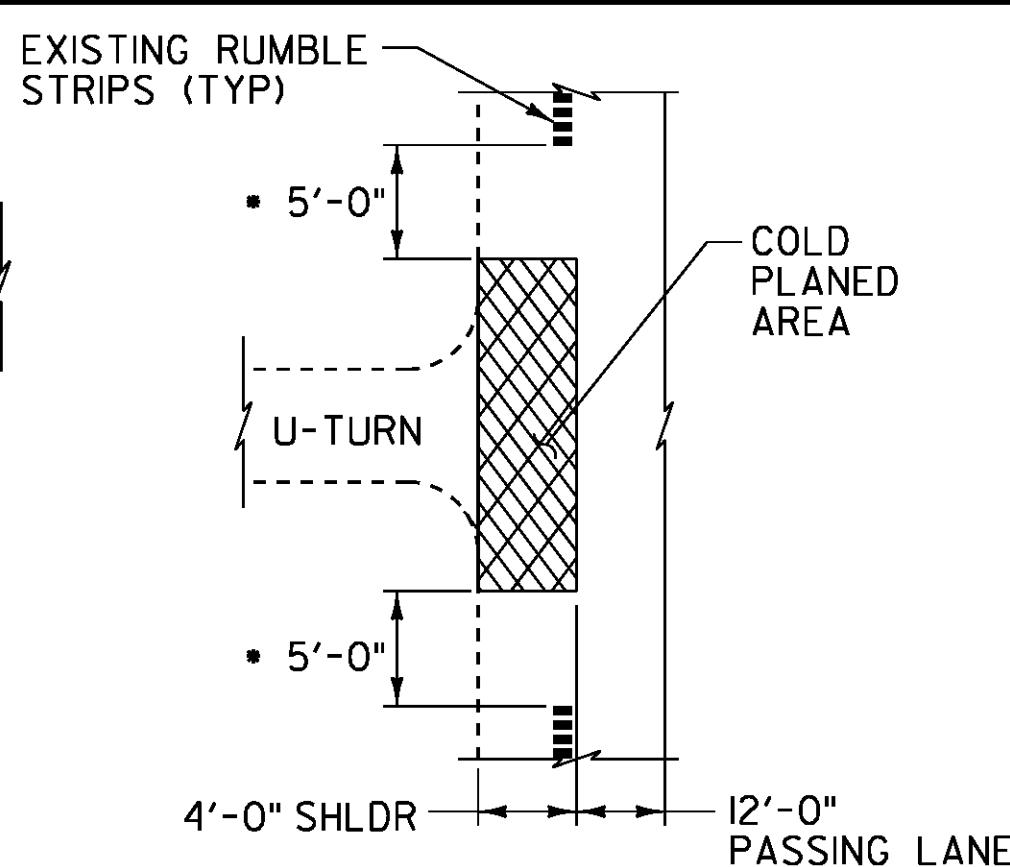
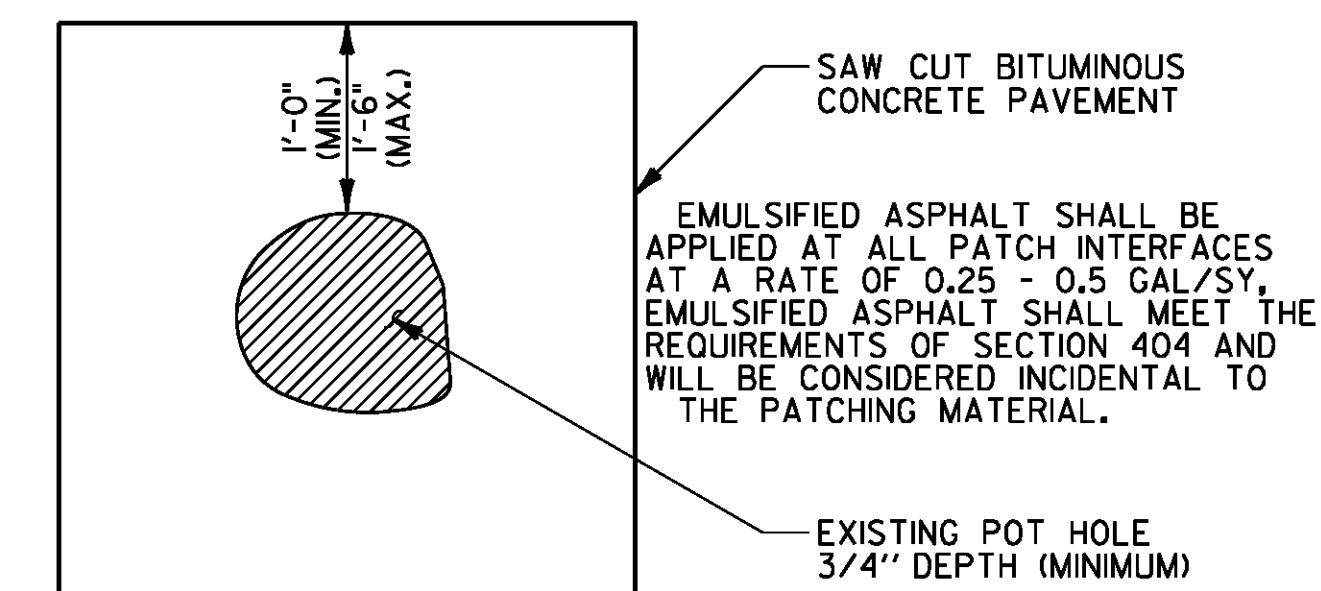
