

**PHASING DIAGRAM AND SPECIAL NOTES FOR EACH LOCATION**

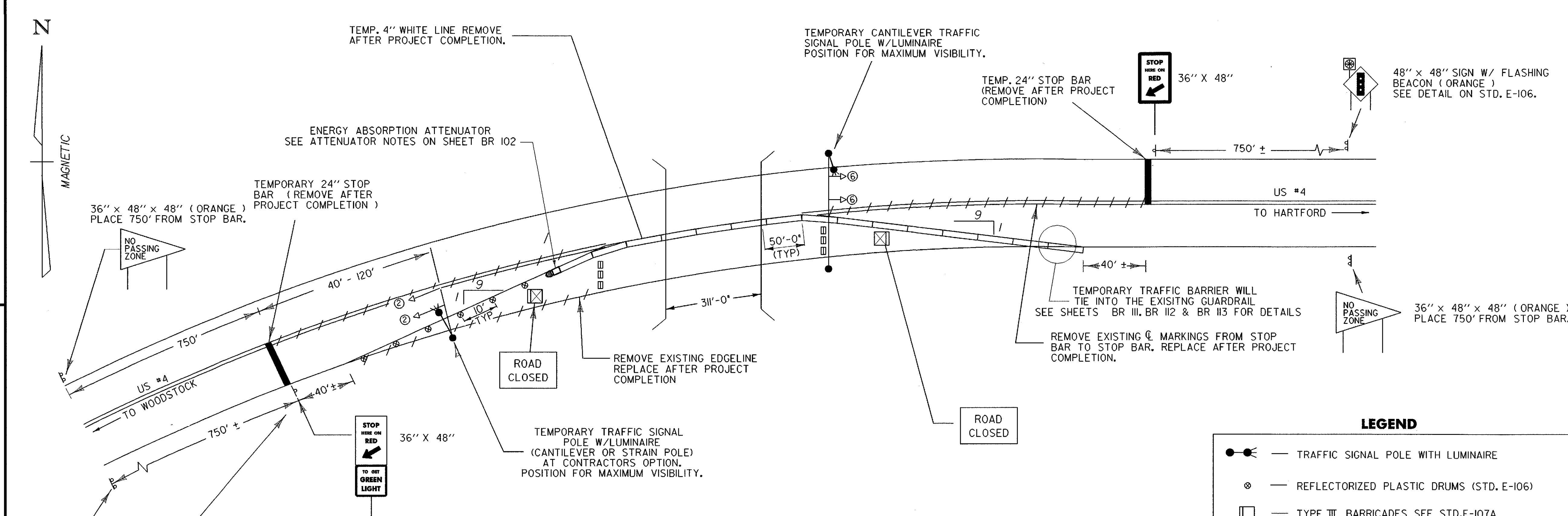
PHASE	2	6	4
MINIMUM	12	3	17
EXTENSION	2		
MAXIMUM	20	3	17
HEAD 2	G	Y	R
HEAD 6	R	R	R
HEAD 4			

APPROACH 4 IS A SIDE STREET APPROACH - IF REQUIRED

**SPECIAL REQUIREMENTS**

APPROACH	TEMPORARY VEHICLE DETECTOR	FLASHING BEACON ON ADVANCED WARNING SIGN
2	X	X
6		
4		

ENTER CHECK MARK IN APPROPRIATE BOX WHEN REQUIRED ON THIS PROJECT



**BRIDGE #8 ON US 4 (MM 4.0)**  
**NOT TO SCALE**

**NOTES:**

- ITEM LOCATIONS ARE APPROXIMATE.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO CONFIRM ANY MEASUREMENTS IN THE FIELD.
- PHASE ONE, TRAFFIC CONTROL FOR THE CLOSURE OF THE SOUTHERN LANE AS SHOWN, PHASE TWO, FOR CLOSURE OF THE NORTHERN LANE, IS OPPOSITE OF THAT SHOWN.

**LEGEND**

- — TRAFFIC SIGNAL POLE WITH LUMINAIRE
- ⊙ — REFLECTORIZED PLASTIC DRUMS (STD. E-106)
- — TYPE III BARRICADES SEE STD. E-107A
- ⊠ — TYPE III BARRICADES (MOD.) SEE STD. E-107A
- /// — PAVEMENT MARKING REMOVAL
- ⊙ — SIGNAL HEAD AND PHASE
- ⊠ — ENERGY ABSORPTION ATTENUATOR
- — TEMPORARY TRAFFIC BARRIER
- ⊙ — FLASHING BEACON

