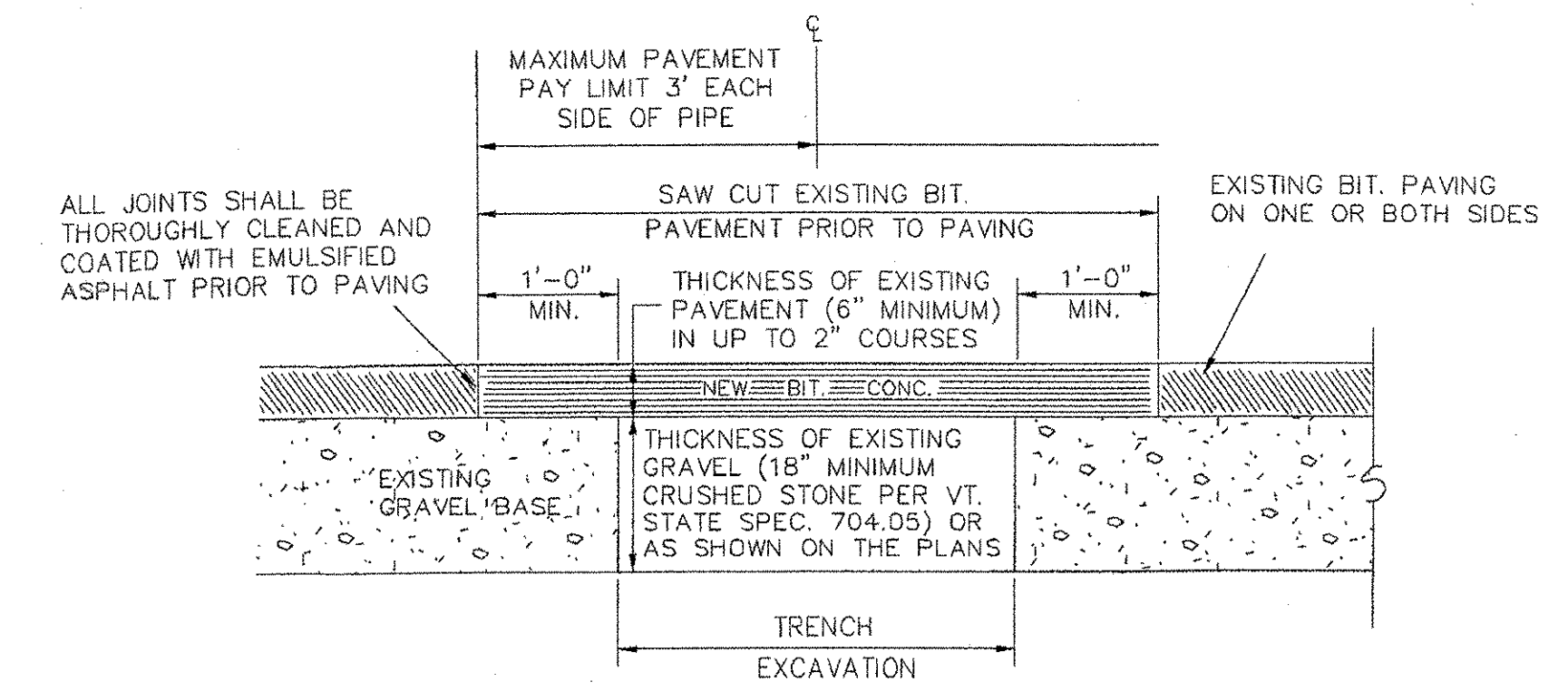


GENERAL CONSTRUCTION SPECIFICATIONS

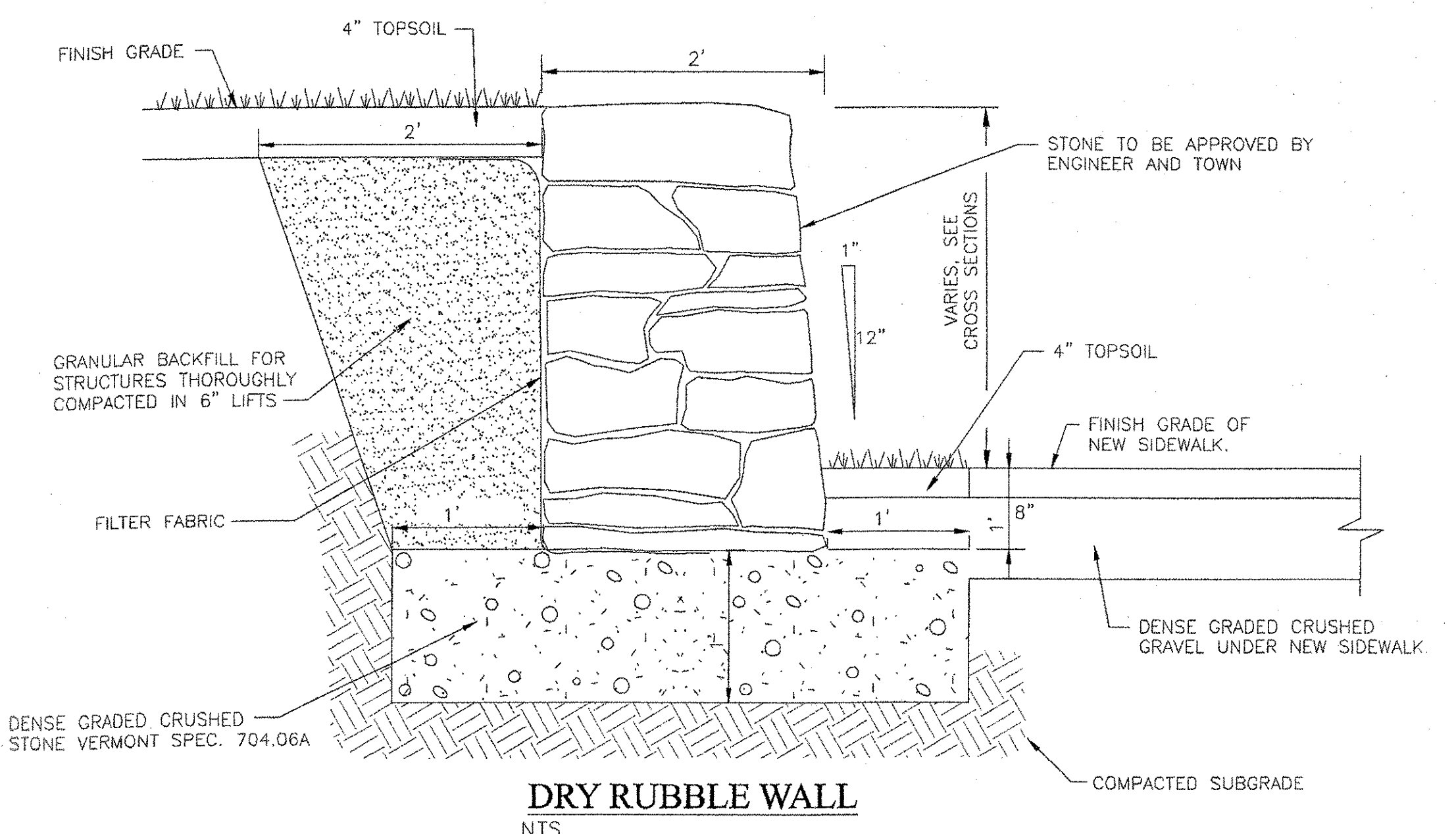
- ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE 2001 VERMONT AGENCY OF TRANSPORTATION STANDARD SPECIFICATIONS FOR CONSTRUCTION, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, 2003 EDITION, THE PROJECT SPECIFICATIONS AND THESE PLANS.
- THE CONTRACTOR SHALL CONTACT ALL UTILITIES BEFORE EXCAVATION TO VERIFY THE LOCATION OF ANY UNDERGROUND LINES. THE CONTRACTOR SHALL NOTIFY "DIGSAFE" AT 1-888-DIG-SAFE PRIOR TO ANY EXCAVATION. PRIOR TO CALLING DIG SAFE IT IS A REQUIREMENT TO PREMARK PROPOSED EXCAVATION AREAS USING WHITE PAINT, STAKES OR OTHER SUITABLE WHITE MARKINGS, IN A MANNER THAT WILL ENABLE THE OPERATORS OF THE UNDERGROUND UTILITY FACILITIES TO KNOW THE BOUNDARIES OF THE PROPOSED EXCAVATION ACTIVITIES. PREMARKING IS NOT REQUIRED IF THE ACTUAL EXCAVATION WILL BE CONTINUOUS AND WILL EXCEED 500 FEET IN LENGTH.
- UTILITIES INFORMATION SHOWN HEREON WERE OBTAINED FROM BEST AVAILABLE SOURCE AND MAY OR MAY NOT BE EITHER ACCURATE OR COMPLETE. CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF EXISTING UTILITIES AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY UTILITY, PUBLIC OR PRIVATE, SHOWN OR NOT SHOWN HEREON. CONTRACTOR SHALL CONNECT OR RECONNECT ALL UTILITIES TO THE NEAREST SOURCE THROUGH COORDINATION WITH UTILITY OWNER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEMOLITION AND REMOVAL OF ALL EXISTING VEGETATION, PAVEMENT AND STRUCTURES NECESSARY TO CONSTRUCT THIS PROJECT UNLESS OTHERWISE NOTED ON THESE PLANS. THE CONTRACTOR SHALL REMOVE ALL EXCESS MATERIAL, DEBRIS AND TRASH FROM THE SITE UPON COMPLETION OF CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE DUST CREATED AS A RESULT OF CONSTRUCTION DOES NOT CREATE A NUISANCE OR A SAFETY HAZARD. WHERE AND WHEN DEEMED NECESSARY BY THE ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO WET SECTIONS OF THE CONSTRUCTION AREA WITH WATER, APPLY CALCIUM CHLORIDE, OR SWEEP THE ROADWAY.