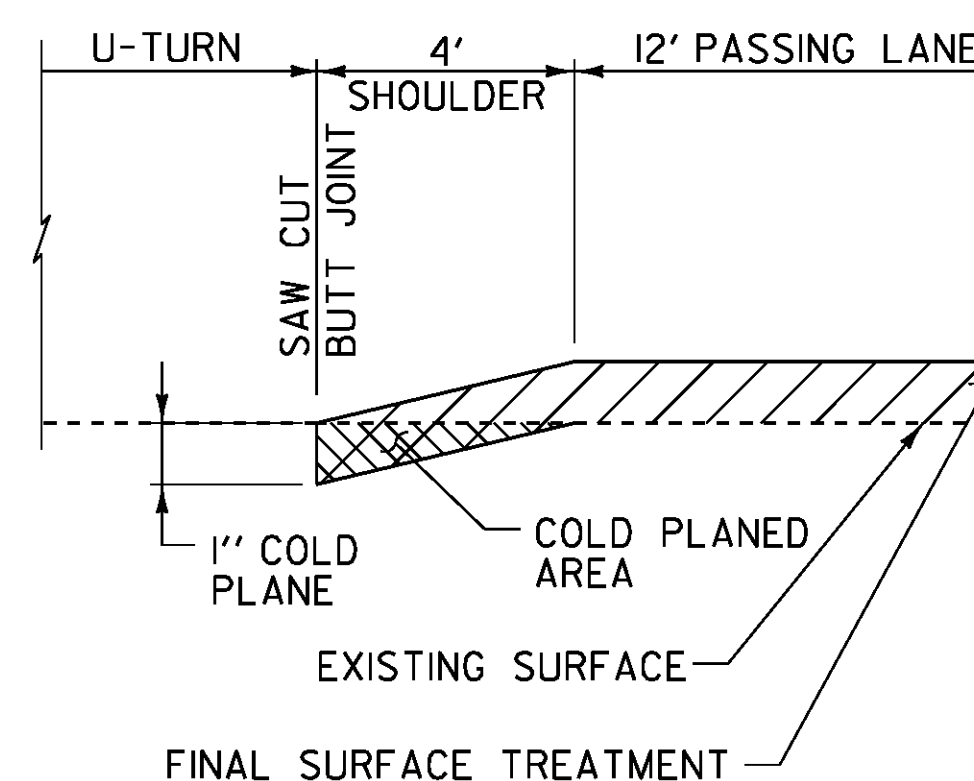


