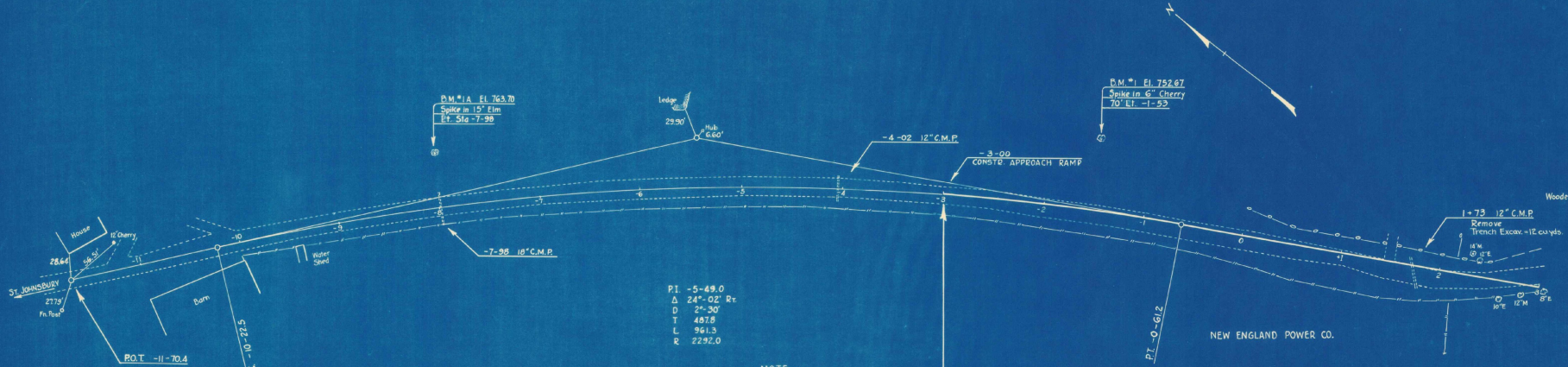






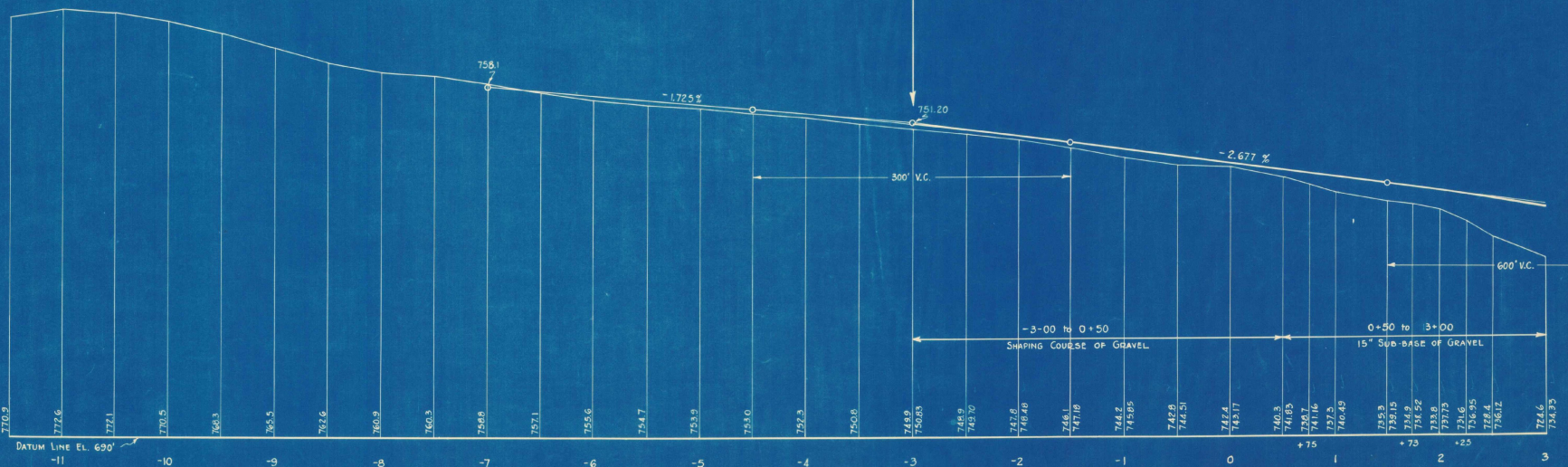
| FILE NO. | DATE  | REV. | BY   | CHKD. | DATE |
|----------|-------|------|------|-------|------|
| 9        | VT-HM | 263  | 1933 | 3     | 27   |



P.I. -5-49.0  
 Δ 24'-02" Rr.  
 D 24-30  
 T 407.9  
 L 961.3  
 R 2292.0

- NOTE -  
 EXCESS CUT TO BE USED TO WEST OF PROJECT  
 TO IMPROVE LINE AND GRADE.

STATION -3-00  
 BEGINNING OF PROJECT



REVISIONS AFTER PROPOSAL  
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| FILE NO. | DATE  | YEAR | SHEET NO. | TOTAL SHEETS |
| 0        | VT-NH | 263  | 1933      | 6            |

SHEET TOTAL  
 CUT = 127 Cu.Yds.  
 FILL = 1410 " "  
 B.C. = 303 " "

C = - Cu.Yds.  
 F = 756 " "

SHAPING COURSE  
 15' SUB-BASE OF

OF GRAVEL ENDS  
 GRAVEL BEGINS

C = 20. Cu.Yds.  
 F = 12. " "  
 B.C. = 44. " "

C = -  
 F = 13.  
 B.C. = 45.

C = -  
 F = 18.  
 B.C. = 53.

C = 6.  
 F = 15.  
 B.C. = 47.

C = 6.  
 F = 10.  
 B.C. = 46.

C = -  
 F = 11.  
 B.C. = 41.

C = 10. Cu.Yds.  
 F = 24. " "  
 B.C. = 24. " "

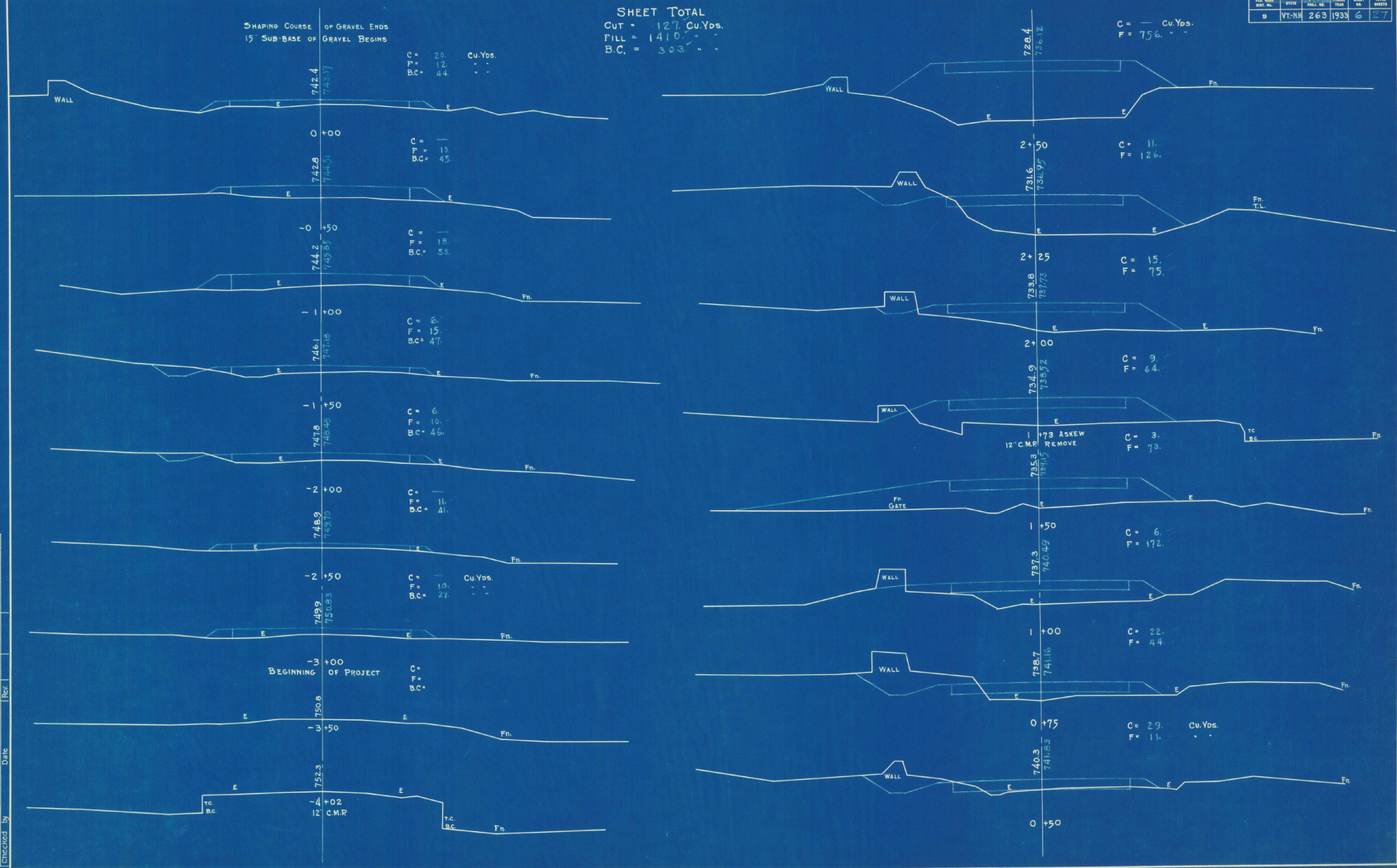
C = -  
 F = -  
 B.C. = -

C = 29. Cu.Yds.  
 F = 11. " "

REVISIONS AFTER PROPOSAL

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Traced by Christensen Date 9-7-34.  
 Checked by \_\_\_\_\_ Date \_\_\_\_\_





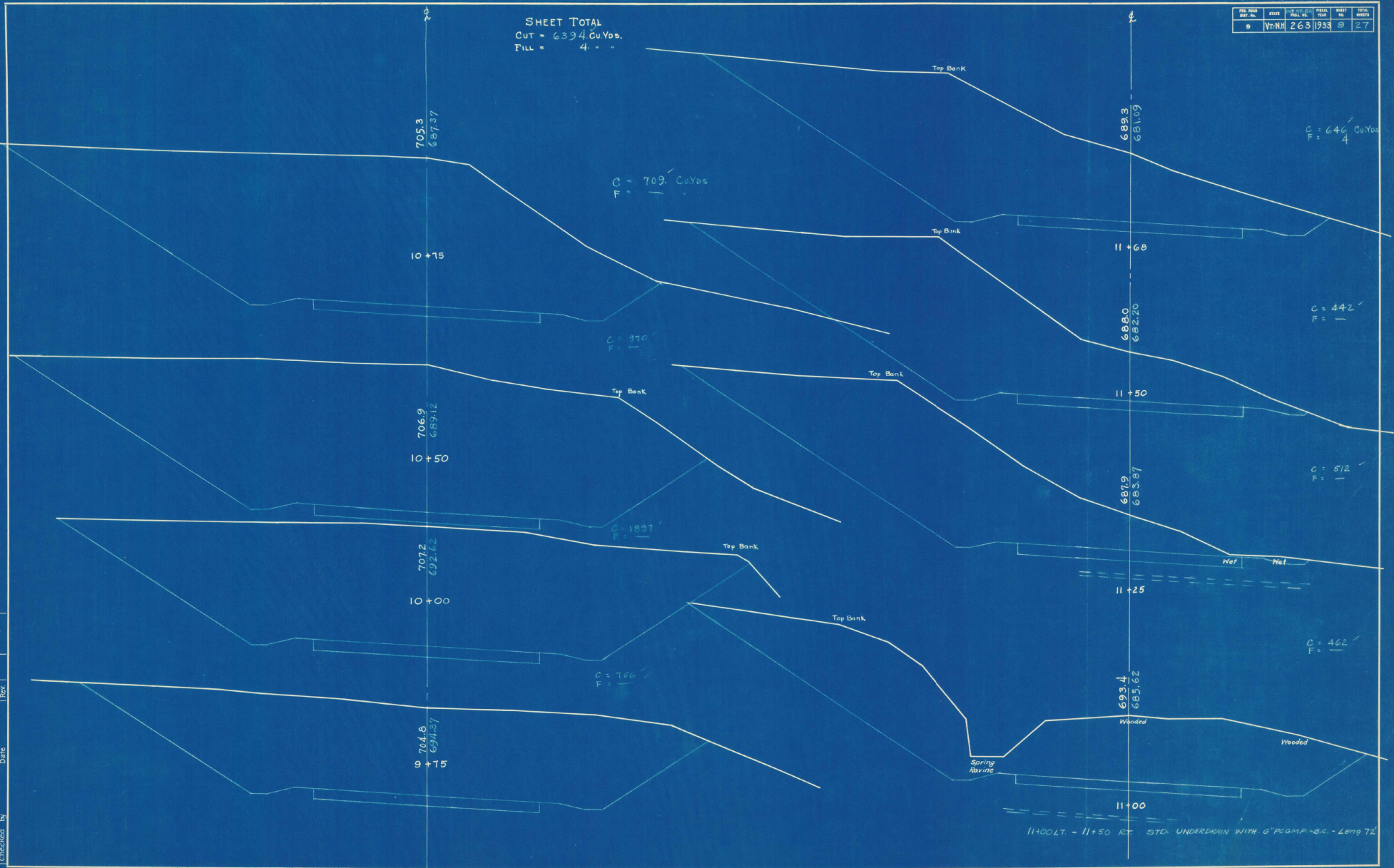


| FILE NO. | DATE  | BY  | SCALE | SHEET NO. | TOTAL SHEETS |
|----------|-------|-----|-------|-----------|--------------|
| B        | Vt-NH | 263 | 1933  | 9         | 27           |

SHEET TOTAL  
 CUT = 6394 Cu.Yds.  
 FILL = 4 - -

| REVISED | DATE | BY | REASON |
|---------|------|----|--------|
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Traced by W.L. ... Date 5-1-34  
 Checked by   Date  



11+00 CLT. - 11+50 RT STD. UNDERDRAIN WITH 6" PCAMP-B.C. - Long 72











| SHEET NO. | TOTAL SHEETS | DATE   | BY   | CHKD. | DATE   |
|-----------|--------------|--------|------|-------|--------|
| 9         | 263          | 3/5/34 | W.S. | W.S.  | 3/5/34 |



SCALE 1" = 5'-0"

STATE OF NEW HAMPSHIRE  
 HIGHWAY DEPARTMENT  
 TOWN: LITTLETON  
 PROJECT:  
 LOCATION: LITTLETON, N.H. WATERFORD, VT.  
 ROAD: THEODORE ROOSEVELT  
 STREAM: CONNECTICUT RIVER  
 DESIGNED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 TRACKED BY: F.W.S. DATE: 5-5-34  
 CHECKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 SHEET 15 OF 27 SHEETS

| Date | Station | From | To |
|------|---------|------|----|
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Prepared by \_\_\_\_\_ Date \_\_\_\_\_  
 Checked by \_\_\_\_\_ Date \_\_\_\_\_

| FILE NO. | DATE     | BY   | CHKD. | DATE    | BY   | CHKD. |
|----------|----------|------|-------|---------|------|-------|
| 9        | 11-21-39 | W.S. | W.S.  | 12-1-39 | W.S. | W.S.  |



SCALE 1"=5'-0"

STATE OF NEW HAMPSHIRE.  
 HIGHWAY DEPARTMENT  
 TOWN: LITTLETON.  
 PROJECT: \_\_\_\_\_  
 LOCATION: LITTLETON N.H. WATERBURY Vt.  
 ROAD: THEODORE ROOSEVELT.  
 STREAM: CONNECTICUT RIVER  
 DESIGNED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 TRACED BY: E.W.G.S. DATE: 5-5-39.  
 CHECKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 SHEET 16 OF 27 SHEETS.