- ANY SURFACES, LINES, MARKERS OR STRUCTURES WHICH HAVE BEEN DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR AT HIS OR HER EXPENSE, TO THE CONDITION AT LEAST EQUAL TO THAT IN WHICH THEY WERE FOUND IMMEDIATELY PRIOR TO THE BEGINNING OF OPERATIONS.
- FOR ANY WORK WITHIN THE HIGHWAY RIGHT-OF-WAY A MINIMUM OF ONE-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. CONTINUOUS TWO-WAY TRAFFIC WILL BE REQUIRED AT NIGHT, DURING PEAK-HOURS, AND WHENEVER POSSIBLE DURING ACTUAL CONSTRUCTION ACTIVITIES. UNIFORMED TRAFFIC CONTROL OFFICERS SHALL DIRECT TRAFFIC WHEN THERE IS ONE-WAY TRAFFIC AND WHEN DEEMED NECESSARY BY THE TOWN. TEMPORARY CONSTRUCTION SIGNS AND TRAFFIC CONTROL SIGNS SHALL BE ERRECTED BY THE CONTRACTOR IN ACCORDANCE WITH STATE AND TOWN STANDARDS AND THESE PLANS.
- TO ENSURE COMPLIANCE WITH THE PLANS AND OTHER REQUIREMENTS, THE DESIGN ON THESE PLANS SHALL BE INSPECTED BY LAMOUREUX AND DICKINSON CONSULTING ENGINEERS, INC., ESSEX JUNCTION, VERMONT. LAMOUREUX AND DICKINSON WAIVES ANY AND ALL RESPONSIBILITY AND LIABILITY FOR PROBLEMS THAT MAY ARISE FROM THE FAILURE OF THE CONTRACTOR TO FOLLOW THESE PLANS, SPECIFICATIONS AND THE DESIGN INTENT THAT THE PLANS CONVEY, AND FROM THE FAILURE TO HAVE BEEN RETAINED OR NOTIFIED TO INSPECT THE WORK AND TESTS IN PROGRESS. AT A MINIMUM, THE CONTRACTOR SHALL NOTIFY THE ENGINEER 24 HOURS IN ADVANCE OF STARTING ANY WORK, CUTTING THE PAVEMENT, BEGINNING THE INSTALLATION OF ANY UTILITIES, PRIOR TO BRINGING IN ANY NEW GRAVEL FOR THE NEW BASE, TO VERIFY LEDGE BLASTING AND REMOVAL, PAVING AND SIGNALS TO VERIFY QUANTITIES, AND FINAL INSPECTION.
- HEALTHY EXISTING TREES ON AND ADJACENT TO THE SITE SHALL BE PROTECTED BY THE CONTRACTOR.
