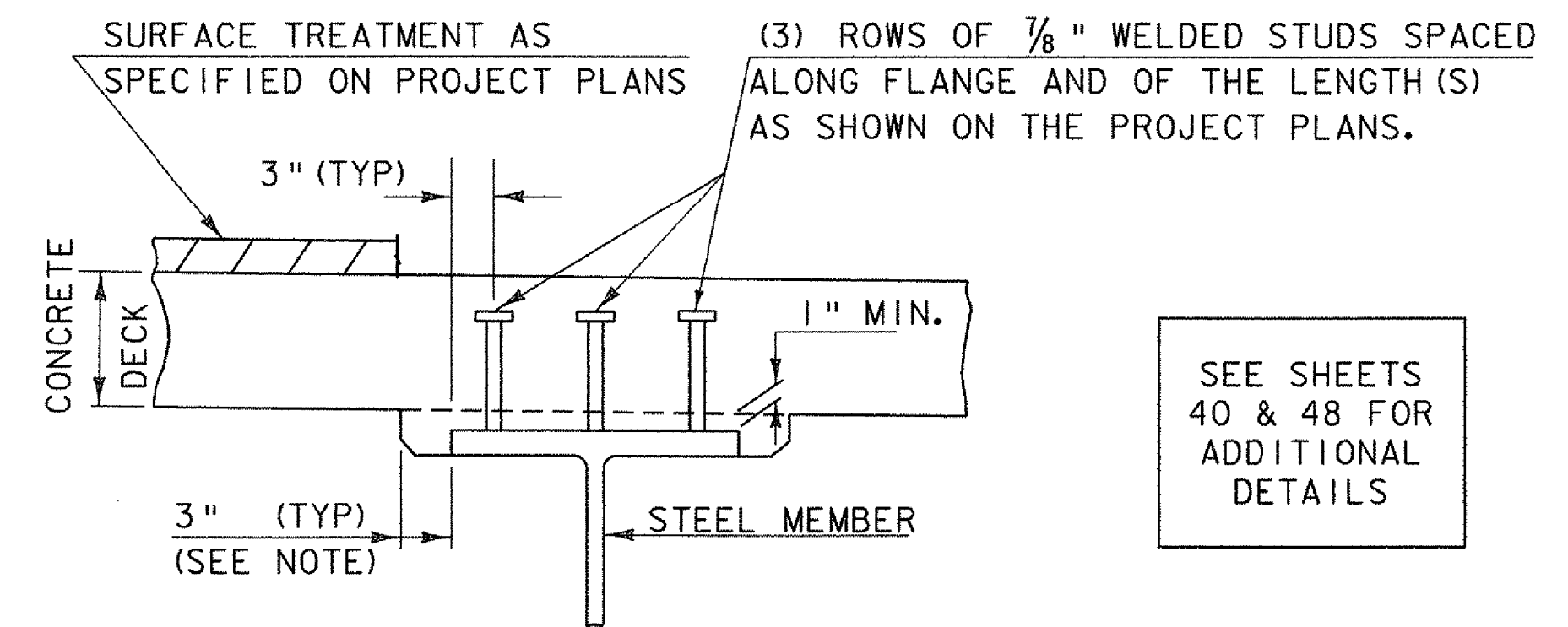


**SAWED PAVEMENT JOINT DETAIL**

• NOT USED ON NB OR SB PER P.M.

N. T. S.

\* JOINT IS TO BE LOCATED ACCURATELY BY STRING LINING, OR OTHER MEANS, PRIOR TO PAVING, SO THAT THE SAW CUTS WILL BE MADE DIRECTLY OVER THE END OF CONCRETE DECK. JOINT SHALL BE CUT DRY IN A SINGLE PASS AND BE SEALED WITHIN 24 HOURS OR PRIOR TO EXPOSURE TO TRAFFIC. JOINT SHALL BE CLEANED PRIOR TO APPLYING THE JOINT SEALER.



SEE SHEETS 40 & 48 FOR ADDITIONAL DETAILS

NOTE: THE 3" HORIZONTAL SECTION MAY BE ELIMINATED FOR FORMING SYSTEMS DESIGNED FOR THE CONSTRUCTION OF VERTICAL HAUNCHES. ANY VOIDS RESULTING FROM FORMING SYSTEM ELEMENTS SHALL BE FILLED WITH JOINT SEALER, POLYURETHANE MEETING THE REQUIREMENTS OF SECTION 524. THE COST OF THE JOINT SEALER, POLYURETHANE SHALL BE INCIDENTAL TO THE ADJACENT CONCRETE.

**HAUNCH AND SHEAR CONNECTOR DETAIL**

(NOT TO SCALE)

**MISCELLANEOUS PAVEMENT & CONCRETE DETAILS**

PROJECT NAME:	GUILFORD
PROJECT NUMBER:	IM 091-(133)
FILE NAME:	95a176\str\sai76miscdet.dgn
PROJECT LEADER:	S. Farnsworth
DESIGNED BY:	VTrans
PLOT DATE:	20-MAY-2008
DRAWN BY:	C. Weeber
CHECKED BY:	S. Farnsworth
BRIISA SHEET	41A OF 114