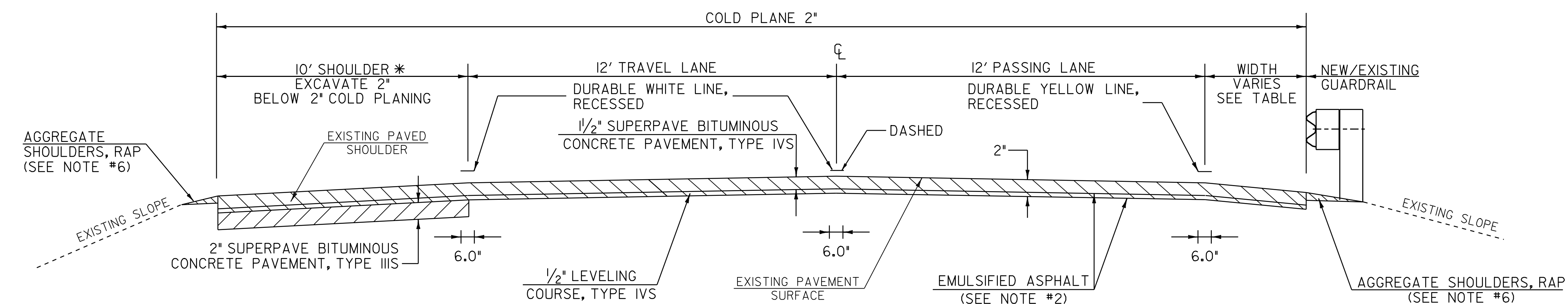


MAINLINE TYPICAL SECTION - NB

LOCATION
 NB 924+00 TO 1157+80
 NB 1167+83 TO 1337+69

* WHEN SHOULDER IS LESS THAN 10' IN WIDTH COLD PLANE 2" WITH 1/2" TYPE IVS, AND PAVE WITH 1 1/2" SUPERPAVE BITUMINOUS CONCRETE PAVEMENT, TYPE IVS.



MAINLINE TYPICAL SECTION - SB

LOCATION
 SB 924+00 TO 1157+80
 SB 1167+83 TO 1341+38

PROJECT PAVING LIMITS

TOWN	BEGIN STATION	END STATION	LEFT SHOULDER	LANE TYPICAL	RIGHT SHOULDER	WEARING DEPTH	BASE DEPTH	LEVELING	NOTES
I-89 NB ROYALTON	924+00	1157+80	4	12-12	10	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	1157+80	1167+83	3	12-12	3	1 1/4" IVS			COLD PLANE 1 1/4" AND OVERLAY 1 1/4", BRIDGE 26N
	1167+83	1172+63	4	12-12	VARIES	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	1172+63	1188+48	4	12-12	10	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	1188+48	1197+98	4	12-12	VARIES	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
BETHEL I-89 SB ROYALTON	1197+98	1254+13	4	12-12	10	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	1254+13	1337+69	4	12-12	10	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	924+00	1157+80	10	12-12	4	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	1157+80	1167+83	3	12-12	3	1 1/4" IVS			COLD PLANE 1 1/4" AND OVERLAY 1 1/4", BRIDGE 26S
	1167+83	1174+53	VARIES	12-12	4	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
BETHEL	1174+53	1181+39	10	12-12	4	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	1181+39	1190+80	VARIES	12-12	4	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	1190+80	1254+13	10	12-12	4	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	1254+13	1341+38	10	12-12	4	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	NB ON RAMP (A)	200+00	215+43	4	16	4	1 1/2" IVS		1/2"
NB OFF RAMP (B)	100+00	101+50	VARIES	VARIES	VARIES	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	101+50	113+37	4	16	4	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
SB ON RAMP (C)	400+00	401+50	VARIES	VARIES	VARIES	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	401+50	411+48	5	16	5	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
SB OFF RAMP (D)	300+00	301+50	VARIES	VARIES	VARIES	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"
	301+50	313+13	4	16	4	1 1/2" IVS		1/2"	COLD PLANE 2", LEVEL 1/2" AND OVERLAY 1 1/2"

SEEDING FORMULA - RURAL AREAS

% WT.	LBS./A.	NAME	PUR %	GERM %
37.5	22.5	CREeping RED FESCUE	98	85
37.5	22.5	TALL FESCUE	95	90
5.0	3.0	RED TOP	95	90
15.0	9.0	BIRDSFOOT TREFOIL	98	85
5.0	3.0	ANNUAL RYEGRASS	95	85
100.0	60.0			

SEEDING NOTES

SEED MIXTURE: SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS SEED.
 SEED: TO BE APPLIED PER SEEDING FORMULAS OR AS DIRECTED BY THE RESIDENT ENGINEER.
 FERTILIZER: FORMULA 10-20-10, TO BE USED WITH SEED, APPLIED AT THE RATE OF 500 LBS./ACRE. (HYDRO SEEDERS MAY USE 9-19-19 FORMULA)
 AGRICULTURAL LIMESTONE: TO BE APPLIED AT THE RATE OF 2 TONS/ACRE, OR AS DIRECTED BY THE RESIDENT ENGINEER.
 TOPSOIL: TO BE USED WITH SEED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER.

