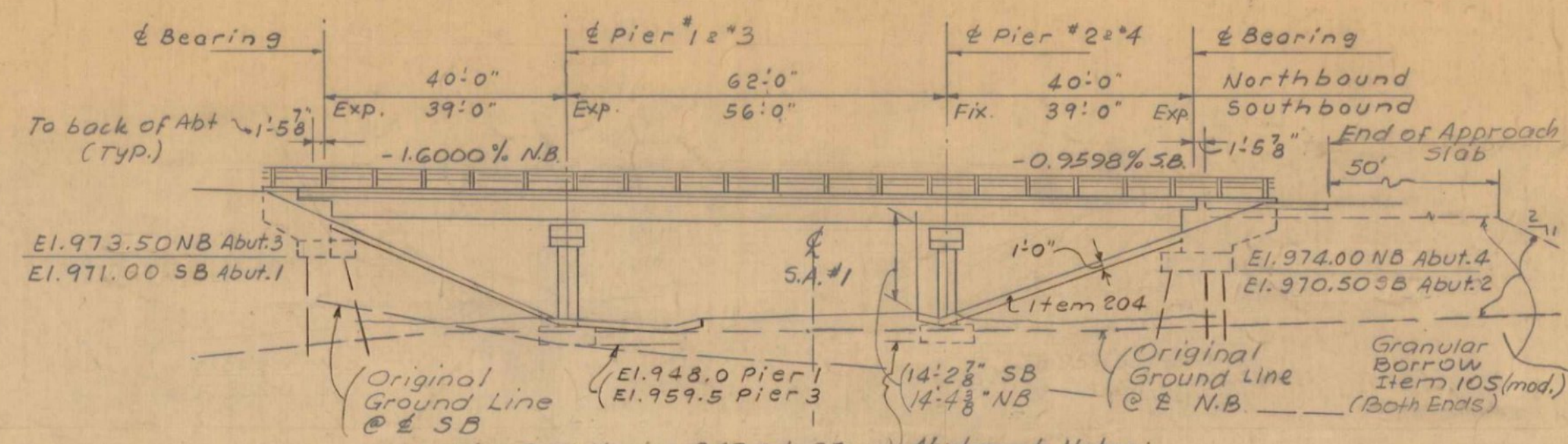
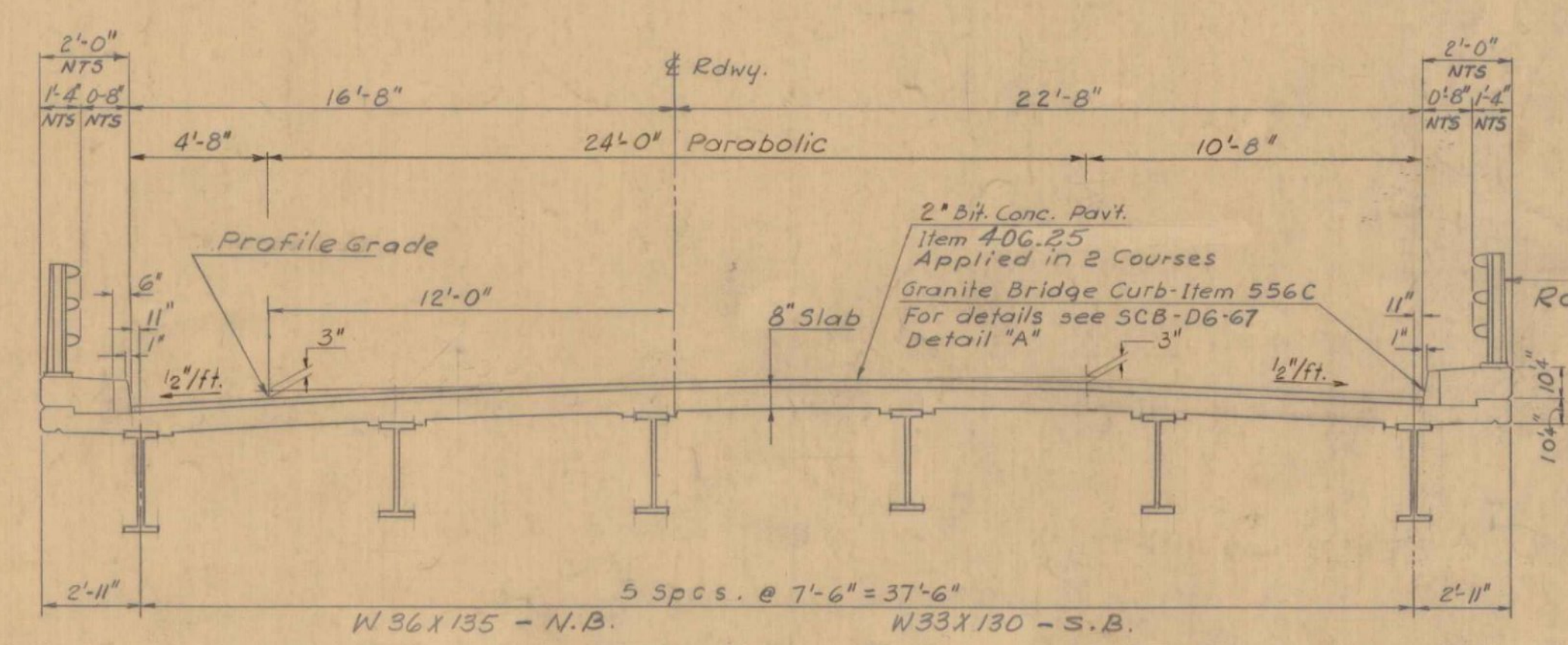