- THE FINISH GRADE SLOPES SHALL BE AS SHOWN ON THE PLANS. FINISH SLOPES, DITCHES AND DISTURBED AREAS SHALL RECEIVE A MINIMUM OF 4" OF TOPSOIL AND SHALL BE FERTILIZED, SEEDED, LIMED, AND MULCHED. TURF ESTABLISHMENT SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 651 OF THE VERMONT AGENCY OF TRANSPORTATION SPECIFICATIONS AND THE SPECIFICATIONS INCLUDED ON THESE PLANS.
- ALL FILL SHALL BE PLACED IN 6 INCH LIFTS AND THOROUGHLY COMPACTED TO 95% OF MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT AS DETERMINED BY ASTM D698 STANDARD PROCTOR, UNLESS OTHERWISE SPECIFIED.
- THE EROSION CONTROL METHODS USED DURING CONSTRUCTION OF THE PROJECT SHALL PROCEED IN THE FOLLOWING SEQUENCE:
 - THE CONTRACTOR SHALL INSTALL AND MAINTAIN STONE FILTERS, SILT FENCES, DITCHES AND OTHER EROSION CONTROL DEVICES AS SHOWN ON THE PLANS. THEY SHALL BE MAINTAINED AND REPAIRED AFTER EVERY RAINFALL UNTIL THE NEW IMPROVEMENTS ARE PAVED AND ALL DISTURBED AREAS HAVE BEEN GRASSSED AND APPROVED BY THE ENGINEER. THE MAINTENANCE OF THE EROSION CONTROL DEVICES WILL INCLUDE REMOVAL OF ANY ACCUMULATED SEDIMENTATION.
 - THE TOPSOIL SHALL BE REMOVED FROM THE AREAS TO BE GRADED AND STOCKPILED. HAY BALES SHALL BE PLACED CONTINUOUSLY AROUND THE BOTTOM OF THE PILE.
 - THE CONTRACTOR WILL TOPSOIL, SEED, AND MULCH THE DISTURBED AREAS AS SOON AS POSSIBLE FOLLOWING COMPLETION OF ADJACENT CONSTRUCTION.
 - AT COMPLETION OF GRADING, THE SLOPES, DITCHES, AND ALL DISTURBED AREAS SHALL BE SMOOTH AND FREE OF POCKETS WITH SUFFICIENT SLOPE TO ENSURE DRAINAGE.
- NEW PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH NOTE #1 ABOVE. ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE NEW IMPROVEMENTS SHALL BE REMOVED BY GRINDING.
- BACKFILL UNDER PIPES IN FILL AREAS SHALL BE COMPACTED TO 95% OF MAXIMUM DENSITY. A MINIMUM OF (2) COMPACTION TESTS SHALL BE TAKEN AT THE CONTRACTOR'S EXPENSE UNDER EACH RUN OF PIPE PRIOR TO INSTALLATION. THE PIPES SHALL ONLY BE INSTALLED OVER ADEQUATELY COMPACTED SOILS.



