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STATE OF VERMONT AGENCY OF TRANSPORTATION

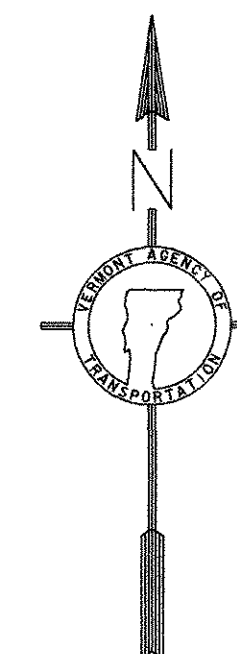
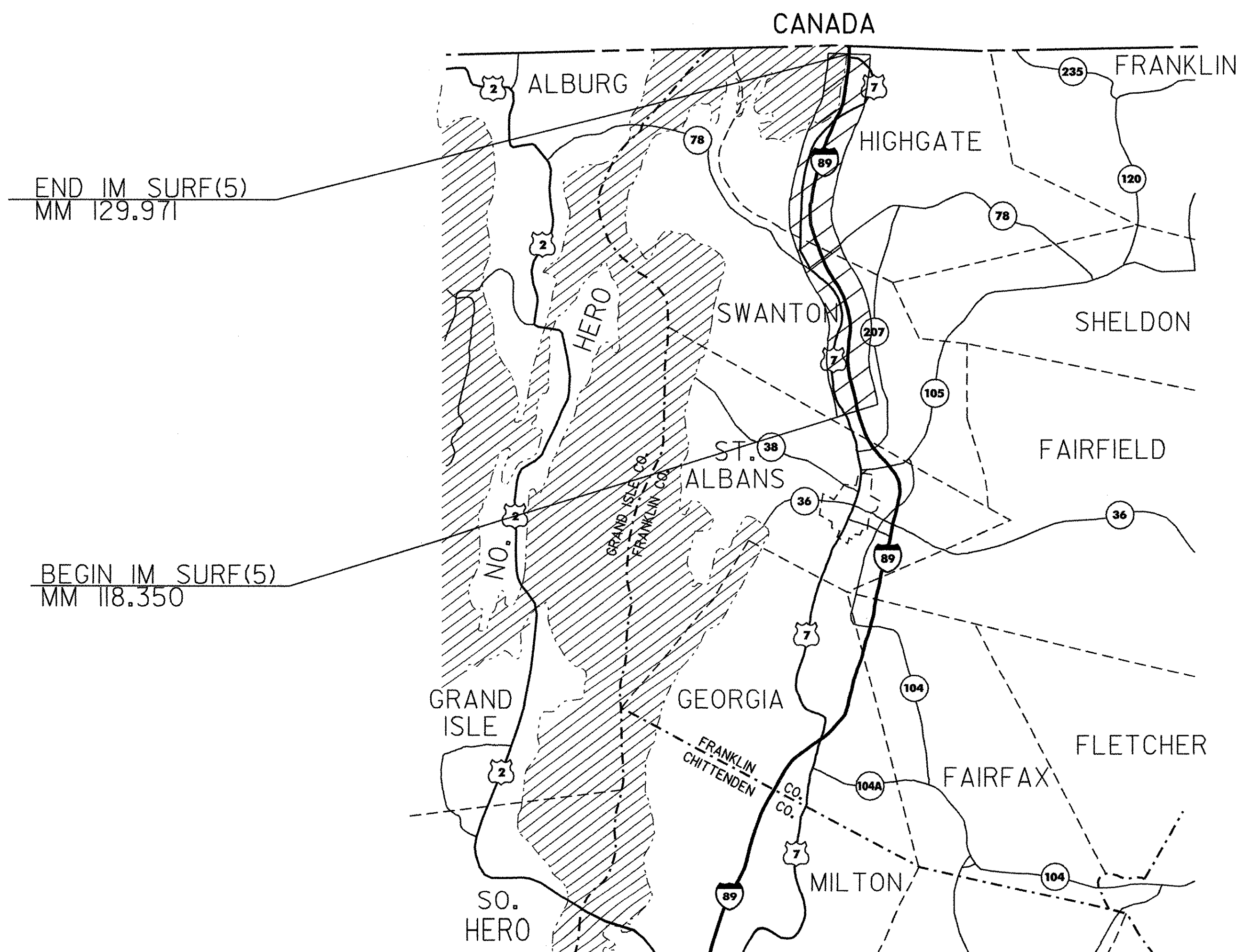
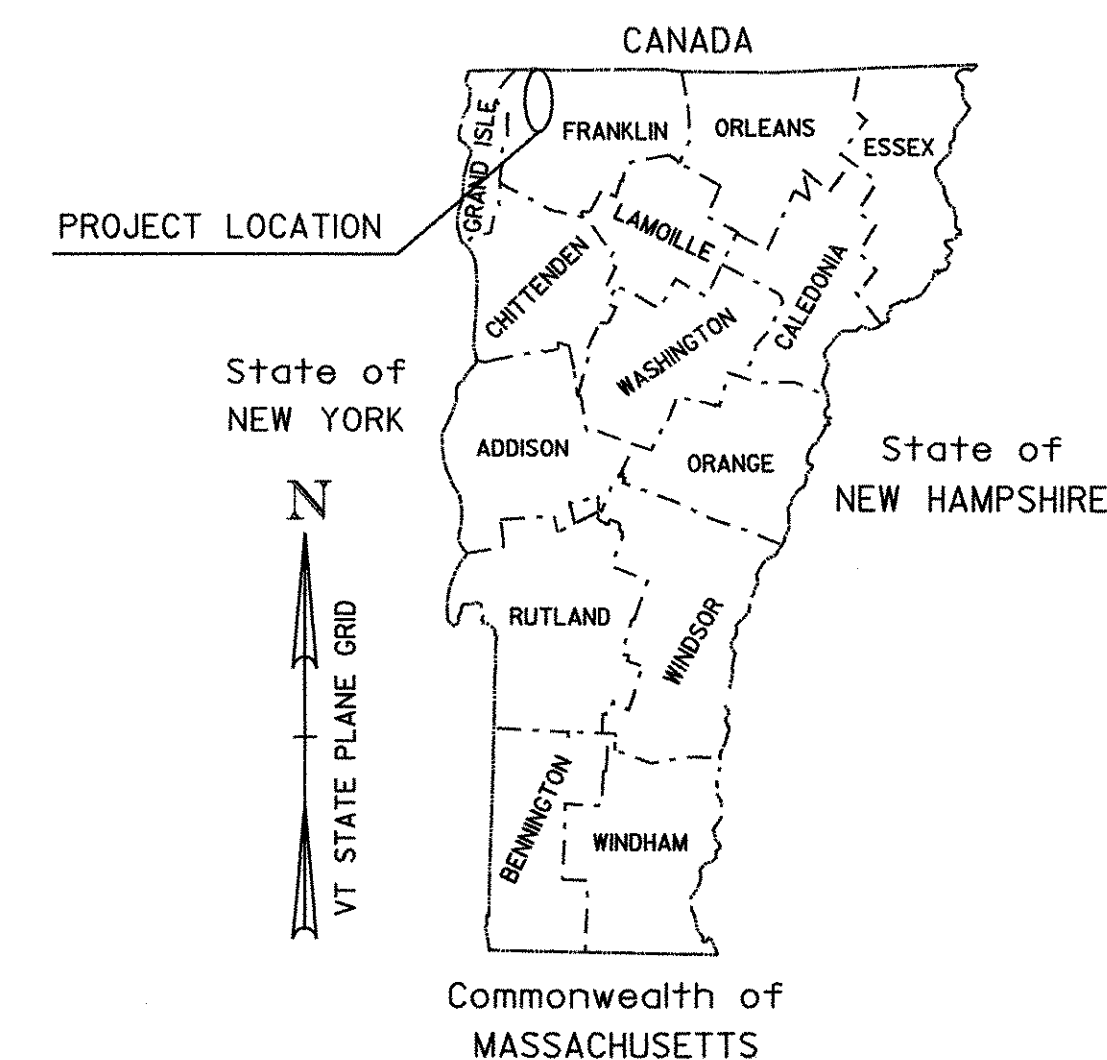


PROPOSED IMPROVEMENT TOWNS OF SWANTON AND HIGHGATE COUNTY OF FRANKLIN INTERSTATE ROUTE 89

BEGINNING AT MM 118.350 ON THE SOUTHBOUND LANES IN THE TOWN OF SWANTON
AND EXTENDING 11.621 MILES NORTH TO MM 129.971 IN THE TOWN OF HIGHGATE.

LENGTH OF PROJECT = 11.621 MILES
LENGTH OF ROADWAY = 11.621 MILES

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES THE CONSTRUCTION OF A
PREVENTATIVE MAINTENANCE TREATMENT SITE ON THE EXISTING INTERSTATE TYPICAL.



RECORD PLANS	
CONTRACTOR:	THE GORMAN GROUP, LLC - ALBANY, NY
RESIDENT ENGINEER:	DELVIN WARNER
CONSTRUCTION BEGAN:	AUGUST 11, 2008
CONSTRUCTION COMPLETE:	SEPTEMBER 18, 2008
RECORD PLANS BY:	D. WARNER & N. GARBACIK
I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.	
BY:	<i>Delvin Warner</i> RESIDENT ENGINEER
DATE:	09/09/09
NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.	

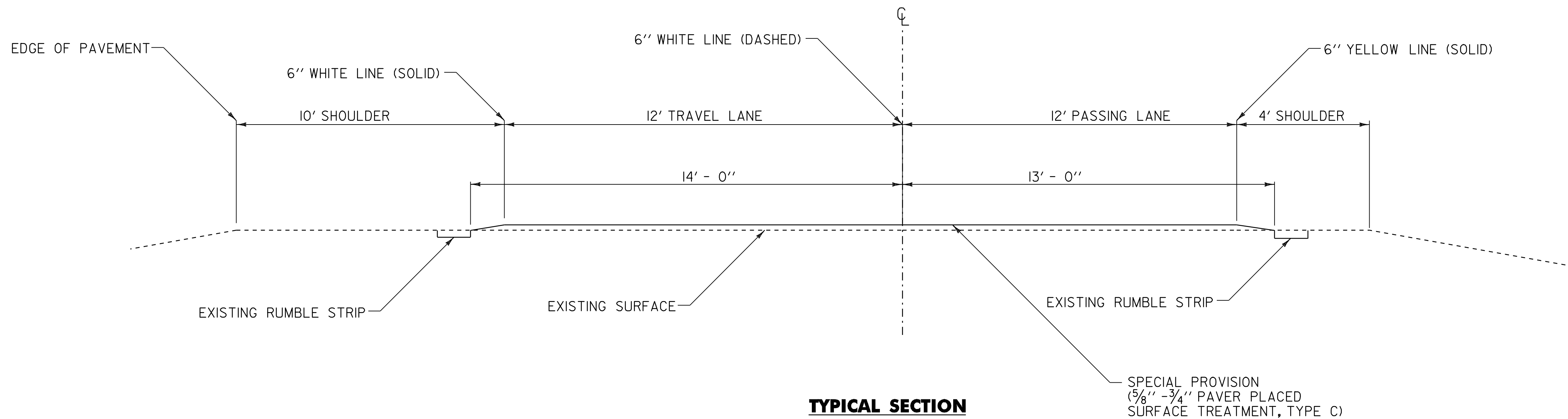
TRAFFIC DATA			
I-89 SOUTHBOUND		AADT	
		2008	2018
SECTION #1	(BEGIN PROJECT TO EXIT #21)	5000	6000
SECTION #2	(EXIT #21 TO CANADIAN BORDER)	1700	2100

