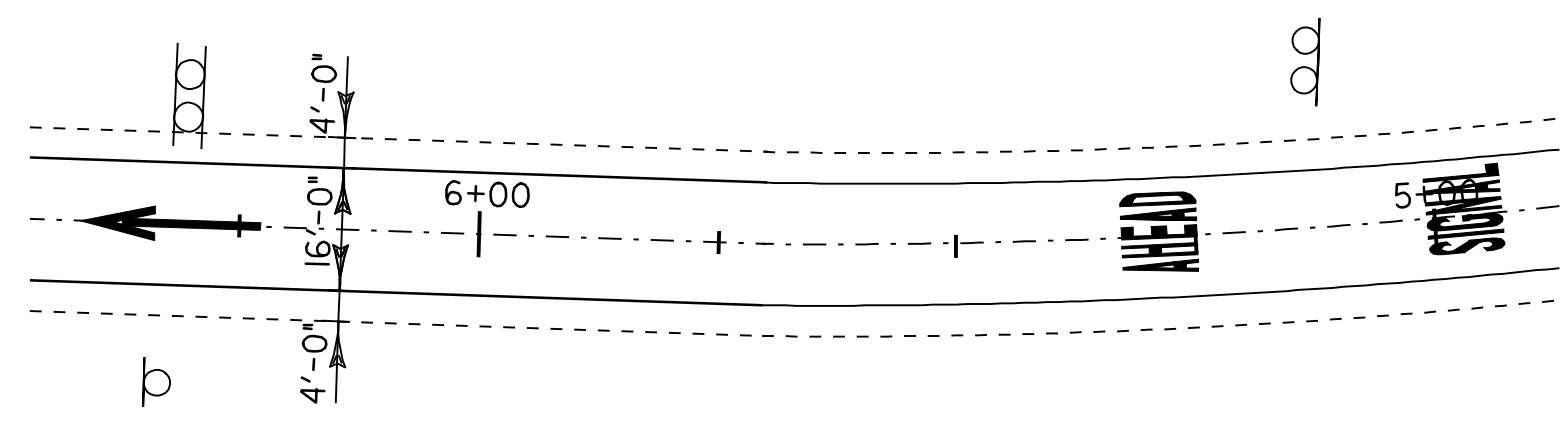


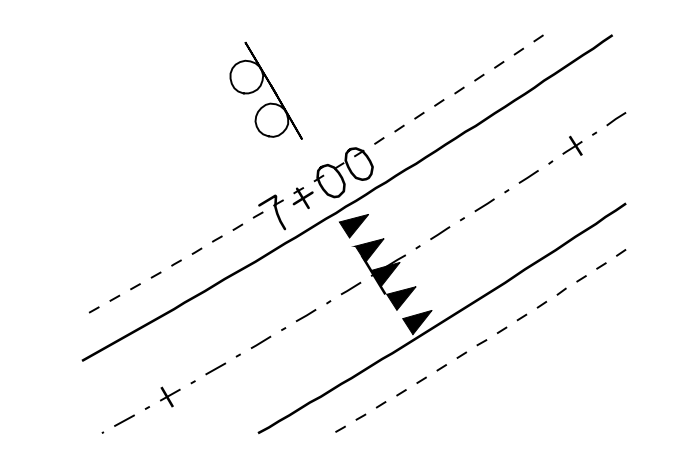
INSET A - GORE DETAIL AT EXIT 17 RAMP B



INSET B - EXIT 17 RAMP B

VEHICLE DETECTOR LOOPS								
LOOP NO.	LANE	CALL Ø	SIZE	TYPE & NO. TURNS	DELAY OR PRESENCE	INDUCTANCE CALC. ACT.	RESISTANCE CALC. ACT.	LEAKAGE TO GROUND
IA	WB RT	Ø 1	6x40	QUAD-2	PRESENCE	347	0.67	
IB	WB LT	Ø 1	6x40	QUAD-2	PRESENCE	350	0.71	

* ALL CALCULATED VALUES ARE AT THE CONTROLLER.
MEASURED VALUES MUST BE FILLED IN PRIOR TO TEST PERIOD.