NEW HIGHWAY SECT. I-91 @ NB 1594+50
SCALE 1" = 20'



NEW HIGHWAY PROFILE ALONG C
SCALE 1" = 20'



TYPICAL SECTION
Northbound shown, Southbound similar

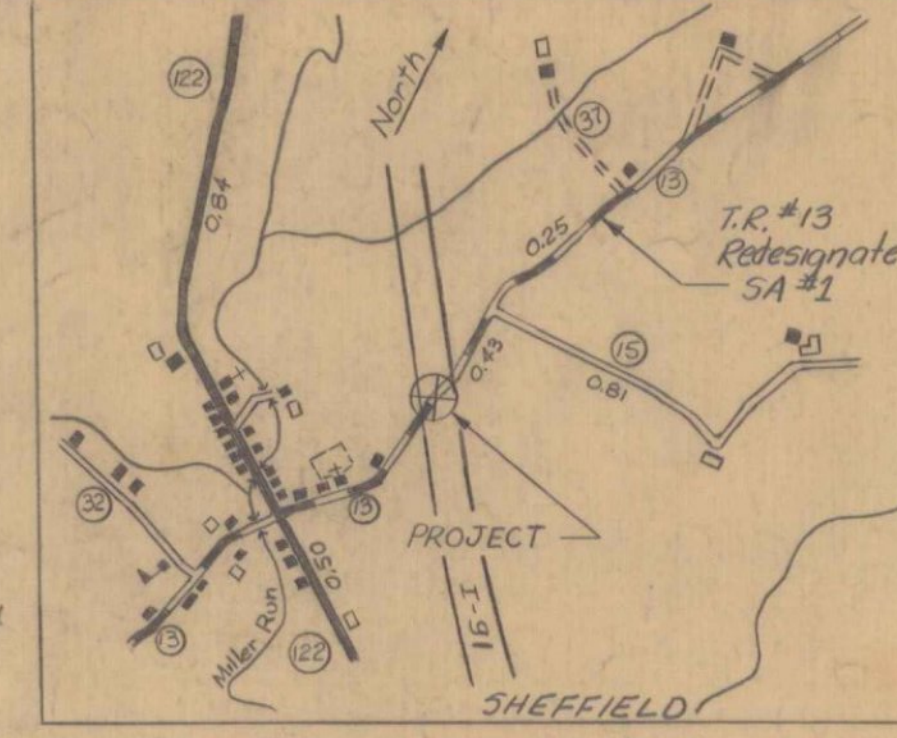


TABLE OF ESTIMATED PILE LENGTHS - 12BP53

Abut. #1 (SB)	25'
Abut. #2 (SB)	40'
Abut. #3 (NB)	30'
Pier #3 (NB)	20'
Pier #4 (NB)	30'
Abut. #4 (NB)	60'

HIGHWAY NO. I-91 NAME OF HIGHWAY INTERSTATE
STRUCTURE NO. 5 COUNTY CALEDONIA TOWN SHEFFIELD
PROJECT NO. I-91-3(10) LOCATION S.A.#1 (Relocated-Redesignated)
(10) LYNDON-BARTON