| Date | Section | From | To |
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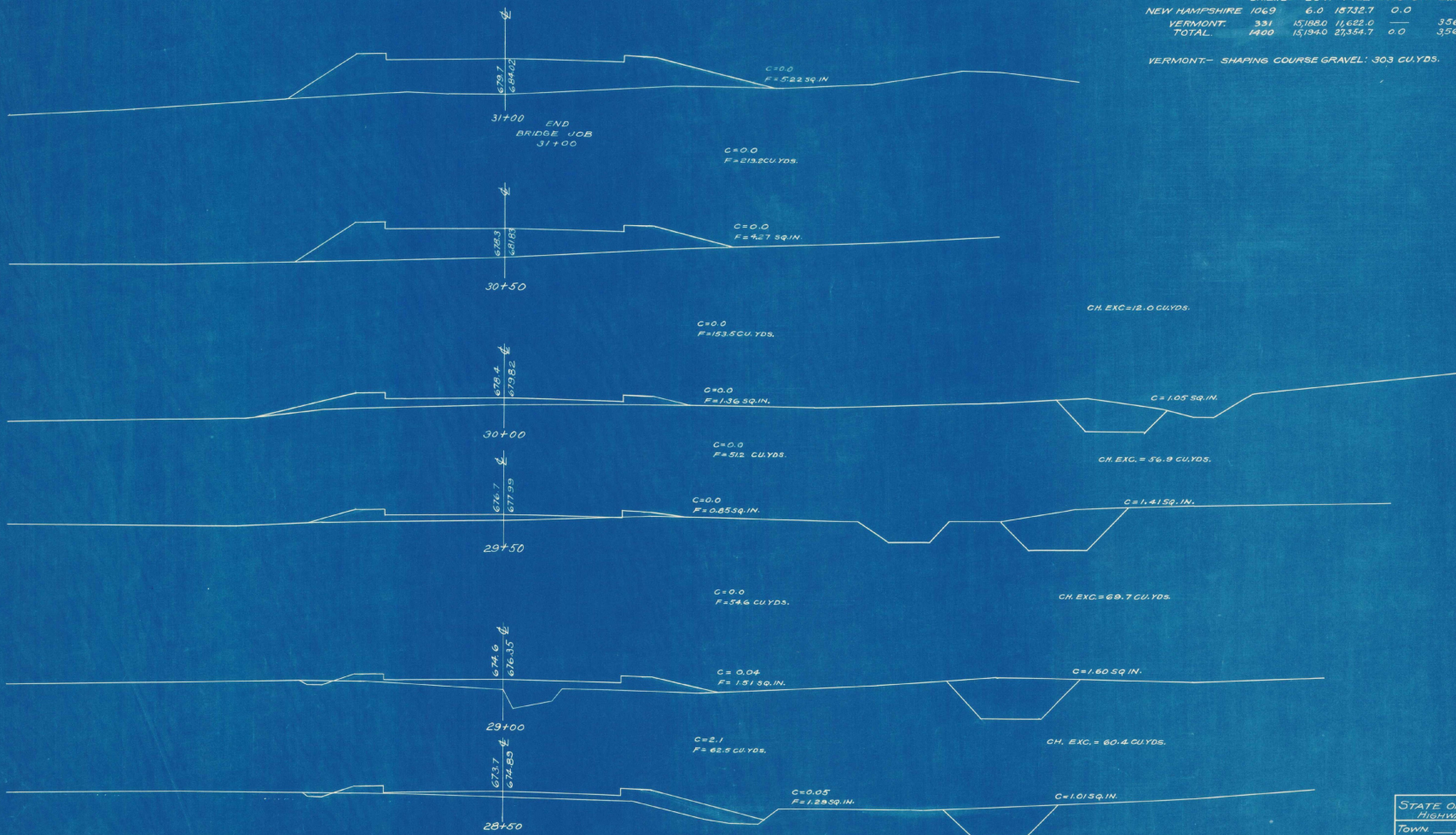
Traced by \_\_\_\_\_ Date \_\_\_\_\_  
 Checked by \_\_\_\_\_ Date \_\_\_\_\_

| P.L. NO. | DATE  | IN. ST. BY | ISS. BY | SHEET NO. | TOTAL SHEETS |
|----------|-------|------------|---------|-----------|--------------|
| 9        | VT-NH | 263        | 1934    | 17        | 27           |

GRAND TOTALS.

|               | CH. EXC. | CUT     | FILL     | SHRINKAGE FACTOR | EXCESS EXC. | BORROW           |
|---------------|----------|---------|----------|------------------|-------------|------------------|
| NEW HAMPSHIRE | 1069     | 6.0     | 18732.7  | 0.0              | 0.0         | 14657.6 CU. YDS. |
| VERMONT       | 381      | 15188.0 | 11,622.0 |                  | 3566.0      |                  |
| TOTAL         | 1450     | 15194.0 | 27,354.7 | 0.0              | 3566.0      | 14657.6          |

VERMONT— SHAPING COURSE GRAVEL: 303 CU. YDS.



SCALE 1" = 5'-0"

STATE OF NEW HAMPSHIRE  
 HIGHWAY DEPARTMENT.  
 TOWN — LITTLETON  
 PROJECT —  
 LOCATION, LITTLETON, N.H. — WATERBURY  
 ROAD — THEODORE ROOSEVELT  
 STREAM, CONNECTICUT RIVER R.  
 DESIGNED BY — DATE —  
 DRAWN BY — DATE —  
 TRACED BY, E.W.G.S. DATE 5-9-34  
 CHECKED BY — DATE —  
 SHEET 17 OF 27 SHEETS.

|       |         |      |    |
|-------|---------|------|----|
| Date  | Station | From | To |
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| Rev.  |         |      |    |
| Rev.  |         |      |    |
| Orig. |         |      |    |
| Orig. |         |      |    |
| Rev.  |         |      |    |
| Date  |         |      |    |
| Date  |         |      |    |

Traced by \_\_\_\_\_ Date \_\_\_\_\_  
 Checked by \_\_\_\_\_ Date \_\_\_\_\_

| FED. ROAD DIST. NO. | STATE    | PROJECT NO. | PROJECT YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|----------|-------------|--------------|-----------|--------------|
| 9                   | Vt.-N.H. | 263         | 1934         | 1         | 30           |

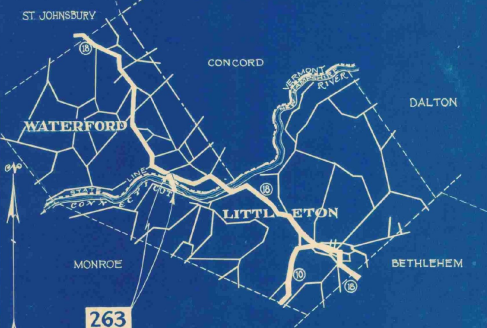
### INDEX OF SHEETS

| SHEET NO | TITLE PAGE             | SHEET NO. | 19-20 BORINGS          |
|----------|------------------------|-----------|------------------------|
| "        | " 2 TYPICAL SECTIONS   | "         | " 21-25 BRIDGE DETAILS |
| "        | " 3-5 PLAN AND PROFILE | "         | " 26 N.H. STANDARDS    |
| "        | " 6-17 CROSS SECTIONS  | "         | " 27-30 VT. STANDARDS  |
| "        | " 18 CONTOUR MAP       | "         | "                      |

STATE OF NEW HAMPSHIRE  
STATE HIGHWAY DEPARTMENT

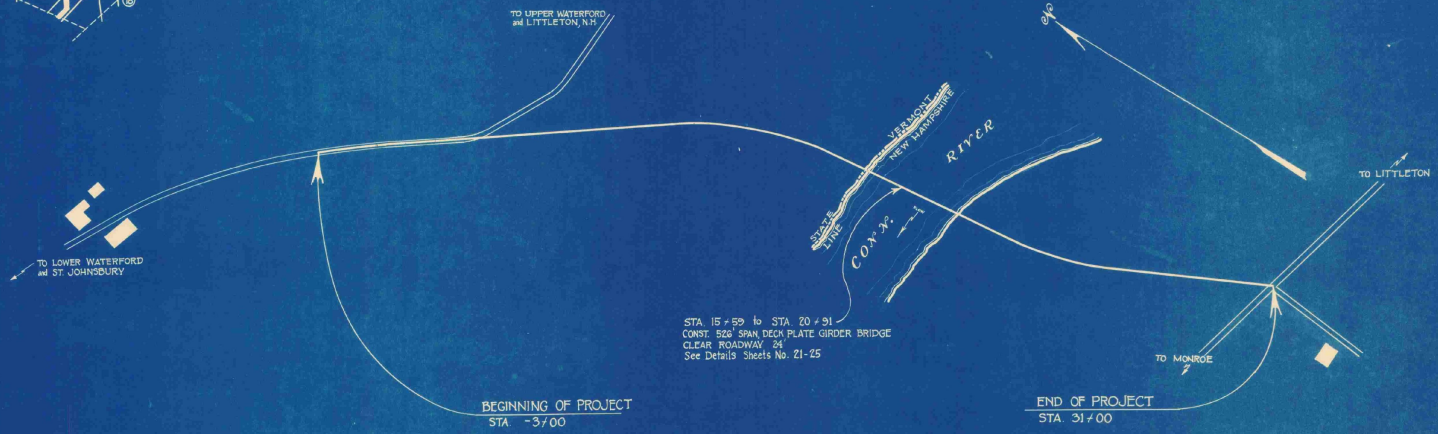
## PLAN AND PROFILE OF PROPOSED INTERSTATE BRIDGE PROJECT No 263

THEODORE ROOSEVELT HIGHWAY



SCALES ( PLAN, 1"=50'-0"  
PROFILE, VERT. 1"=10'-0" HOR. 1"=50'-0"  
CROSS SECTION, VERT. 1"=5'-0" HOR. 1"=5'-0"

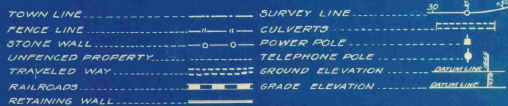
NOTE. This project to be constructed in accordance with standard specifications on file with the Bureau of Public Roads, and Special Provisions attached to Proposal.



STA 15+59 to STA 20+91  
CONST. 526' SPAN DECK PLATE GIRDER BRIDGE  
CLEAR ROADWAY 24'  
See Details' Sheets No. 21-25

## TOWNS OF WATERFORD, VT. and LITTLETON, N.H. CALEDONIA and GRAFTON COUNTIES

### CONVENTIONAL SIGNS



LAYOUT  
SCALE 1"= 300'

Plans drawn by .....  
Plans traced by .....  
Plans checked by .....

APPROVED: *H.E. Sargent*  
VT. COMMISSIONER OF HIGHWAYS

APPROVED: *H.C. Scott*  
N.H. HIGHWAY COMMISSIONER

RECOMMENDED FOR APPROVAL: \_\_\_\_\_

DISTRICT ENGINEER, BUREAU OF PUBLIC ROADS.

RECOMMENDED FOR APPROVAL: \_\_\_\_\_

CHIEF ENGINEER, BUREAU OF PUBLIC ROADS.

APPROVED: \_\_\_\_\_

DIRECTOR, BUREAU OF PUBLIC ROADS.

WATERFORD, VT.  
DALTON, N.H.  
N.H. TO Vt.  
+ BUILT  
Clerk Frank W. Bennett





Guilford  
Brandy Bridge

2/11/37  
WHD

Abuts. Class C.

$$2.5' \times 4' \times 31' \times \frac{1}{27} \times 2 = 23 \text{ C.Y. say } 30 \text{ C.Y.}$$

Superstr.

$$\text{Class A} = 48.3 \overset{\text{Reel}}{+ 15} = 63 \text{ C.Y.}$$

$$\text{Reinf.} = 11,169$$

$$\text{Str. Steel} = 107,479$$

Str. Exc.

$$\#1 \quad 6' \times 2' \times 31' \times \frac{1}{27} = 15 \text{ C.Y.}$$

$$\#2 \quad 6' \times 2' \times 31' \times \frac{1}{27} = \frac{15}{30} \text{ say } 50 \text{ C.Y.}$$

CSR 260 L.F

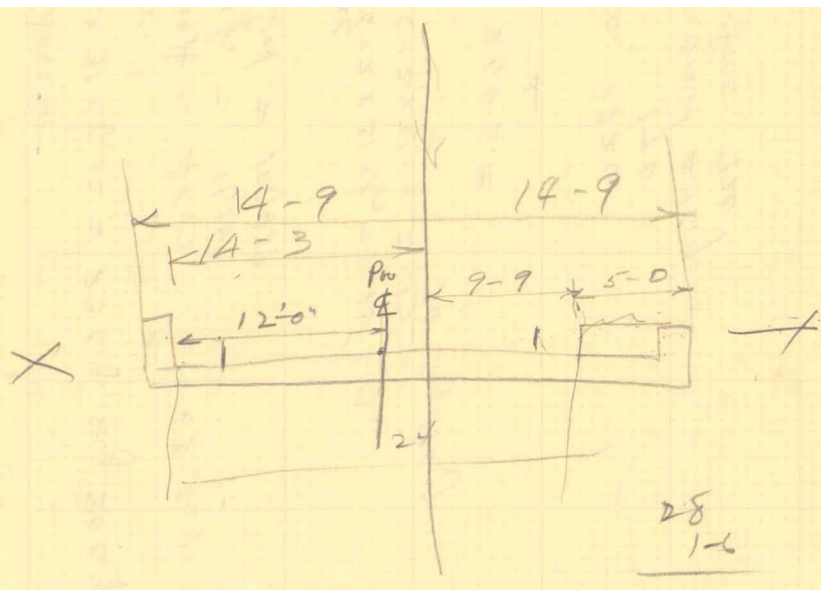
Anchors 4

Conc Exc. 1326

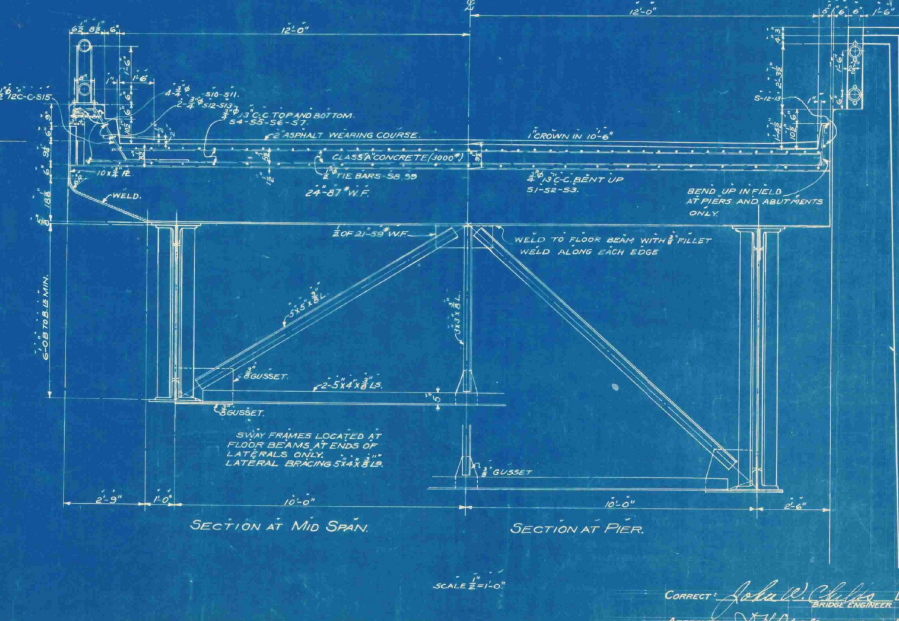
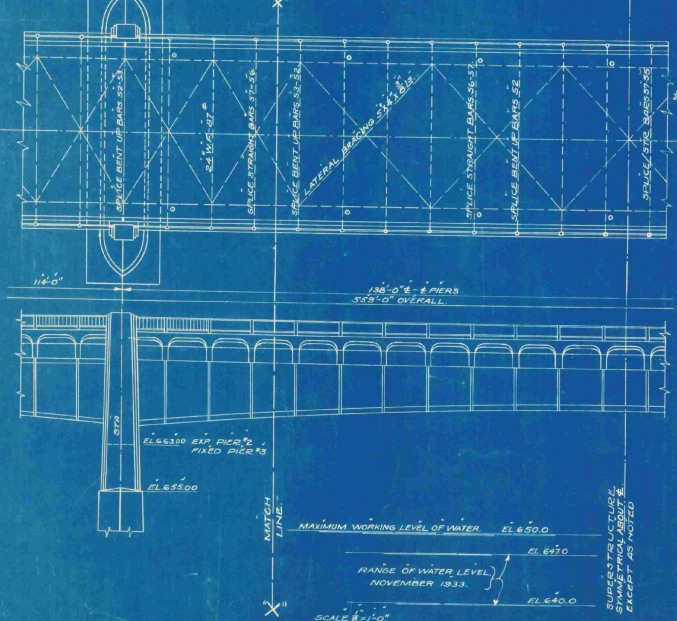
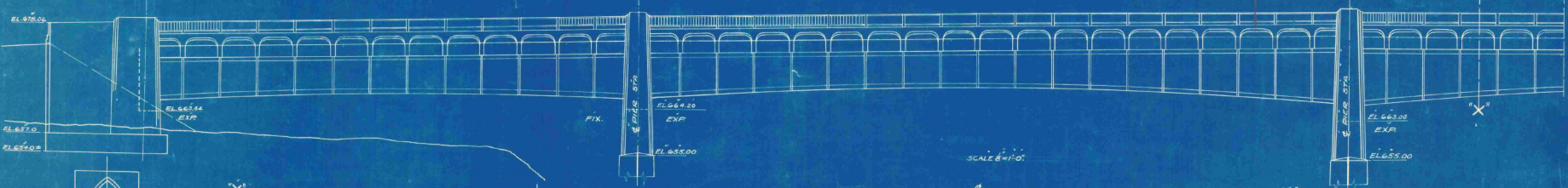
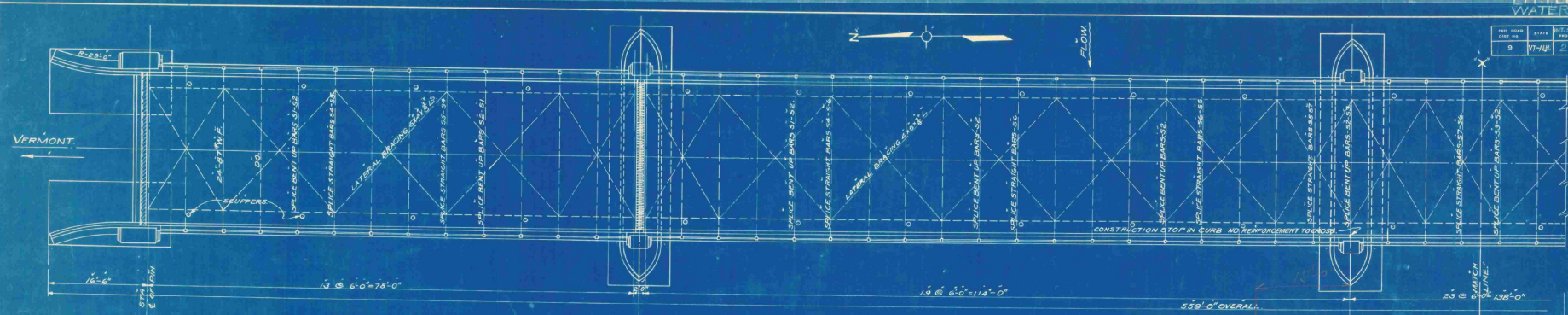
Rock " 170

Subbase grad 416 C.Y.

grad. surface 320



| DESIGN DIST NO. | STATE | DIST. NO. | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|-----------------|-------|-----------|-------------|-----------|--------------|
| 9               | VT-NH | 263       | 1334        | 21        | 27           |



| SUPERSTRUCTURE QUANTITIES     |             |
|-------------------------------|-------------|
| 27 CLASS "A" CONCRETE (3000#) | 455 CU YD   |
| 29 REINFORCING STEEL          | 82,675 LBS  |
| 30 STRUCTURAL STEEL           | 808,000 LBS |
| 72 ASPHALT WEARING COURSE     | 1391 SQ YD  |
| 85 STEEL BRIDGE RAIL          | 1023 LNF    |

DESIGN LOADS THIS SPECIFICATION AS SHOWN, MINIMUM  
PIERS 8" HOLES & GENERAL REPAIRS, NOT REQUIRED.  
PAINT: 1 SHOP COAT RED LEAD AND OIL  
1 FIELD COAT APPROVED PAINT-SEE NOTE BELOW.  
THE CONTRACTOR SHALL DELIVER TO THE STATE  
TENSILES OF SHOP DRAWINGS BEFORE  
FINAL PAYMENT IS MADE  
NO CONCRETE SHALL BE PLACED UNTIL  
REINFORCING STEEL HAS BEEN CHECKED  
BY ENGINEER  
ALL EXPOSED CORNERS TO BE CHAMFERED 1/4" EXCEPT  
AS NOTED  
CURB CORNERS TO BE ROUNDED TO 1" RADIUS  
MINIMUM CEMENT FACTOR CLASS "A" CONCRETE "101"

NOTE:  
FIELD COAT TO BE GREEN, OF A SHADE  
APPROXIMATELY THAT OF WEATHERED COPPER  
AS MANUFACTURED BY THE INDUSTRIAL  
DARTER & CO. PAINT CO. OF HAYSVILLE, PENN., OR EQUIVALENT.  
SAMPLES MAY BE SEEN AT THE N.H. HIGHWAY LAB.