INSET E - EXIT 17 RAMP F

I-89 SOUTHBOUND

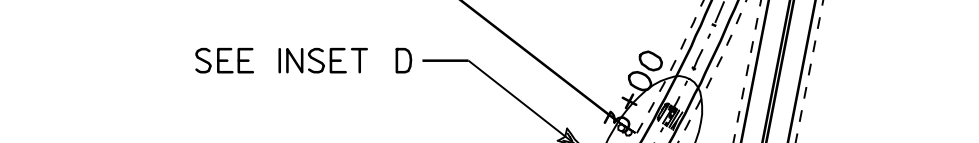
INSET D - EXIT 17 RAMP F

END EXIT 17 RAMP A AT STA. 11+00.00
END PROJECT IM 089-3(59)

BEGIN EXIT 17 RAMP F AT STA. 1+00.00



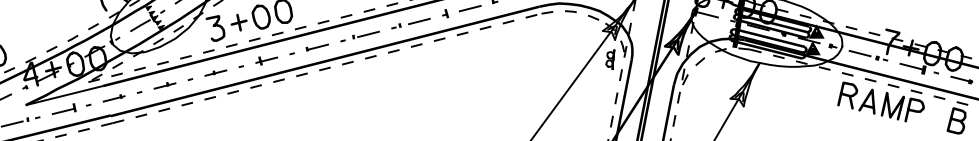
U.S. ROUTE 2



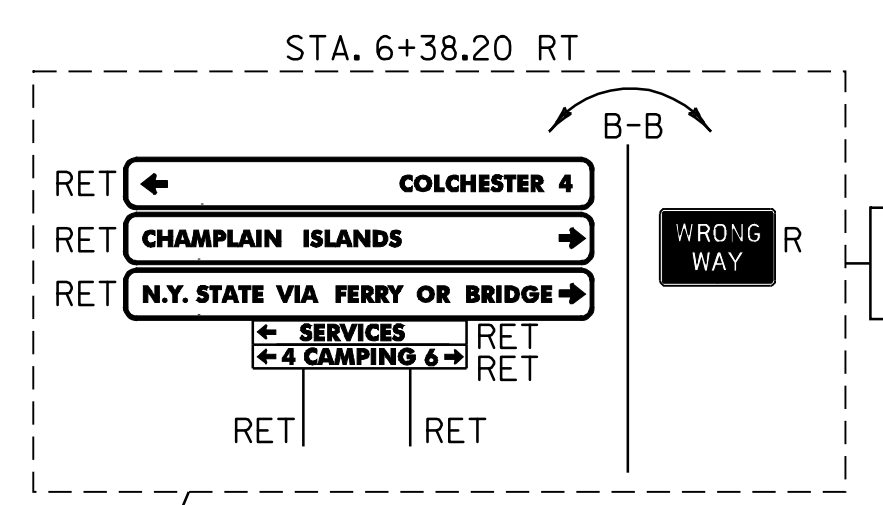
BEGIN EXIT 17 RAMP A AT STA. 1+00.00



END EXIT 17 RAMP B AT STA. 8+23.00



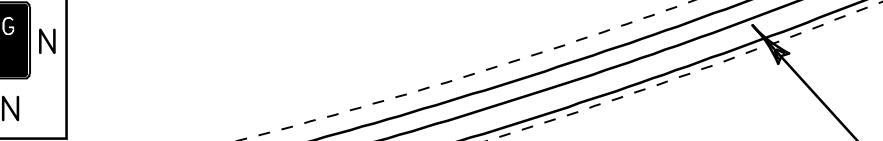
6+38.20 LT



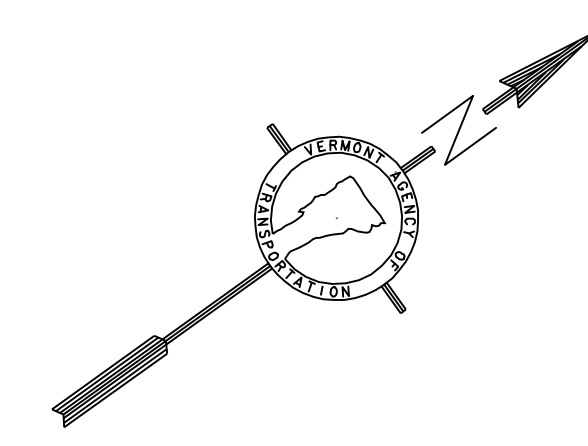
STA. 4+90.91 RT



BASELINE STA. 5176+11.00
BEGIN EXIT 17 RAMP B AT STA. 1+00.00



STA. 5174+40.00 (MM 98.000)
BEGIN PROJECT IM 089-3(59)