EXISTING STRUCTURE - None

1. RECOMMENDED TYPE OF STRUCTURE 39'-4" Roadway; 3-Cont. Spans, Rolled Beams
2. RECOMMENDED CLEAR SPAN OR SPANS Composite in Span 2 Only
MEASURED PARALLEL TO NEW HIGHWAY 40'-62'-40'-N.B. 39'-56'-39'-S.B.
MEASURED AT RIGHT ANGLES TO NEW HIGHWAY S.A.#1 (26'-10")-(46'-7")-(26'-10") (26'-2")-(37'-7")-(26'-2")

NEW STRUCTURE

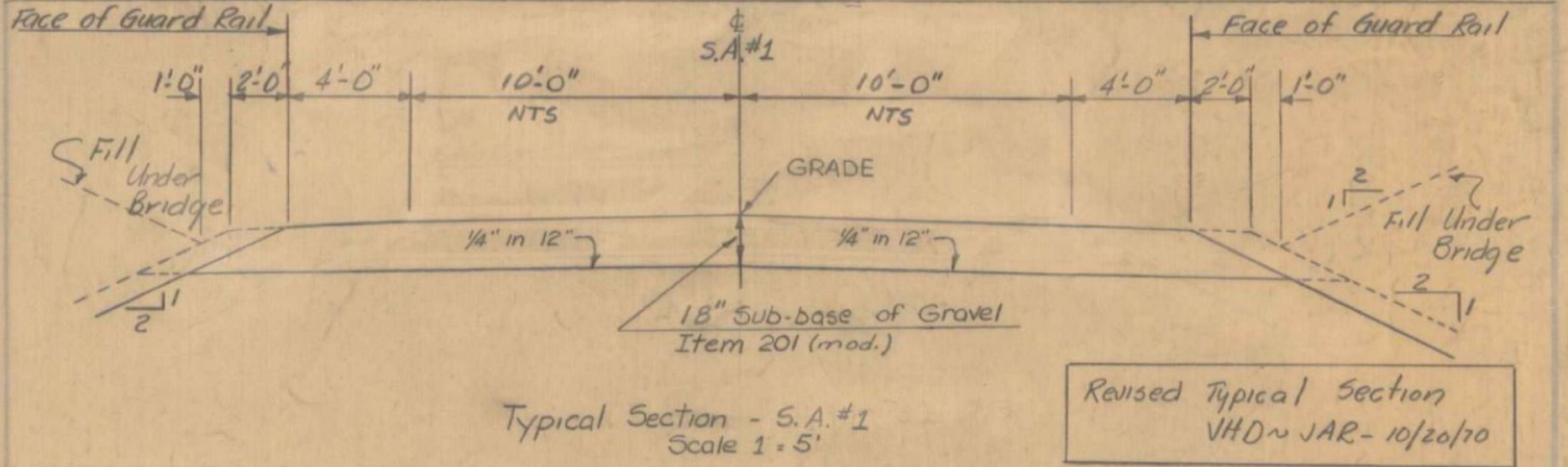
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MEASURED AT RIGHT ANGLES TO NEW HIGHWAY S.A.#1 (26'-10")-(46'-7")-(26'-10") (26'-2")-(37'-7")-(26'-2")

16. ARE SIDEWALKS REQUIRED? If so on what side? BOTH SIDES
17. RECOMMENDED TYPE OF PAVEMENT 2" Bituminous Concrete; 8" Concrete Slab
18. PROBABLE COST OF CLEARING AND GRUBBING STREAM CHANNEL AT STRUCTURE SITE
20. SHOULD PROVISIONS BE MADE FOR PUBLIC UTILITIES? No.
21. ESTIMATED ALLOWABLE LOAD ON FOUNDATIONS 6 T/ft. on ledge. Should piles be used? Yes EST. COST 12BP53 + see Table