STATE OF NEW HAMPSHIRE  
HIGHWAY DEPARTMENT  
TOWN: LITTLETON  
PROJECT:  
LOCATION:  
ROAD: I.R. HIGHWAY  
STREAM: CONNECTICUT RIVER  
DESIGNED BY: R.D.F. DATE: 1-24-34  
DRAWN BY: M.E.S. DATE: 11-23-33  
CHECKED BY: H.R.H. DATE: 2-15-34  
APPROVED: *[Signature]* DATE: 5-12-34  
CHIEF ENGINEER

SHEET 21 OF 27 SHEETS

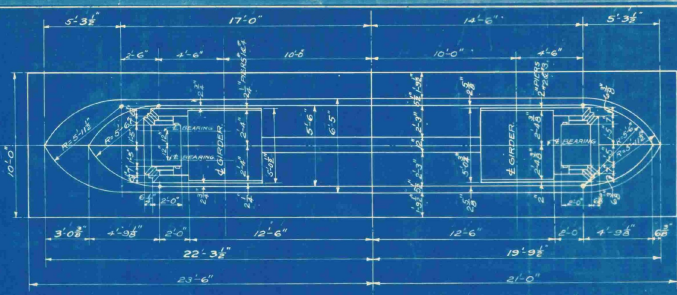
DATE: 5-12-34  
CHECKED BY: H.R.H.  
DATE: 2-15-34  
DRAWN BY: M.E.S.  
DATE: 11-23-33  
DESIGNED BY: R.D.F.  
DATE: 1-24-34





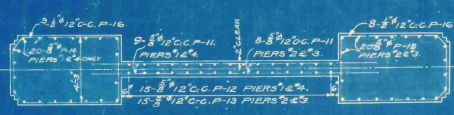


|            |          |      |       |           |              |
|------------|----------|------|-------|-----------|--------------|
| DESIGN NO. | DRAWN BY | DATE | SCALE | SHEET NO. | TOTAL SHEETS |
| 9          | NHVT     | 263  | 1934  | 25        | 27           |

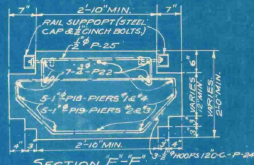


HALF PLAN-UP STREAM END  
SCALE 1/4"=1'-0"

HALF PLAN-DOWN STREAM END  
SCALE 1/4"=1'-0"



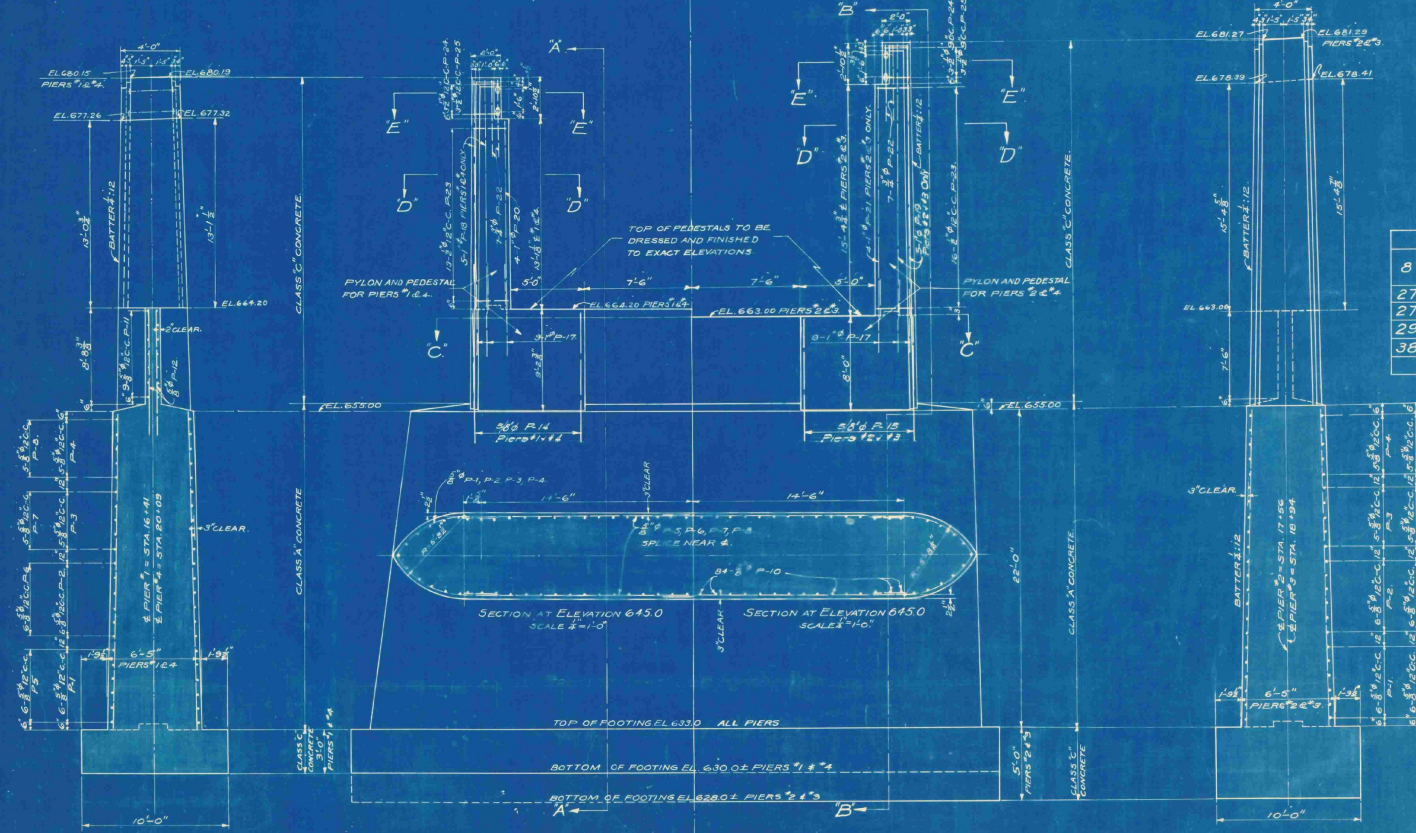
SECTION C-C  
SCALE 1/4"=1'-0"



SECTION E-E  
SCALE 1/4"=1'-0"



SECTION D-D  
SCALE 1/4"=1'-0"



SECTION A-A  
SCALE 1/4"=1'-0"

PIERS 1 AND 4  
HALF ELEVATION-UP STREAM END  
SCALE 1/4"=1'-0"

PIERS 2 AND 3  
HALF ELEVATION-DOWN STREAM END  
SCALE 1/4"=1'-0"

SECTION B-B  
SCALE 1/4"=1'-0"

|    | ESTIMATED QUANTITIES.             | PIER NO.1  | PIER NO.2  | PIER NO.3  | PIER NO.4  |
|----|-----------------------------------|------------|------------|------------|------------|
| 8  | STRUCTURE EXCAVATION EARTH        | 180 CU YDS | 110 CU YDS | 110 CU YDS | 210 CU YDS |
|    | LEDGE                             | 50 CU YDS  | 15 CU YDS  | 15 CU YDS  | 15 CU YDS  |
| 27 | CONCRETE CLASS A                  | 102 CU YDS | 158 CU YDS | 158 CU YDS | 152 CU YDS |
| 27 | CONCRETE CLASS C                  | 92 CU YDS  | 168 CU YDS | 166 CU YDS | 92 CU YDS  |
| 29 | REINFORCING STEEL                 | 7033 LBS   | 7039 LBS   | 7039 LBS   | 7033 LBS   |
| 38 | RIP RAP FOR FOUNDATION PROTECTION | 25 CU YDS  | 25 CU YDS  | 25 CU YDS  | 25 CU YDS  |

NOTES:  
ALL PIER FOOTINGS TO BE CARRIED TO SUITABLE FOUNDATION.  
NO CONCRETE TO BE PLACED UNTIL AFTER REINFORCING STEEL HAS BEEN CHECKED BY THE ENGINEER.  
EXPOSED CORNERS OF CONCRETE TO BE CHAMFERED EXCEPT PYLONS.  
BACKFILL AROUND PIERS WITH RIP RAP FOR FOUNDATION PROTECTION.  
CLASS A CONCRETE TO HAVE MINIMUM CEMENT FACTOR OF 1.61  
CLASS C CONCRETE TO HAVE MINIMUM CEMENT FACTOR OF 1.30

STATE OF NEW HAMPSHIRE  
HIGHWAY DEPARTMENT  
TOWN - LITTLETON  
PROJECT  
LOCATION LITTLETON, N.H. - WATERFORD, V.T.  
ROAD - T.R. HIGHWAY  
STREAM, CONNECTICUT RIVER  
DESIGNED BY M.A.C. DATE 3-1-34  
DRAWN BY E.B. DATE 3-22-34  
TRACED BY E.W.G.S. DATE 3-3-34  
CHECKED BY W.B.S. DATE 3-3-34  
SHEET 25 OF 27 SHEETS

DATE Station From of to  
 ORG. ORG. ORG. ORG. ORG. ORG.  
 DATE DATE DATE DATE DATE DATE  
 CHECKED BY



| DES. YEAR | DESIGN | PLAN NO. | SECTION | SHEET | TOTAL SHEETS |
|-----------|--------|----------|---------|-------|--------------|
| 19        | N. H.  |          |         | 1     |              |

### INDEX OF SHEETS

| SHEET NO. | TITLE PAGE                       |
|-----------|----------------------------------|
| " "       | 2 TYPICAL SECTION OF IMPROVEMENT |
| " "       | PLAN AND PROFILE                 |
| " "       | CROSS SECTIONS                   |
| " "       | STANDARDS                        |

STATE OF NEW HAMPSHIRE  
STATE HIGHWAY DEPARTMENT

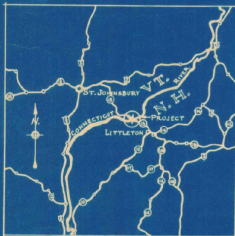
## PLAN AND PROFILE OF PROPOSED U.S. PUBLIC WORKS PROJECT

NO - N.R.H.  
INTERSTATE BRIDGE PROJECT  
T. R. HIGHWAY.

(PLAN, 1"=50' 0")  
SCALES: (PROFILE VERT. 1"=10' 0" HOR. 1"=80' 0")  
(CROSS SECTION VERT. 1"=5' 0" HOR. 1"=5' 0")

NOTE. This project to be constructed in accordance with standard specifications on file with the Bureau of Public Roads, and special provisions attached to proposal.

### KEY MAP



### CONVENTIONAL SIGNS

|                   |                  |    |     |
|-------------------|------------------|----|-----|
| TOWN LINE         | SURVEY LINE      | 30 | 140 |
| FENCE LINE        | CULVERTS         |    |     |
| STONE WALL        | POWER POLE       |    |     |
| UNFENCED PROPERTY | TELEPHONE POLE   |    |     |
| TRAVELED WAY      | GROUND ELEVATION |    |     |
| RAILROADS         | GRADE ELEVATION  |    |     |
| RETAINING WALL    |                  |    |     |

Plans drawn by .....  
Plans traced by .....  
Plans checked by .....

### LAYOUT SCALE 1"

TOWNS OF - WATERFORD, VT. & LITTLETON, N.H.  
COUNTIES OF - CALEDONIA, VT. & GRAFTON, N.H.

APPROVED: \_\_\_\_\_  
HIGHWAY COMMISSIONER

RECOMMENDED FOR APPROVAL: \_\_\_\_\_

DISTRICT ENGINEER, BUREAU OF PUBLIC ROADS

RECOMMENDED FOR APPROVAL: \_\_\_\_\_

CHIEF ENGINEER, BUREAU OF PUBLIC ROADS

APPROVED: \_\_\_\_\_

DIRECTOR, BUREAU OF PUBLIC ROADS



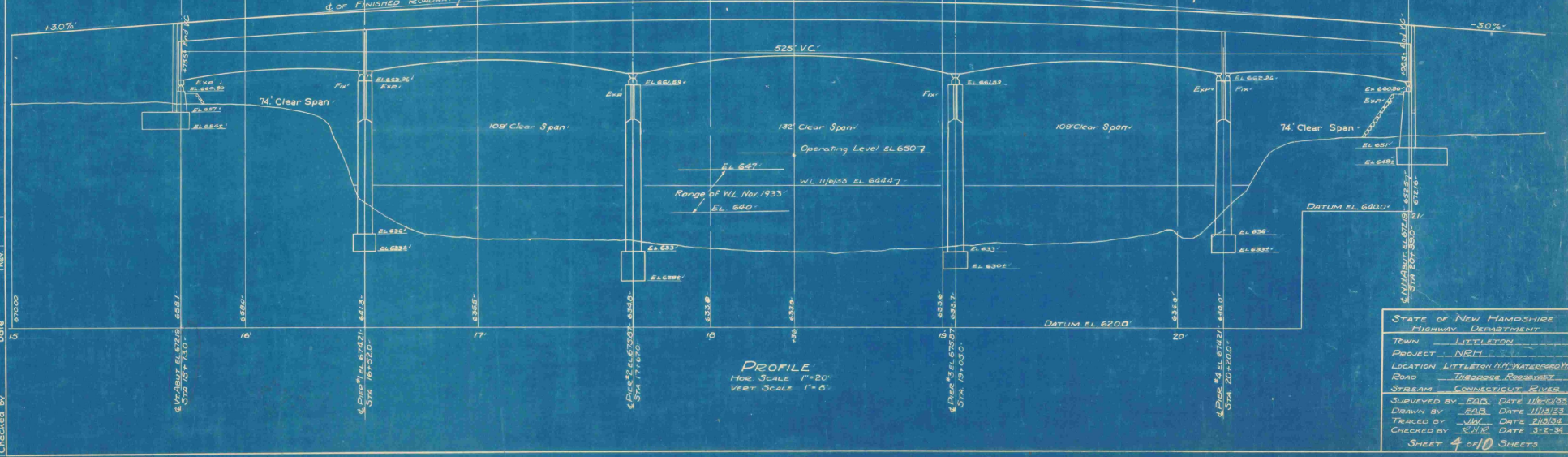
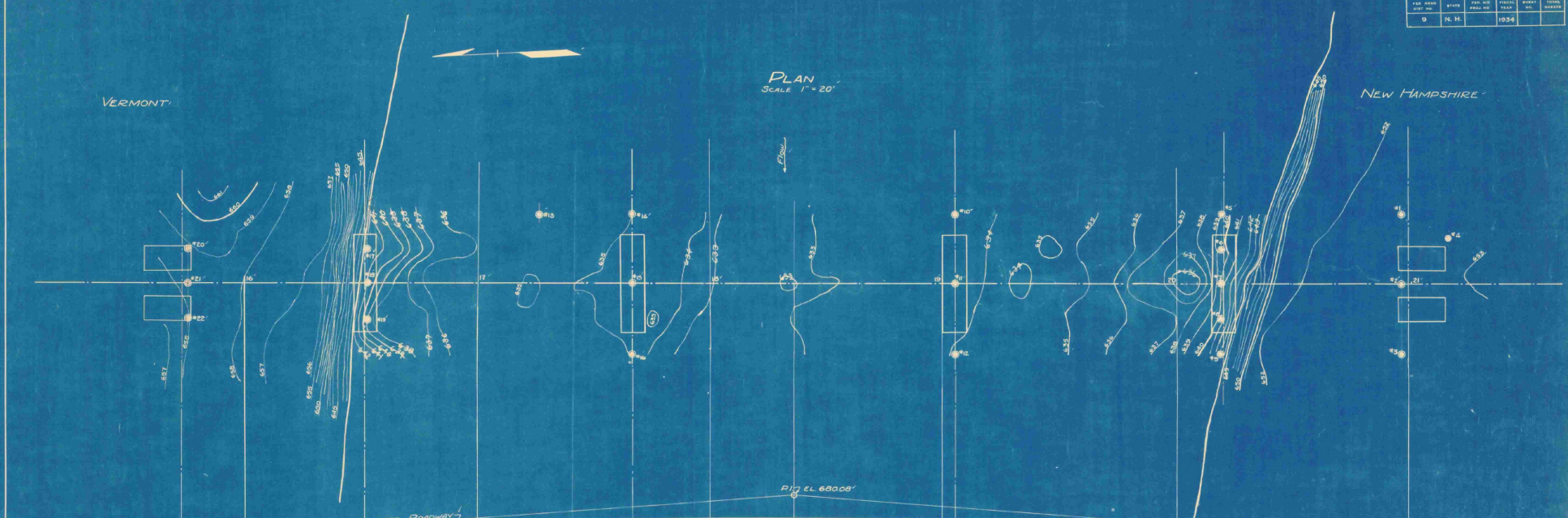


| DES. NO. | DATE  | DES. BY | CHKD. BY | APPROVED |
|----------|-------|---------|----------|----------|
| 9        | N. H. |         |          | 1934     |