RAMP RADII INFORMATION

RAMP	RADII, FT	DESC.
A	716	RADII AT I-89 END OF RAMP
B	716	RADII AT I-89 END OF RAMP

LOOP DETECTOR NOTES

- LOOPS ON US 2 ARE NOT SHOWN ON THIS SHEET AS THERE WILL NOT BE ANY WORK DONE TO THESE LOOPS. NOT ALL SIGNAL EQUIPMENT IS SHOWN ON THIS SHEET.
- THE CONTRACTOR SHALL CONFIRM ALL LOCATIONS, INCLUDING, BUT NOT LIMITED TO, UTILITIES, POLES, PULL BOXES, STRIPING, AND LOOP DETECTORS. THE CONTRACTOR SHALL CONFIRM ALL LOCATIONS IN THE FIELD WITH THE RESIDENT ENGINEER PRIOR TO INSTALLATION.
- PRIOR TO COLD PLANING, THE CONTRACTOR SHALL DISCONNECT THE VEHICLE DETECTOR LOOP IN THE CONTROLLER CABINET AND CUT IT AT THE CURB OR SHOULDER. ONCE THE VEHICLE DETECTOR LOOP IS DISCONNECTED, THE SIGNAL PHASE THAT IT WAS CALLING SHALL BE SET ON MAXIMUM RECALL OR THE SIGNAL SHALL BE SET TO FLASH WHILE TRAFFIC IS BEING CONTROLLED BY A UNIFORMED TRAFFIC OFFICER.
- ALL PROPOSED VEHICLE DETECTOR LOOPS SHALL BE INSTALLED IN THE LEVELING COURSE, WHICH IS IMMEDIATELY BELOW THE WEARING SURFACE. ONCE THE PROPOSED VEHICLE DETECTOR LOOP IS INSTALLED, THE INDUCTANCE, RESISTANCE AND LEAKAGE TO GROUND MUST BE TESTED USING PROPERLY CALIBRATED EQUIPMENT. THESE TEST RESULTS SHALL BE COMPARED WITH THE CALCULATED VALUES SHOWN ON THE LAYOUT PLANS AND THE FIELD MEASURED VALUES SHALL BE RECORDED ON THE LAYOUT PLANS. UPON COMPLETION OF THE INSTALLATION OF A PROPOSED VEHICLE LOOP DETECTOR, THE SIGNAL SHALL BE RETURNED TO NORMAL OPERATION.
- EXISTING TIMINGS WILL BE USED.
- WORK IMPROVEMENTS CONSISTING OF THOSE SHOWN ON PLANS SHALL BE PERFORMED ACCORDING TO SPECIFICATIONS AND STANDARD DRAWINGS OF VERMONT AGENCY OF TRANSPORTATION. VEHICLE DETECTOR LOOPS SHALL COMPLY WITH VTRANS STANDARD E-172.

646.414 DURABLE 6" WHITE LINE (MOD. - RETROREFLECTIVITY/INLAID)

- STA. 5176+11LT - STA. 5182+11LT (SOLID)
- STA. 5176+11LT - STA. 5178+21LT (DASHED)
- STA. 5178+21LT - STA. 5182+11LT (DOTTED)
- STA. 1+00 RT - STA. 4+25 RT EXIT 17 RAMP A (SOLID)
- STA. 5+72 RT - STA. 11+00 RT EXIT 17 RAMP A (SOLID)
- STA. 1+00 RT - STA. 8+23 RT EXIT 17 RAMP B (SOLID)
- STA. 1+00 RT - STA. 9+25 RT EXIT 17 RAMP F (SOLID)

