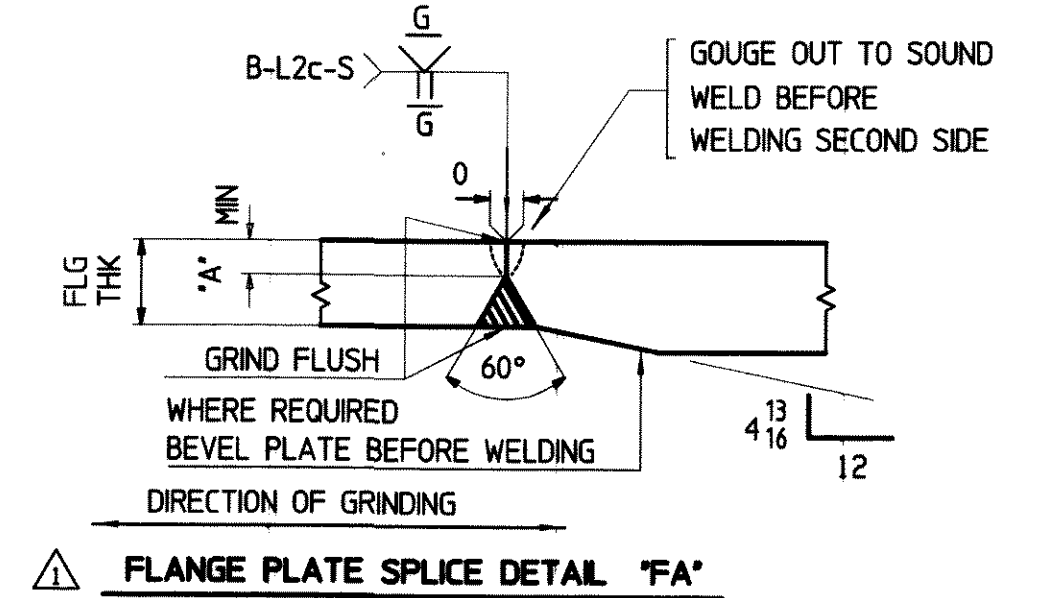
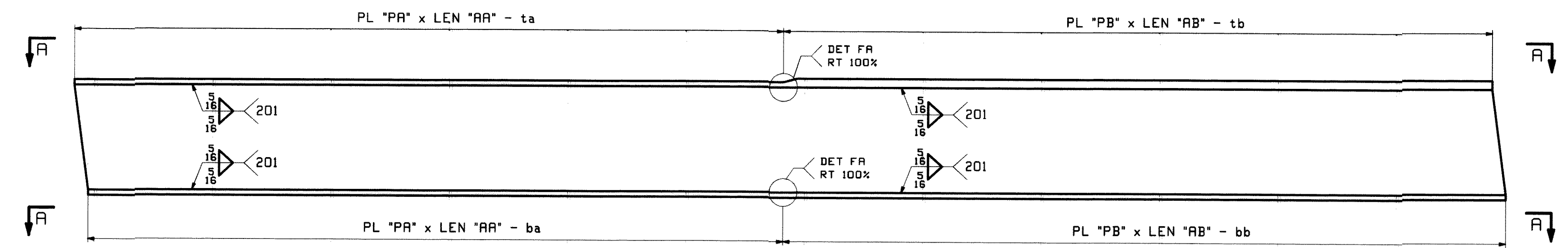


