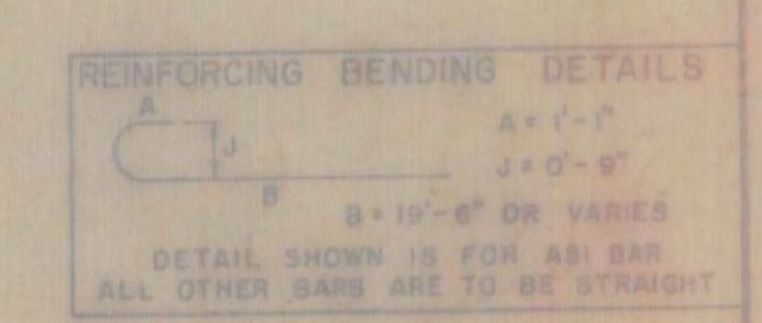


- GENERAL NOTES**
- ALL WORK AND MATERIALS SHALL CONFORM TO THE STATE OF VERMONT, DEPARTMENT OF HIGHWAYS, STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION DATED APRIL 1964, AND THE A.A.S.H.O. SPECIFICATIONS DATED 1969, DESIGNED FOR HS20-44 LOADING.
 - ALL REINFORCING STEEL SHALL BE DETAILED ON THE REINFORCING STEEL SCHEDULE. ALL SPLICES SHALL BE A MINIMUM OF 40 BAR DIAMETERS.
 - For Curtain Wall and Approach Slab Bracket details see STD 5CB-D2-67A
 - Bituminous concrete pmt depth shall be 2" at the Bridge end and vary to 3" at the roadway end.

VOID

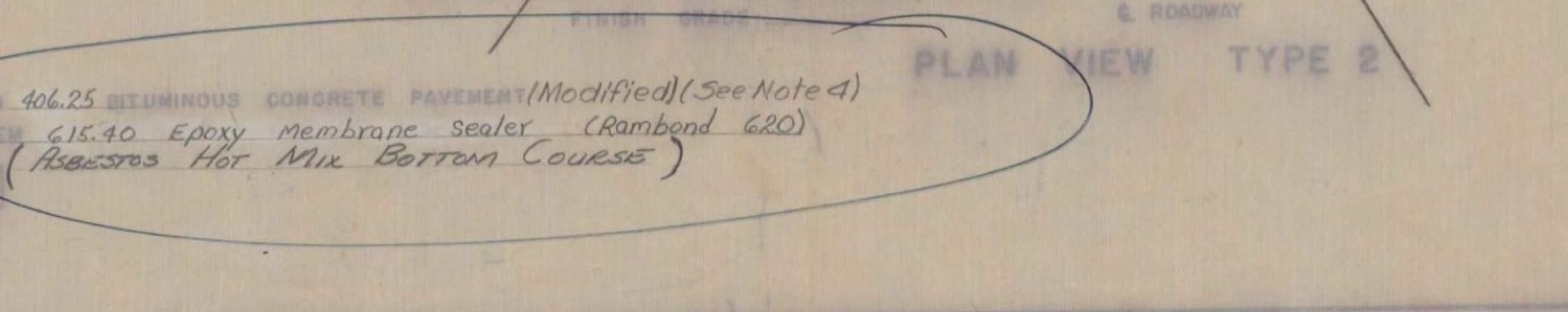
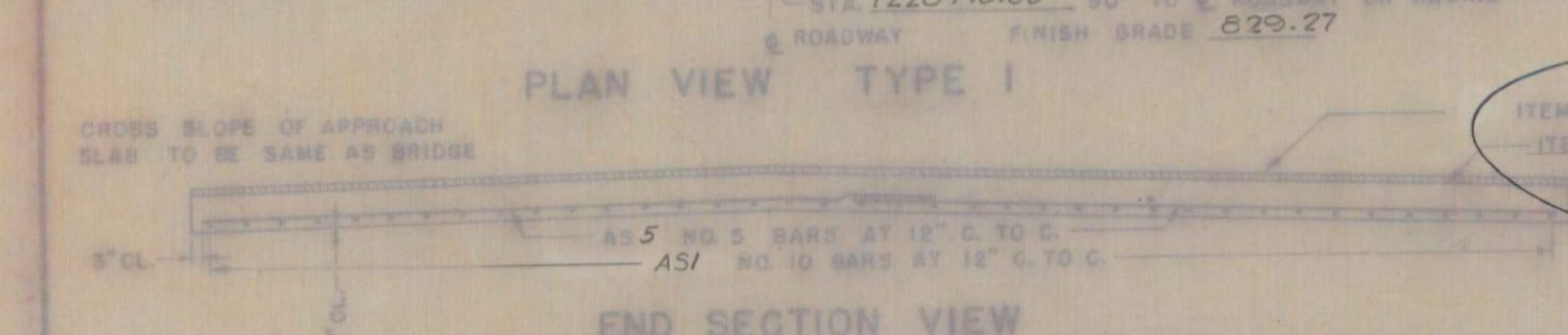
LYNDON-IRASBURG
 (M MEMB(29)
 SHEET 19 OF 55
 BRIDGE NO. 955
 FOR REFERENCE ONLY



LIST OF QUANTITIES

ITEM NO.	ITEM	UNIT
318	TAR EMULSION FOR BRIDGE FLOORS	GAL.
361-B	BITUMINOUS CONCRETE PAVEMENT	TONS
372-A	JOINT SEALER - HOT POURING	L.F.
372-C	JOINT SEALER - PREPARED, TYPE A	L.F.
401-B	CONCRETE CLASS B	CY.
402	REINFORCING STEEL	LB.

Stage 2 Construction
 CONTRACT 1
 STAGE 1 CONSTRUCTION



REVISIONS AND CORRECTIONS

NO.	DESCRIPTION	DATE
1.	DIMENSIONS OF JOINT SEALER TYPE A REVISED.	5/15/65 W.B.T.
2.	DIMENSIONS OF JOINT SEALER TYPE B REVISED.	6/23/65 W.B.T.
3.	JOINT BETWEEN CURB AND SLAB REVISED, BITUMINOUS CONCRETE REVISED TO 2" QUANTITY TOTALS REMOVED. 12/7/65 W.B.T.	

DRAWN BY: W.B.T. Jan 1964
 TRACED BY: W.B.T. Jan 1964
 CHECKED BY: W.M.S. Feb 1965

RECOMMENDED FOR APPROVAL: [Signature] BRIDGE ENGINEER DATE 2/1/65
 RECOMMENDED FOR APPROVAL: [Signature] ASSISTANT CHIEF ENGINEER DATE 4/1/65
 APPROVED BY: [Signature] CHIEF ENGINEER DATE 4/1/65

DETAILS OF APPROACH SLAB FOR 39'-4" FOOT BRIDGE
 TO BE USED FOR BRIDGE AT STATION SB 1227+09.31
 LOCATION I-91 SB OVER TOWN ROAD No. 34
 Updated 5/8/70 AGC, CSR, JLA.

STATE OF VERMONT DEPARTMENT OF HIGHWAYS STANDARD STRUCTURE SB-AS-65

PROJECT LYNDON - BARTON
 TOWN OF LYNDON
 ROUTE NO. 1-91 STA. 1227
 1-91 OVER TOWN ROAD No. 34
 APPROACH SLAB NO. 2

NOT TO SCALE
 IN CHARGE OF H.C. MARGOLIS
 DESIGNED BY LBR CHECKED BY CFP
 PROJECT NO. 1-91-365 (18)
 SHEET 163 OF 365 369 BR 118