646.415 DURABLE 6" YELLOW LINE (MOD. - RETROREFLECTIVITY/INLAID)

- STA. 1+00 LT - STA. 11+00 LT EXIT 17 RAMP A (SOLID)
- STA. 2+53 LT - STA. 8+23 LT EXIT 17 RAMP B (SOLID)
- STA. 1+00 LT - STA. 7+75 LT EXIT 17 RAMP F (SOLID)

646.44 DURABLE 12" WHITE LINE (MOD. - RETROREFLECTIVITY/INLAID)

- STA. 5174+57 LT - STA. 5176+11LT (SOLID)
- STA. 1+00 LT - STA. 2+53 LT EXIT 17 RAMP B (SOLID)
- STA. 5174+57 LT - STA. 5176+11LT (HATCHED)

646.46 DURABLE 24" STOP BAR (THERMOPLASTIC)
STA. 8+00 EXIT 17 RAMP B

EXISTING	NEW	LEGEND
⊠	⊠	UTILITY POLE
—○—	—○—	LUMINAIRE
○	○	LIGHT OR WOOD POLE
⊙	⊙	STRAIN POLE
⊠	⊠	CONTROLLER CABINET
⊠	⊠	PULLBOX/JUNCTION BOX
—○—	—○—	SIGNAL HEAD
—BX—	—BX—	CONDUIT
⊠	⊠	VEHICLE LOOPS
⊙	⊙	PEDESTAL POST
—S	—S	STANCHION
—S	—S	SWEEP

646.50 DURABLE LETTER OR SYMBOL (THERMOPLASTIC)

- STA. 4+71 EXIT 17 RAMP B "SIGNAL"
- STA. 5+11 EXIT 17 RAMP B "AHEAD"
- STA. 6+39 EXIT 17 RAMP B "ARROW"
- STA. 3+03 EXIT 17 RAMP F "YIELD"
- STA. 3+43 EXIT 17 RAMP F "AHEAD"
- STA. 7+00 EXIT 17 RAMP F "YIELD TRIANGLES" (5)

646.614 TEMPORARY 6" WHITE LINE (PAINT)

- STA. 5176+11LT - STA. 5182+11LT (SOLID)
- STA. 5176+11LT - STA. 5178+21LT (DASHED)
- STA. 5178+21LT - STA. 5182+11LT (DOTTED)
- STA. 1+00 RT - STA. 4+25 RT EXIT 17 RAMP A (SOLID)
- STA. 5+72 RT - STA. 11+00 RT EXIT 17 RAMP A (SOLID)
- STA. 1+00 RT - STA. 8+23 RT EXIT 17 RAMP B (SOLID)
- STA. 1+00 RT - STA. 9+25 RT EXIT 17 RAMP F (SOLID)

646.615 TEMPORARY 6" YELLOW LINE (PAINT)

- STA. 1+00 LT - STA. 11+00 LT EXIT 17 RAMP A (SOLID)
- STA. 2+53 LT - STA. 8+23 LT EXIT 17 RAMP B (SOLID)
- STA. 1+00 LT - STA. 7+75 LT EXIT 17 RAMP F (SOLID)

646.64 TEMPORARY 12" WHITE LINE (PAINT)

- STA. 5174+57 LT - STA. 5176+11LT (SOLID)
- STA. 1+00 LT - STA. 2+53 LT EXIT 17 RAMP B (SOLID)
- STA. 5174+57 LT - STA. 5176+11LT (HATCHED)

646.66 TEMPORARY 24" STOP BAR (PAINT)

- STA. 8+00 EXIT 17 RAMP B

646.70 TEMPORARY LETTER OR SYMBOL (PAINT)

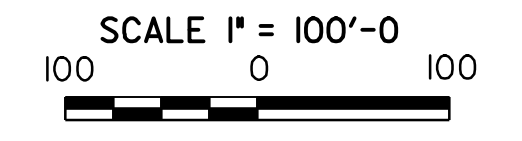
- STA. 4+71 EXIT 17 RAMP B "SIGNAL"
- STA. 5+11 EXIT 17 RAMP B "AHEAD"
- STA. 6+39 EXIT 17 RAMP B "ARROW"
- STA. 3+03 EXIT 17 RAMP F "YIELD"
- STA. 3+43 EXIT 17 RAMP F "AHEAD"
- STA. 7+00 EXIT 17 RAMP F "YIELD TRIANGLES" (5)