VERMONT

PLAN  
SCALE 1" = 20'

NEW HAMPSHIRE



PROFILE  
HOR SCALE 1" = 20'  
VERT SCALE 1" = 5'

| REVISIONS | DATE    | BY     | FOR      |
|-----------|---------|--------|----------|
| 1         | 1/10/34 | J.W.C. | DESIGNED |
| 2         | 1/10/34 | J.W.C. | TRACED   |
| 3         | 1/10/34 | J.W.C. | CHECKED  |
| 4         | 1/10/34 | J.W.C. | APPROVED |

STATE OF NEW HAMPSHIRE  
Highway Department  
TOWN - LITTLETON  
PROJECT - NH  
LOCATION - LITTLETON, N.H. WATERCROFT  
ROAD - THROUGH ROADWAY  
STREAM - CONNECTICUT RIVER  
SURVEYED BY - E.A. DATE 11/1/33  
DRAWN BY - J.W.C. DATE 11/1/33  
TRACED BY - J.W.C. DATE 2/1/34  
CHECKED BY - J.W.C. DATE 3-2-34  
SHEET 4 of 10 SHEETS

| FILE NO. | DATE  | FILE NO. | DATE | FILE NO. | DATE | FILE NO. | DATE |
|----------|-------|----------|------|----------|------|----------|------|
| 9        | N. H. |          |      | 1934     |      |          |      |

WASH BORINGS TAKEN NOV. 1933.  
SCALE 1"=10'  
BORINGS TAKEN FOR PURPOSE OF DESIGN AND SHOW CONDITIONS AT BORING POINTS ONLY, BUT DO NOT NECESSARILY SHOW NATURE OF MATERIAL TO BE ENCOUNTERED IN CONNECTION WITH CONSTRUCTION OF THE BRIDGE.  
FIGURES IN COLUMNS INDICATE BLOWS PER FOOT DROPPING 135 LB. HAMMER 2 TOS ON A 1" PIPE IN A CASING.

| EL. 628.0 | SURFACE                             | EL. 629.3 | SURFACE                       | EL. 630.7 | SURFACE                       |
|-----------|-------------------------------------|-----------|-------------------------------|-----------|-------------------------------|
| 10        | LOAMY SAND                          |           | LOAMY SAND                    |           | LOAMY SAND                    |
| 25        | COARSE SAND AND COARSE GRAVEL       | 31        | COARSE SAND AND COARSE GRAVEL | 31        | COARSE SAND AND COARSE GRAVEL |
| 64        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 74        | BLUE SAND AND COARSE GRAVEL   | 74        | CEMENTED SAND AND GRAVEL      |
| 64.3      |                                     | 74        | LITTLE CLAY                   | 64.3      |                               |

FOOTING Vt ABUT. EL. 654.07

| EL. 628.0 | RIVER BED                           | EL. 628.5 | RIVER BED                           | EL. 629.0 | RIVER BED                           | EL. 629.5 | RIVER BED                           | EL. 630.0 | RIVER BED                           | EL. 630.5 | RIVER BED                           | EL. 631.0 | RIVER BED                           |
|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|
| 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            |
| 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY |
| 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        |

FOOTING PIER N°1 EL. 633.07

| EL. 628.0 | RIVER BED                           | EL. 628.5 | RIVER BED                           | EL. 629.0 | RIVER BED                           | EL. 629.5 | RIVER BED                           | EL. 630.0 | RIVER BED                           | EL. 630.5 | RIVER BED                           | EL. 631.0 | RIVER BED                           | EL. 631.5 | RIVER BED                           | EL. 632.0 | RIVER BED                           | EL. 632.5 | RIVER BED                           | EL. 633.0 | RIVER BED                           | EL. 633.5 | RIVER BED                           | EL. 634.0 | RIVER BED                           |    |                                     |
|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|----|-------------------------------------|
| 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10        | COARSE SAND AND BOULDERS            | 10 | COARSE SAND AND BOULDERS            |
| 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10 | CEMENTED SAND, GRAVEL & LITTLE CLAY |
| 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10        | LEGE OR BOULDER BROKE CASING        | 10 | LEGE OR BOULDER BROKE CASING        |

FOOTING PIER N°2 EL. 628.0

BORING N°22, BORING N°21, BORING N°20, BORING N°19A, BORING N°19, BORING N°18A, BORING N°18, BORING N°17, BORING N°16, BORING N°15, BORING N°14, BORING N°13, BORING N°12, BORING N°11, BORING N°10

| EL. 628.0 | SURFACE                  | EL. 628.5 | SURFACE                  | EL. 629.0 | SURFACE                  | EL. 629.5 | SURFACE                  | EL. 630.0 | SURFACE                  | EL. 630.5 | SURFACE                  | EL. 631.0 | SURFACE                  | EL. 631.5 | SURFACE                  | EL. 632.0 | SURFACE                  | EL. 632.5 | SURFACE                  | EL. 633.0 | SURFACE                  | EL. 633.5 | SURFACE                  | EL. 634.0 | SURFACE                  |    |                          |
|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|-----------|--------------------------|----|--------------------------|
| 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10        | LOAMY SAND               | 10 | LOAMY SAND               |
| 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10 | COARSE SAND AND BOULDERS |
| 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10 | COARSE SAND AND BOULDERS |
| 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10        | COARSE SAND AND BOULDERS | 10 | COARSE SAND AND BOULDERS |

FOOTING NH ABUT. EL. 643.0

FOOTING PIER N°4 EL. 633.0

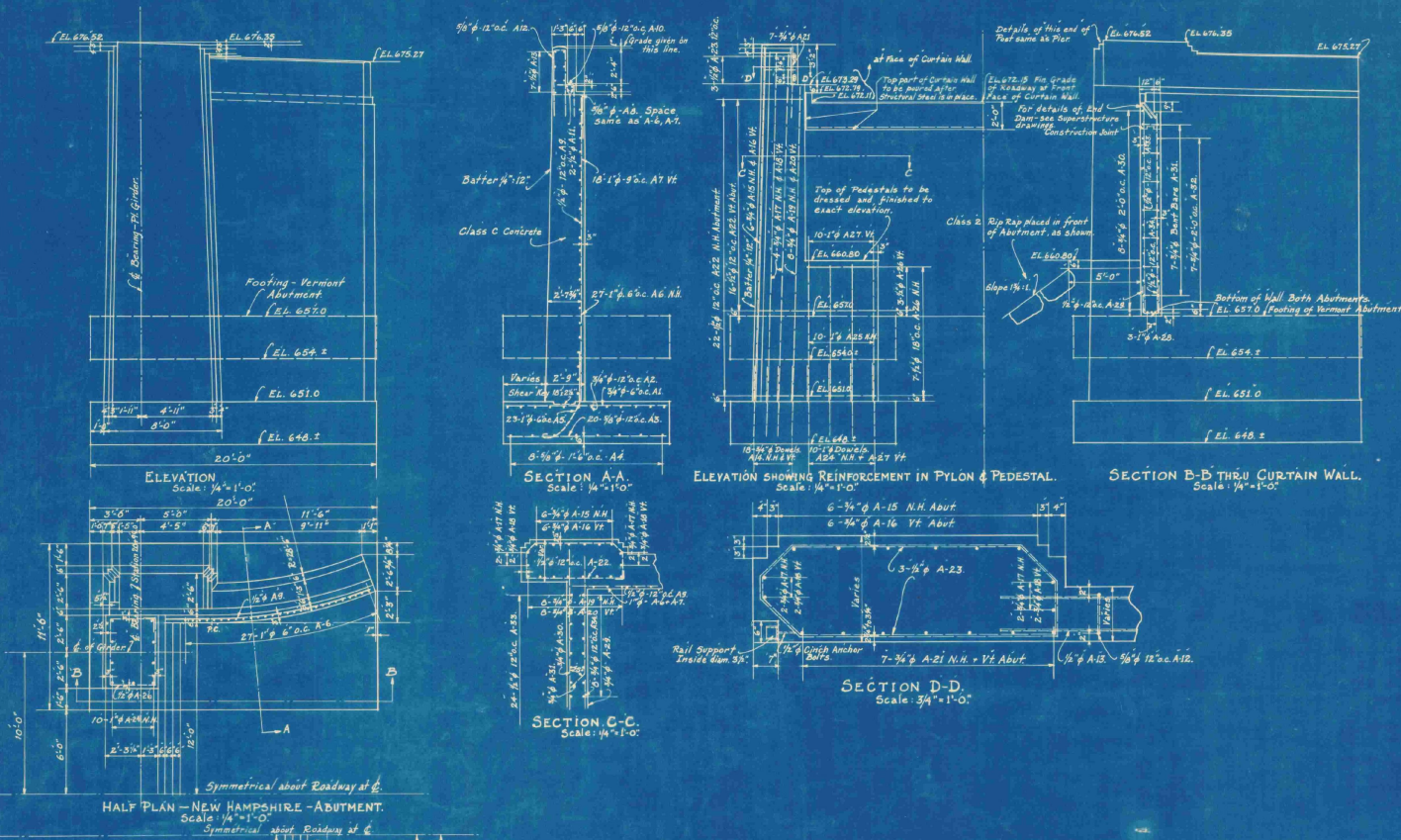
| EL. 628.0 | RIVER BED                           | EL. 628.5 | RIVER BED                           | EL. 629.0 | RIVER BED                           | EL. 629.5 | RIVER BED                           | EL. 630.0 | RIVER BED                           | EL. 630.5 | RIVER BED                           | EL. 631.0 | RIVER BED                           | EL. 631.5 | RIVER BED                           | EL. 632.0 | RIVER BED                           | EL. 632.5 | RIVER BED                           | EL. 633.0 | RIVER BED                           | EL. 633.5 | RIVER BED                           | EL. 634.0 | RIVER BED                           |    |                                     |
|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|-----------|-------------------------------------|----|-------------------------------------|
| 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10        | COARSE SAND, GRAVEL AND BOULDERS    | 10 | COARSE SAND, GRAVEL AND BOULDERS    |
| 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10        | CEMENTED SAND, GRAVEL & LITTLE CLAY | 10 | CEMENTED SAND, GRAVEL & LITTLE CLAY |
| 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10        | LEGE OR BOULDER                     | 10 | LEGE OR BOULDER                     |

BORING N°9, BORING N°8, BORING N°7, BORING N°6A, BORING N°6, BORING N°5, BORING N°4B, BORING N°4A, BORING N°4, BORING N°3, BORING N°2A, BORING N°2, BORING N°1B

STATE OF NEW HAMPSHIRE  
HIGHWAY DEPARTMENT  
TOWN LITTLETON  
PROJECT  
LOCATION, LITTLETON, N.H. WATERPAGES Vt ROAD, THROUGH ROUBIDOUX STREAM, CORNHURST RIVER  
SURVEYED BY F.W.G.S. DATE 11-27-33  
TRACED BY F.W.G.S. DATE 11-29-33  
CHECKED BY F.W.G.S. DATE 3-1-34  
SHEET 5 OF 10 SHEETS.

REVISIONS  
DATE  
BY  
CHECKED BY  
DATE

| FED. ROAD DIST. NO. | STATE | FED. AID PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|--------------------|-------------|-----------|--------------|
| 9                   | N. H. |                    |             |           |              |



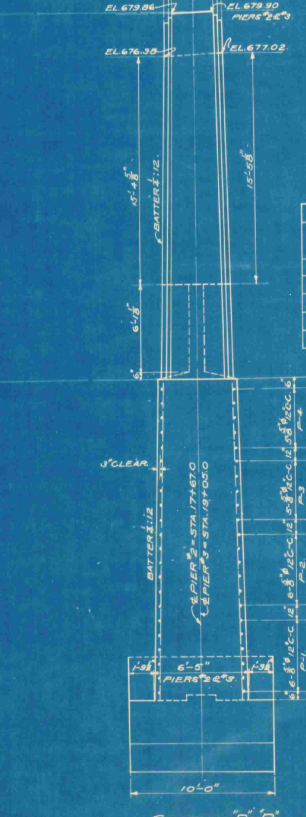
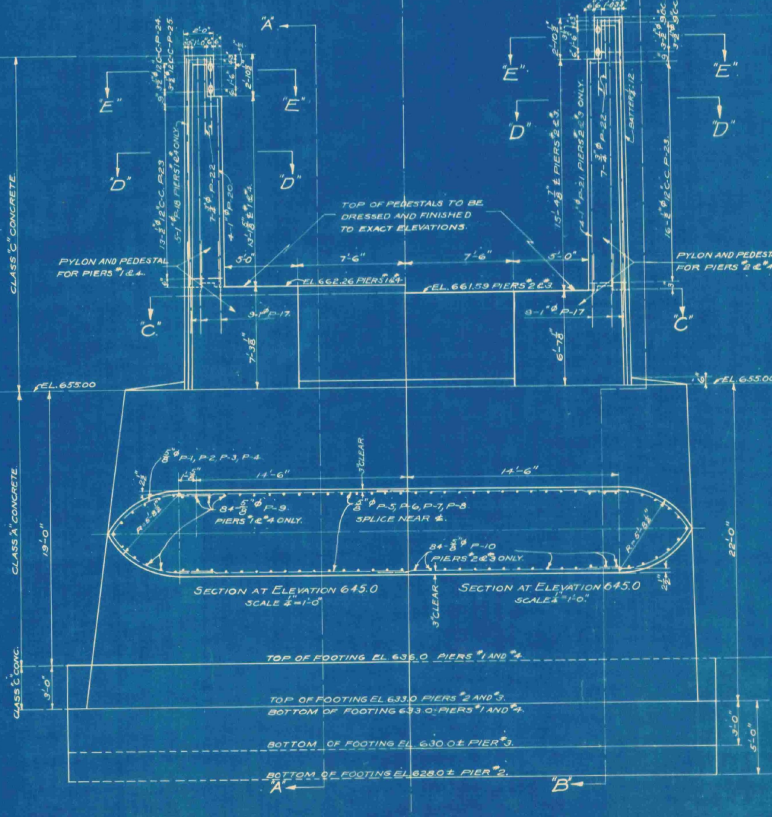
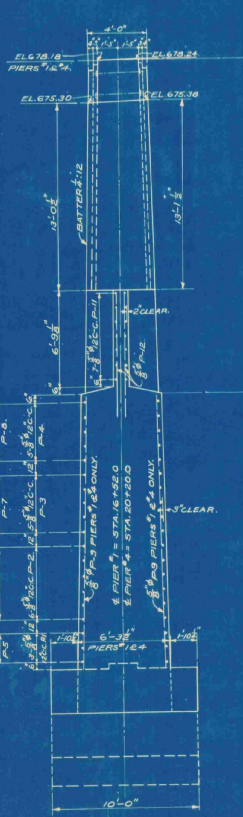
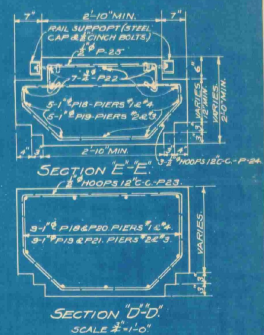
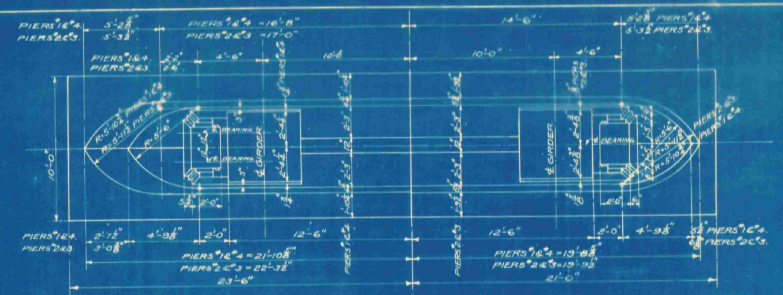
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| ESTIMATED QUANTITIES.  |              |               |
|------------------------|--------------|---------------|
| MATERIALS.             | VERMONT.     | NEW HAMPSHIRE |
| STRUCTURAL EXCAVATION. | 85 cu. YDS.  | 100 cu. YDS.  |
| CONCRETE - CLASS C.    | 144 cu. YDS. | 178 cu. YDS.  |
| REINFORCING STEEL.     | 10142 LBS.   | 13000 LBS.    |
| RIP RAP - CLASS 2.     | 7 cu. YDS.   | 25 cu. YDS.   |

Note: Exposed corners of concrete to be chamfered 1/2". Corners of curb to be rounded to 4" radius. No concrete to be placed until after reinforcing steel has been checked by engineers. Minimum cement factor of Class C Concrete = 1.30. Footings to be carried to a firm foundation, subject to the approval of the engineers. Excess structure excavation to be used for fill under Rip Rap without additional compensation.