CONVENTIONAL SIGNS	
COUNTY LINE	---
TOWN LINE	- - - - -
LIMITS OF ACCESS	○-○-○-○
POINT OF ACCESS	X
FENCE LINE	-x-x-
STONE WALL	o-o-o-o-o
TRAVELED WAY	- - - - -
GUARD RAIL	o-o-o-o-o
RAILROAD	+
SURVEY LINE	+
CULVERT	- - - - -
POWER POLE	⊕
TELEPHONE POLE	⊕
TREES	⊗
CONTROL OF ACCESS	///
PROPERTY LINE	---
R.O.W. TAKING LINE	---
SLOPE RIGHTS	○ SR
TOP OF CUT	△
TOE OF SLOPE	○

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT. CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JANUARY 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

APPROVED _____ DATE _____	
DIRECTOR OF PROGRAM DEVELOPMENT	
PROJECT MANAGER:	DOMÉY
PROJECT	SWANTON - HIGHGATE IM SURF (5)
SHEET 1 OF 14 SHEETS	



TYPICAL SECTION
I 89 SOUTHBOUND MM ~~118.350~~ TO MM 129.971

NOT TO SCALE **117.855**

NOTES:

1. ALL NECESSARY SURFACE PREPARATION INVOLVING PATCHING, POTHOLE REPAIR, AND CRACK SEALING SHALL BE PERFORMED PRIOR TO APPLICATION OF THE PAVER PLACED SURFACE TREATMENT. ALL CRACKS GREATER THAN 0.10" AND UP TO 1.0" IN WIDTH SHALL BE SEALED USING THE "BLOW AND GO" METHOD. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE PAID FOR UNDER ITEM 900.635 SPECIAL PROVISION (BITUMINOUS CRACK SEALING, BLOW AND GO METHOD). ALL OTHER PATCHING AND POT-HOLE REPAIR SHALL BE COMPLETED USING BITUMINOUS CONCRETE PAVEMENT, TYPE IV. AN ESTIMATED QUANTITY FOR ITEM 900.680 SPECIAL PROVISION (BITUMINOUS CONCRETE PAVEMENT SURFACE PREPARATION, TYPE I) HAS BEEN INCLUDED TO COVER ALL COSTS ASSOCIATED WITH THIS WORK. EXISTING SHOULDER PAVEMENT SURFACES BEYOND THE LIMITS OF THE PAVER PLACED SURFACE TREATMENT SHALL ALSO RECEIVE CRACK SEALING AND RELATED PATCHING AND POTHOLE REPAIR TREATMENTS.

2. NO BRIDGES ON THIS PROJECT WILL HAVE SURFACE PREPARATION.

3. ALL EXISTING PAVEMENT MARKINGS SHALL BE REMOVED PRIOR TO APPLYING THE CRACK SEAL. AN ESTIMATED QUANTITY OF 50% REMOVAL OF EXISTING MARKINGS HAS BEEN INCLUDED TO COVER THIS PROVISION. LANE DELINEATION IS TO BE MAINTAINED DURING CONSTRUCTION BY USING LINE STRIPING TARGETS.

4. A 25' COLD PLANED WEDGE SHALL BE CONSTRUCTED AT THE PROJECT BEGIN, PROJECT END, INTERCHANGE RAMPS, AND AT ALL BEGIN/END BRIDGE APPROACHES WITHIN THE PROJECT LIMITS, OR AS DIRECTED BY THE RESIDENT ENGINEER. THE LONGITUDINAL EDGES OF THE SURFACE TREATMENTS SHALL BE FEATHERED AS DIRECTED BY THE RESIDENT ENGINEER.

5. AT MILEMARKER 129.971, THE BITUMINOUS SURFACE TERMINATES AND ABUTS CONCRETE PAVEMENT. FOLLOWING THE APPLICATION OF THE PAVER PLACED SURFACE TREATMENT A FULL WIDTH (EDGE-OF-PAVEMENT TO EDGE-OF-PAVEMENT) ASPHALTIC PLUG JOINT SHALL BE INSTALLED TO SEAL THIS TRANSITION OF PAVEMENT TYPES. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE PAID FOR UNDER ITEM 516.10 BRIDGE EXPANSION JOINT, ASPHALTIC PLUG.

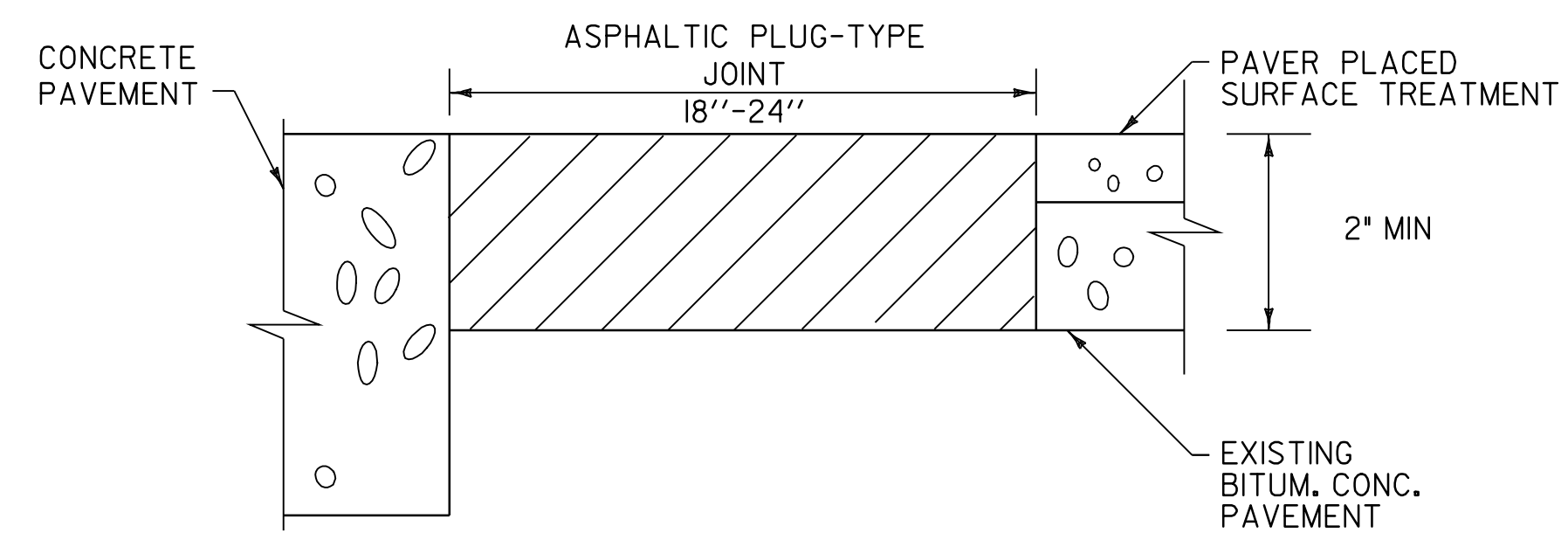
6. WINTER SAND AND OTHER DEBRIS THAT HAS ACCUMULATED ALONG THE BASE OF GUARDRAIL SHALL BE REMOVED AS DIRECTED BY THE RESIDENT ENGINEER PRIOR TO PERFORMING PAVEMENT SURFACE REPAIRS. AN ESTIMATED QUANTITY FOR ITEM 203.40 SHOULDER BERM REMOVAL HAS BEEN INCLUDED TO COVER THE COSTS ASSOCIATED WITH THIS WORK.

BRIDGES WITHIN THE PROJECT ARE:

BRIDGE NUMBER	MM	WORK REQUIRED
94-S	121.338	NO
95	122.402	NO
96-S	122.794	NO
97-S	123.121	NO
98-S	123.368	NO
100-S	128.373	NO

**TYPICAL SECTION -
PAVER PLACED
SURFACE TREATMENT**

PROJECT NAME:	SWANTON - HIGHGATE
PROJECT NUMBER:	IM SURF (5)
FILE NAME: ...07A278\...07A278.dgn	PLOT DATE: 11-FEB-2009 14:38
PROJECT LEADER: DOMEY	DRAWN BY: HUNT
DESIGNED BY: HUNT	CHECKED BY: PAVT MGMT
IPARM FILE NAME: 07A278+yp.l	SHEET 2 OF 14



ASPHALTIC PLUG-TYPE JOINT DETAIL

NOT TO SCALE

INSTALLATION:

THE JOINT SHALL BE LOCATED CENTRALLY OVER THE DECK EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.

THE JOINT SHALL BE EXCAVATED AS SHOWN ON THE PLANS BY USE OF SAWS AND PNEUMATIC HAMMER OR A HAMMER AND CHISEL.

THE JOINT AREA SHALL BE BLAST CLEANED OF DEBRIS AND ASPHALT. THE JOINT AREA SHALL BE THOROUGHLY DRIED USING HOT COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.

SPALLED AND DEFECTIVE CONCRETE SHALL BE REPAIRED WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.

THE INSTALLATION OF MATERIAL, COMPACTION, AND TOPCOATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.

IMMEDIATELY AFTER TOPCOATING, AN ANTI-SKID MATERIAL SHALL BE CAST OVER THE JOINT TO REDUCE THE RISK OF TRACKING.

JOINT SHALL BE PROTECTED FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 52 C +/- (125 F).

ANY ADDITIONAL MATERIALS DEEMED NECESSARY TO COMPLETE THE JOINT INSTALLATION SHALL BE INCIDENTAL TO ITEM 516.10, BRIDGE EXPANSION JOINT, ASPHALTIC PLUG.

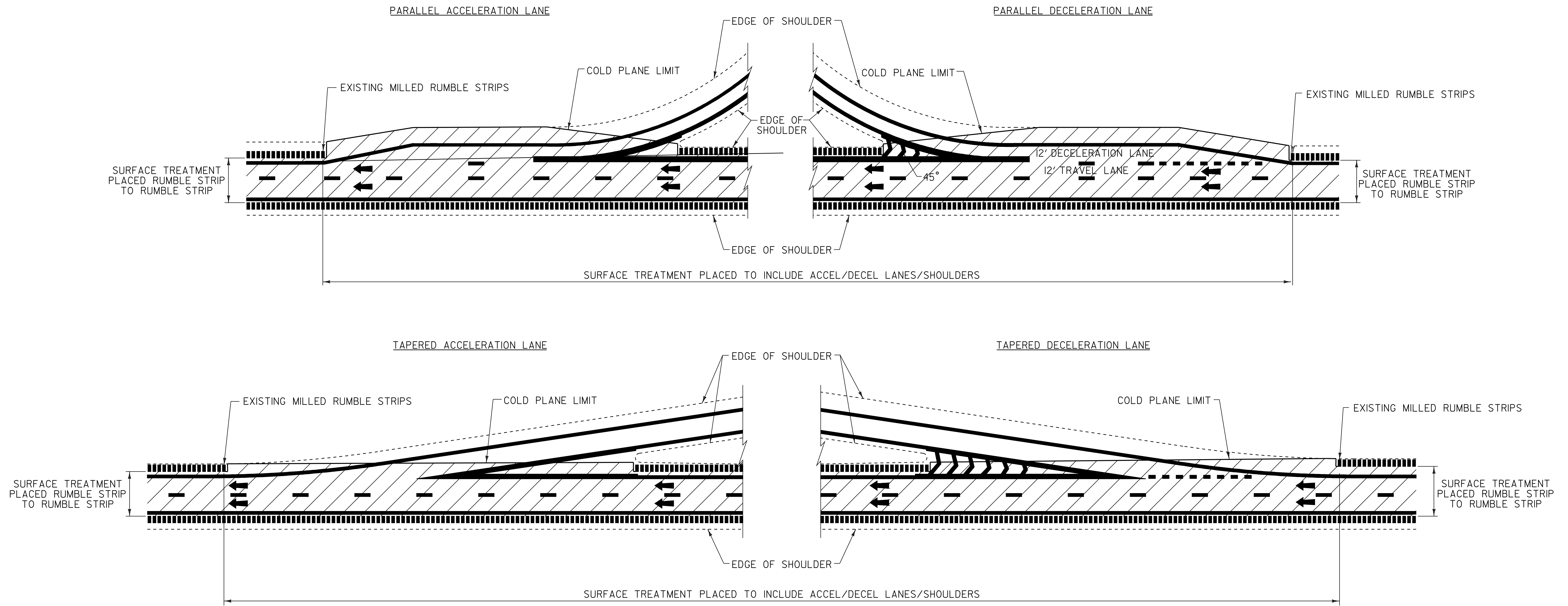
**MISCELLANEOUS
DETAILS
SHEET**

PROJECT NAME: SWANTON - HIGHGATE
PROJECT NUMBER: IM SURF (5)

FILE NAME: ...07A278\...07A278.dgn PLOT DATE: 11-FEB-2009
PROJECT LEADER: DOMEY DRAWN BY: HUNT
DESIGNED BY: HUNT CHECKED BY: PAVT MGMT
IPARM FILE NAME: 07A278mde+.i SHEET 3 OF 14

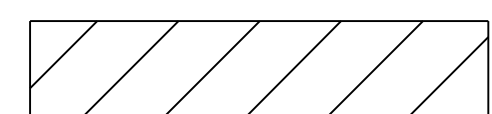
TYPICAL INTERCHANGE CONSTRUCTION DETAILS

NOT TO SCALE



NOTES:
1. LINE STRIPING SHOWN FOR REFERENCE ONLY.