**GENERAL TEMPORARY TRAFFIC SIGNAL NOTES**

- DESIGN OF THE SIGNAL SUPPORT(S) AND ANY REQUIRED GUYING IS THE RESPONSIBILITY OF THE CONTRACTOR.
- SIGNAL TIMING/TIMING ADJUSTMENTS REQUESTED BY THE RESIDENT ENGINEER SHALL BE ACCOMPLISHED WITHIN A 48 HOUR PERIOD AND PAYMENT SHALL BE SUBSIDIARY TO THE TRAFFIC SIGNAL ITEM. THE ALL-RED CLEARANCE INTERVAL IS BASED ON AN ASSUMED SPEED OF 10-20 MPH. THE RESIDENT ENGINEER SHALL MAKE SEVERAL TRIAL RUNS TO DETERMINE THE PROPER ALL-RED CLEARANCE INTERVAL.
- SIGNAL FACES SHALL CONSIST OF 12" LENSES. (RED, YELLOW, AND GREEN)
- THE BOTTOM OF THE HOUSING OF A SIGNAL FACE SUSPENDED OVER A ROADWAY SHALL NOT BE LESS THAN 16 1/2 FEET NOR MORE THAN 19 FEET ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY. THE BOTTOM OF A SIGNAL FACE NOT MOUNTED OVER A ROADWAY, SHALL NOT BE LESS THAN 8 FEET NOR MORE THAN 15 FEET ABOVE THE ROADWAY. CAUTION SHOULD BE USED TO INSURE COMPLIANCE WITH THE HEIGHT REQUIREMENTS IN THE EVENT THE NEW APPROACH GRADES DIFFER SIGNIFICANTLY FROM THE OLD ROAD GRADE.
- SIGNAL FACES FOR ANY ONE APPROACH SHALL NOT BE LESS THAN 8 FEET APART MEASURED HORIZONTALLY BETWEEN CENTER OF FACES.
- SIGNAL HEADS MAY BE HUNG ON A SPAN WIRE OR ON A CANTILEVER MAST ARM. AT LEAST ONE SIGNAL HEAD SHALL BE UNMISTAKABLY IN LINE WITH THE CENTER OF APPROACHING TRAFFIC AT ALL TIMES. THE SECOND SIGNAL HEAD MAY BE POST MOUNTED, LOCATED AT A DISTANCE NO GREATER THAN 14 1/2 FEET FROM THE CENTER OF THE APPROACH LANE WHEN THE STOP BAR IS 40 FEET FROM THE SIGNAL HEAD. CONSULT THE M.U.T.C.D. FOR ADDITIONAL INFORMATION CONCERNING SIGNAL PLACEMENT.
- SIGNAL HEAD PLACEMENT IS CRITICAL. HEADS SHALL BE ADJUSTED TO REFLECT LANE LOCATION CHANGES.
- THE SIGNAL SYSTEM SHALL CONSIST OF POLES, SIGNS AND POSTS, WARNING SIGN, LUMINAIRES, FLASHING BEACONS AND SIGNAL EQUIPMENT TO PROVIDE FOR AN ADEQUATE DESIGN. IT ALSO INCLUDES PERMITS AND COST ASSOCIATED WITH PROVIDING ELECTRICAL POWER.
- THE CONTRACTOR SHALL PROVIDE AN ACTUATED CONTROLLER. THE APPROACHES NOTED SHALL HAVE A TEMPORARY VEHICLE DETECTOR. THE CONTROLLER, DETECTOR AND ALL OTHER SIGNAL EQUIPMENT SHALL MEET OR EXCEED ALL NEMA STANDARDS.
- ON SEMI-ACTUATED SIGNAL, PARTICULARLY WITH LONG BRIDGES, THE CONTROLLER SHOULD BE LOCATED ON THE SAME SIDE OF THE BRIDGE AS THE DETECTOR.
- INTERVAL TIMING SHOWN IN SECONDS.
- INTERCONNECT BETWEEN SIGNAL POLES BY WHATEVER MEANS POSSIBLE OR CONVENIENT TO PROVIDE FOR A SAFE INSTALLATION.
- PLACE TEMPORARY POLES BEHIND GUARDRAIL WHERE POSSIBLE.
- POLES SUPPORTING SPAN WIRES AND/OR MAST ARMS SHALL BE ADEQUATELY BRACED OR GUYED AND SHALL NOT BE PLACED SO AS TO CREATE A HAZARD TO THE TRAVELLING PUBLIC.
- ALL TEMPORARY SIGNAL EQUIPMENT, SIGNS, ETC., SHALL BELONG TO THE CONTRACTOR AT THE END OF THE PROJECT AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR ITS REMOVAL, INCLUDING ANY TEMPORARY PAVEMENT MARKINGS, UTILITY POLES, WIRES, ETC.