STATE OF NEW HAMPSHIRE  
HIGHWAY DEPARTMENT  
TOWN: LITTLETON  
PROJECT: LOCATION LITTLETON, NH - WATERBURY, VT. ROAD T. R. HIGHWAY. STREAM CONNECTICUT RIVER.  
DESIGNED BY M.F.S. DATE 11-33  
DRAWN BY M.B.S. DATE 11-33  
TRACED BY C.N.P. DATE 2-28-34  
CHECKED BY H.L.Z. DATE 3-1-37  
SHEET 6 OF 10 SHEETS.

| REV. NO. | DATE | BY | CHKD. BY | APP. BY | SCALE |
|----------|------|----|----------|---------|-------|
| 9        | N.H. |    |          |         |       |



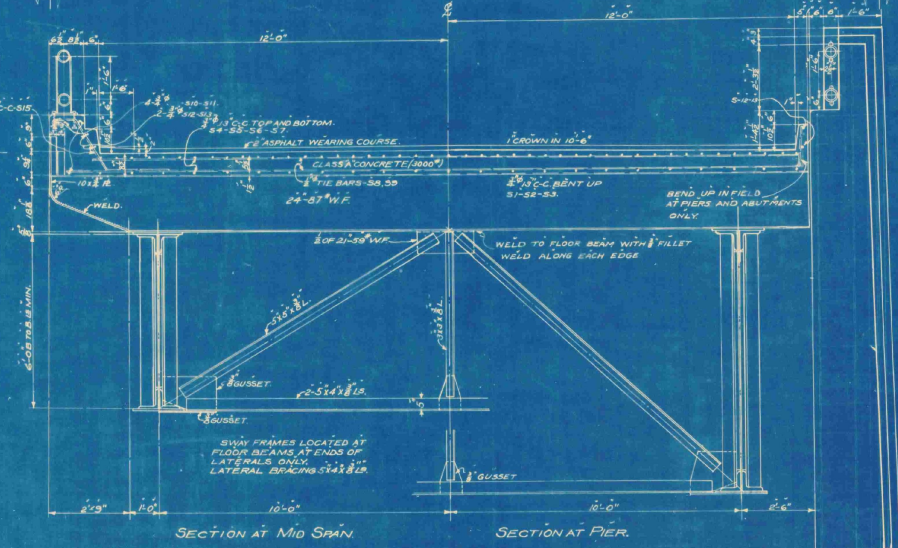
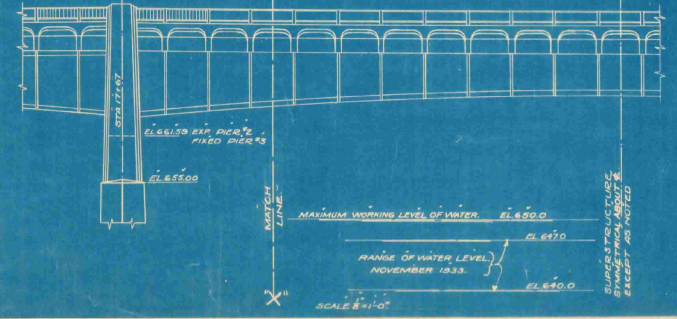
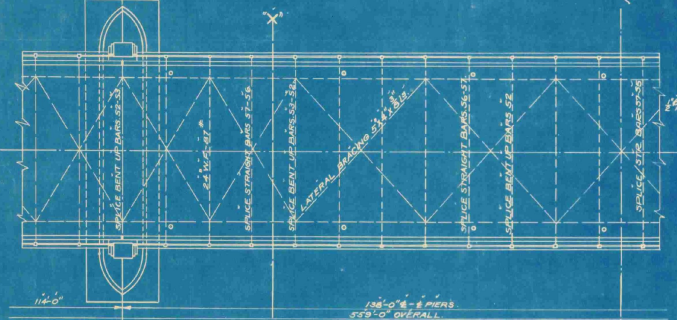
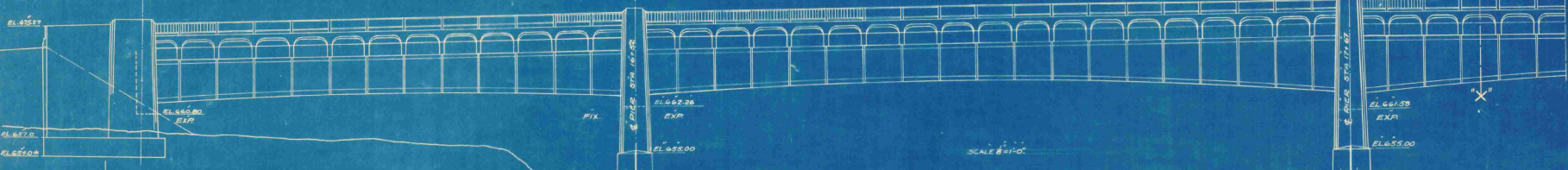
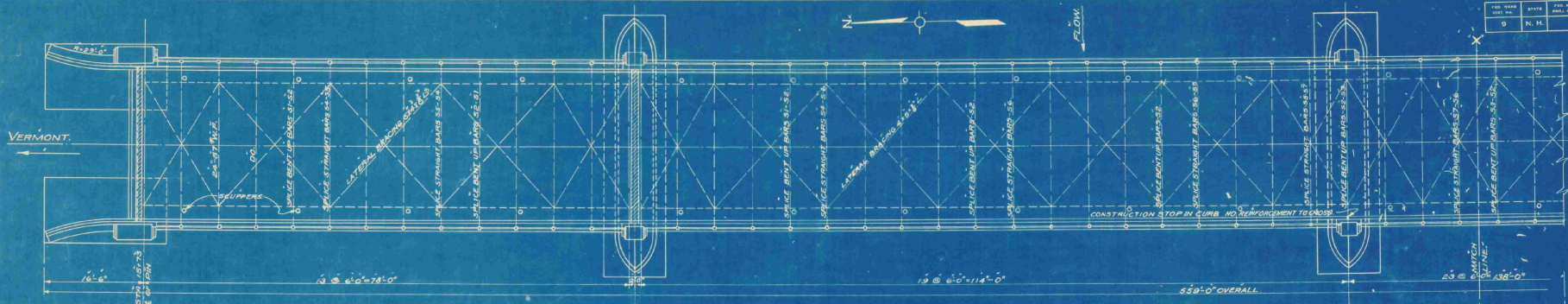
| ESTIMATED QUANTITIES.                | PIER N°1   | PIER N°2   | PIER N°3   | PIER N°4   |
|--------------------------------------|------------|------------|------------|------------|
| 8 STRUCTURE EXCAVATION EARTH LEDGE   | 125 CU YDS | 133 CU YDS | 75 CU YDS  | 140 CU YDS |
| 27 CONCRETE CLASS A                  | 153 CU YDS | 153 CU YDS | 153 CU YDS | 153 CU YDS |
| 27 CONCRETE CLASS C                  | 96 CU YDS  | 147 CU YDS | 14 CU YDS  | 86 CU YDS  |
| 39 REINFORCING STEEL                 | 6156 LBS   | 6781 LBS   | 6781 LBS   | 6156 LBS   |
| 28 RIP RAP FOR FOUNDATION PROTECTION | 25 CU YDS  | 25 CU YDS  | 25 CU YDS  | 25 CU YDS  |

NOTES:-  
 ALL PIER FOOTINGS TO BE CARRIED TO SUITABLE FOUNDATION.  
 NO CONCRETE TO BE PLACED UNTIL AFTER REINFORCING STEEL HAS BEEN CHECKED BY THE ENGINEER.  
 EXPOSED CORNERS OF CONCRETE TO BE CHAMFERED 1/4".  
 BACKFILL AROUND PIERS WITH RIP RAP FOR FOUNDATION PROTECTION.  
 CLASS A CONCRETE TO HAVE MINIMUM CEMENT FACTOR OF 1.61  
 CLASS C CONCRETE TO HAVE MINIMUM CEMENT FACTOR OF 1.30

|   |      |    |          |         |
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| REVISIONS AFTER PROFESSIONAL DATE OF DESIGN | DATE | BY | CHKD. BY | APP. BY |
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STATE OF NEW HAMPSHIRE  
 HIGHWAY DEPARTMENT  
 TOWN - LITTLETON  
 PROJECT  
 LOCATION  
 ROAD - T.R. HIGHWAY  
 STREAM - CONNECTICUT RIVER  
 DESIGNED BY - DATE  
 DRAWN BY - DATE 3-2-34  
 CHECKED BY - DATE 3-3-34  
 SHEET 7 OF 10 SHEETS

| FED. ROAD DIST. NO. | STATE | FED. AID PROJ. NO. | FUND. YEAR | ROUTE NO. | TOTAL SHEETS |
|---------------------|-------|--------------------|------------|-----------|--------------|
| 9                   | N. H. |                    |            |           |              |



| SUPERSTRUCTURE QUANTITIES  |        |       |
|----------------------------|--------|-------|
| 27 CLASS A CONCRETE (1000) | 485    | Cu Yd |
| 29 REINFORCING STEEL       | 82475  | Lbs   |
| 30 STRUCTURAL STEEL        | 808000 | Lbs   |
| 72 ASPHALT WEARING COURSE  | 1381   | Sq Yd |
| 85 STEEL BRIDGE RAIL       | 1023   | Ln Ft |

DESIGN LOADING HIS SPECIFICATIONS AASHO 1933, MILD RIVETS 5/8" Holes 1/2" GENERAL REPAIRING NOT REQUIRED. PAINT 1 SINGLE COAT RED LEAD AND OIL. 1 FIELD COAT APPROVED ALUMINUM. THE CONTRACTOR SHALL DELIVER TO THE STATE TRACINGS OF SHOP DRAWINGS BEFORE FINAL PAYMENT IS MADE. NO CONCRETE SHALL BE PLACED UNTIL REINFORCING STEEL HAS BEEN CHECKED BY ENGINEER. ALL EXPOSED CORNERS TO BE CHAMFERED 1/2" EXCEPT AS NOTED. CURB CORNERS TO BE ROUNDED TO 1" RADIUS. MINIMUM CEMENT FACTOR CLASS A CONCRETE 161.

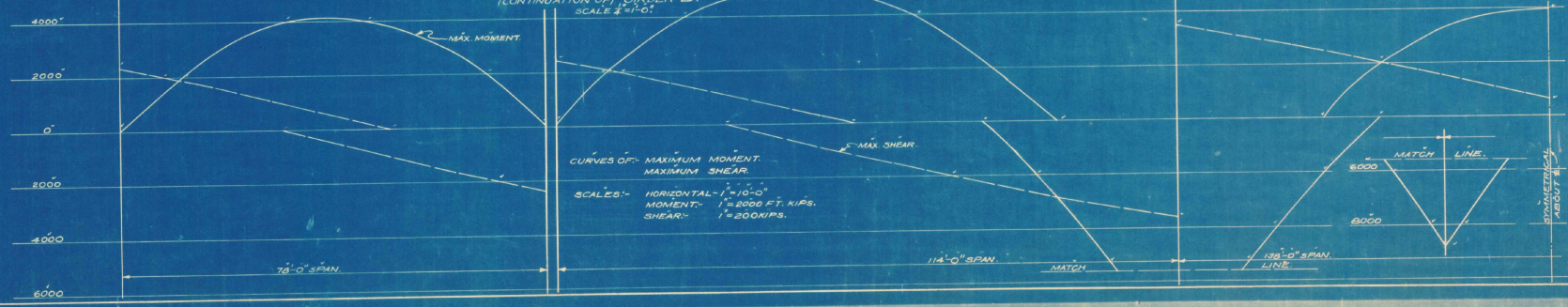
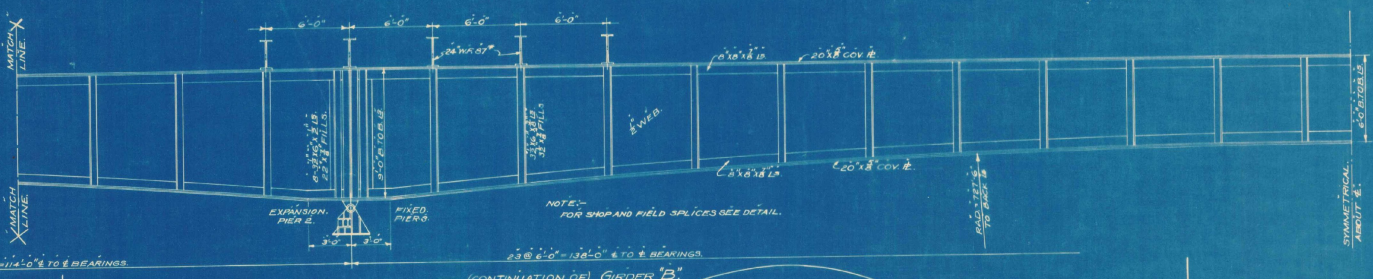
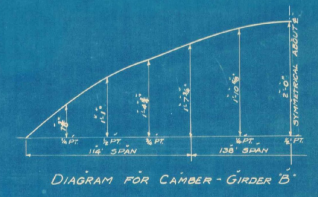
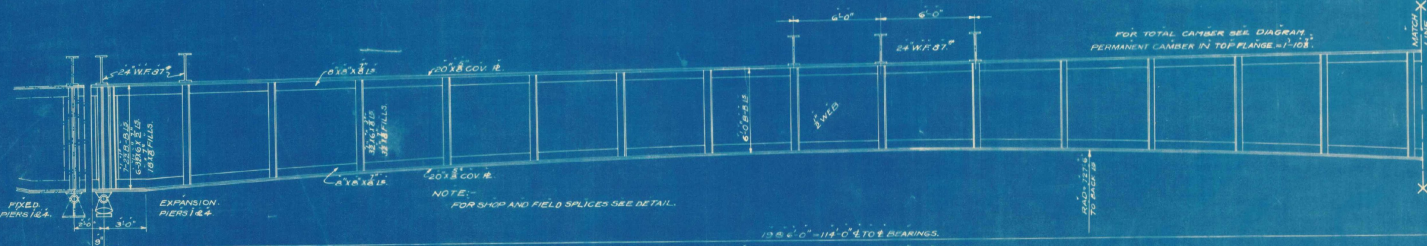
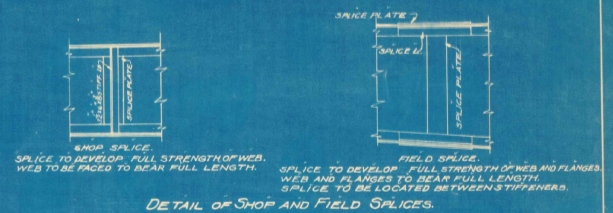
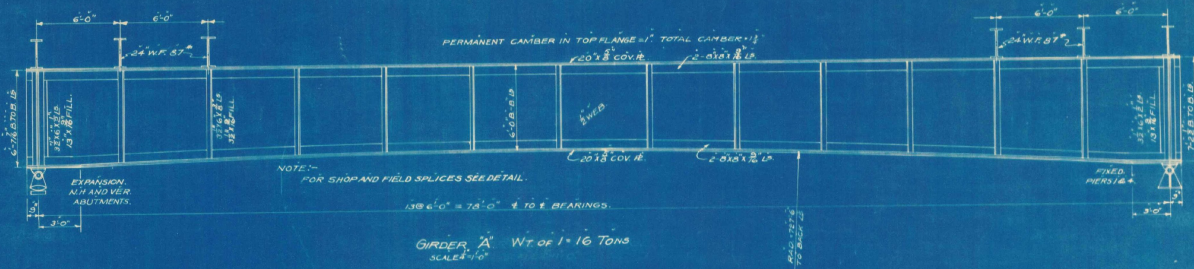
BATTER 1/4"

STATE OF NEW HAMPSHIRE  
HIGHWAY DEPARTMENT  
TOWN LITTLETON  
PROJECT  
LOCATION T. R. HIGHWAY  
ROAD T. R. HIGHWAY  
STREAM CONNECTICUT RIVER  
DESIGNED BY R. D. F. DATE 1-24-34  
DRAWN BY M. P. S. DATE 11-23-33  
TRACED BY F. W. S. DATE 2-15-34  
CHECKED BY H. R. K. DATE 3-2-34  
SHEET 8 OF 10 SHEETS

| REVISIONS | DATE | BY | REASON |
|-----------|------|----|--------|
| 1         |      |    |        |
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DATE DATE  
DESIGNED BY

|             |          |            |      |
|-------------|----------|------------|------|
| DESIGNED BY | DRAWN BY | CHECKED BY | DATE |
| B. N. H.    |          |            |      |



STATE OF NEW HAMPSHIRE. HIGHWAY DEPARTMENT.

TOWN: LITTLETON.

PROJECT: \_\_\_\_\_

ROAD: T. R. HIGHWAY.

STREAM: CONNECTICUT RIVER.