LEGEND



SPECIAL PROVISION
(PAVER PLACED SURFACE TREATMENT, TYPE C)



DIRECTION OF TRAFFIC FLOW



EXISTING MILLED RUMBLE STRIPS

**INTERCHANGE
DETAIL
SHEET**

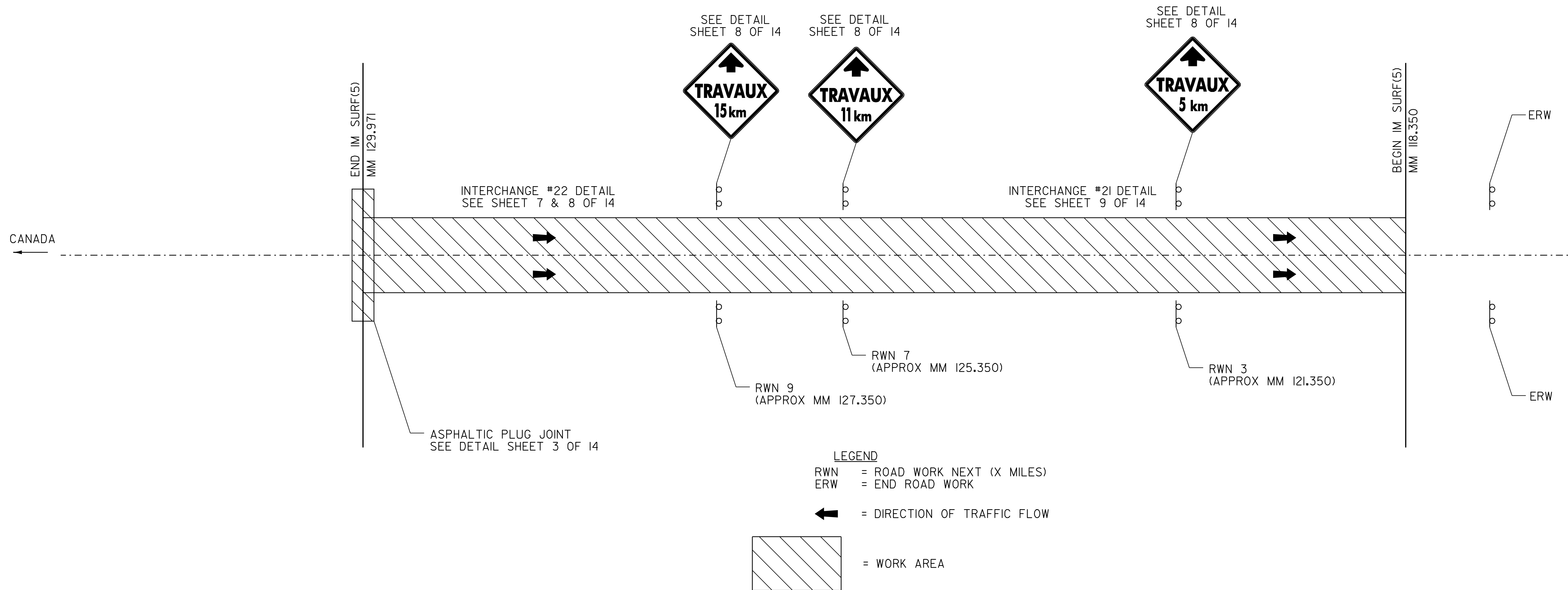
PROJECT NAME: SWANTON - HIGHGATE
PROJECT NUMBER: IM SURF (5)

FILE NAME: ...07A278\...07A278.dgn
PROJECT LEADER: DOMEY
DESIGNED BY: HUNT
IPARM FILE NAME: 07A2781d.1

PLOT DATE: 11-FEB-2009
DRAWN BY: HUNT
CHECKED BY: PAVT MGMT
SHEET 5 OF 14

**BEGIN /END PROJECT
CONSTRUCTION APPROACH SIGNING**

NOT TO SCALE



NOTES:

1. ALL WORK FROM THE END OF THE PROJECT TO APPROXIMATE STATION 129.600 SHALL BE COMPLETED FIRST. ONCE ALL WORK IS COMPLETED IN THIS AREA, RESUME ALL NORMAL TRAFFIC CONTROL DEVICES; IN COMPLIANCE WITH THE APPROPRIATE VTRANS STANDARD AND THE MUTCD.

2. THE DISTANCE SHOWN ON THE "ROAD WORK NEXT XX MILES" (G20-1) SIGN SHALL BE STATED TO THE NEAREST WHOLE MILE. REFER TO PART 6 OF THE 2003 MUTCD SECTION 6F.51. THESE SIGNS SHOULD BE SPACED APPROXIMATELY EVERY 2-3 MILES ALONG THE PROJECT AS A REMINDER TO THE TRAVELLING MOTORIST.

3. EXISTING SPEED LIMIT SIGNS SHALL BE COVERED WHEN REDUCED SPEED SIGNS ARE POSTED, AS DIRECTED BY THE RESIDENT ENGINEER. KEEP RECORDS WHEN POSTING THE WORK ZONE SPEED LIMIT FOR LEGAL PURPOSES; DOCUMENTING DATES, TIMES, AND LOCATIONS OF SIGNS. WHEN WORK ZONE SPEED LIMIT IS NOT IN USE ALL ASSOCIATED SIGNS SHALL BE COVERED, TURNED AND/OR LAID FLAT IN ORDER THAT MOTORISTS CANNOT READ THESE SIGNS.

4. PORTABLE OR STATIONARY WORK ZONE SPEED LIMIT SIGNS SHOULD BE SPACED EVERY 1.5 TO 2 MILES WHERE APPLICABLE AS A REMINDER TO THE MOTORIST TRAVELING THROUGH THE WORK ZONE WHAT SPEED THEY SHOULD BE TRAVELLING.

5. WHEN REDUCED REGULATORY SPEED LIMIT SIGNS ARE USED, THE RESUMPTION OF THE USUAL SPEED LIMIT SHALL BE INDICATED BY AN APPROPRIATE SPEED LIMIT SIGN AT THE END OF THE WORK ZONE.

6. FOR THE APPROVED SPEED REDUCTION, THE "SPEED LIMIT XX" AND OTHER RELATED SIGNS SHALL BE REMOVED OR COVERED WHEN WORK IS NOT IN PROGRESS.

7. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE RESIDENT ENGINEER FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION. THE COST OF PREPARING THIS PLAN (AND MAKING CHANGES IF NECESSARY) SHALL BE PAID INCIDENTAL TO ITEM 641.10, "TRAFFIC CONTROL".

8. THE CONTRACTOR SHALL INCLUDE A CONSTRUCTION SIGN APPROACH PACKAGE FOR EXPECTED LANE CLOSURES AND WORK ZONE SPEED REDUCTIONS IN COMPLIANCE WITH VTRANS STANDARD E-103. PAYMENT FOR PROVIDING THIS PACKAGE SHALL BE INCIDENTAL TO ITEM 641.10, "TRAFFIC CONTROL".

9. ADDITIONAL RAMP SIGNING MAY BE REQUIRED AS DIRECTED BY THE RESIDENT ENGINEER.

10. THE BID PRICE FOR "TRAFFIC CONTROL", ITEM 641.10, SHALL INCLUDE ALL APPROACH AND ON-PROJECT CONSTRUCTION SIGNING, PORTABLE ARROW BOARDS, BARRIERS, BARRELS, CONES, BARRICADES, TEMPORARY REGULATORY AND WARNING SIGNS, AND POSTS AS DETAILED IN VTRANS STANDARDS. ALL ADJUSTING, RELOCATING, AND REMOVING OF THESE DEVICES AS DIRECTED BY THE RESIDENT ENGINEER SHALL ALSO BE INCLUDED. THE FOLLOWING ITEMS WILL BE PAID FOR SEPARATELY: 630.10 AND 630.15 - UNIFORMED TRAFFIC OFFICERS AND FLAGGERS.

11. PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) SHALL BE PROVIDED FOR USE ALONG THIS PROJECT. THE PLACEMENT OF THESE UNITS AS WELL AS THE MESSAGE WILL BE APPROVED BY THE RESIDENT ENGINEER. THESE SIGNS WILL BE PAID FOR UNDER ITEM 641.15, "PORTABLE CHANGEABLE MESSAGE SIGN".

12. COORDINATION WITH THE BORDER PATROL WILL BE REQUIRED FOR ALL WORK FROM THE BEGINNING OF THE PROJECT TO APPROXIMATE STATION 129.600. CONTACT PORT DIRECTOR JAMES MCMILLAN AT (802) 868-2778 EXT. 136.

13. THE PCMS SHALL CONSIST OF EITHER ONE OR TWO PHASES. TYPICALLY, A PHASE SHALL CONSIST OF UP TO THREE LINES OF EIGHT CHARACTERS PER LINE, OR AS DIRECTED BY THE RESIDENT ENGINEER. THE PCMS SHOULD BE USED AS A SUPPLEMENT AND NOT AS A SUBSTITUTE FOR CONVENTIONAL SIGNS.

14. THE PCMS SHOULD COMMUNICATE WHAT INFORMATION MOTORISTS NEED TO KNOW. UNNECESSARY INFORMATION SHOULD BE AVOIDED. MESSAGES SHOULD BE UPDATED PERIODICALLY TO DESCRIBE THE WORK ACTIVITY OCCURRING SO THAT THE PCMS CONTINUES TO COMMAND THE ATTENTION OF MOTORISTS, AS DIRECTED BY THE RESIDENT ENGINEER.