FOUNDATION INFORMATION

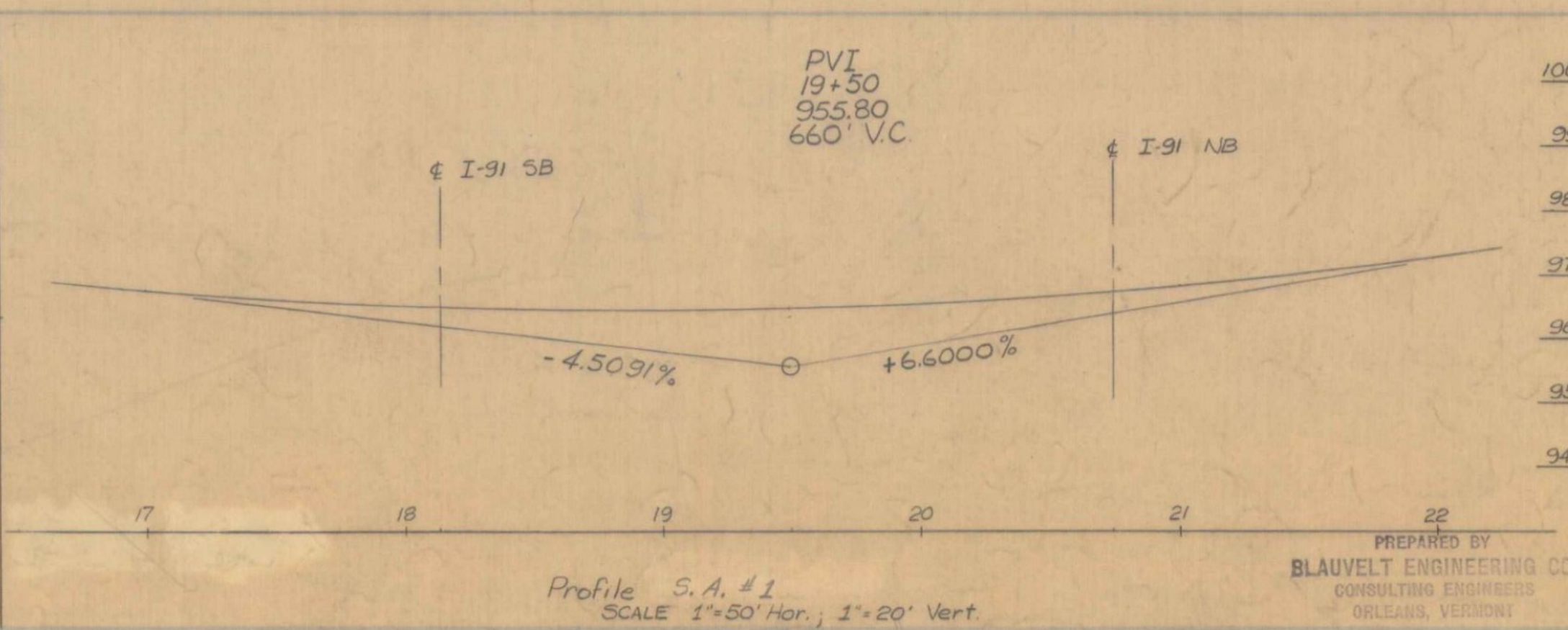
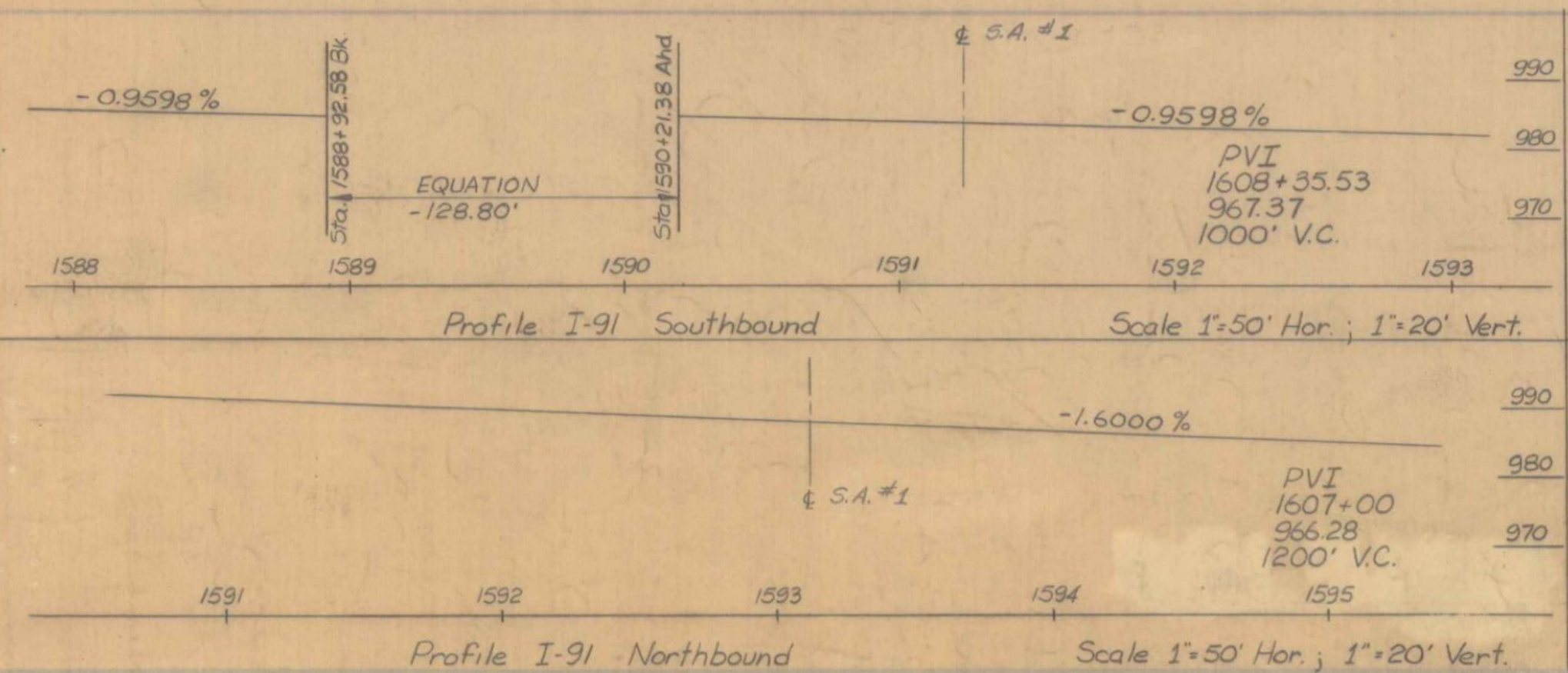
OBTAINED FOR DESIGN PURPOSES ONLY, AND THE STATE ASSUMES NO RESPONSIBILITY WHATSOEVER FOR THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN. BOULDERS MAY BE ENCOUNTERED AT ANY PIER OR ABUTMENT LOCATION.