- SETUP AND MAINTAIN SIGNS AND OTHER SAFETY CONTROL DEVICES.
- RESHAPE HOLE AND PATCH AREA BY CUTTING WITH A CONCRETE SAW INTO SQUARE OR RECTANGULAR SHAPE AND CUT SIDE FACES VERTICALLY. RESHAPE DOWNWARD TO SOLID MATERIAL AND AROUND HOLE TO SOUND PAVEMENT.
- BACKFILL TRENCH IN 6" LIFTS AND COMPACT EACH LIFT TO 95% OF MAXIMUM DENSITY OF OPTIMUM MOISTURE CONTENT AS DETERMINED BY ASTM D698 STANDARD PROCTOR.
- REMOVE ALL LOOSE MATERIAL AND THOROUGHLY SWEEP THE HOLE AREA CLEAN OF MUD AND STANDING WATER.
- APPLY LIQUID ASPHALT TACK TO VERTICAL FACES IN A UNIFORM MANNER. DO NOT PUDDLE TACK COAT ON BOTTOM OF HOLE.
- FILL TOP OF HOLE WITH TYPE III BITUMINOUS CONCRETE AND COMPACT IN LIFTS NO MORE THAN 2" THICK. FINAL UNCOMPACTED LIFT SHOULD BE 1/2" TO 1" ABOVE ADJOINING PAVEMENT SO THAT AFTER COMPACTION THE PATCH IS LEVEL WITH THE ORIGINAL PAVEMENT. EACH LIFT SHOULD BE THOROUGHLY COMPACTED WITH A VIBRATORY PLATE COMPACTOR OR A PORTABLE ROLLER. EXPERIENCE HAS SHOWN THAT 15 TO 20 PASSES WITH A VIBRATORY ROLLER AND MIX TEMPERATURE ABOVE 250 F (121 C) ARE NECESSARY TO ENSURE GOOD COMPACTION. HAND TAMP SHOULD ONLY BE USED FOR SMALL AREAS (LESS THAN 1 S.F.).
- CLEAN UP AREA. DO NOT LEAVE EXCESS FILL OR EXCAVATED MATERIAL ON THE PAVEMENT. REMOVE SAFETY SIGNS.