FLG THK	'A'
25	6
42	12



SECTION A-A FOR 11G1C THRU 15G5C

LOCATION	RAD	ARC	CHD	AA	AB	A1	A2	A3	A4	A5	AA	CA	B1	B2	B3	B4	B5	CB	ECL	ECR	WL1	WL2	HDI	PL PA (M270-50WT2)	PL PB (M270-50WT2)
11G1C TOP FL	1130.92	100'-4 ¹⁵ / ₁₆	100'-4 ⁹ / ₁₆	60'-5 ¹ / ₁₆	39'-11 ⁷ / ₈	4 ¹³ / ₁₆	4 ⁵ / ₈	4 ¹ / ₁₆	3 ¹ / ₈	1 ³ / ₄	6'-0 ¹ / ₂	60'-5	2 ¹ / ₈	2 ¹ / ₁₆	1 ³ / ₄	1 ³ / ₈	3 ³ / ₄	39'-11 ⁷ / ₈	3 ³ / ₈	3 ³ / ₈	60'-4 ⁷ / ₈	39'-11 ¹¹ / ₁₆	1'-0 ¹³ / ₁₆	PL 1 x 16-ta (2/S)	PL 1 ¹ / ₂ x 16-tb (2/L)
11G1C BTM FL	1130.92	100'-4 ³ / ₈	100'-4 ³ / ₈	60'-4 ¹⁵ / ₁₆	39'-11 ¹³ / ₁₆	4 ¹³ / ₁₆	4 ⁵ / ₈	4 ¹ / ₁₆	3 ¹ / ₈	1 ³ / ₄	6'-0 ¹ / ₂	60'-4 ¹³ / ₁₆	2 ¹ / ₈	2 ¹ / ₁₆	1 ³ / ₄	1 ³ / ₈	3 ³ / ₄	39'-11 ¹³ / ₁₆	3 ³ / ₈	3 ³ / ₈	60'-4 ³ / ₄	39'-11 ⁵ / ₈	1'-0 ¹³ / ₁₆	PL 1 ¹ / ₂ x 16-ba (1/E)	PL 1 ¹ / ₂ x 16-bb (2/L)
12G2C TOP FL	1138.42	99'-5 ³ / ₄	99'-5 ³ / ₈	59'-5 ³ / ₈	39'-11 ⁷ / ₈	4 ¹¹ / ₁₆	4 ¹ / ₂	3 ¹⁵ / ₁₆	3	1 ¹¹ / ₁₆	5'-11 ³ / ₈	59'-5 ¹³ / ₁₆	2 ¹ / ₈	2	1 ³ / ₄	1 ³ / ₈	3 ³ / ₄	39'-11 ⁷ / ₈	3 ³ / ₈	3 ³ / ₈	59'-5 ¹¹ / ₁₆	39'-11 ¹¹ / ₁₆	1'-0 ⁹ / ₁₆	PL 1 x 16-ta (2/U)	PL 1 ¹ / ₂ x 16-tb (2/L)
12G2C BTM FL	1138.42	99'-5 ¹¹ / ₁₆	99'-5 ⁵ / ₁₆	59'-5 ¹³ / ₁₆	39'-11 ⁷ / ₈	4 ¹¹ / ₁₆	4 ¹ / ₂	3 ¹⁵ / ₁₆	3	1 ¹¹ / ₁₆	5'-11 ³ / ₈	59'-5 ³ / ₄	2 ¹ / ₈	2	1 ³ / ₄	1 ³ / ₈	3 ³ / ₄	39'-11 ¹³ / ₁₆	3 ³ / ₈	3 ³ / ₈	59'-5 ⁵ / ₈	39'-11 ¹¹ / ₁₆	1'-0 ⁹ / ₁₆	PL 1 ¹ / ₂ x 16-ba (1/G)	PL 1 ¹ / ₂ x 16-bb (2/L)
13G3C TOP FL	1145.92	98'-7 ¹ / ₈	98'-6 ³ / ₄	58'-7 ¹ / ₄	39'-11 ⁷ / ₈	4 ¹ / ₂	4 ⁵ / ₁₆	3 ³ / ₄	2 ⁷ / ₈	1 ⁵ / ₈	5'-10 ⁵ / ₁₆	58'-7 ¹ / ₈	2 ¹ / ₁₆	2	1 ³ / ₄	1 ⁵ / ₁₆	3 ³ / ₄	39'-11 ⁷ / ₈	3 ³ / ₈	5 ¹ / ₁₆	58'-7	39'-11 ³ / ₄	1'-0 ¹ / ₄	PL 1 x 16-ta (2/W)	PL 1 ¹ / ₂ x 16-tb (2/L)
13G3C BTM FL	1145.92	98'-7 ¹ / ₈	98'-6 ³ / ₄	58'-7 ¹ / ₄	39'-11 ⁷ / ₈	4 ¹ / ₂	4 ⁵ / ₁₆	3 ³ / ₄	2 ⁷ / ₈	1 ⁵ / ₈	5'-10 ⁵ / ₁₆	58'-7 ¹ / ₈	2 ¹ / ₁₆	2	1 ³ / ₄	1 ⁵ / ₁₆	3 ³ / ₄	39'-11 ⁷ / ₈	3 ³ / ₈	5 ¹ / ₁₆	58'-7	39'-11 ³ / ₄	1'-0 ¹ / ₄	PL 1 ¹ / ₂ x 16-ba (1/J)	PL 1 ¹ / ₂ x 16-bb (2/L)