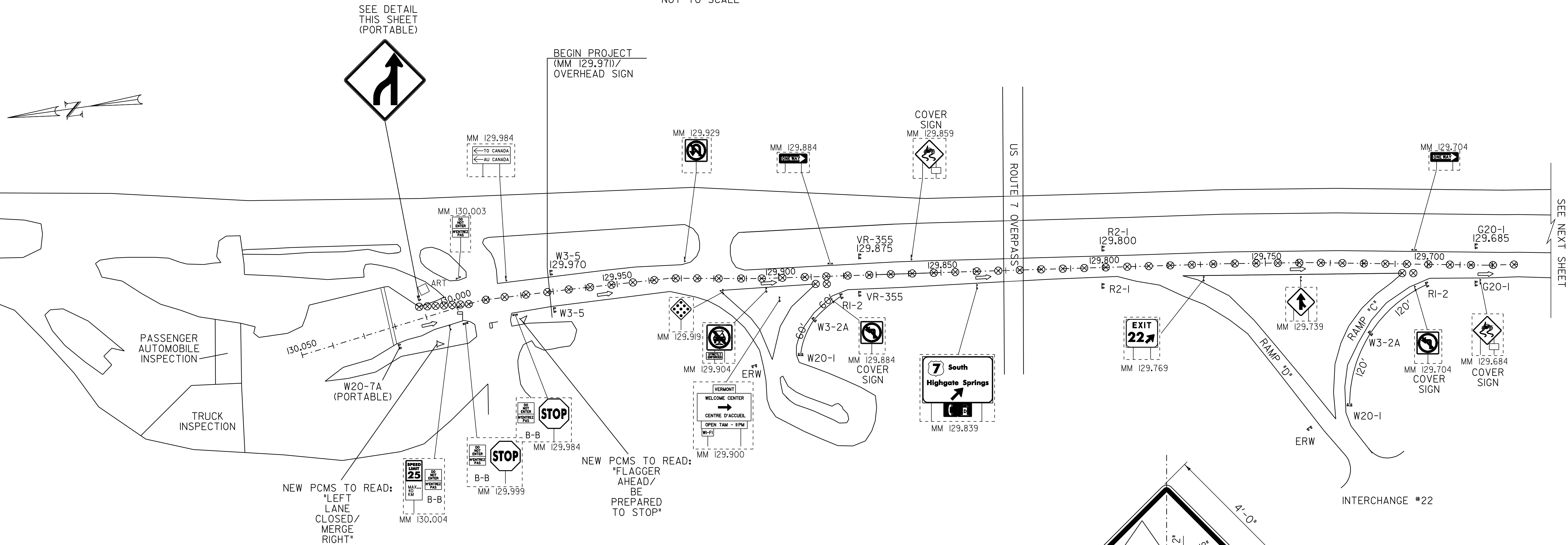
15. THE LATEST REVISION OF THE 2003 MUTCD SHALL BE THE STANDARD FOR ALL TRAFFIC CONTROL DEVICES. EXISTING SIGNS SHALL BE VALID UNTIL SUCH TIME AS THEY ARE REPLACED OR RECONSTRUCTED. WHEN NEW TRAFFIC CONTROL DEVICES ARE ERECTED OR PLACED OR EXISTING TRAFFIC CONTROL DEVICES ARE REPLACED OR REPAIRED THE EQUIPMENT, DESIGN, METHOD OF INSTALLATION, PLACEMENT OR REPAIR SHALL CONFORM TO SUCH STANDARDS.

16. REFER TO VT. STATE STANDARDS AND THE MUTCD FOR TEMPORARY TRAFFIC CONTROL SIGN DIMENSIONS AND COLORS.

17. DURING CONSTRUCTION IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAINTAIN ONE-LANE TRAFFIC. IN NO CASE SHALL THE PAVED WIDTH FOR THIS ONE-LANE TRAFFIC, INCLUDING SHOULDERS, BE REDUCED TO LESS THEN 15 FEET IN WIDTH. THIS PAVED WIDTH SHALL REMAIN FREE OF OBSTRUCTIONS AND OBSTACLES AT ALL TIMES.

TEMPORARY CONSTRUCTION SIGNING SHEET #1	PROJECT NAME:	SWANTON - HIGHGATE
	PROJECT NUMBER:	IM SURF (5)
	FILE NAME: ...07A278\...07A278.dgn	PLOT DATE: 11-FEB-2009
	PROJECT LEADER: DOMEY	DRAWN BY: HUNT
DESIGNED BY: HUNT	CHECKED BY: PAVT MGMT	
IPARM FILE NAME: 07A278casl.I	SHEET 6 OF 14	

PASSING LANE CLOSURE
NOT TO SCALE

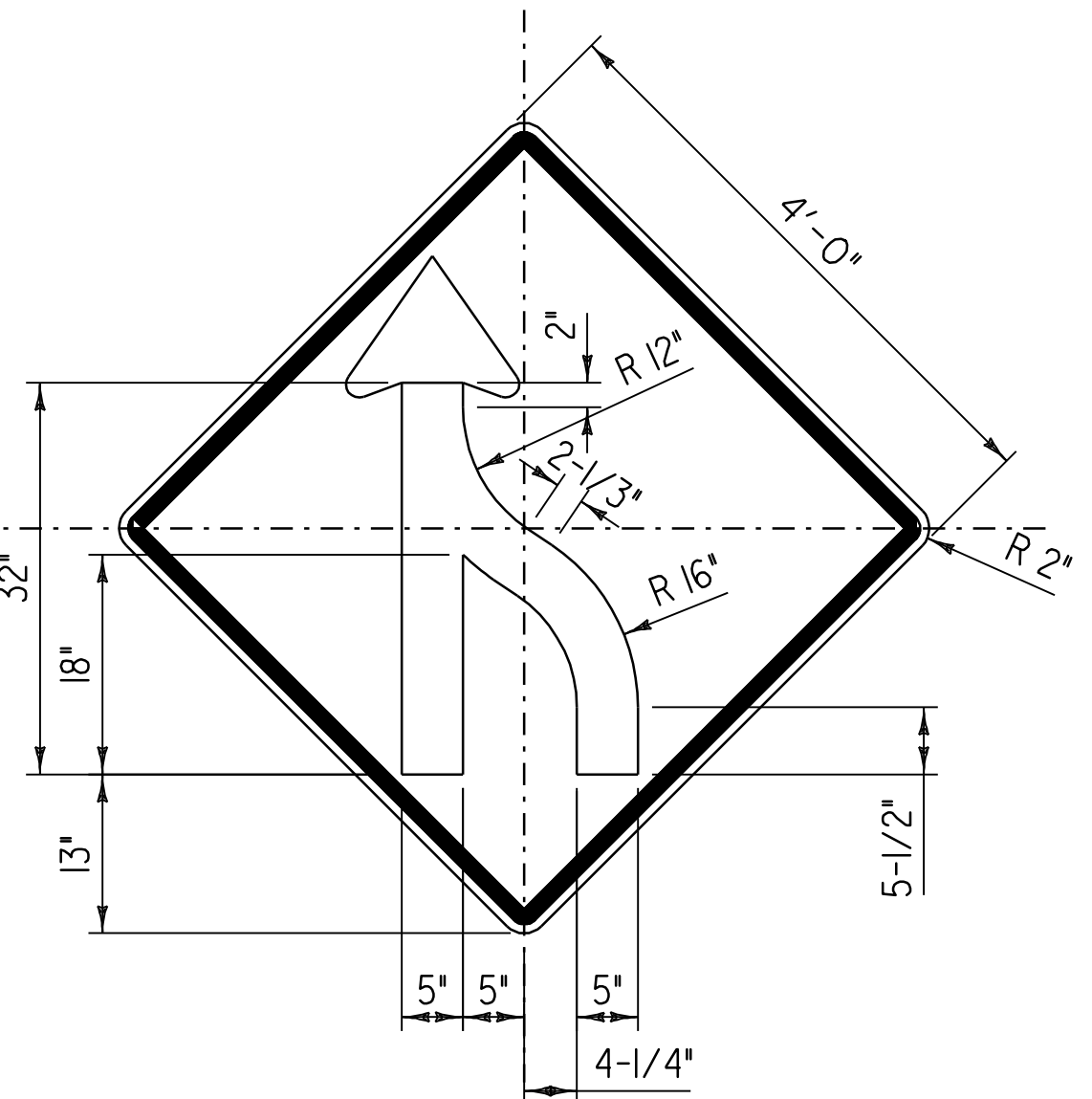


NEW PCMS TO READ:
"LEFT LANE CLOSED/MERGE RIGHT"

NEW PCMS TO READ:
"FLAGGER AHEAD/ BE PREPARED TO STOP"

LEGEND

- ⇐ DIRECTION OF TRAFFIC FLOW
- ⊗ REFL. PLASTIC DRUMS
- ▷ PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)
- ⌒ FLAGGER
- W20-1 = ROAD WORK AHEAD
- W3-2A = YIELD AHEAD
- RI-2 = YIELD
- VR-355 = FINES DOUBLED
- R2-1 = WORK ZONE SPEED LIMIT
- G20-1 = ROAD WORK NEXT X MILES
- W3-5 = SPEED REDUCTION
- W20-7A = FLAGGER AHEAD



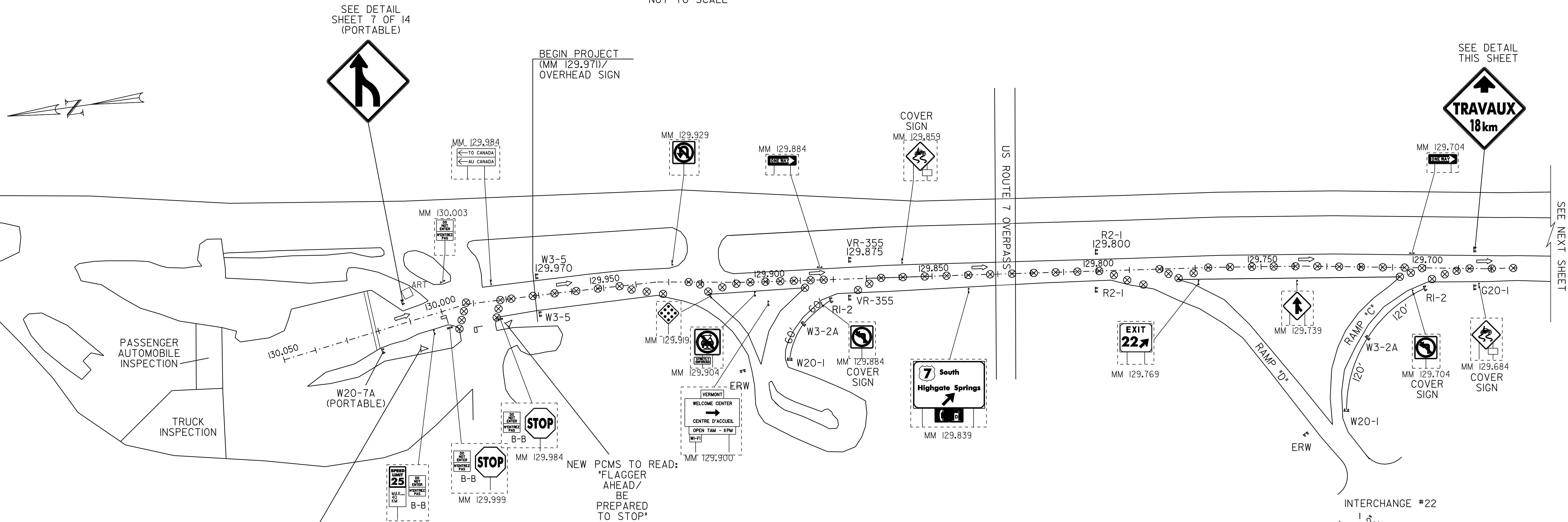
COLORS
BORDER AND SYMBOL: BLACK, NON-REFL
BACKGROUND: ORANGE REFL TYPE VIII SHEETING

NOTES:
1. ALL WORK FROM THE END OF THE PROJECT TO APPROXIMATE STATION 129,600 SHALL BE COMPLETED FIRST. ONCE ALL WORK IS COMPLETED IN THIS AREA, RESUME ALL NORMAL TRAFFIC CONTROL DEVICES; IN COMPLIANCE WITH THE APPROPRIATE VTRANS STANDARD AND THE MUTCD, ONCE WORK IS COMPLETED TO MM 129,600, THE PCMS CAN BE REMOVED AND STATIONARY SIGNS CAN BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER.
2. USE VERMONT STANDARD DRAWING E-103 FOR CONSTRUCTION APPROACH SIGNING.
3. 2 FLAGGERS WILL BE PLACED AT APPROXIMATE STATION 129,990 TO HELP TRAFFIC FLOW. THESE FLAGGERS WILL HELP FACILITATE THE TURNING OF LARGE TRUCKS ENTERING THE PROJECT AREA, AND ASSIST MOTORISTS COMING IN FROM THE INSPECTION BUILDING.
4. A UTO WILL BE REQUIRED AT RAMPS WHEN EQUIPMENT IS WORKING ACROSS RAMP AREAS.
5. NO MORE THAN 2 MILES OF ANY LANE SHALL BE CLOSED AT ANY TIME DURING CONSTRUCTION.