REPLACEMENT of EXISTING BITUMINOUS PAVEMENT

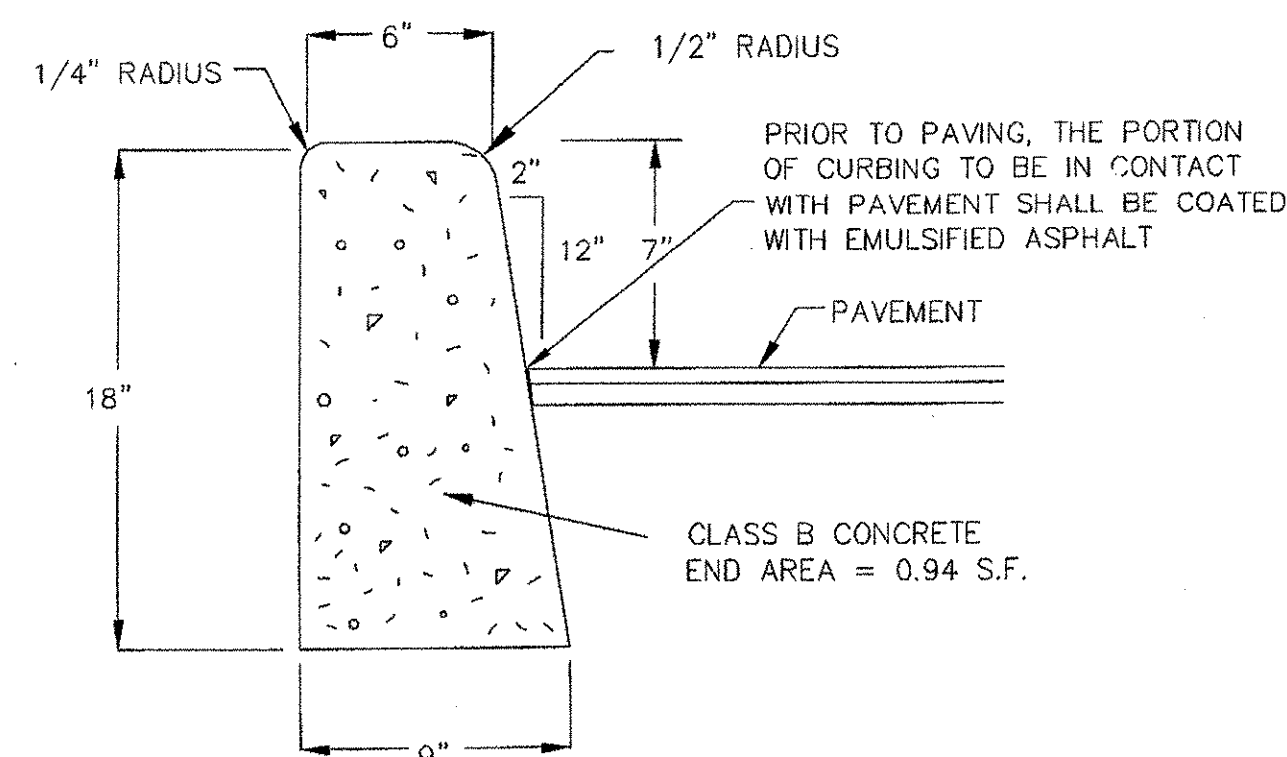
NTS



FIELD STONE NOTES

- FIELD STONE SHALL BE NATIVE TO THE AREA ROUGHLY RECTANGULAR AND HAVE AT LEAST ONE WEATHERED FACE.
- FIELD STONE FACES SHALL MEASURE BETWEEN 8"-20" ON ALL FACES.
- WEATHERED SIDE OF FIELD STONE TO BE PLACED ON WALL FACE.
- HORIZONTAL AND VERTICAL JOINT WIDTH NOT TO EXCEED 3" MAXIMUM. HORIZONTAL JOINTS SHALL BE PARALLEL. STAGER VERTICAL JOINTS 3" MINIMUM.
- FIELD STONE SHALL BE FOR UNDER ITEM 602.20, DRY RUBBLE MASONRY.
- TOP OF WALL SHALL SLOPE TO PARALLEL ADJACENT FINISH GRADE. TOP OF WALL FINISH GRADE SLOPE SHALL NOT EXCEED 0.050. FIELD STONE COURSING BEDS SHALL BE ROUGHLY PARALLEL TO THE TOP OF WALL.