GENERAL NOTES

- COLD PLANE MAINLINE FULL WIDTH 2". EXCAVATE 10' SHOULDER AN ADDITIONAL 2". PAVE 10' SHOULDER 2" WITH TYPE IIIS, LEVEL MAIN LINE FULL WIDTH WITH TYPE IVS, PAVE MAINLINE FULL WIDTH WITH 1 1/2" TYPE IVS. COLD PLANE RAMP A 3", LEVEL WITH TYPE IVS, PAVE WITH 2-1 1/2" LIFTS TYPE IVS. COLD PLANE RAMPS B, C & D 2", LEVEL WITH TYPE IVS, PAVE WITH 1 1/2" TYPE IVS. PAVE U-TURNS 1 1/2" WITH TYPE IVS. PAVING SHALL BE COMPLETED AS SHOWN ON THE TYPICALS, UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
- EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES, COLD PLANED SURFACES AND BETWEEN ALL COURSES OF PAVEMENT AT THE RATE OF 0.025 GAL/SY OR AS DIRECTED BY THE RESIDENT ENGINEER.
- SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = 1/4" ± (TOTAL THICKNESS EXCLUDING LEVELING COURSE).
- COLD PLANING SHALL BE COMPLETED ACCORDING TO THE TYPICAL SECTION OR AS DENOTED OTHERWISE ON THE PLANS. A FULL DEPTH BUTT JOINT SHALL BE CONSTRUCTED AT THE PROJECT BEGIN/END AND AT ALL RAMP APPROACHES AS NOTED ON THE PROJECT PLANS OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. ALL BUTT JOINTS SHALL BE SAW CUT, INCIDENTAL TO ITEM 210.10, COLD PLANING, BITUMINOUS PAVEMENT.
- ESTIMATED QUANTITIES OF ITEM 608.25, ALL PURPOSE EXCAVATOR RENTAL, TYPE I, 608.37, TRUCK RENTAL AND 608.40, LOADER RENTAL, TYPE I HAVE BEEN INCLUDED FOR THE PROVISION OF CONSTRUCTING GUARDRAIL FLARES. 25 CUBIC YARDS OF EARTH BORROW HAS BEEN INCLUDED TO PROVIDE FOR FLARE CONSTRUCTION. THE GUARDRAIL FLARES SHALL BE CAPPED WITH AN ESTIMATED 3 INCH DEPTH OF DITCHING MATERIAL OR TOPSOIL UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. ITEM 653.20, TEMPORARY EROSION MATTING, SHALL BE PLACED ON SLOPES GREATER THAN 1:6 CREATED BY THE GUARDRAIL FLARE. THE QUANTITIES INCLUDED REFLECT 25 SY OF ITEM 653.20, TEMPORARY EROSION MATTING FOR EACH NEW GUARDRAIL FLARE.
- ALL EDGES OF PAVEMENT SHALL BE BACKED UP FULL HEIGHT WITH COLD PLANE GRINDINGS AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.13, AGGREGATE SHOULDERS, RAP.
- ITEM 604.40, CHANGING ELEVATION OF DROP INLETS CATCH BASINS OR MANHOLES AND ITEM 604.41, REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I ARE ESTIMATED QUANTITIES AND SHALL BE PERFORMED AT LOCATIONS INDICATED ON THE LAYOUT SHEETS AND AS DIRECTED BY THE RESIDENT ENGINEER. ALL DROP INLETS AND CATCH BASINS SHALL BE RAISED OR REHABILITATED SUCH THAT THE GRATE ELEVATION IS LEVEL WITH THE SURROUNDING TERRAIN.
- COMPACTION, GRADING AND CLEAN UP ITEM 402.13, AGGREGATE SHOULDERS, RAP AND ITEM 651.35, TOPSOIL IS TO BE INCLUDED IN THE CONTRACT UNIT PRICE OF EACH ITEM.
- THE CONTRACTOR SHALL MAINTAIN COLD PLANE AND PAVING DEPTHS THAT DO NOT REDUCE THE CLEARANCE HEIGHTS OF THE ROADWAY OR SHOULDERS UNDER THE OVERPASS BRIDGES (BR-21 AND BR-28). THESE HEIGHTS SHALL BE RECORDED BY THE RESIDENT ENGINEER PRIOR TO THE START OF CONSTRUCTION AND SHALL BE REMEASURED AFTER PAVING OPERATIONS ARE COMPLETE. IF IT IS DETERMINED THAT A REDUCTION IN CLEAR HEIGHT HAS OCCURRED, THE CONTRACTOR SHALL RE- ESTABLISH THE ORIGINAL CLEAR HEIGHT AT NO ADDITIONAL EXPENSE TO THE STATE.
- AREAS ADJACENT TO THE SHOULDER, WHERE GUARDRAIL IS BEING RETAINED, THAT HAVE BUILT UP EXCESS MATERIAL ARE TO BE GRADED TO ALLOW THE SHOULDER TO DRAIN. PAYMENT IS TO BE INCLUDED UNDER ITEM 203.40, SHOULDER BERM REMOVAL.
- ALL DELINEATORS WITHIN THE PROJECT LIMITS ARE TO BE REPLACED. THESE ARE TO BE PAID FOR UNDER ITEM 676.10, DELINEATOR WITH STEEL POST AND 676.12, REMOVAL OF EXSITING DELINEATOR.

PROJECT TYPICAL SHEET 1

PROJECT NAME: ROYALTON-BETHEL	PLOT DATE: 28-JUN-2010 15:11
PROJECT NUMBER: IM 089-1(54)	DRAWN BY: MRS
FILE NAME: p06a198.dgn	CHECKED BY: PTS
PROJECT LEADER: PTS	SHEET 3 OF 37
DESIGNED BY: NULL	
IPARM FILE NAME: p06a198p1.i	

NOT TO SCALE