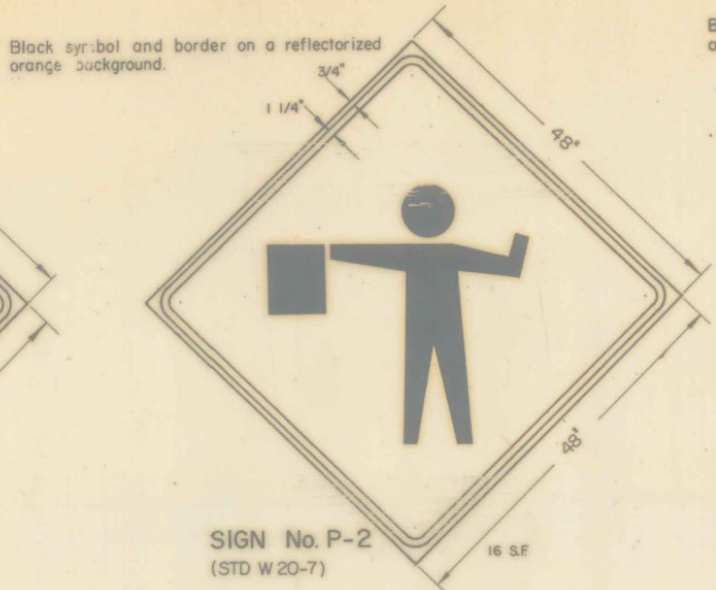
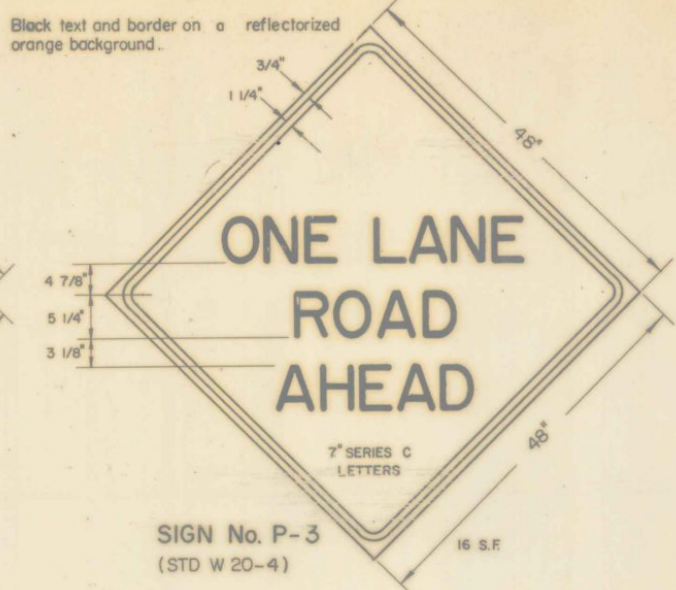


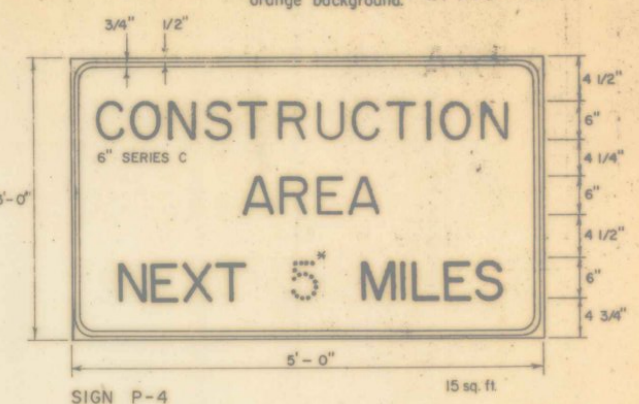
SIGN No. P-1
(STD W20-1)



SIGN No. P-2
(STD W20-7)



SIGN No. P-3
(STD W20-4)



SIGN P-4
(G 20-1)

Show mileage to nearest 1/4 mile.
Black text and border on reflectorized orange background.

TO BE MOUNTED ON A PORTABLE SUPPORT AND DISPLAYED IN ADVANCE OF A FLAGPERSON WHEN ON DUTY.