TEMPORARY CONSTRUCTION SIGNING SHEET #2

PROJECT NAME:	SWANTON - HIGHGATE	
PROJECT NUMBER:	IM SURF (5)	
FILE NAME: ...07A278\...07A278.dgn	PLOT DATE:	11-FEB-2009
PROJECT LEADER: DOMEY	DRAWN BY:	HUNT
DESIGNED BY: HUNT	CHECKED BY:	PAVT MGMT
IPARM FILE NAME: 07A278cas2.1	SHEET	7 OF 14

TRAVEL LANE CLOSURE
NOT TO SCALE



SEE DETAIL SHEET 7 OF 14 (PORTABLE)

SEE DETAIL THIS SHEET

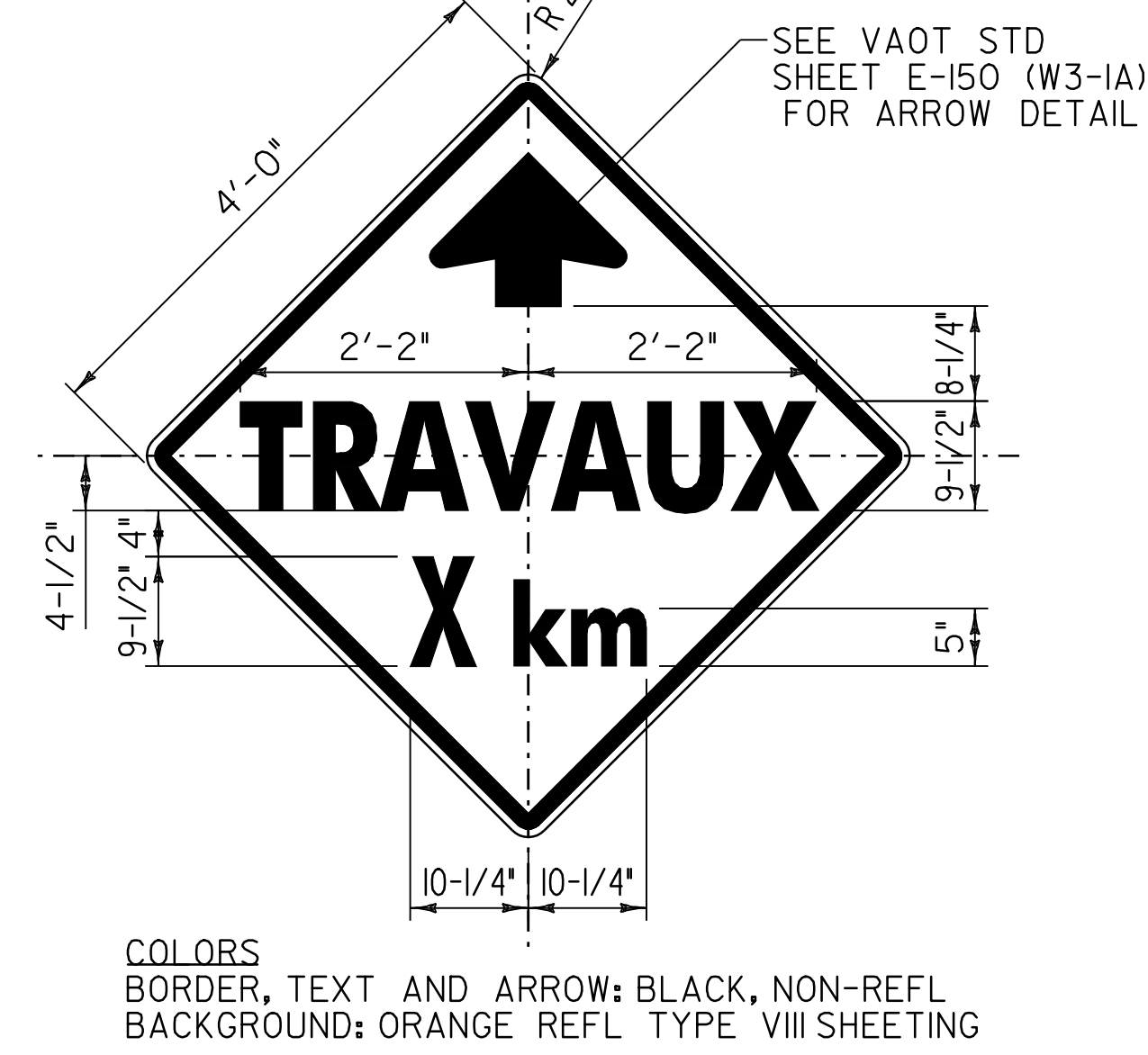
NEW PCMS TO READ:
"RIGHT LANE CLOSED / MERGE LEFT"

NEW PCMS TO READ:
"FLAGGER AHEAD / BE PREPARED TO STOP"

LEGEND

- ⇐ DIRECTION OF TRAFFIC FLOW
- ⊗ REFL. PLASTIC DRUMS
- ▷ PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)
- ⌋ FLAGGER
- W20-1 = ROAD WORK AHEAD
- W3-2A = YIELD AHEAD
- RI-2 = YIELD
- VR-355 = FINES DOUBLED
- R2-1 = WORK ZONE SPEED LIMIT
- G20-1 = ROAD WORK NEXT X MILES
- W3-5 = SPEED REDUCTION
- W20-7A = FLAGGER AHEAD

- NOTES:**
1. ALL WORK FROM THE END OF THE PROJECT TO APPROXIMATE STATION 129.600 SHALL BE COMPLETED FIRST. ONCE ALL WORK IS COMPLETED IN THIS AREA, RESUME ALL NORMAL TRAFFIC CONTROL DEVICES; IN COMPLIANCE WITH THE APPROPRIATE VTRANS STANDARD AND THE MUTCD, ONCE WORK IS COMPLETED TO MM 129.600, THE PCMS CAN BE REMOVED AND STATIONARY SIGNS CAN BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER.
 2. USE VERMONT STANDARD DRAWING E-103 FOR CONSTRUCTION APPROACH SIGNING.
 3. 2 FLAGGERS WILL BE PLACED AT APPROXIMATE STATION 129.990 TO HELP TRAFFIC FLOW. THESE FLAGGERS WILL HELP FACILITATE THE TURNING OF LARGE TRUCKS ENTERING THE PROJECT AREA, AND ASSIST MOTORISTS COMING IN FROM THE INSPECTION BUILDING.
 4. A UTO WILL BE REQUIRED AT RAMPS WHEN EQUIPMENT IS WORKING ACROSS RAMP AREAS.
 5. NO MORE THAN 2 MILES OF ANY LANE SHALL BE CLOSED AT ANY TIME DURING CONSTRUCTION.



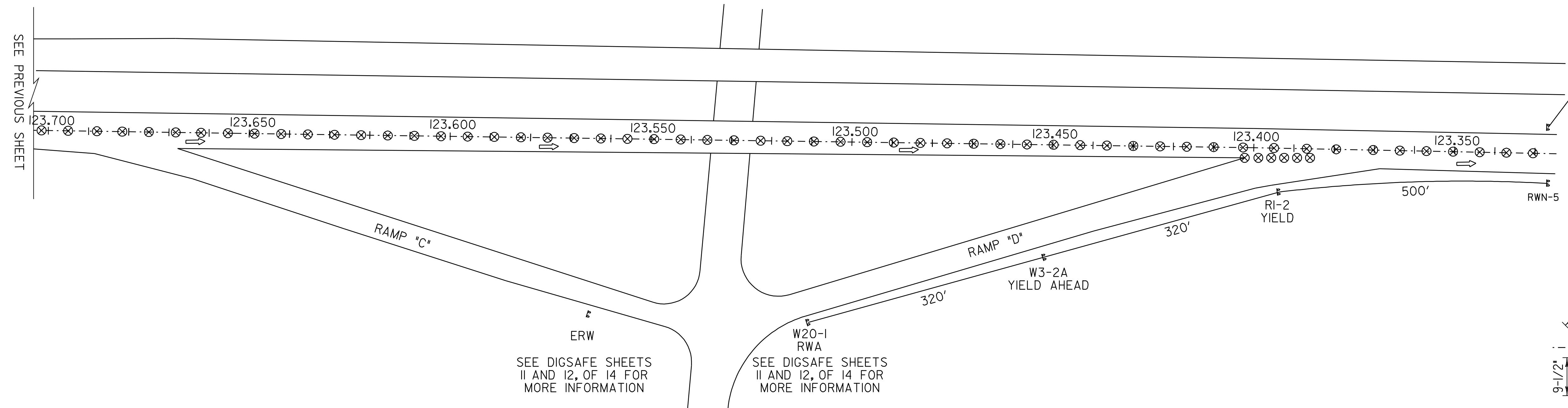
COLORS
BORDER, TEXT AND ARROW: BLACK, NON-REFL
BACKGROUND: ORANGE REFL TYPE VIII SHEETING

TEMPORARY CONSTRUCTION SIGNING SHEET #3	PROJECT NAME:	SWANTON - HIGHGATE	
	PROJECT NUMBER:	IM SURF (5)	
	FILE NAME:	...07A278\...07A278.dgn	
	PLOT DATE:	11-FEB-2009	
DESIGNED BY:	HUNT	DRAWN BY:	HUNT
IPARM FILE NAME:	07A278cas3.1	CHECKED BY:	PAVT MGMT
		SHEET	8 OF 14

