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STANDARDS

E-100	CONSTRUCTION APPROACH SIGNS	01/02/04
E-100A	SIDE ROAD CONSTRUCTION APPROACH SIGNS	01/02/04
E-101	CONSTRUCTION SIGN DETAILS	05/30/03
E-102	CONSTRUCTION SIGN DETAILS	06/30/03
E-102A	CONSTRUCTION SIGN DETAILS	05/01/04
E-103	MAINLINE TRAFFIC CONTROL DIVIDED HIGHWAY ONE LANE CLOSED	03/01/04
E-105	TRAFFIC CONTROL FOR CONSTRUCTION VEHICLE U-TURNS ON DIVIDED HIGHWAY	05/01/04
E-106	TRAFFIC CONTROL - MISCELLANEOUS DETAILS	03/01/04
E-107A	BREAKAWAY BARRICADE DETAILS	08/08/95
E-191	PAVEMENT MARKING DETAILS	02 / 01 / 99
E-192	PAVEMENT MARKING DETAILS	10 / 12 / 00

STATE OF VERMONT
AGENCY OF TRANSPORTATION



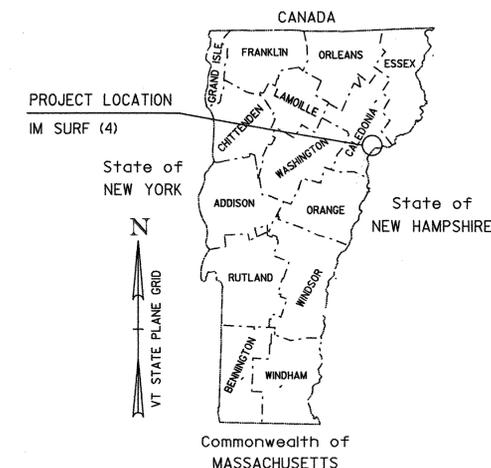
PROPOSED IMPROVEMENT

**TOWNS OF BARNET-WATERFORD
COUNTY OF CALEDONIA
INTERSTATE ROUTE 91**

BEGINNING AT MM ^{119.25}~~119.23~~ ON THE SOUTHBOUND BARREL IN THE TOWN OF BARNET
AND EXTENDING NORTHERLY 9.05 MILES TO MM ^{128.30}~~128.28~~ IN THE TOWN OF WATERFORD.

LENGTH OF PROJECT 9.05 MILES (47784 FEET)
LENGTH OF ROADWAY 9.05 MILES (47784 FEET)

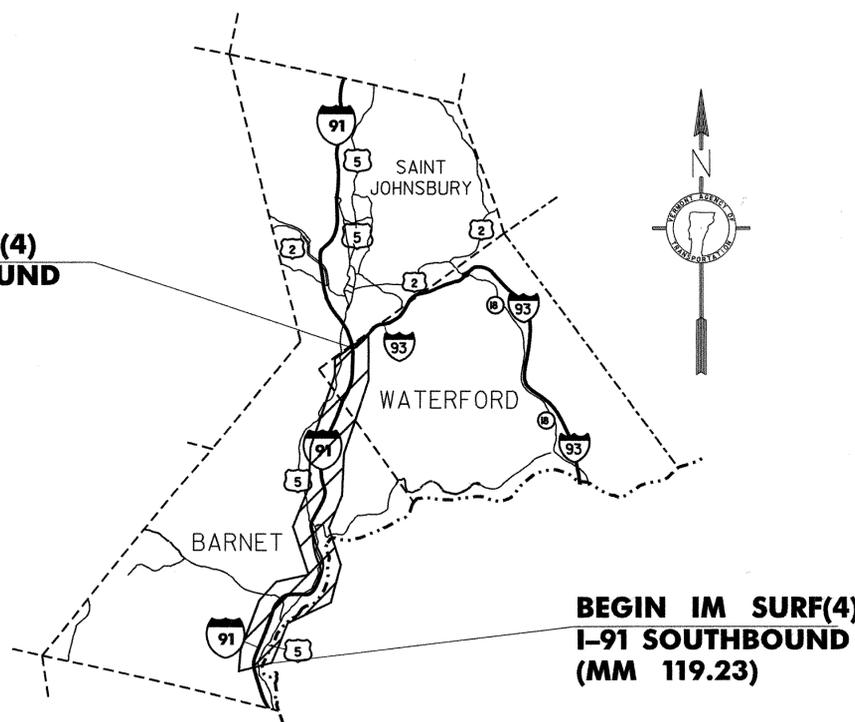
WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES FOG SEALING,
MICRO-SURFACING, AND NEW PAVEMENT MARKINGS ON THE EXISTING INTERSTATE TYPICAL.



TRAFFIC DATA

MM 119.23 ~ MM 120.45
2007 ADT = 2500
2007 DHV = 300
2017 ADT = 3000
2017 DHV = 360
2007 ~ 2017 ESALS = 2,166,000
MM 120.45 ~ MM 128.28
2007 ADT = 3000
2007 DHV = 360
2017 ADT = 3600
2017 DHV = 440
2007 ~ 2017 ESALS = 3,120,000

**END IM SURF(4)
I-91 SOUTHBOUND
(MM 128.28)**



**BEGIN IM SURF(4)
I-91 SOUTHBOUND
(MM 119.23)**

NOT TO SCALE

RECORD PLANS

CONTRACTOR: THE GORMAN GROUP, LLC. - ALBANY, NY

RESIDENT ENGINEER: BRIGITTE CODLING

CONSTRUCTION BEGAN: JULY 17, 2007

CONSTRUCTION COMPLETE: SEPTEMBER 14, 2007

RECORD PLANS BY: BRIGITTE CODLING & NICK GARBACIK

I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.

BY *Brigitte M. Codling* RESIDENT ENGINEER
DATE *October 22, 2008*

NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.

CONVENTIONAL SIGNS

COUNTY LINE	---
TOWN LINE	- - - - -
LIMITS OF ACCESS	○-○-○-○-○
POINT OF ACCESS	X
FENCE LINE	-x-x-x-
STONE WALL	○-○-○-○-○-○-○-○
TRAVELED WAY	- - - - -
GUARD RAIL	○-○-○-○-○
RAILROAD	—+—+—+—+—
SURVEY LINE	—+—+—+—+—
CULVERT	—+—+—+—+—
POWER POLE	⊕
TELEPHONE POLE	⊕
TREES	⊗
CONTROL OF ACCESS	///
PROPERTY LINE	—+—+—+—+—
R.O.W. TAKING LINE	—+—+—+—+—
SLOPE RIGHTS	○-○-○-○-○
TOP OF CUT	△-△-△-△-△
TOE OF SLOPE	○-○-○-○-○

DATUM

VERTICAL	N/A
HORIZONTAL	N/A

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT. CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

/pave/06a180/p06a180.dgn/pa180ti.i

APPROVED *[Signature]* DATE *5-10-07*
DIRECTOR OF PROGRAM DEVELOPMENT

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED *[Signature]* DATE *5-18-07*
DIVISION ADMINISTRATOR

PROJECT BARNET-WATERFORD
IM SURF (4)

SHEET 1 OF 11 SHEETS