Abutments are founded on Steel H-Piles (12BP53-SB) (pile, Point Bearing)
S.B. - Piers are founded on Ledge
N.B. - Piers are founded on Steel H-Piles (12BP53-SB) (pile, Point Bearing)



SPECIFICATIONS: All materials and construction shall conform to the State of Vermont, Department of Highways, Standard Specifications, for Highway and Bridge construction dated April 1964 and the A.A.S.H.O Standard Specifications for Highway Bridges dated 1969.

DESIGN LOADING: HS 20-44 as modified for National System of Interstate Highways
DESIGN STRESSES: Reinforcing Steel - $f_s = 20,000$ p.s.i., Concrete - $f_c = 3000$ p.s.i., $n = 10$, $f_r = 1200$ p.s.i.



STATE OF VERMONT
DEPARTMENT OF HIGHWAYS

INTERSTATE IN THE TOWNS OF
SHEFFIELD
PROJECT - LYNDON-BARTON
ROUTE NO I-91 LDG STA BR 99-5
I-91 over S.A.#1
Stage 2 Construction

RECOMMENDED FOR APPROVAL [Signature] 12/30/66
CONSTRUCTION ENGINEER DATE

RECOMMENDED FOR APPROVAL [Signature] 12/30/66
DATE

RECOMMENDED FOR APPROVAL [Signature]
DATE

APPROVED BY [Signature] CHIEF

LYNDON-IRASBURG (M MEMB(29)
SHEET NO. 99S & 100N
FOR REFERENCE ONLY

REvised typical to 2'-0" brush curb, changed curb reinforcing ~ VHD ~ JAR ~ NKN 6-19-70

PREPARED BY BLAUVELT ENGINEERING CO. CONSULTING ENGINEERS ORLEANS, VERMONT

PROJECT NO I-91-3(10) SHEET 100 of 100 BR102
(10) 382 389