DESIGNED BY: B.N.H. DATE: 1-24-34

DRAWN BY: M.B.S. DATE: 11-25-33

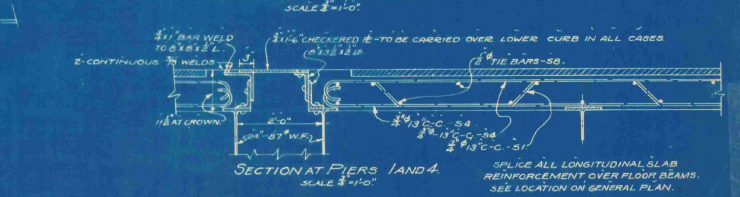
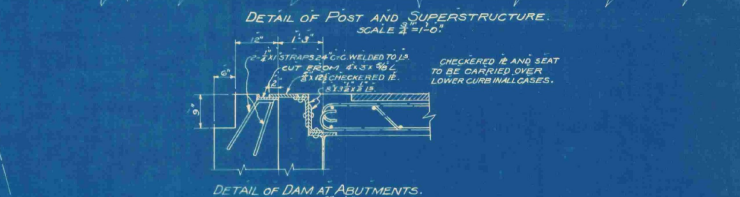
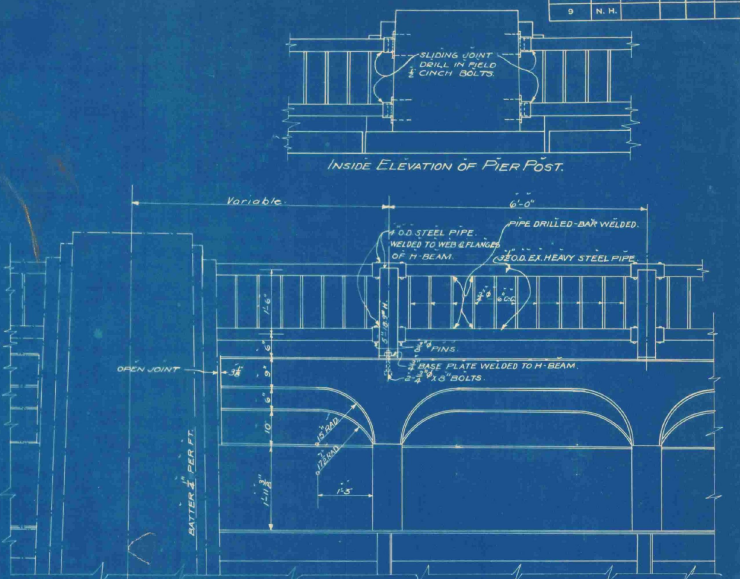
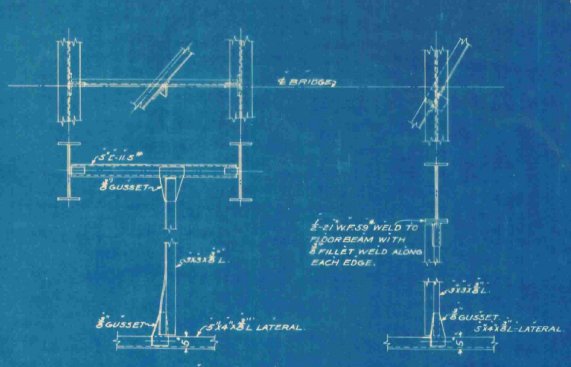
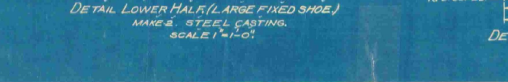
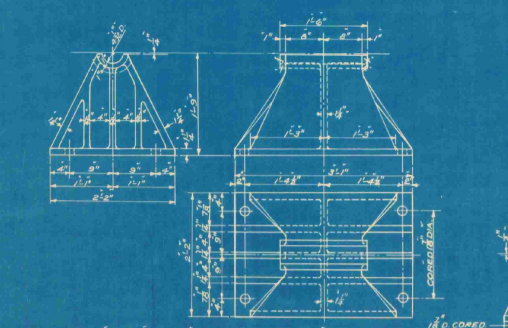
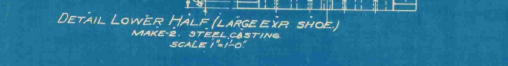
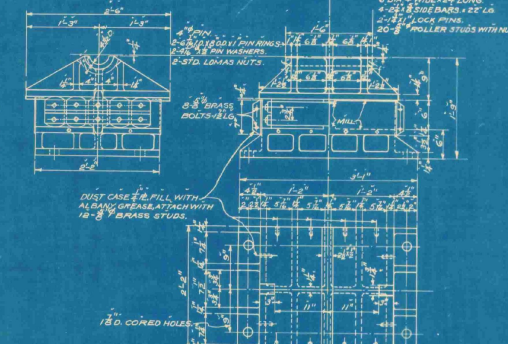
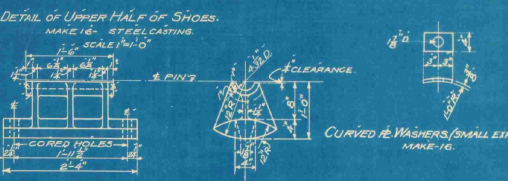
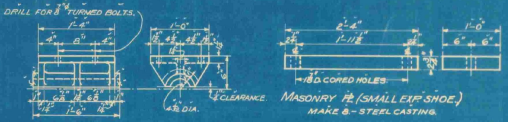
TRACED BY: E.W.G.S. DATE: 2-20-34

CHECKED BY: B.N.H. DATE: 3-12-34

SHEET 5 OF 10 SHEETS.

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|---------------|------------------|---------------|-------------|
| Original Date | Original Station | Original From | Original To |
| Rev.          | Rev.             | Rev.          | Rev.        |
| Checked by    | Date             | Checked by    | Date        |

| DESIGN NO. | STATE | PROJ. NO. | PIECE NO. | SHEET NO. | TOTAL SHEETS |
|------------|-------|-----------|-----------|-----------|--------------|
| 9          | N. H. |           |           |           |              |



INSIDE ELEVATION OF PIER POST.

DETAIL OF LATERAL HANGERS. SCALE 3/4"

DETAIL OF POST AND SUPERSTRUCTURE. SCALE 3/4"

DETAIL OF DAM AT ABUTMENTS. SCALE 3/4"

SECTION AT PIERS 1 AND 4. SCALE 3/4"

DETAIL OF SCUPPER. MAKE 4 - STEEL CASTING. SCALE 3/4"

| REVISIONS | AFTER | PROPOSED | DATE | BY | CHECKED |
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STATE OF NEW HAMPSHIRE.  
 HIGHWAY DEPARTMENT  
 TOWN, LITTLETON  
 PROJECT  
 LOCATION, ROAD, T. R. HIGHWAY  
 STREAM, CONNECTICUT RIVER  
 DESIGNED BY R.I.D.E. DATE 1-28-34  
 DRAWN BY M.H.B. DATE 2-21-34  
 TRACED BY E.W.G.S. DATE 2-24-34  
 CHECKED BY R.N.K. DATE 3-2-34  
 SHEET 10 OF 10 SHEETS.

| REV. NO. | DATE  | BY | CHKD. BY | TOTAL SHEETS |
|----------|-------|----|----------|--------------|
| 0        | N. H. |    | 1934     | 13           |

### INDEX OF SHEETS

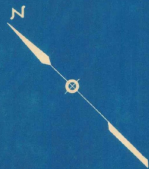
- SHEET NO 1 TITLE PAGE
- " 2 TYPICAL SECTION OF IMPROVEMENT
- " 3 PLAN AND PROFILE
- " 4,5,6 CROSS SECTIONS
- " 7 BRIDGES
- " 8 ABUTMENTS
- " 9 PIERIS
- " 10 MAINTENANCE SPAN
- " 11 PLATE GIRDER APP SPANS
- " 12 EXPANSION JOINT DETAILS
- " 13 STANDARDS

STATE OF NEW HAMPSHIRE  
STATE HIGHWAY DEPARTMENT

## PLAN AND PROFILE OF PROPOSED U.S. PUBLIC WORKS PROJECT

NO - N.R.H. 202-4  
INTERSTATE BRIDGE PROJECT  
ROAD

END OF PROJECT  
STATION 15+60.71



BEGINNING OF PROJECT  
STATION 0+00

SCALES (PLAN: 1"=50'-0"  
PROFILE VERT: 1"=10'-0" HOR: 1"=50'-0"  
CROSS SECTION VERT: 1"=5'-0" HOR: 1"=5'-0")

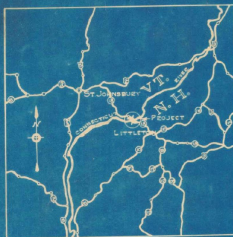
NOTE: This project to be constructed in accordance with standard specifications on file with the Bureau of Public Roads, and special provisions attached to proposal.

P.I. = STA 12+93.00  
P.V. = STA 12+48.18  
L = 637.27  
D = 9° 00'  
T = 856.07  
T = 199.28'

P.I. = STA 15+15.27  
P.V. = STA 14+59.71  
L = 637.27  
D = 9° 00'  
T = 856.07  
T = 151.00'

P.I. = STA 3+54.28  
P.V. = STA 3+20.17  
L = 637.27  
D = 9° 00'  
T = 856.07  
T = 193.22'

### KEY MAP



### CONVENTIONAL SIGNS

- |                   |                  |     |
|-------------------|------------------|-----|
| TOWN LINE         | SURVEY LINE      | 30' |
| FENCE LINE        | CULVERTS         |     |
| STONE WALL        | POWER POLE       |     |
| UNFENCED PROPERTY | TELEPHONE POLE   |     |
| TRAVELED WAY      | GROUND ELEVATION |     |
| RAILROADS         | GRADE ELEVATION  |     |
| RETAINING WALL    | BOUNDARY         |     |

LAYOUT  
SCALE 1"=50 FEET

TOWNS OF - WATERFORD, VT. & LITTLETON, N.H.  
COUNTIES OF - CALEDONIA, VT. & GRAFTON, N.H.

APPROVED: *J. G. Grant*  
HIGHWAY COMMISSIONER

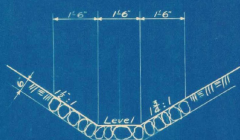
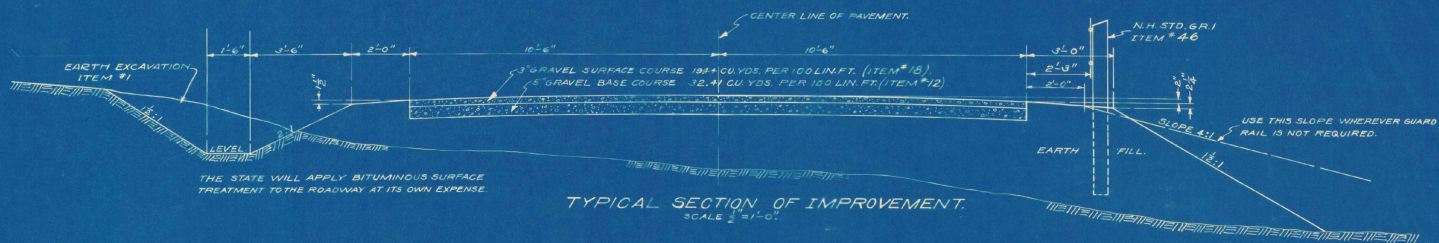
RECOMMENDED FOR APPROVAL:

DISTRICT ENGINEER, BUREAU OF PUBLIC ROADS  
RECOMMENDED FOR APPROVAL:

CHIEF ENGINEER, BUREAU OF PUBLIC ROADS  
APPROVED:

DIRECTOR, BUREAU OF PUBLIC ROADS

| FED. ROAD DIST. NO. | STATE | FED. AID PROJ. NO. | FED. PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|--------------------|----------------|-----------|--------------|
| 9                   | N. H. |                    | 1933           | 2         | 10           |

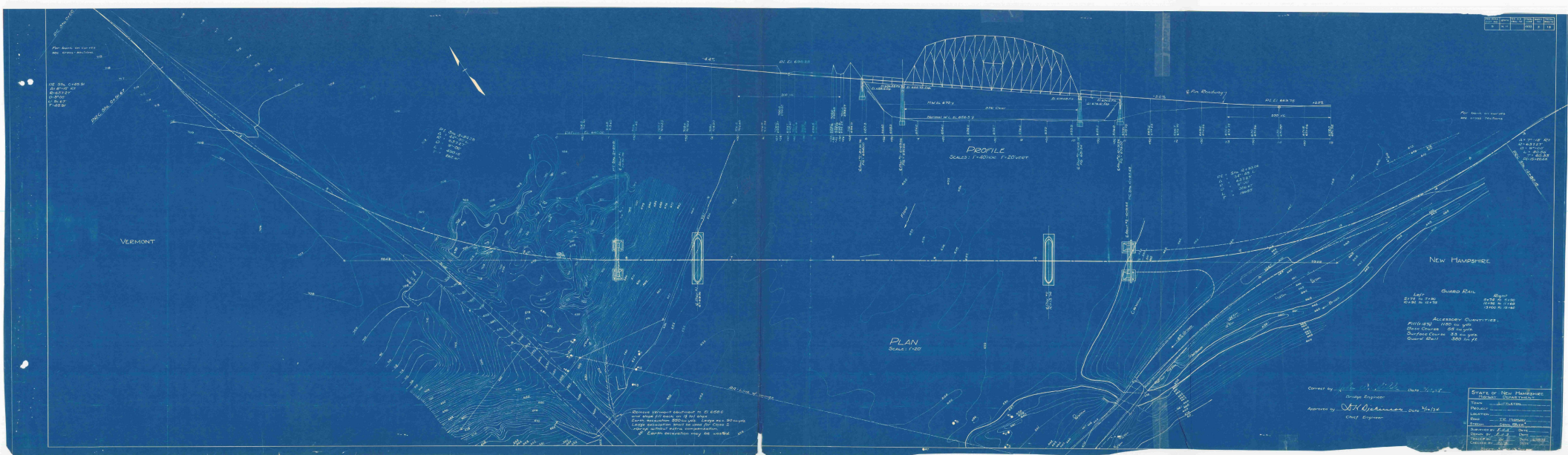


THE STATE WILL APPLY BITUMINOUS SURFACE TREATMENT TO THE ROADWAY AT ITS OWN EXPENSE.

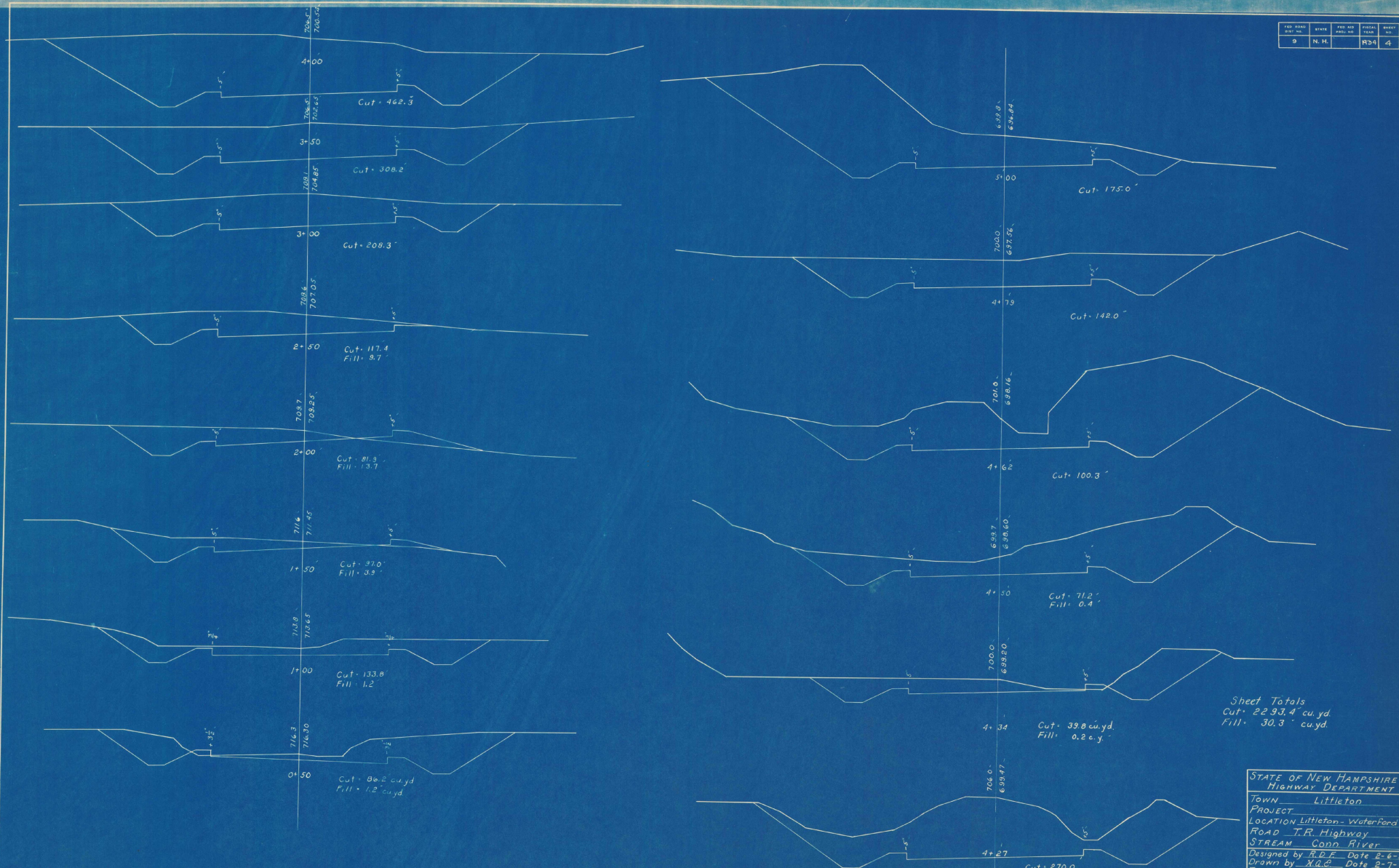
| REVISION | DATE | BY | REASON |
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Traced by \_\_\_\_\_ Date \_\_\_\_\_  
 Checked by \_\_\_\_\_ Date \_\_\_\_\_

STATE OF NEW HAMPSHIRE.  
 HIGHWAY DEPARTMENT.  
 TOWN LITTLETON  
 PROJECT \_\_\_\_\_  
 LOCATION \_\_\_\_\_  
 ROAD LITTLETON-WATERFORD  
 STREAM CONNECTICUT RIVER  
 DESIGNED BY R.W.F. DATE 12-19-33  
 DRAWN BY L.W.G.S. DATE 12-19-33  
 TRACED BY E.W.G.S. DATE 12-19-33  
 CHECKED BY M.F.S. DATE 12-21-33  
 SHEET 2 OF 13 SHEETS



| FED. ROAD DIST. NO. | STATE | FED. AID PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|--------------------|-------------|-----------|--------------|
| 9                   | N. H. |                    | 1934        | 4         | 13           |