- A 400 FOOT WATT MERCURY OR 200 WATT HPS LUMINAIRE AND MAST ARM SHALL BE PROVIDED ON A POLE ON EACH APPROACH AT A MOUNTING HEIGHT OF 30' ABOVE ROADWAY CENTERLINE. THE INTENT IS TO LIGHT UP THE AREA AROUND THE SIGNAL HEADS AND STOP BAR FOR INCREASED VISIBILITY. THE RESIDENT ENGINEER SHALL DETERMINE THE ADEQUACY OF THE LIGHTING AND DIRECT CHANGES IF THE LIGHTING IS INSUFFICIENT.
- STOP BARS SHALL BE LOCATED A MINIMUM OF 40' AND A MAXIMUM OF 120' FROM THE NEAREST SIGNAL HEAD.
- PAYMENT FOR THE VEHICLE LOOP DETECTOR ITEM SHALL BE ITEM 678.42, 'TEMPORARY DETECTOR' (EACH).
- THE SIGNS AND ASSOCIATED POSTS AS SHOWN ON THIS SHEET AND NOTED BELOW ARE SUBSIDIARY TO THE TEMPORARY TRAFFIC SIGNAL SYSTEM ('STOP HERE ON RED', 'SIGNAL AHEAD', 'NO PASSING ZONE', AND 'TO GET GREEN LIGHT' ETC.) THE TEMPORARY STOP BARS SHOULD BE PAID UNDER THE TEMPORARY 24" STOP BAR ITEM.
- SEE STD. E-140 FOR 'STOP HERE ON RED' SIGN DETAIL AND E-101 FOR 'SIGNAL AHEAD' SYMBOL SIGN. THE 'SIGNAL AHEAD' SIGN SHALL HAVE AN ORANGE BACKGROUND (REFLECTORIZED). SEE STANDARD E-12IF FOR SIGN PLACEMENT. SEE STANDARD E-171A AND E-172 FOR ADDITIONAL INFORMATION ON SIGNALS AND DETECTORS.
- A 'SIGNAL AHEAD' SIGN SHALL BE PLACED AT LEAST 750' FROM THE SIGNAL OR AT A POSITION TO BE DETERMINED BY THE ENGINEER. ALL POSTS SHALL BE CONSIDERED AS SUBSIDIARY TO THE TEMPORARY TRAFFIC SIGNAL ITEM.
- THE 'NO PASSING' SIGN SHALL BE USED TO PREVENT PASSING FOR 750' IN ADVANCE OF THE STOP BAR. THE SIGN SHALL BE PER STANDARD E-102.
- ALL ELECTRICAL WORK SHALL MEET THE REQUIREMENTS OF THE NATIONAL ELECTRICAL CODE AND STATE INSPECTOR.
- ALL SIGNAL RELATED SIGNS SHALL BE REMOVED OR COVERED WHEN THE SIGNALS ARE NOT OPERATING.
- REMOVE EXISTING CENTERLINE PAVEMENT MARKINGS FROM THE STOP BAR TO THE BRIDGE DECK ON THE APPROACHES. THIS SHALL BE PAID FOR AS 'REMOVAL OF EXISTING PAVEMENT MARKINGS'. REPLACEMENT OF THE LINE WILL BE PAID FOR UNDER THE ITEM 646.41 - 'DURABLE 4" YELLOW LINE'.
- THE 'TO GET GREEN LIGHT' SIGN IS TO BE USED ONLY ON APPROACHES WITH VEHICLE DETECTORS.
- IN SITUATIONS WHERE EXISTING PASSING ZONES EXTEND THROUGH THE AREA BETWEEN THE STOP BAR AND THE 'NO PASSING ZONE' SIGN, THEN TEMPORARY DOUBLE YELLOW LINES SHALL BE INSTALLED FROM THE STOP BAR TO THE 'NO PASSING ZONE' SIGN. THESE MARKINGS SHALL BE PAID UNDER THE 'TEMPORARY 4' YELLOW LINE' ITEM.
- PAYMENT FOR TEMPORARY BARRIER USED SHALL BE MADE UNDER THE APPROPRIATE ITEM.
- ANY ADDITIONAL TEMPORARY PAVEMENT MARKINGS REQUIRED SHALL BE PAID FOR UNDER THE PAY ITEM 'TEMPORARY 4' YELLOW LINE' OR 'TEMPORARY 4' WHITE LINE'.

/traf/misc/onewayrd.dgn : onewayrd.i

ORIGINAL PREPARED NOV. 1986

DATE	REVISIONS	BY
5/88	LENGTHEN LOOPS, ADDED 'GREEN LIGHT' SIGN, UPDATE STD.'S	DSP
3/91	ADD PAVEMENT MARKING NOTES & PHASE DIAGRAM	DSP
6/93	DELETE DETOUR & CHG. NOTES	RPD
9/95	MAJOR REVISIONS	EGF

**ONE-WAY ROADWAY WITH TEMPORARY TRAFFIC SIGNAL**

BRIDGE NO. 8 (US 4)

PREPARED BY M. PELOQUIN DATE 6/98  
 CHECKED BY C. MEUNIER DATE 11/98  
 DESIGN SUPERVISOR ROGERS DATE 11/98  
 PROJ. HARTFORD-SHARON-ROYALTON (MIRO89-18)  
**SHEET BR 103**  
 SHEET 60 OF 260 SHEETS