675.50 REMOVING SIGN

- AS SHOWN - 5

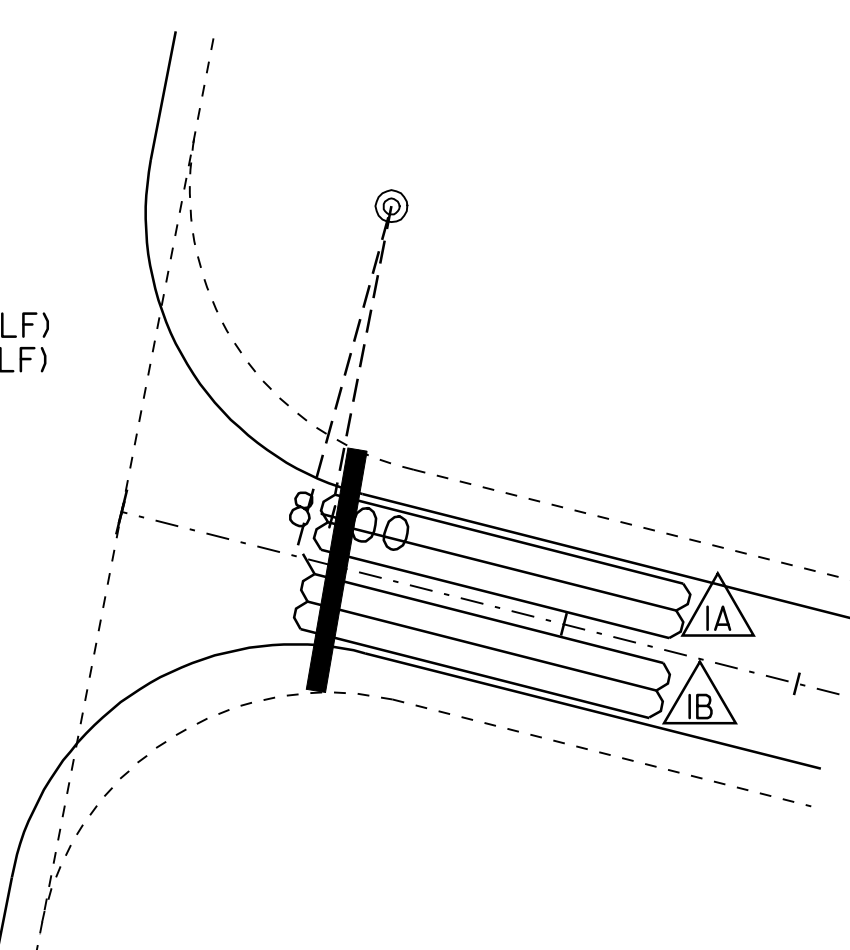
678.22 VEHICLE LOOP DETECTOR

- STA. 7+65 RT - STA. 8+05 RT EXIT 17 RAMP B (92 LF)
- STA. 7+65 LT - STA. 8+05 LT EXIT 17 RAMP B (92 LF)



LEGEND

- N = NEW
 - R = REMOVE
 - B-B = BACK TO BACK
 - RET = RETAIN
- REFER TO STANDARDS E-191 & E-192 FOR ADDITIONAL DETAILS.



INSET C - EXIT 17 RAMP B

NOTE:

BURIED COMMUNICATION CABLES EXIST AT EXIT 17. BUT ARE NOT SHOWN ON THIS PLAN. THESE CABLES ARE TO BE AVOIDED DURING CONSTRUCTION.

RAMP DETAIL SHEET INTERCHANGE #17

PROJECT NAME: COLCHESTER - GEORGIA	PLOT DATE: 12-OCT-2010 16:39
PROJECT NUMBER: IM 089-3(59)	DRAWN BY: RHB/SJB
FILE NAME: p04g030.DGN	CHECKED BY: EPD
PROJECT LEADER: EPD	DESIGNED BY: JDA
DESIGNED BY: JDA	PLOT FILE:
PLOT FILE:	SHEET 18 OF 22