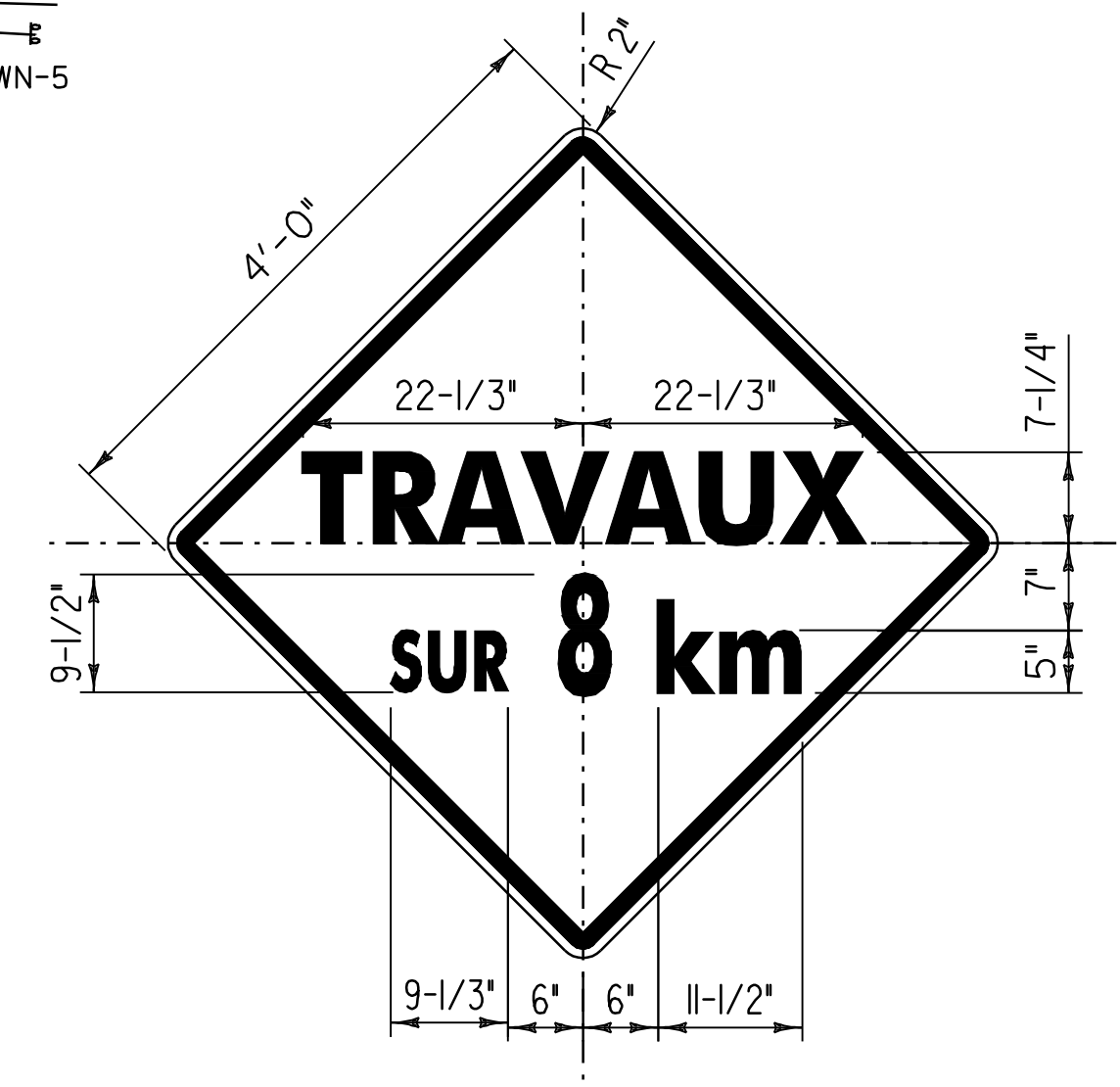


PASSING LANE CLOSURE

NOT TO SCALE



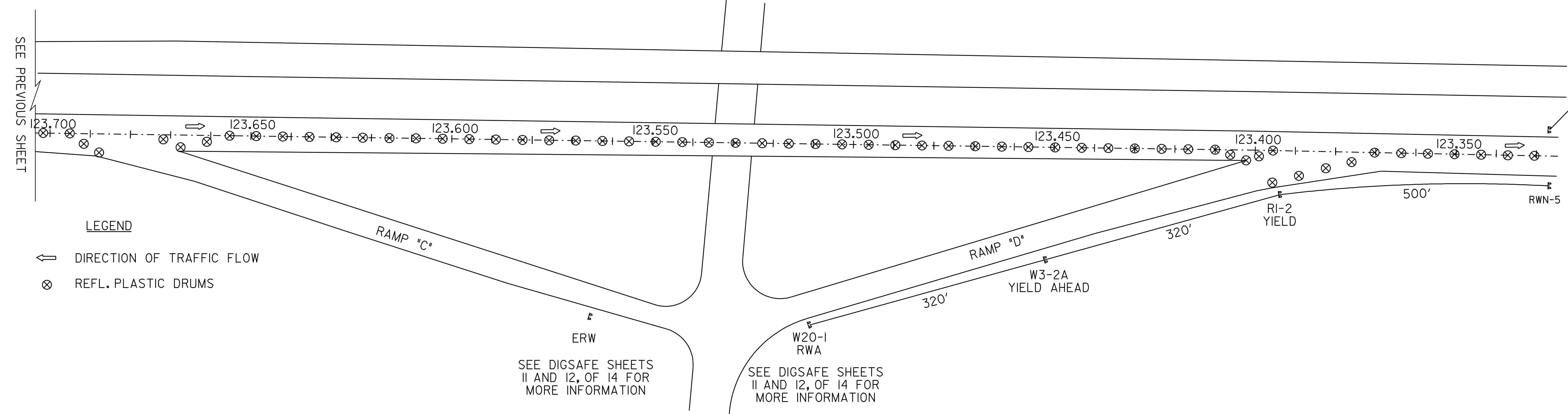
SEE DETAIL THIS SHEET



INTERCHANGE #21

TRAVEL LANE CLOSURE

NOT TO SCALE



COLORS
BORDER AND TEXT: BLACK, NON-REFL
BACKGROUND: ORANGE REFL TYPE VIII SHEETING



SEE DETAIL THIS SHEET

LEGEND

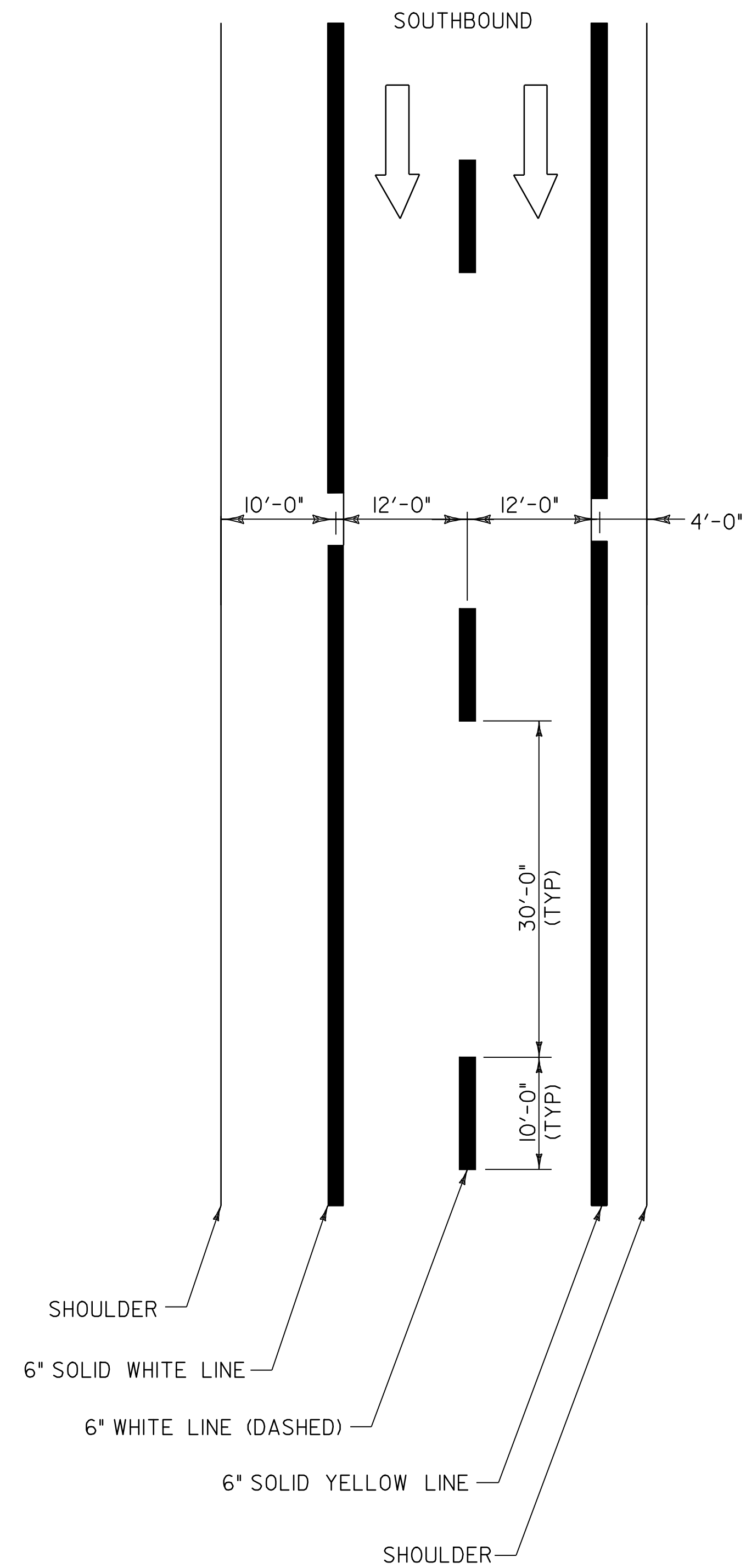
- ← DIRECTION OF TRAFFIC FLOW
- ⊗ REFL. PLASTIC DRUMS

NOTES:

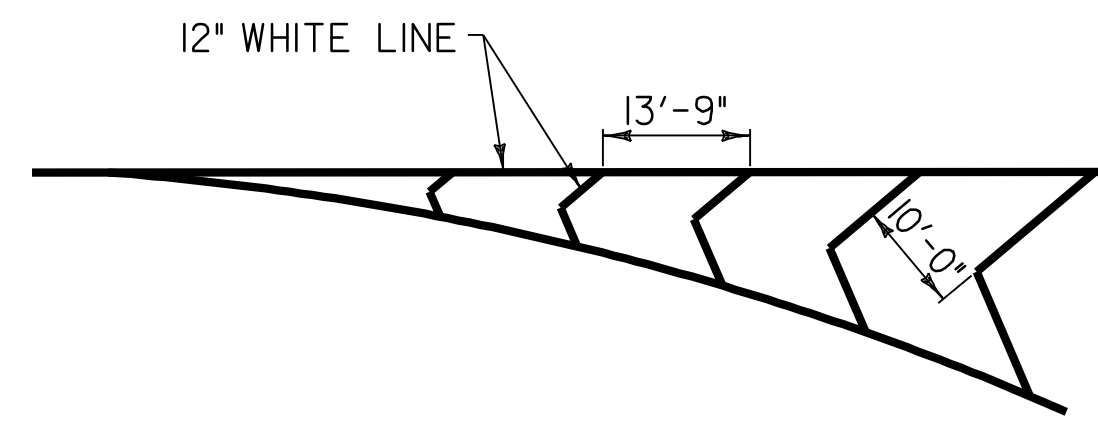
1. ALL WORK FROM THE END OF THE PROJECT TO APPROXIMATE STATION 129.600 SHALL BE COMPLETED FIRST. ONCE ALL WORK IS COMPLETED IN THIS AREA, RESUME ALL NORMAL TRAFFIC CONTROL DEVICES; IN COMPLIANCE WITH THE APPROPRIATE VTRANS STANDARD AND THE MUTCD. ONCE WORK IS COMPLETED TO MM 129.600, THE PCMS CAN BE REMOVED AND STATIONARY SIGNS CAN BE INSTALLED AS DIRECTED.
2. USE VERMONT STANDARD DRAWING E-103 FOR CONSTRUCTION APPROACH SIGNING.
3. A UTO WILL BE REQUIRED AT RAMPS WHEN EQUIPMENT IS WORKING ACROSS RAMP AREAS.
4. NO MORE THAN 2 MILES OF ANY LANE SHALL BE CLOSED AT ANY TIME DURING CONSTRUCTION.