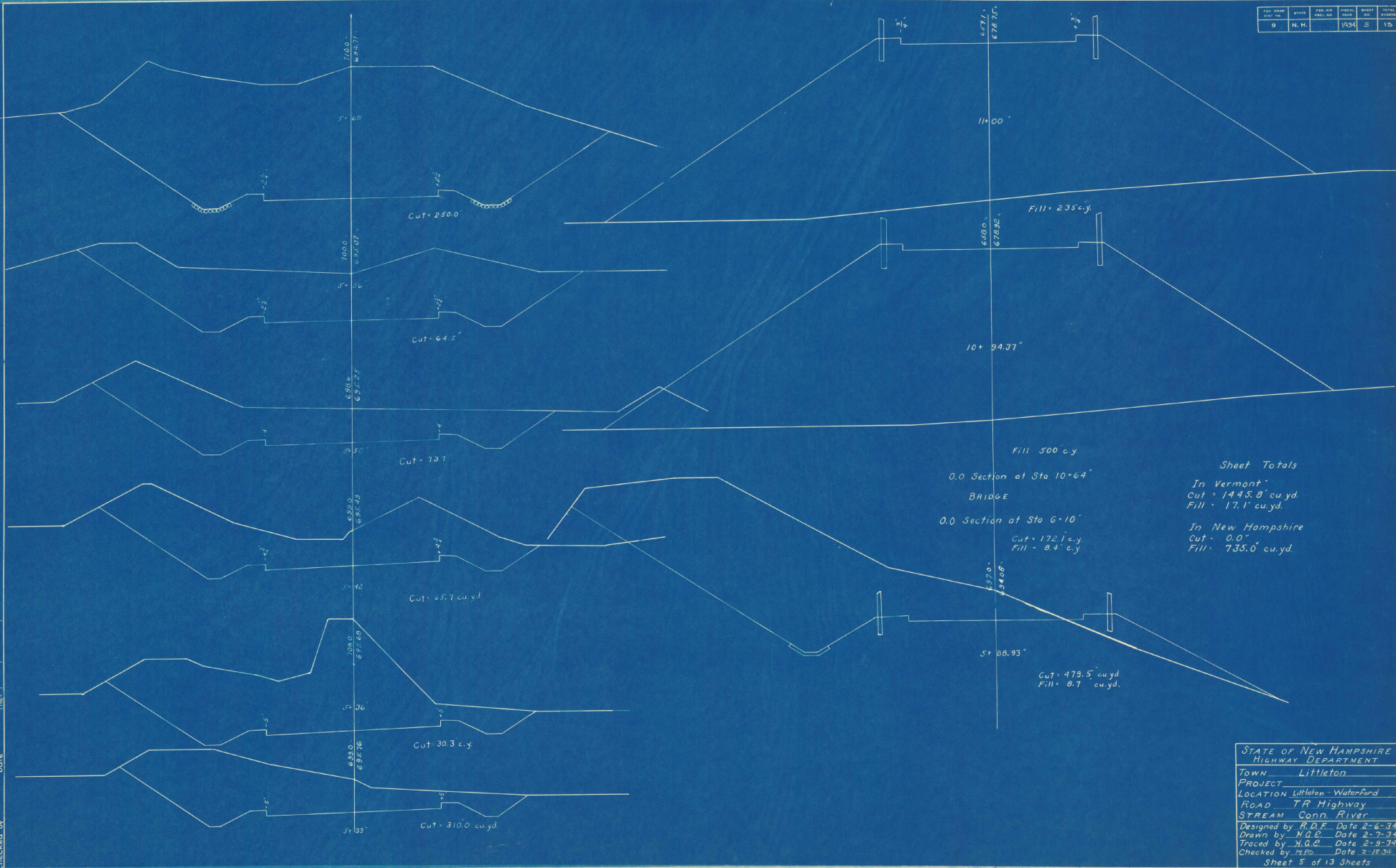
Sheet Totals  
 Cut = 2293.4 cu. yd.  
 Fill = 30.3 cu. yd.

STATE OF NEW HAMPSHIRE  
 HIGHWAY DEPARTMENT  
 TOWN Littleton  
 PROJECT LOCATION Littleton - Water Pond  
 ROAD T.R. Highway  
 STREAM Conn. River  
 Designed by R.D.F. Date 2-6-34  
 Drawn by N.G.E. Date 2-7-34  
 Traced by N.G.E. Date 2-8-34  
 Checked by M.E.S. Date 2-12-34  
 Sheet 4 of 13 Sheets

Beginning of Project Sta. 0+00

| DESIGNED BY | CHECKED BY | DATE |
|-------------|------------|------|
|             |            |      |

| PROJECT NO. | DATE  | DESIGNER | CHECKED | DATE | SCALE | TOTAL SHEETS |
|-------------|-------|----------|---------|------|-------|--------------|
| 9           | N. H. |          |         |      | 1/32" | 13           |



0.0 Section at Sta 10+64  
BRIDGE  
0.0 Section at Sta 6+10  
Cut = 172.1 c.y.  
Fill = 8.4 c.y.

Sheet Totals  
In Vermont  
Cut = 1445.8 cu.yd.  
Fill = 17.1 cu.yd.  
In New Hampshire  
Cut = 0.0  
Fill = 735.0 cu.yd.

Cut = 473.5 cu.yd.  
Fill = 8.7 cu.yd.

| DATE | BY | REVISION |
|------|----|----------|
|      |    |          |
|      |    |          |
|      |    |          |
|      |    |          |
|      |    |          |
|      |    |          |

Traced by \_\_\_\_\_ Date \_\_\_\_\_  
Checked by \_\_\_\_\_ Date \_\_\_\_\_

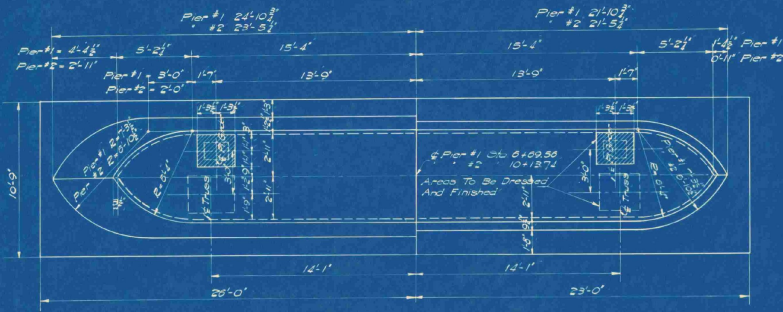
STATE OF NEW HAMPSHIRE  
HIGHWAY DEPARTMENT  
TOWN Littleton  
PROJECT Location Littleton - Waterford  
Road TR Highway  
STREAM Conn. River  
Designed by R.D.F. Date 2-6-34  
Drawn by M.G.S. Date 2-7-34  
Traced by H.A.S. Date 2-9-34  
Checked by W.P.S. Date 3-18-34  
Sheet 5 of 13 Sheets







| FIG. NO. | DATE  | FIG. NO. | FIG. NO. | SHEET NO. | TOTAL SHEETS |
|----------|-------|----------|----------|-----------|--------------|
| 9        | N. H. |          |          | 9         | 15           |

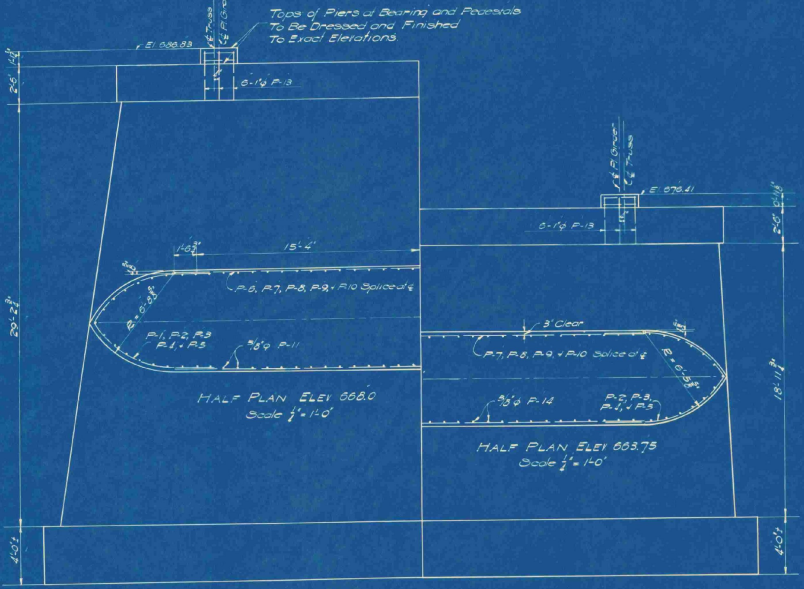


HALF PLAN PIER NO 1  
Scale 1/4" = 1'-0"

HALF PLAN PIER NO 2  
Scale 1/4" = 1'-0"

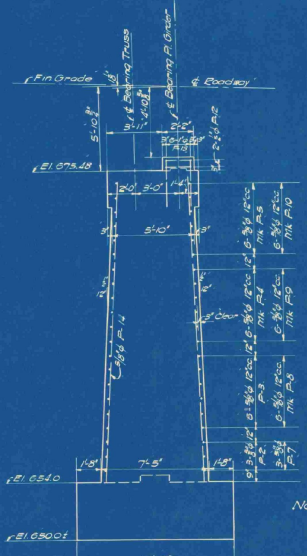


SECTION PIER NO 1  
Scale 1/4" = 1'-0"



HALF ELEVATION PIER NO 1  
Scale 1/4" = 1'-0"

HALF ELEVATION PIER NO 2  
Scale 1/4" = 1'-0"



SECTION PIER NO 2  
Scale 1/4" = 1'-0"

| ESTIMATED QUANTITIES            |            |
|---------------------------------|------------|
| PIER NO 1                       |            |
| EXCAVATION                      | 120 CU YDS |
| CONCRETE CLASS 'B'              | 414 "      |
| REINFORCING STEEL               | 6650 LBS   |
| RIP RAP (FOUNDATION PROTECTION) | 50 CU YDS  |

| PIER NO 2                       |            |
|---------------------------------|------------|
| EXCAVATION                      | 195 CU YDS |
| CONCRETE CLASS 'B'              | 287 "      |
| REINFORCING STEEL               | 4120 LBS   |
| RIP RAP (FOUNDATION PROTECTION) | 50 CU YDS  |

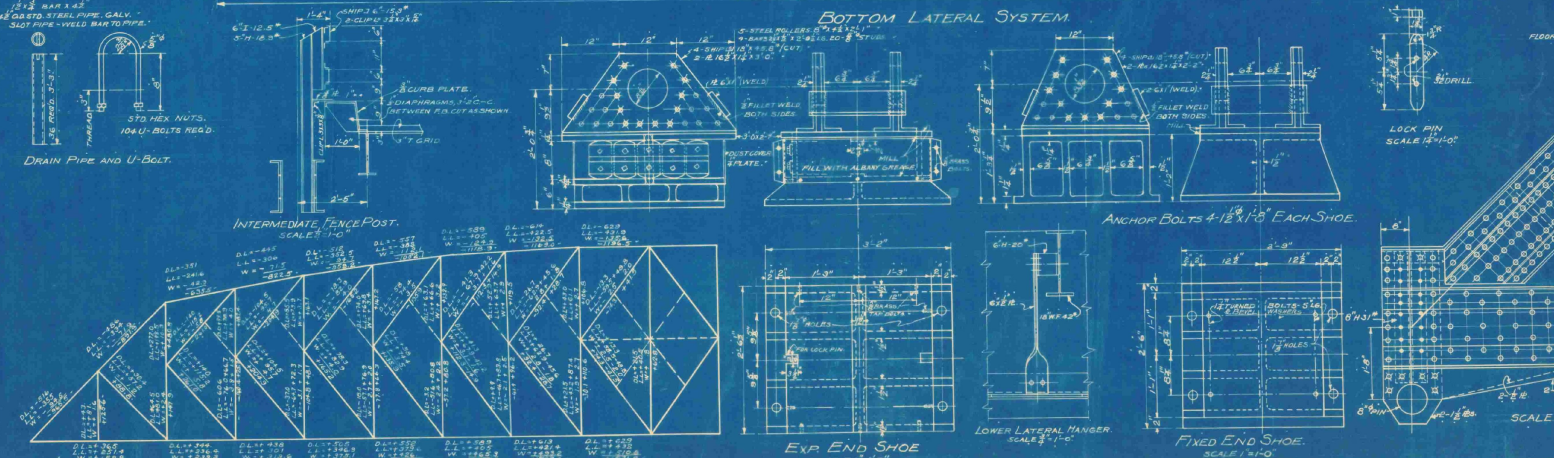
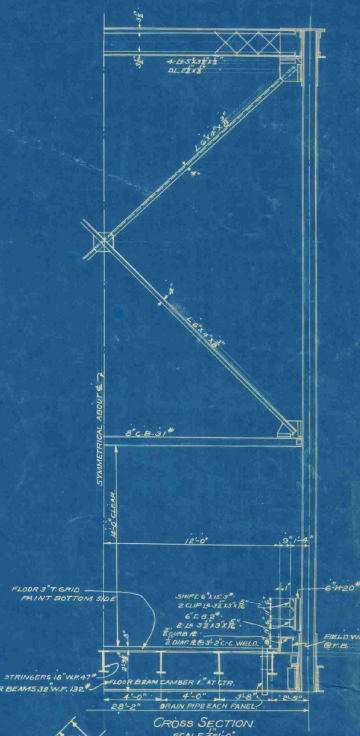
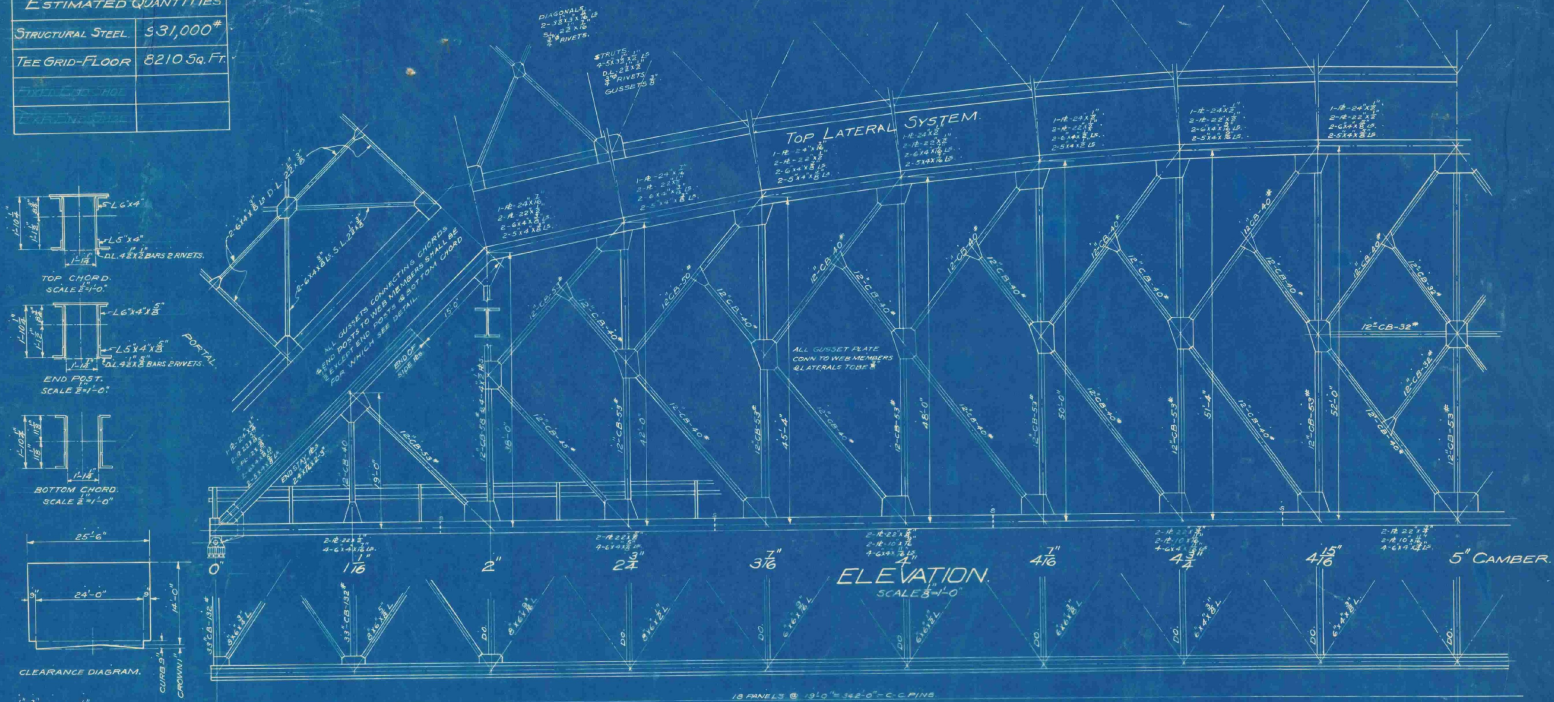
STATE OF NEW HAMPSHIRE  
HIGHWAY DEPARTMENT  
TOWN LITTLETON  
PROJECT \_\_\_\_\_  
LOCATION \_\_\_\_\_  
ROAD LITTLETON - WATERFORD  
DRAWN CONNECTICUT ENGINEERS  
DESIGNED BY C.B. DATE 2-2-34  
DRAWN BY C.B. DATE 2-6-34  
CHECKED BY M.R.S. DATE 2-7-34  
DATE 2-8-34  
SHEET 9 OF 13 SHEETS

NOTE:  
Backfill Around Piers With  
Riprap For Foundation Protection  
Piers To Be Class 'B' (2800#) Conc.  
Min. Cement Factor = 1.45

REVISIONS AFTER PROPOSAL  
DATE Station FROM OF TO  
Orig. \_\_\_\_\_  
Rev. \_\_\_\_\_  
Checked by \_\_\_\_\_  
Date \_\_\_\_\_

|            |       |          |         |       |              |
|------------|-------|----------|---------|-------|--------------|
| DESIGN NO. | DATE  | DESIGNER | CHECKER | SCALE | TOTAL SHEETS |
| 9          | N. H. | 1933     | 10      | 13    |              |

| ESTIMATED QUANTITIES |              |
|----------------------|--------------|
| STRUCTURAL STEEL     | 531,000*     |
| TEE GRID-FLOOR       | 8210 Sq. Ft. |
| FLOOR FINISH         |              |
| CONCRETE             |              |



**NOTES.**  
 SPECIFICATIONS: AASHTO 1931 & N.H.S.T.D.  
 DESIGN LOADING: H-15  
 RIVETS: 3/8" & 1/2" WHERE SECTIONS REQUIRE.  
 PAINT: ONE SHOP COAT RED LEAD OIL.  
 ONE FIELD COAT APPROX. ALUMINUM.  
 DEAD LOAD: 25 LB. PER LIN. FT. ONE TOPS.  
 CAMBER TRUSS AS SHOWN.  
 UNIT STRESSES ON SHED ON 1/4" BASIS.  
 SEE SHEET 12 FOR END DAMS.  
 THE CONTRACTOR SHALL FURNISH THE  
 STATE OF NEW HAMPSHIRE WITH TRACINGS  
 OF SHOP DRAWINGS BEFORE FINAL  
 PAYMENT IS MADE.

