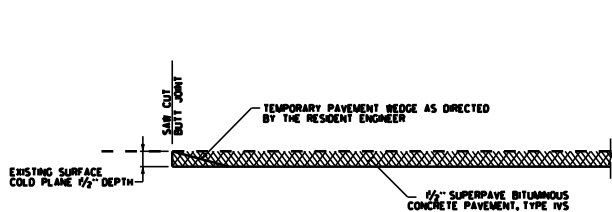


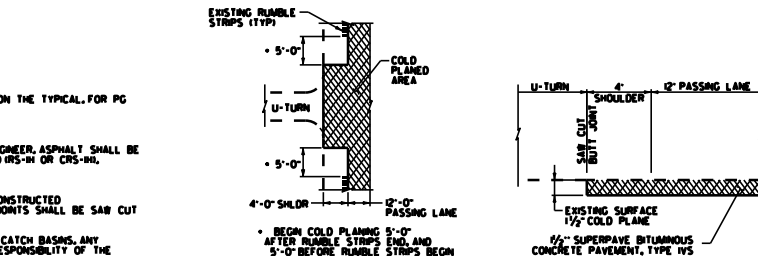
TYPICAL SECTION



COLD PLANE DETAIL AT BEGIN/END PROJECT & RAMPS

NOTES:
1. SURFACE PREPARATION IS REQUIRED ON ALL RAMPS.

TYPICAL RAMP SECTION



COLD PLANE DETAIL AT U-TURNS

MM 4820 LT
MM 5405 LT
MM 5205A LT
MM 5354S LT
MM 5630 LT

NOT TO SCALE

ALTERNATE B
TYPICAL
SECTIONS

PROJECT NAME: SPRINGFIELD - WINDSOR
PROJECT NUMBER: IM SURF (23)

FILE NAME: \\0408\0408.dgn
PROJECT LEADER: MUNT
DESIGNED BY: MUNT
DRAWN BY: MUNT
CHECKED BY: PAVI MONT
PLOT DATE: 23-MAY-2004 09:06
SHEET 3 OF 8

NOTES:

- THE PAVEMENT BEARING COURSE SHALL BE 80 GYRATION TYPE IVS ON THE ROADWAY AND BRIDGE SURFACES, ITEM 490.30 AS SHOWN ON THE TYPICAL, FOR PG BINDER GRADE SEE SECTION 490 OF THE GENERAL SPECIAL PROVISIONS.
- SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = +/- 1/4" (TOTAL PAVEMENT THICKNESS)
- EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL COLD PLANNED SURFACES AT A RATE OF 0.080 GAL/SY OR AS DIRECTED BY THE ENGINEER, ASPHALT SHALL BE RS-1H OR CRS-1H PER THE MANUFACTURER'S RECOMMENDATION, AND SHALL BE PAID AS 900.683 SPECIAL PROVISION (EMULSIFIED ASPHALT) RS-1H OR CRS-1H.
- IN AREAS OF RUTTING THE COLD PLANE DEPTH SHOULD BE MEASURED FROM THE HIGH POINTS OF THE ROADWAY SECTION.
- COLD PLANNING TO BE COMPLETED ACCORDING TO THE TYPICAL OR AS NOTED ON THE PLANS, A FULL DEPTH BUTT JOINT SHALL BE CONSTRUCTED AT THE PROJECT BEGIN/END AND AT ALL RAMP APPROACHES AS NOTED ON THE PLANS OR AS DIRECTED BY THE ENGINEER, ALL BUTT JOINTS SHALL BE SAW CUT INCIDENTAL TO ITEM 203.40.
- THE CONTRACTOR SHALL USE CAUTION WHEN COLD PLANNING AND PAVING OPERATIONS OCCUR ADJACENT TO EXISTING DROP INLETS OR CATCH BASINS, ANY DAMAGE WHICH OCCURS TO THESE DRAINAGE STRUCTURES OR BRIDGE MEMBRANES AS A RESULT OF THESE OPERATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE STATE OF VERMONT.
- IF IT IS DETERMINED IN AREAS ALONG THE BASE OF THE GUARDRAIL WHERE WINTER SAND AND OTHER DEBRIS HAS ACCUMULATED SUFFICIENTLY TO AFFECT PROPER CRACK SEALING AND RELATED PATCHING AND POTHOLE REPAIR TREATMENTS, THIS MATERIAL SHALL BE REMOVED PRIOR TO CRACK SEALING, PATCHING, AND POTHOLE REPAIR AS DIRECTED BY THE RESIDENT ENGINEER, AN ESTIMATED QUANTITY FOR ITEM 203.40 SHOULDER BERM REMOVAL HAS BEEN INCLUDED TO COVER THE COSTS ASSOCIATED WITH THIS WORK.
- ALL NECESSARY SURFACE PREPARATION INVOLVING PATCHING, POTHOLE REPAIR, AND CRACK SEALING SHALL BE PERFORMED PRIOR TO PAVING, ALL CRACKS GREATER THAN 0.10" AND UP TO 1.0" IN BOTH SHALL BE SEALED USING THE "BLOW AND GO" METHOD, ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE PAID UNDER ITEM 417.20 BITUMINOUS CRACK SEALING, "BLOW AND GO" METHOD, THE PATCHING OF ALL CRACKS GREATER THAN 1.0" AND ALL OTHER PATCHING AND POTHOLE REPAIR SHALL BE COMPLETED USING BITUMINOUS CONCRETE PAVEMENT IN ACCORDANCE WITH ITEM 500.680 SPECIAL PROVISION (BITUMINOUS CONCRETE PAVEMENT SURFACE PREPARATION, TYPE II), ALL COLD PLANNED BRIDGE DECKS AND SHOULDERS BEYOND THE LIMITS OF COLD PLANNING AND PAVING WITHIN THE PROJECT LIMITS SHALL ALSO RECEIVE CRACK SEALING AND RELATED SURFACE PREPARATION PRIOR TO PAVING, AN ESTIMATED QUANTITY OF THIS ITEM HAS BEEN INCLUDED TO COVER ALL COSTS ASSOCIATED WITH THIS WORK.