14G4C TOP FL	1153.42	97'-8 ¹⁵ / ₁₆	97'-8 ⁹ / ₁₆	57'-9 ¹ / ₁₆	39'-11 ⁷ / ₈	4 ⁵ / ₁₆	4 ³ / ₁₆	3 ⁵ / ₈	2 ³ / ₄	1 ⁹ / ₁₆	5'-9 ⁵ / ₁₆	57'-9	2 ¹ / ₁₆	2	1 ³ / ₄	1 ⁵ / ₁₆	3 ³ / ₄	39'-11 ⁷ / ₈	5 ¹ / ₁₆	5 ¹ / ₁₆	57'-8 ⁷ / ₈	39'-11 ¹¹ / ₁₆	1'-0	PL 1 x 16-ta (2/Y)	PL 1 ¹ / ₂ x 16-tb (2/L)
14G4C BTM FL	1153.42	97'-8 ¹⁵ / ₁₆	97'-8 ⁹ / ₁₆	57'-9 ¹ / ₁₆	39'-11 ⁷ / ₈	4 ⁵ / ₁₆	4 ³ / ₁₆	3 ⁵ / ₈	2 ³ / ₄	1 ⁹ / ₁₆	5'-9 ⁵ / ₁₆	57'-9	2 ¹ / ₁₆	2	1 ³ / ₄	1 ⁵ / ₁₆	3 ³ / ₄	39'-11 ¹³ / ₁₆	5 ¹ / ₁₆	5 ¹ / ₁₆	57'-8 ⁷ / ₈	39'-11 ¹¹ / ₁₆	1'-0	PL 1 ¹ / ₂ x 16-ba (1/L)	PL 1 ¹ / ₂ x 16-bb (2/L)
15G5C TOP FL	1160.92	96'-11 ¹ / ₄	96'-10 ¹⁵ / ₁₆	56'-11 ³ / ₈	39'-11 ⁷ / ₈	4 ³ / ₁₆	4	3 ¹ / ₂	2 ¹¹ / ₁₆	1 ¹ / ₂	5'-8 ⁵ / ₁₆	56'-11 ⁵ / ₁₆	2 ¹ / ₁₆	2	1 ³ / ₄	1 ⁵ / ₁₆	3 ³ / ₄	39'-11 ¹³ / ₁₆	5 ¹ / ₁₆	5 ¹ / ₁₆	56'-11 ¹ / ₄	39'-11 ¹¹ / ₁₆	11 ³ / ₄	PL 1 x 16-ta (3/A)	PL 1 ¹ / ₂ x 16-tb (2/L)
15G5C BTM FL	1160.92	96'-11 ³ / ₁₆	96'-10 ⁷ / ₈	56'-11 ³ / ₈	39'-11 ¹³ / ₁₆	4 ³ / ₁₆	4	3 ¹ / ₂	2 ¹¹ / ₁₆	1 ¹ / ₂	5'-8 ⁵ / ₁₆	56'-11 ⁵ / ₁₆	2 ¹ / ₁₆	2	1 ³ / ₄	1 ⁵ / ₁₆	3 ³ / ₄	39'-11 ³ / ₄	5 ¹ / ₁₆	5 ¹ / ₁₆	56'-11 ¹ / ₄	39'-11 ⁵ / ₈	11 ³ / ₄	PL 1 ¹ / ₂ x 16-ba (1/N)	PL 1 ¹ / ₂ x 16-bb (2/L)



FLANGE DIAGRAM FOR 11G1C THRU 15G5C

APPROVED FOR CONSTRUCTION
 DATE: 2/23/10
 DRAWN BY: JB
 CHECKED BY: SOB
 TENSOR 2345

NOTES:
 1. FOR GENERAL SHOP NOTES SEE DRAWING GNI.

REV.	DATE	REMARKS	DWN	CHK	APVL	SHOP
0	2/22/2010	APPROVAL COMMENT ADD THK TRANSITION	SOB	SOB		

MATERIAL: M270-50W SURFACE PREP. & PAINT: AS NOTED HOLES: AS NOTED SHOP BOLTS: AS NOTED

DESCRIPTION: FLANGE DIAGRAM

CASCO BAY STEEL STRUCTURES, INC.
 75 SPRING HILL ROAD SACO, MAINE 04072
 PHONE (207) 282-7360 FAX (207) 282-1179

STRUCTURE: U.S. 5 over I-91
 Bridge No. 19A
 Putney
 County of Windham

DATE: 01/05
 DATE: 01/08
 JOB NO. 438
 DWG NO. F3
 REV. 1