STATE OF NEW HAMPSHIRE  
 HIGHWAY DEPARTMENT  
 TOWN: LITTLETON  
 PROJECT: LOCATION LITTLETON-WATERFORD  
 ROAD T.R. HIGHWAY  
 STREAM, CONNECTICUT RIVER  
 DESIGNED BY: DATE: 2-10-33  
 DRAWN BY: E.W.G. DATE: 2-6-34  
 TRACED BY: R.D.L. DATE: 2-10-34  
 CHECKED BY: DATE: 2-10-34

SHEET 10 OF 13 SHEETS.

|           |         |      |         |
|-----------|---------|------|---------|
| REVISIONS | DATE    | BY   | REASON  |
| 1         | 10/1/33 | W.G. | REVISED |
| 2         | 10/1/33 | W.G. | REVISED |
| 3         | 10/1/33 | W.G. | REVISED |
| 4         | 10/1/33 | W.G. | REVISED |
| 5         | 10/1/33 | W.G. | REVISED |
| 6         | 10/1/33 | W.G. | REVISED |
| 7         | 10/1/33 | W.G. | REVISED |
| 8         | 10/1/33 | W.G. | REVISED |
| 9         | 10/1/33 | W.G. | REVISED |
| 10        | 10/1/33 | W.G. | REVISED |
| 11        | 10/1/33 | W.G. | REVISED |
| 12        | 10/1/33 | W.G. | REVISED |
| 13        | 10/1/33 | W.G. | REVISED |
| 14        | 10/1/33 | W.G. | REVISED |
| 15        | 10/1/33 | W.G. | REVISED |
| 16        | 10/1/33 | W.G. | REVISED |
| 17        | 10/1/33 | W.G. | REVISED |
| 18        | 10/1/33 | W.G. | REVISED |
| 19        | 10/1/33 | W.G. | REVISED |
| 20        | 10/1/33 | W.G. | REVISED |
| 21        | 10/1/33 | W.G. | REVISED |
| 22        | 10/1/33 | W.G. | REVISED |
| 23        | 10/1/33 | W.G. | REVISED |
| 24        | 10/1/33 | W.G. | REVISED |
| 25        | 10/1/33 | W.G. | REVISED |
| 26        | 10/1/33 | W.G. | REVISED |
| 27        | 10/1/33 | W.G. | REVISED |
| 28        | 10/1/33 | W.G. | REVISED |
| 29        | 10/1/33 | W.G. | REVISED |
| 30        | 10/1/33 | W.G. | REVISED |
| 31        | 10/1/33 | W.G. | REVISED |
| 32        | 10/1/33 | W.G. | REVISED |
| 33        | 10/1/33 | W.G. | REVISED |
| 34        | 10/1/33 | W.G. | REVISED |
| 35        | 10/1/33 | W.G. | REVISED |
| 36        | 10/1/33 | W.G. | REVISED |
| 37        | 10/1/33 | W.G. | REVISED |
| 38        | 10/1/33 | W.G. | REVISED |
| 39        | 10/1/33 | W.G. | REVISED |
| 40        | 10/1/33 | W.G. | REVISED |
| 41        | 10/1/33 | W.G. | REVISED |
| 42        | 10/1/33 | W.G. | REVISED |
| 43        | 10/1/33 | W.G. | REVISED |
| 44        | 10/1/33 | W.G. | REVISED |
| 45        | 10/1/33 | W.G. | REVISED |
| 46        | 10/1/33 | W.G. | REVISED |
| 47        | 10/1/33 | W.G. | REVISED |
| 48        | 10/1/33 | W.G. | REVISED |
| 49        | 10/1/33 | W.G. | REVISED |
| 50        | 10/1/33 | W.G. | REVISED |
| 51        | 10/1/33 | W.G. | REVISED |
| 52        | 10/1/33 | W.G. | REVISED |
| 53        | 10/1/33 | W.G. | REVISED |
| 54        | 10/1/33 | W.G. | REVISED |
| 55        | 10/1/33 | W.G. | REVISED |
| 56        | 10/1/33 | W.G. | REVISED |
| 57        | 10/1/33 | W.G. | REVISED |
| 58        | 10/1/33 | W.G. | REVISED |
| 59        | 10/1/33 | W.G. | REVISED |
| 60        | 10/1/33 | W.G. | REVISED |
| 61        | 10/1/33 | W.G. | REVISED |
| 62        | 10/1/33 | W.G. | REVISED |
| 63        | 10/1/33 | W.G. | REVISED |
| 64        | 10/1/33 | W.G. | REVISED |
| 65        | 10/1/33 | W.G. | REVISED |
| 66        | 10/1/33 | W.G. | REVISED |
| 67        | 10/1/33 | W.G. | REVISED |
| 68        | 10/1/33 | W.G. | REVISED |
| 69        | 10/1/33 | W.G. | REVISED |
| 70        | 10/1/33 | W.G. | REVISED |
| 71        | 10/1/33 | W.G. | REVISED |
| 72        | 10/1/33 | W.G. | REVISED |
| 73        | 10/1/33 | W.G. | REVISED |
| 74        | 10/1/33 | W.G. | REVISED |
| 75        | 10/1/33 | W.G. | REVISED |
| 76        | 10/1/33 | W.G. | REVISED |
| 77        | 10/1/33 | W.G. | REVISED |
| 78        | 10/1/33 | W.G. | REVISED |
| 79        | 10/1/33 | W.G. | REVISED |
| 80        | 10/1/33 | W.G. | REVISED |
| 81        | 10/1/33 | W.G. | REVISED |
| 82        | 10/1/33 | W.G. | REVISED |
| 83        | 10/1/33 | W.G. | REVISED |
| 84        | 10/1/33 | W.G. | REVISED |
| 85        | 10/1/33 | W.G. | REVISED |
| 86        | 10/1/33 | W.G. | REVISED |
| 87        | 10/1/33 | W.G. | REVISED |
| 88        | 10/1/33 | W.G. | REVISED |
| 89        | 10/1/33 | W.G. | REVISED |
| 90        | 10/1/33 | W.G. | REVISED |
| 91        | 10/1/33 | W.G. | REVISED |
| 92        | 10/1/33 | W.G. | REVISED |
| 93        | 10/1/33 | W.G. | REVISED |
| 94        | 10/1/33 | W.G. | REVISED |
| 95        | 10/1/33 | W.G. | REVISED |
| 96        | 10/1/33 | W.G. | REVISED |
| 97        | 10/1/33 | W.G. | REVISED |
| 98        | 10/1/33 | W.G. | REVISED |
| 99        | 10/1/33 | W.G. | REVISED |
| 100       | 10/1/33 | W.G. | REVISED |





SUPER-ELEVATION AND WIDENING  
HARD SURFACE AND SURFACE TREATED GRANULOID ROADS

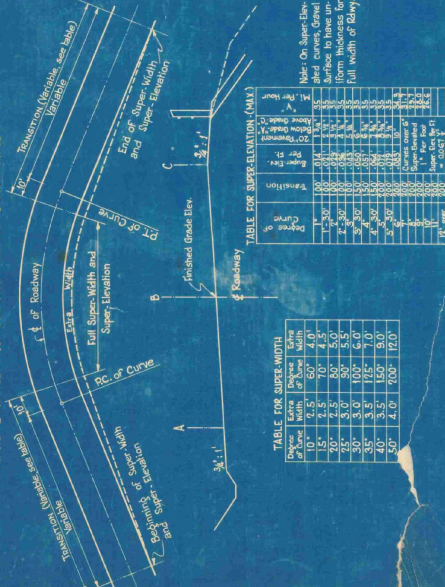


TABLE FOR COMPUTING SUPER-ELEVATIONS  
FOR PROPORTIONAL PARTS OF TRANSITION

| Distance from P.C. (Feet) | Super-Elevation (Mill) |
|---------------------------|------------------------|
| 0                         | 0.00                   |
| 10                        | 0.00                   |
| 20                        | 0.00                   |
| 30                        | 0.00                   |
| 40                        | 0.00                   |
| 50                        | 0.00                   |
| 60                        | 0.00                   |
| 70                        | 0.00                   |
| 80                        | 0.00                   |
| 90                        | 0.00                   |
| 100                       | 0.00                   |
| 110                       | 0.00                   |
| 120                       | 0.00                   |
| 130                       | 0.00                   |
| 140                       | 0.00                   |
| 150                       | 0.00                   |
| 160                       | 0.00                   |
| 170                       | 0.00                   |
| 180                       | 0.00                   |
| 190                       | 0.00                   |
| 200                       | 0.00                   |
| 210                       | 0.00                   |
| 220                       | 0.00                   |
| 230                       | 0.00                   |
| 240                       | 0.00                   |
| 250                       | 0.00                   |
| 260                       | 0.00                   |
| 270                       | 0.00                   |
| 280                       | 0.00                   |
| 290                       | 0.00                   |
| 300                       | 0.00                   |
| 310                       | 0.00                   |
| 320                       | 0.00                   |
| 330                       | 0.00                   |
| 340                       | 0.00                   |
| 350                       | 0.00                   |
| 360                       | 0.00                   |
| 370                       | 0.00                   |
| 380                       | 0.00                   |
| 390                       | 0.00                   |
| 400                       | 0.00                   |
| 410                       | 0.00                   |
| 420                       | 0.00                   |
| 430                       | 0.00                   |
| 440                       | 0.00                   |
| 450                       | 0.00                   |
| 460                       | 0.00                   |
| 470                       | 0.00                   |
| 480                       | 0.00                   |
| 490                       | 0.00                   |
| 500                       | 0.00                   |
| 510                       | 0.00                   |
| 520                       | 0.00                   |
| 530                       | 0.00                   |
| 540                       | 0.00                   |
| 550                       | 0.00                   |
| 560                       | 0.00                   |
| 570                       | 0.00                   |
| 580                       | 0.00                   |
| 590                       | 0.00                   |
| 600                       | 0.00                   |
| 610                       | 0.00                   |
| 620                       | 0.00                   |
| 630                       | 0.00                   |
| 640                       | 0.00                   |
| 650                       | 0.00                   |
| 660                       | 0.00                   |
| 670                       | 0.00                   |
| 680                       | 0.00                   |
| 690                       | 0.00                   |
| 700                       | 0.00                   |
| 710                       | 0.00                   |
| 720                       | 0.00                   |
| 730                       | 0.00                   |
| 740                       | 0.00                   |
| 750                       | 0.00                   |
| 760                       | 0.00                   |
| 770                       | 0.00                   |
| 780                       | 0.00                   |
| 790                       | 0.00                   |
| 800                       | 0.00                   |
| 810                       | 0.00                   |
| 820                       | 0.00                   |
| 830                       | 0.00                   |
| 840                       | 0.00                   |
| 850                       | 0.00                   |
| 860                       | 0.00                   |
| 870                       | 0.00                   |
| 880                       | 0.00                   |
| 890                       | 0.00                   |
| 900                       | 0.00                   |
| 910                       | 0.00                   |
| 920                       | 0.00                   |
| 930                       | 0.00                   |
| 940                       | 0.00                   |
| 950                       | 0.00                   |
| 960                       | 0.00                   |
| 970                       | 0.00                   |
| 980                       | 0.00                   |
| 990                       | 0.00                   |
| 1000                      | 0.00                   |

**PIPE CULVERT HEADWALLS - FOR CM. OR C.I. PIPE** (NH.Sto.No.PC-1)

**PIPE CULVERT HEADWALLS - FOR CONCRETE PIPE** (NH.Sto.No.PC-2)

**CATCH BASINS** (NH.Sto.No.CB-1)

**CONCRETE DROP INLETS** (NH.Sto.No.DI-1)

**WIRE CABLE RAILING - FOR ROADWAY** (NH.Sto.No.GR-2)

**WIRE CABLE RAILING - FOR BRIDGES** (NH.Sto.No.GR-3)

**WOOD RAILING** (NH.Sto.No.GR-2)

**WOOD RAILING** (NH.Sto.No.GR-2)

**WOOD RAILING** (NH.Sto.No.GR-3)

TRAFFIC SIGN SUMMARY SHEET

| MILEMARKER, STATION, OR SIGN NUMBER | SIGN LEGEND | SIGN DIMENSIONS | NEW AND SALVAGED SIGNS |     |            |              | EXIST. POSTS |       | NEW SIGN POSTS |                         |     |     |                  |       |            |               |       |       |       |               |        |           | REMARKS | SIGN DETAIL |                        |                   |     |  |        |       |
|-------------------------------------|-------------|-----------------|------------------------|-----|------------|--------------|--------------|-------|----------------|-------------------------|-----|-----|------------------|-------|------------|---------------|-------|-------|-------|---------------|--------|-----------|---------|-------------|------------------------|-------------------|-----|--|--------|-------|
|                                     |             |                 | "A"                    | "B" | SALV. SIGN | SALV. T.J.S. | RET.         | SALV. | NO. OF POSTS   | FLANGED CHANNEL LB./FT. |     |     | TUBULAR ALUMINUM |       |            | TUBULAR STEEL |       |       |       | W-SHAPE STEEL |        | FTG. SIZE |         | SIGN FRAME  | DETAIL ON SHEET NUMBER | STD. SHEET NUMBER |     |  |        |       |
|                                     |             |                 |                        |     |            |              |              |       |                | 1/2                     | 2.0 | 3.0 | 3.0"Ø            | 4.0"Ø | FOUNDATION | 3"Ø           | 3.5"Ø | 4.0"Ø | 5.0"Ø | POST SIZE     | WEIGHT | 24"       |         |             |                        |                   | 30" |  |        |       |
| 2+10 RT                             |             | 48' X 48'       | 16                     |     |            |              |              | 1     |                |                         |     | X   |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  | E-155  |       |
| RAMP A                              |             | 48' X 48'       | 16                     |     |            |              |              | 1     |                |                         |     | X   |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  | E-155  |       |
| 7+55 LT                             |             | 30' X 30'       | 6.25                   |     |            |              |              | 1     |                |                         |     | X   |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  | E-140  |       |
| 9+60 LT                             |             | 30' X 30'       | 6.25                   |     |            |              |              | 1     |                |                         |     | X   |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  | E-145A |       |
| 11+65 LT                            |             | 72' X 10'       | 5                      |     |            |              |              |       |                | 2                       |     |     | X                |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  | E-123  |       |
|                                     |             | 72' X 10'       | 5                      |     |            |              |              |       |                |                         |     |     |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        | E-123 |
| 12+20 RT                            |             | 72' X 10'       | 5                      |     |            |              |              |       |                | 2                       |     |     | X                |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        | E-123 |
|                                     |             | 72' X 10'       | 5                      |     |            |              |              |       |                |                         |     |     |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        | E-123 |
|                                     |             | 72' X 10'       | 5                      |     |            |              |              |       |                |                         |     |     |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        | E-123 |
|                                     |             | 72' X 10'       | 5                      |     |            |              |              |       |                |                         |     |     |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        | E-123 |
| 13+45 RT                            |             |                 |                        |     |            | 1            |              |       |                | 2                       |     |     | X                |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        |       |
|                                     |             |                 |                        |     |            | 1            |              |       |                |                         |     |     |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        |       |
|                                     |             | 36' X 48'       | 12                     |     |            |              |              |       |                |                         |     |     |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        | 52    |
| 14+00 LT                            |             | 2 @ 24' X 12'   | 4                      |     |            |              |              |       |                | 2                       |     |     | X                |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        | E-135 |
|                                     |             | 2 @ 24' X 24'   | 8                      |     |            |              |              |       |                |                         |     |     |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        | E-135 |
|                                     |             | 2 @ 21' X 15'   | 4.30                   |     |            |              |              |       |                |                         |     |     |                  |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        | E-135 |
| 17+10 LT                            |             |                 |                        |     |            | 1            |              |       |                | 1                       |     |     | X                |       |            |               |       |       |       |               |        |           |         |             |                        |                   |     |  |        |       |

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD.  
 POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED  
 ON THE STANDARD SHEETS AND THE TRAFFIC & SAFETY DIVISION'S

DRAWN BY KML DATE 5/95  
 SQUAD LEADER STEWART  
 DESIGN FILE NO. ZSDA/898018/DB081R  
 IPARM DB081SI DATE PLOTTED 15-FEB-1996

VERMONT AGENCY OF  
TRANSPORTION  
PHASE III-INTERSTATE

HANGER 4396

Bridge  
19 34  
35

Waterford Vt. and Littleton  
# 263

N.H. 19 34  
35