TEMPORARY CONSTRUCTION SIGNING SHEET #4

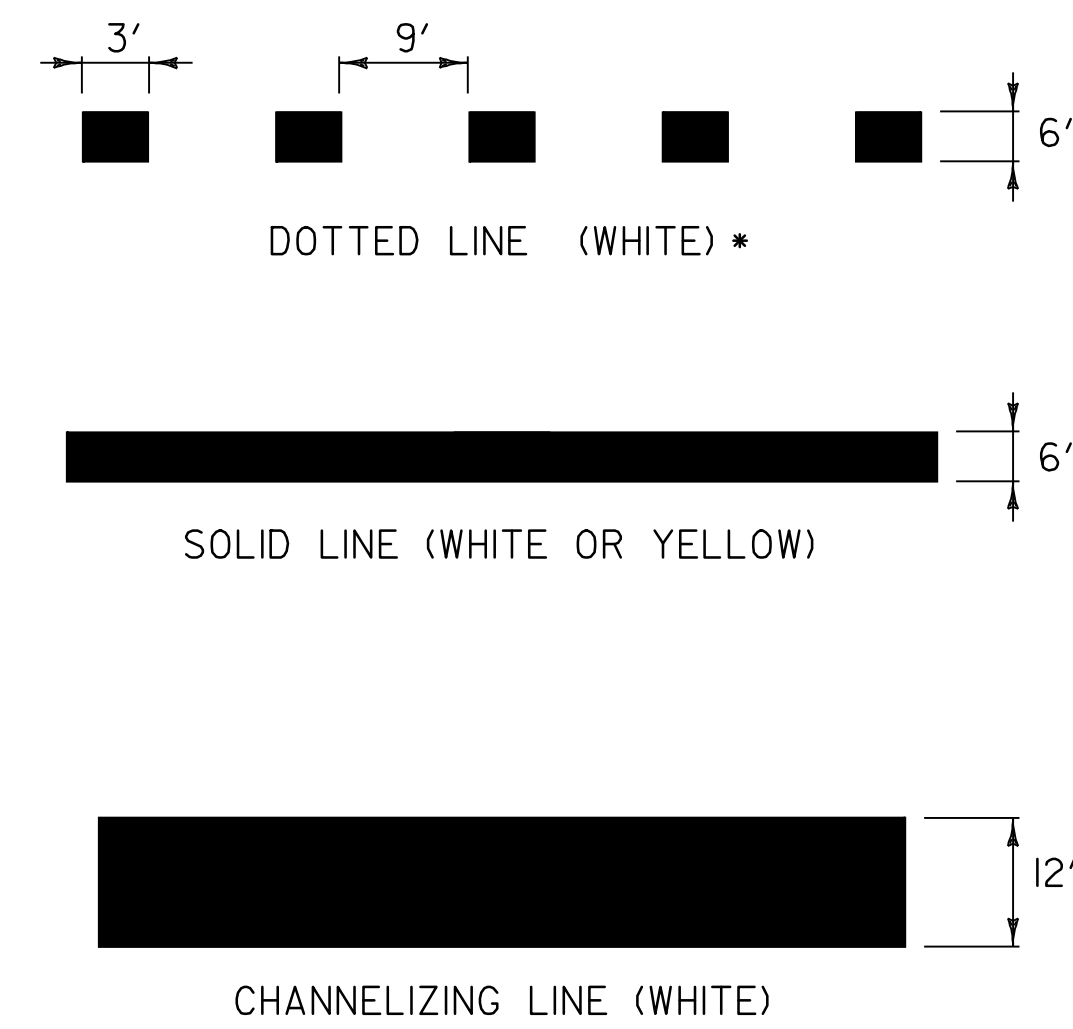
PROJECT NAME:	SWANTON - HIGHGATE		
PROJECT NUMBER:	IM SURF (5)		
FILE NAME:	...07A278\...07A278.dgn	PLOT DATE:	11-FEB-2009
PROJECT LEADER:	DOMNEY	DRAWN BY:	HUNT
DESIGNED BY:	HUNT	CHECKED BY:	PAVT MGMT
IPARM FILE NAME:	07A278cas4.1	SHEET	9 OF 14



TYPICAL MAINLINE MARKING PLAN
NOT TO SCALE



GORE MARKING DETAIL
NOT TO SCALE



PAVEMENT MARKING LINE DETAILS
NOT TO SCALE

* TO BE INSTALLED ONLY AT THE
DIRECTION OF THE RESIDENT ENGINEER

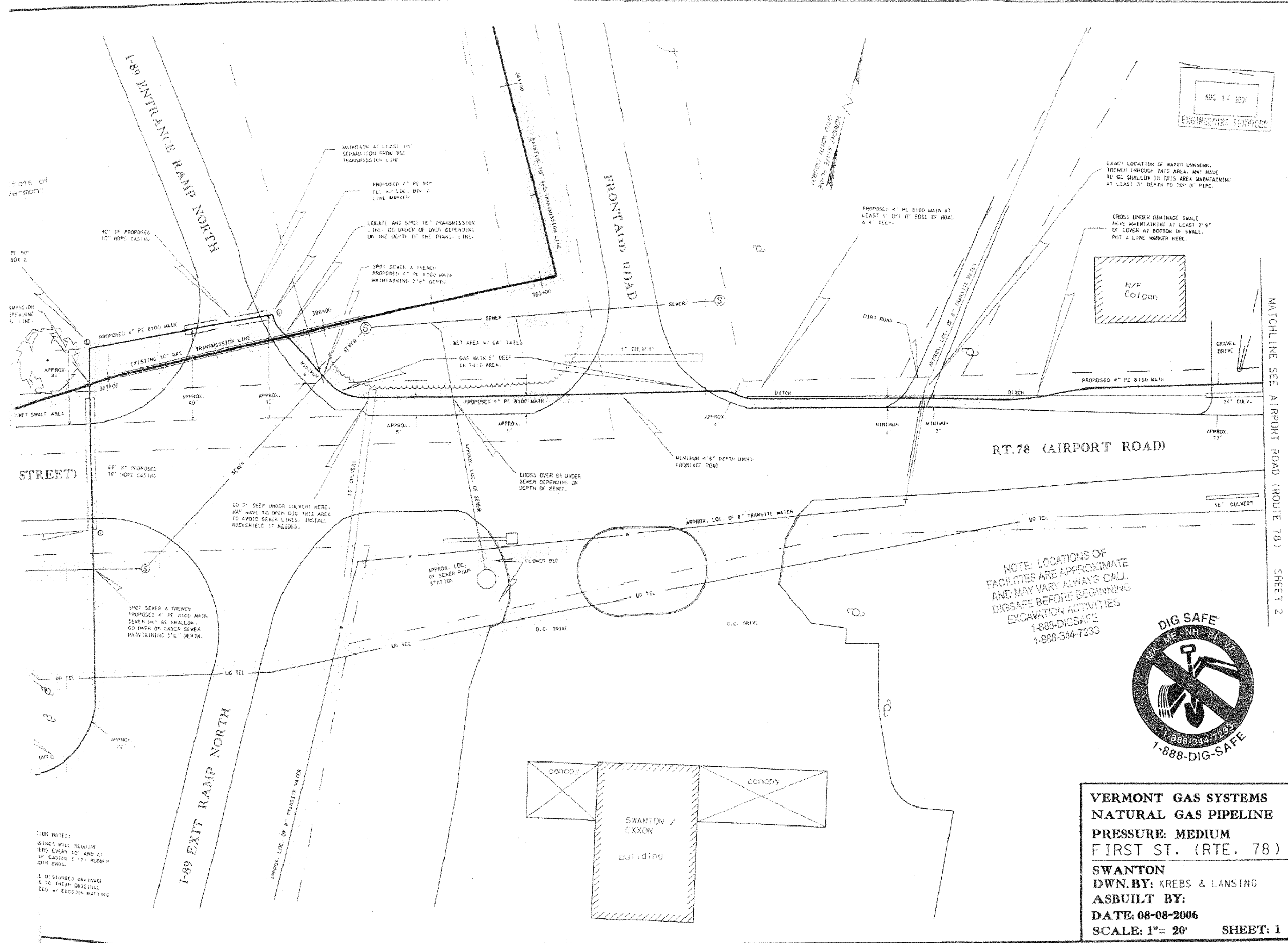
LEGEND

← DIRECTION OF TRAFFIC FLOW

**PAVEMENT
MARKING
DETAIL
SHEET**

PROJECT NAME: SWANTON - HIGHGATE
PROJECT NUMBER: IM SURF (5)

FILE NAME: ...07A278\...07A278.dgn PLOT DATE: 11-FEB-2009
PROJECT LEADER: DOMEY DRAWN BY: HUNT
DESIGNED BY: HUNT CHECKED BY: PAVT MGMT
IPARM FILE NAME: 07A278pmdet.i SHEET 10 OF 14



AUG 14 2006
ENGINEERING SERVICES

EXACT LOCATION OF WATER UNKNOWN. TRENCH THROUGH THIS AREA MAY HAVE TO GO SHALLOW IN THIS AREA MAINTAINING AT LEAST 3' DEPTH TO TOP OF PIPE.

CROSS UNDER DRAINAGE SWALE HERE MAINTAINING AT LEAST 3" OF COVER AT BOTTOM OF SWALE. PUT A LINE MARKER HERE.

N/F Colgar

RT. 78 (AIRPORT ROAD)

NOTE: LOCATIONS OF FACILITIES ARE APPROXIMATE AND MAY VARY. ALWAYS CALL DIGSAFE BEFORE BEGINNING EXCAVATION ACTIVITIES
1-888-DIGSAFE
1-888-344-7233



VERMONT GAS SYSTEMS
NATURAL GAS PIPELINE
PRESSURE: MEDIUM
FIRST ST. (RTE. 78)
SWANTON
DWN. BY: KREBS & LANSING
ASBUILT BY:
DATE: 08-08-2006
SCALE: 1"= 20' SHEET: 1

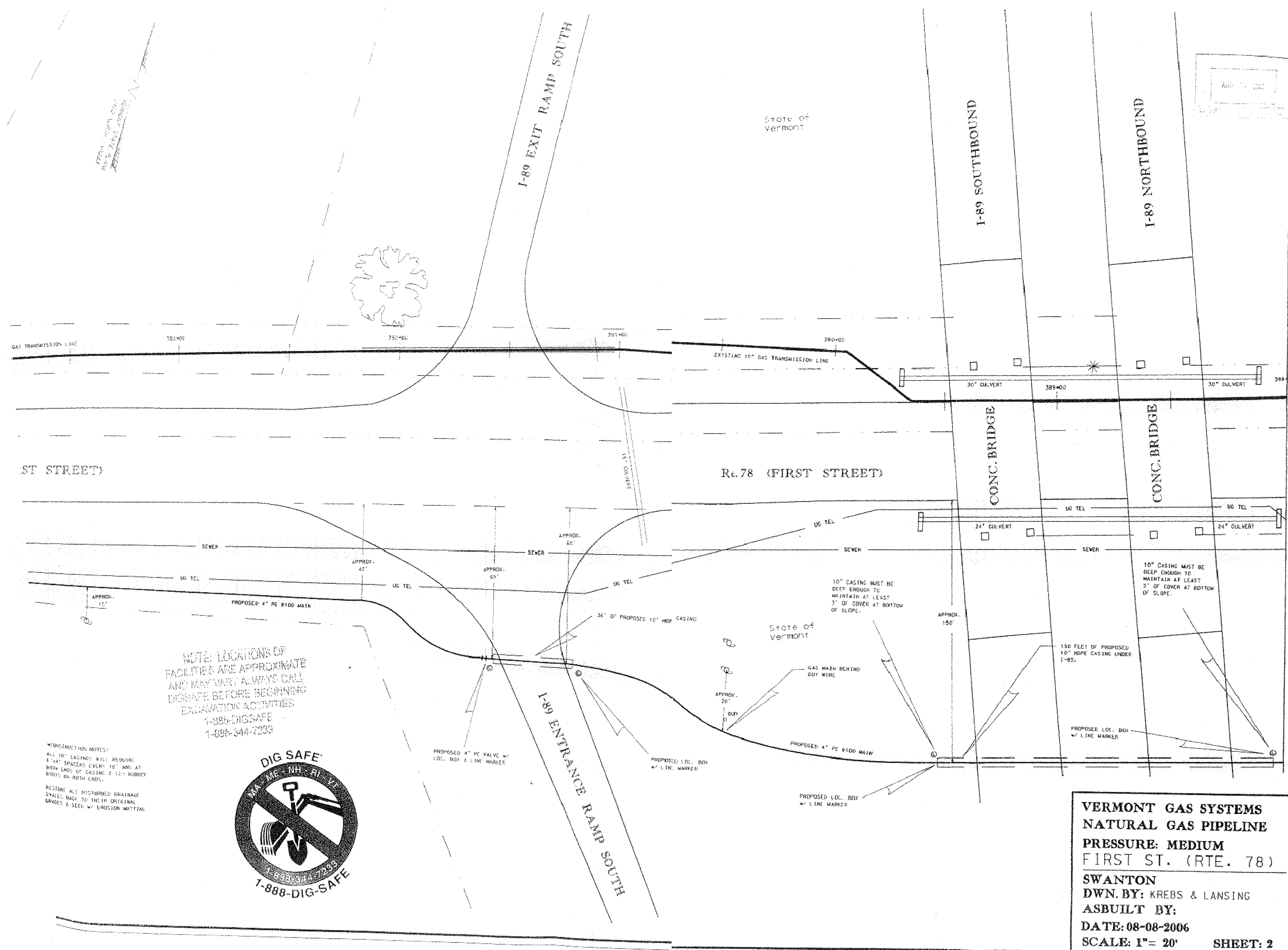
NOTE: ISINGS WILL REQUIRE 1200 EXPOSURE 10" AND AT OF CASING & 12" RUBBER 10" END.