- THE LARGEST STONES SHALL BE USED AT THE BASE OF THE WALL AND THE SIZE OF THE STONES SHALL DECREASE AS COURSES ARE LAID. STONES USED IN THE TOP COURSE BELOW THE CAP STONE SHALL NOT BE SMALLER IN HEIGHT THAN THE SPECIFIED CAP STONE.
- CAP STONE SHALL BE 6" X 24" X 24" MINIMUM (2" TOLERANCE).

GRADATION REQUIREMENTS		
MATERIAL	SIEVE SIZE	PERCENT (%) PASSING
CRUSHED GRAVEL FOR SUBBASE VAOT SPEC 704.05A FINE	2"	100 %
	1 1/2"	90-100 %
	#4	30-60 %
	#100	0-12 %
DENSE GRADED CRUSHED STONE VAOT SPEC 704.06	#200	0-6 %
	3 1/2"	100 %
	3"	90-100%
	2"	75-100%
	1"	50-80%
	1/2"	30-60%
	#4	15-40%
	#200	0-6 %

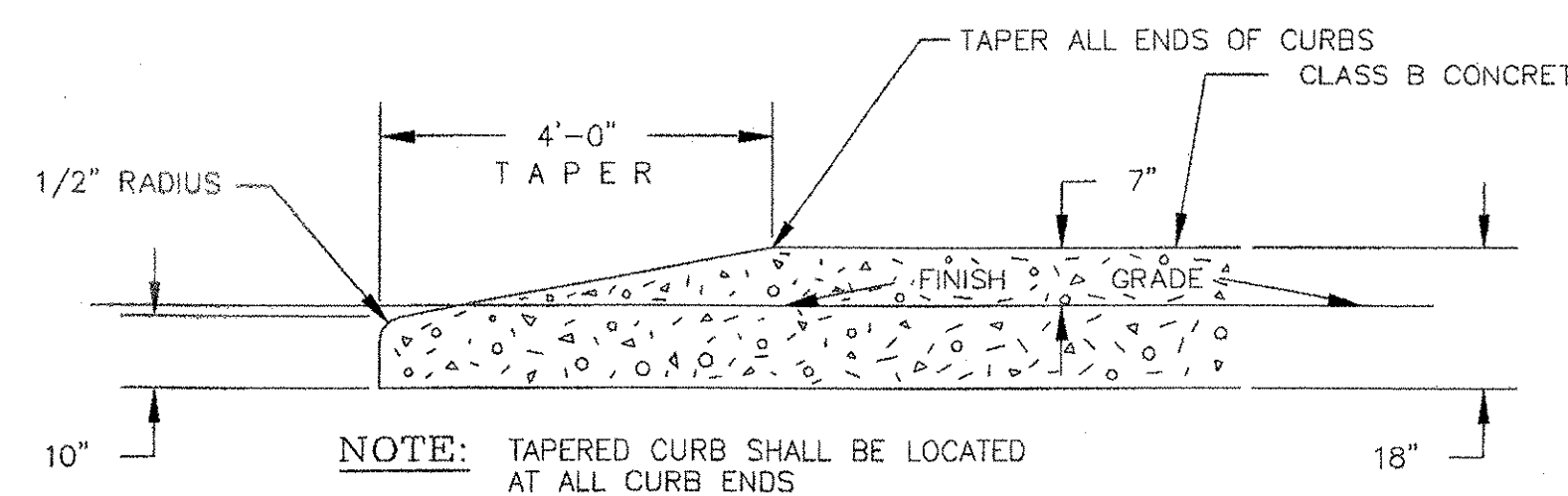


NOTES:

- CURBING SHALL BE CONSTRUCTED IN 10' SECTIONS WITH 1/8" JOINT BETWEEN SECTIONS.
- CURBING EXPANSION JOINTS SHALL BE CONSTRUCTED EVERY 20' AND SHALL BE CONSTRUCTED OF MATERIAL CONFORMING TO AASHTO DESIGNATION M-153 (1/2" SPONGE RUBBER OR CORK.)
- ALL CONCRETE SHALL BE AIR ENTRAINED NOT LESS THAN 5% AND NOT MORE THAN 7%
- SLIP FORMING SHALL NOT BE USED IN CURB CONSTRUCTION

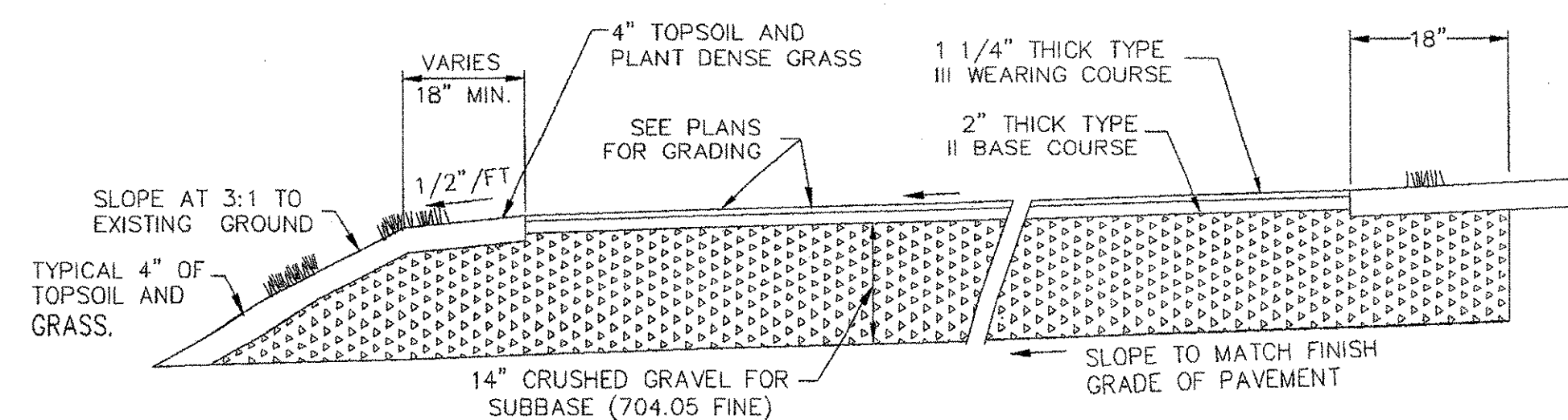
CONCRETE CURB

N.T.S.



TYPICAL TAPERED CURB

N.T.S.



DRIVE/PARKING AREA DETAIL

N.T.S.

10-20-05	REVISED FOR CONTRACT PLANS	BH/DH
REVISIONS		
THESE PLANS WITH LATEST REVISIONS SHOULD ONLY BE USED FOR THE PURPOSE SHOWN BELOW:		# OF SHEETS
STP WALK (9) TOWN OF ESSEX		proj. no. 02-132
ROUTE 15 IN ESSEX CENTER, VT		survey L&D/OTHERS
DETAILS & SPECIFICATIONS		design DLH/LAL
		drawn DB
		checked LAL/DLH
		date 01-24-03
		scale
		AS NOTED sht. no.
LAMOUREUX & DICKINSON Consulting Engineers, Inc. 14 Morse Drive Essex Junction, VT 05452 (802) 878-4450		3