TYPICAL APPROACH AREA DETAIL MAINLINE & RAMPS
NOT TO SCALE

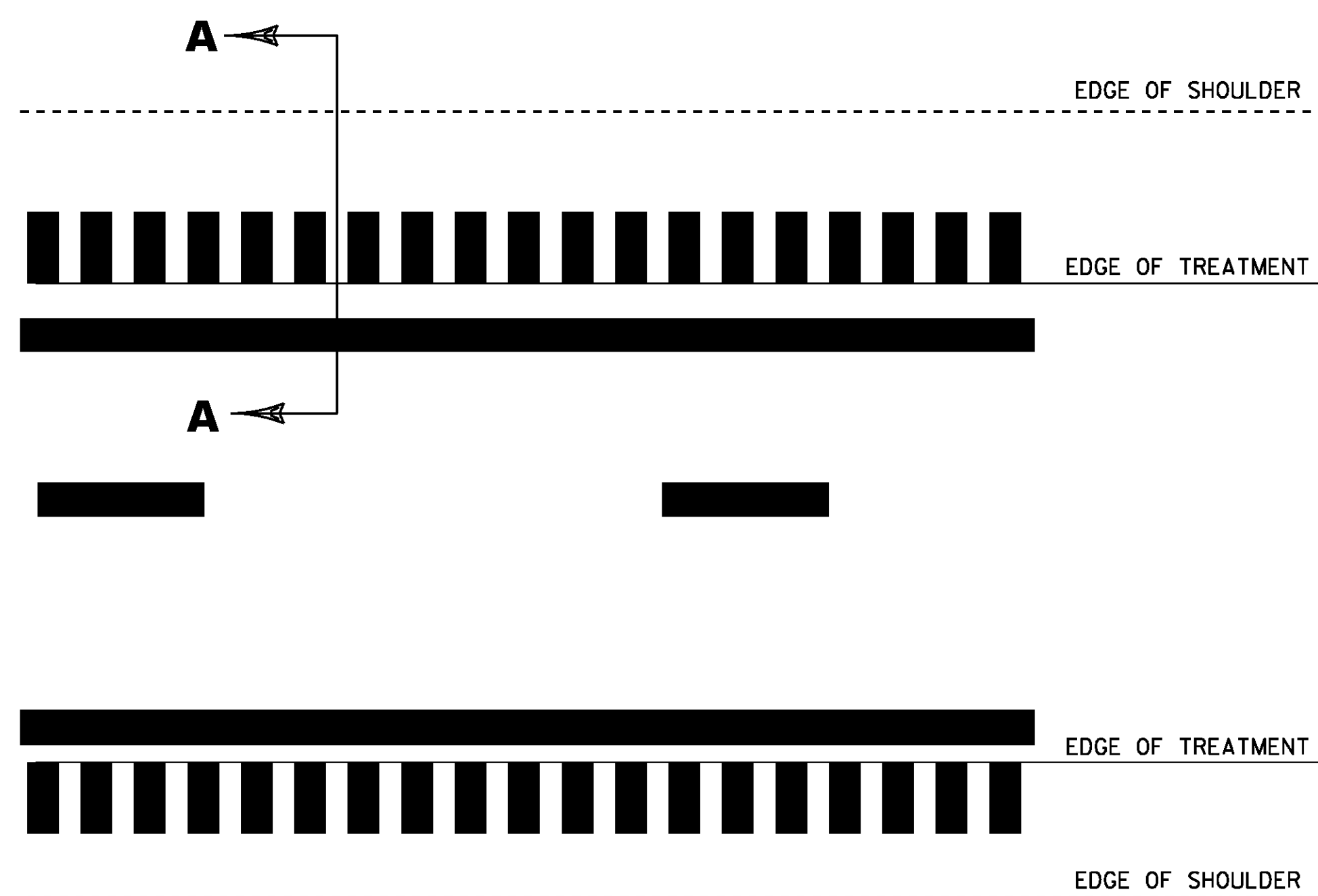


- BEGIN COLD PLANING 5'-0" AFTER RUMBLE STRIPS END, AND END COLD PLANING 5'-0" BEFORE RUMBLE STRIPS BEGIN

COLD PLANE DETAIL AT U-TURNS
NOT TO SCALE

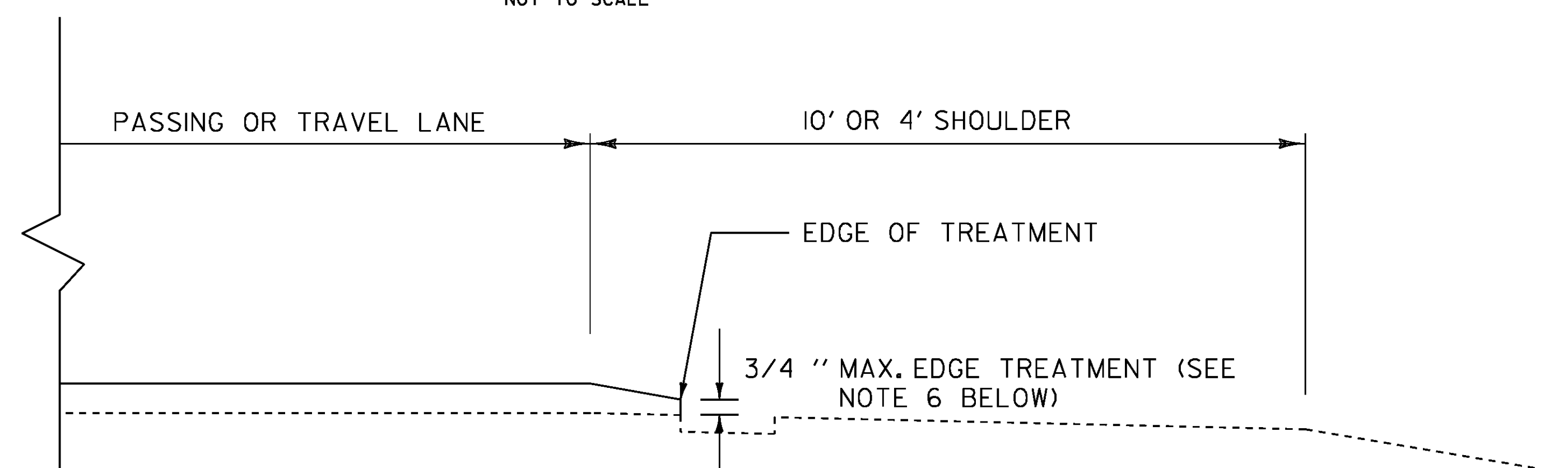


TYPICAL POT HOLE REPAIR
NOT TO SCALE



EDGE OF TREATMENT TYPICAL PLAN

NOT TO SCALE



EDGE OF TREATMENT TYPICAL SECTION A-A

NOT TO SCALE

NOTES:

1. ALL NECESSARY SURFACE PREPARATION INVOLVING PATCHING, POT HOLE REPAIR, AND CRACK-SEALING SHALL BE PERFORMED PRIOR TO APPLICATION OF THE FINAL SURFACE TREATMENT. ALL CRACKS GREATER THAN 0.10" AND UP TO 1.0" IN WIDTH SHALL BE SEALED USING THE "BLOW AND GO" FILL METHOD. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE PAID FOR UNDER ITEM 417.20, BITUMINOUS CRACK SEALING, "BLOW AND GO" METHOD. THE PATCHING OF ALL CRACKS GREATER THAN 1.0" AND ALL OTHER PATCHING AND POT HOLE REPAIR SHALL BE COMPLETED USING BITUMINOUS CONCRETE PAVEMENT IN ACCORDANCE WITH ITEM 900.680 SPECIAL PROVISION (BITUMINOUS CONCRETE PAVEMENT SURFACE PREPARATION, TYPE D). AN ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN INCLUDED TO COVER ALL COSTS ASSOCIATED WITH THIS WORK.

2. EXISTING SHOULDER PAVEMENT SURFACES BEYOND THE LIMITS OF THE FINAL SURFACE TREATMENT SHALL ALSO RECEIVE CRACK-SEALING AND RELATED PATCHING AND POT HOLE REPAIR TREATMENTS.

3. FOLLOWING COMPLETION OF COLD PLANING, THE MILLED SURFACE FOR ALL BRIDGES SHALL ALSO RECEIVE CRACK-SEALING AND RELATED PATCHING AND POT HOLE REPAIR TREATMENTS, AS DIRECTED BY THE ENGINEER.

4. ALL EXISTING PAVEMENT MARKINGS SHALL BE REMOVED PRIOR TO ANY CRACK SEALING BEING PERFORMED AND PRIOR TO APPLYING THE FINAL SURFACE TREATMENT. ALL LANE DELINEATION IS TO BE MAINTAINED DURING CONSTRUCTION BY THE USE OF LINE STRIPING TARGETS OR TEMPORARY PAINT.

5. A 50' COLD PLANED WEDGE SHALL BE CONSTRUCTED AT THE PROJECT BEGIN, PROJECT END, RAMPS, AND AT ALL BRIDGE APPROACHES OR AS DIRECTED BY THE ENGINEER. THE LONGITUDINAL EDGES OF THE SURFACE TREATMENT SHALL BE FEATHERED AS SHOWN ON THE TYPICAL SECTION, OR AS DIRECTED BY THE ENGINEER. ANY SAWCUTTING AT BUTT JOINTS SHALL BE PAID INCIDENTAL TO ITEM 900.675 SPECIAL PROVISION (MICRO-MILLING BITUMINOUS CONCRETE PAVEMENT). ALL PROJECT COLD PLANING WILL BE PAID AS ITEM 900.675 SPECIAL PROVISION (MICRO-MILLING BITUMINOUS CONCRETE PAVEMENT).

6. THE SCREED OF THE PAVER SHALL BREAK AT THE BREAK POINT OF THE SHOULDER SUCH THAT THE DESIGNED NOMINAL THICKNESS IS CARRIED ONTO THE SHOULDER AND BROKEN OR PINCHED BY ROLLING. EDGE HEIGHT SHALL BE MEASURED AT MINIMUM OF FIVE RANDOMLY SPACED POINTS PER TENTH OF A MILE. IF IT IS FOUND THAT THE AVERAGE EDGE HEIGHT IS GREATER THAN 3/4" OVER THE TENTH OF A MILE, WORK SHALL BE PERFORMED SUCH THAT NO MORE THAN THE 3/4" AVERAGE HEIGHT IS ATTAINED. THIS APPLIES TO BOTH THE 10' AND 4' SHOULDER SIDES OF THE HIGHWAY.

**NOTES & DETAILS
COMMON TO
BOTH
ALTERNATES**

PROJECT NAME: WATERFORD
PROJECT NUMBER: IM SURF(47)

FILE NAME: I4a122\pl4a122.dgn
PROJECT LEADER: M. FOWLER
DESIGNED BY: K. LOCKE
IPARM FILE NAME: pl4a122_05.1

PLOT DATE: 10-DEC-2014
DRAWN BY: K. LOCKE
CHECKED BY: M. FOWLER
SHEET 5 OF 22