1. DISTURBED DRAINAGE K TO THEIR CASING BED W/ EROSION MATTING

FOR INFORMATIONAL PURPOSES ONLY

DIG SAFE INFORMATION SHEET #1

PROJECT NAME:	SWANTON - HIGHGATE
PROJECT NUMBER:	IM SURF (5)
FILE NAME:	...07A278\...07A278.dgn
PROJECT LEADER:	DOMY
DESIGNED BY:	HUNT
IPARM FILE NAME:	07A278dsl1
PLOT DATE:	27-JAN-2009
DRAWN BY:	HUNT
CHECKED BY:	PAVT MGMT
SHEET #	OF 14



NOTE: LOCATIONS OF FACILITIES ARE APPROXIMATE AND MAY VARY. ALWAYS CALL DIGSAFE BEFORE BEGINNING EXCAVATION ACTIVITIES.
 1-888-DIGSAFE
 1-888-344-7239

WORKING NOTES:
 ALL 10" CASINGS WILL REQUIRE 6"x4" SPACERS EVERY 10' AND AT BOTH ENDS OF CASING. 2 1/2" RUBBER BODS ON BOTH ENDS.
 RESTORE ALL DISTURBED DRAINAGE CHANNELS BACK TO THEIR ORIGINAL GRADE & SEED W/ EROSION MATTING.



VERMONT GAS SYSTEMS
NATURAL GAS PIPELINE
PRESSURE: MEDIUM
FIRST ST. (RTE. 78)
SWANTON
DWN. BY: KREBS & LANSING
ASBUILT BY:
DATE: 08-08-2006
SCALE: 1" = 20' SHEET: 2

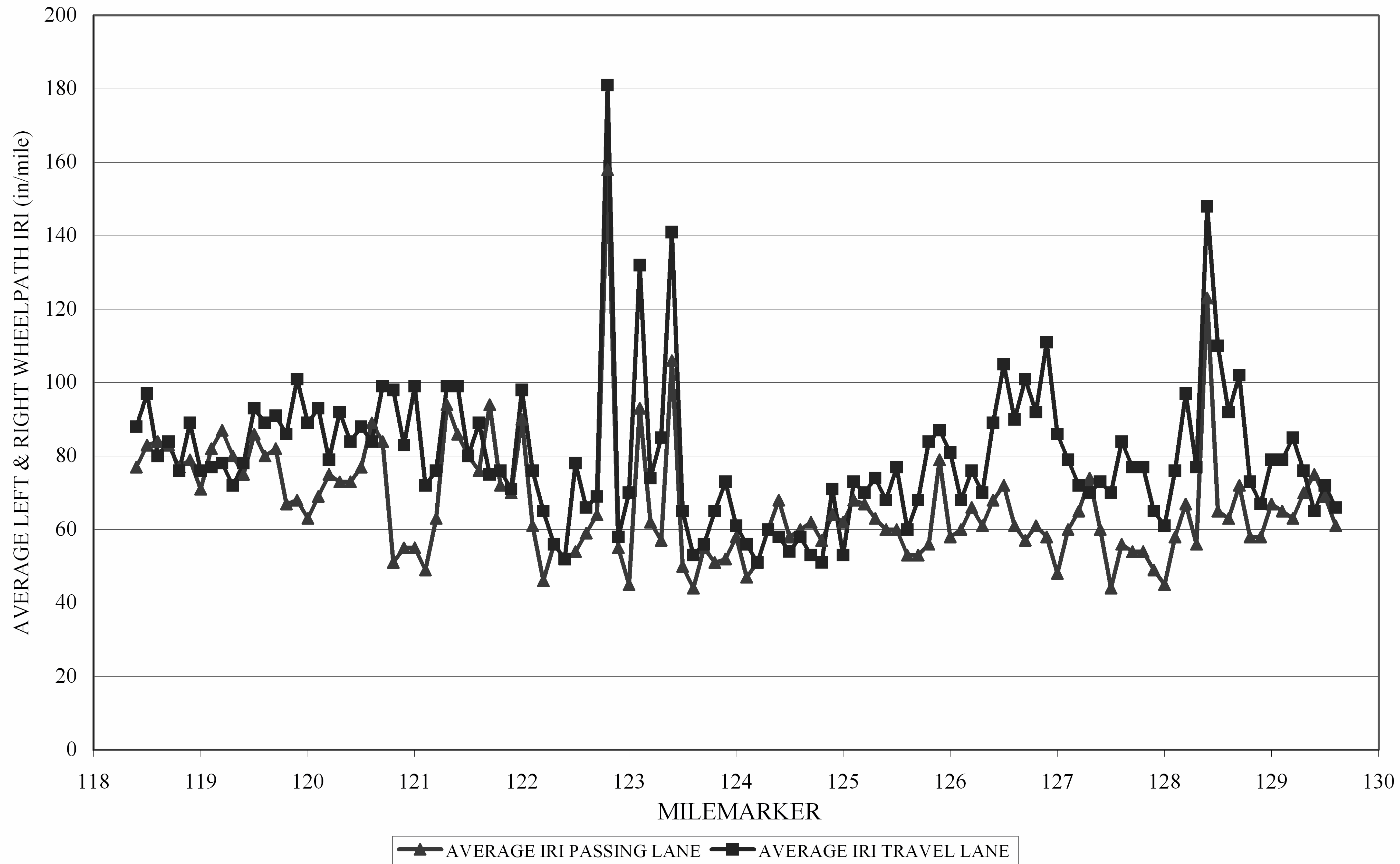
MATCHLINE SEE FIRST STREET (ROUTE 78) SHEET 1

FOR INFORMATIONAL PURPOSES ONLY

DIG SAFE
INFORMATION
SHEET #2

PROJECT NAME:	SWANTON - HIGHGATE
PROJECT NUMBER:	IM SURF (5)
FILE NAME: ...07A278\...07A278.dgn	PLOT DATE: 27-JAN-2009
PROJECT LEADER: DOMEY	DRAWN BY: HUNT
DESIGNED BY: HUNT	CHECKED BY: PAVT MGMT
IPARM FILE NAME: 07A278ds2.i	SHEET 12 OF 14

189 SB 2007 IRI DATA FOR SWANTON-HIGHGATE IM SURF(5)

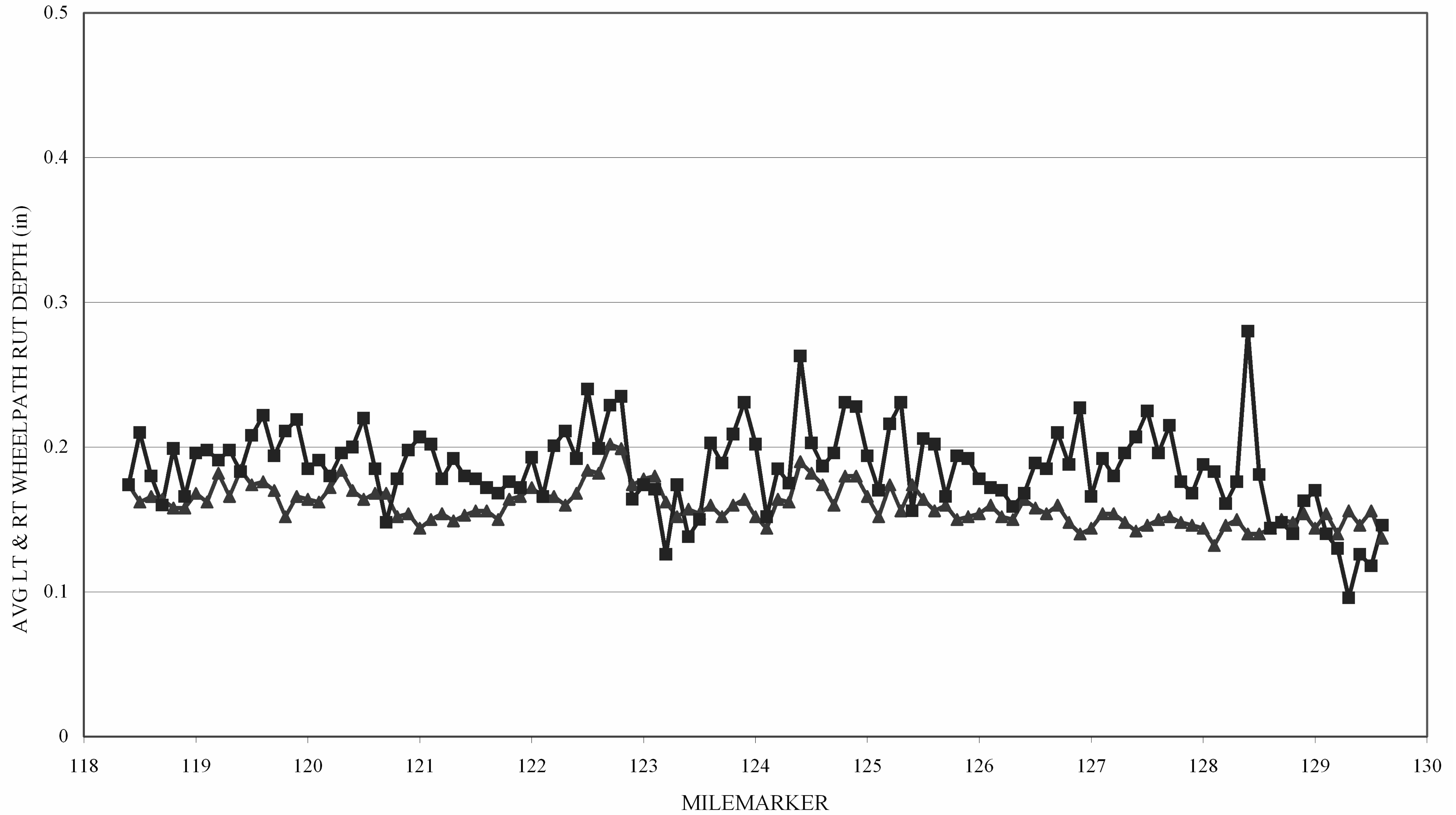


FOR INFORMATIONAL PURPOSES ONLY

**ROUGHNESS
DATA
INFORMATION
SHEET**

PROJECT NAME:	SWANTON - HIGHGATE
PROJECT NUMBER:	IM SURF (5)
FILE NAME: ...07A278\...07A278.dgn	PLOT DATE: 11-FEB-2009
PROJECT LEADER: DOMEY	DRAWN BY: HUNT
DESIGNED BY: HUNT	CHECKED BY: PAVT MGMT
IPARM FILE NAME: 07A278rod.1	SHEET 13 OF 14

I89 SB 2007 RUT DATA FOR SWANTON-HIGHGATE IM SURF(5)



▲ AVERAGE RUT PASSING LANE
 ■ AVERAGE RUT TRAVEL LANE

FOR INFORMATIONAL PURPOSES ONLY

**RUTTING
DATA
INFORMATION
SHEET**

PROJECT NAME:	SWANTON - HIGHGATE
PROJECT NUMBER:	IM SURF (5)
FILE NAME: ...07A278\...07A278.dgn	PLOT DATE: 11-FEB-2009
PROJECT LEADER: DOMEY	DRAWN BY: HUNT
DESIGNED BY: HUNT	CHECKED BY: PAVT MGMT
IPARM FILE NAME: 07A278rud.l	SHEET 14 OF 14