TO BE MOUNTED ON A PORTABLE SUPPORT AND DISPLAYED IN ADVANCE OF THE WORK AREA WHEN REQUIRED

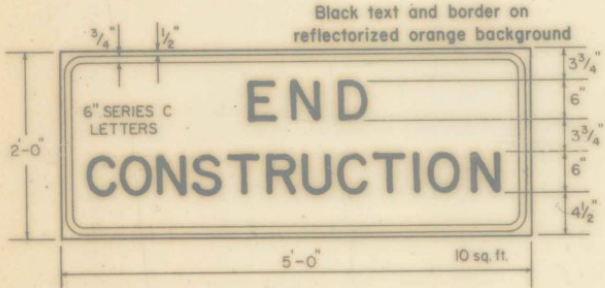
LOCATION

Construction paving signs shall be located as detailed on this sheet or ordered by the Engineer. They shall appear at each end of the highway under construction, and on all intersecting public highways. The exact placement of any sign will depend upon the alignment of the highway and the character of the road sides. The location measurements on this sheet are intended to indicate the sequence to be followed, and the minimum spacing to be observed by the Engineer in determining exact locations.

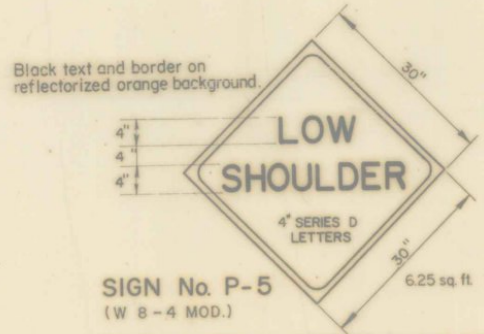
DESIGN

The designs of the signs shall conform with the details shown on this sheet and with the standards prescribed in the Manual on Uniform Traffic Control Devices prepared by National Joint Committee on Uniform Traffic Control Devices.

NOTES

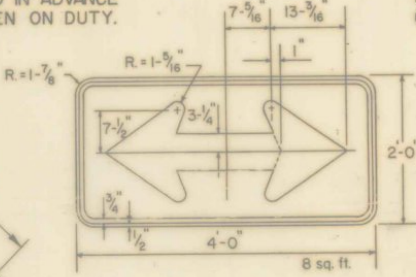


SIGN No. P-7 (STD. G20-2)



SIGN No. P-5
(W 8-4 MOD.)

TO BE LOCATED AS ORDERED BY THE ENGINEER ON POSTS SET SECURELY IN THE GROUND, NEAR EDGE OF SIGN TO BE AT LEAST 2 FT FROM EDGE OF PAVEMENT, WITH BOTTOM AT LEAST 3 FT ABOVE ROAD LEVEL.



SIGN No. P-6
(STD W1-7)

Black arrow and border on reflectorized orange background

MATERIALS

The signs shall be of metal, wood, plywood, hardboard or any other material satisfactory to the Engineer. No material will be approved that will deteriorate by exposure to the weather during the required life of the sign.

REFLECTORIZATION

All reflectorized material shall consist of encapsulated lens reflective sheeting.

INSTALLATION

The signs shall be in place at the time the project commences. Each sign shall be erected in a neat and workmanlike manner on wood or metal posts set securely in the ground, unless otherwise noted on this sheet. The bottom of a sign, unless otherwise noted shall be at least 5 feet above road level, and the nearest edge of a sign shall be at least 6 feet outside the shoulder point or 2 feet outside guard rail, curbing or sidewalk. Posts and signs shall be braced or reinforced in back as necessary. The installation of all signs shall be subject to the approval of the Engineer. In urban areas, the bottom of a sign shall be at least 7' above the sidewalk.

MAINTENANCE

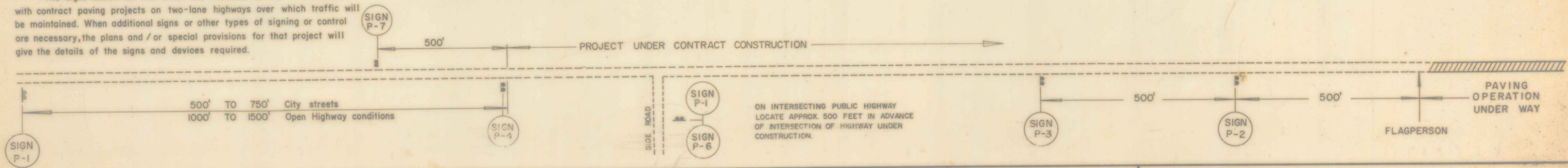
Signs shall be maintained in a clean and legible condition satisfactory to the Engineer. They shall be completely visible to approaching traffic at all times. They shall be kept plumb and level, and always present a neat appearance. Damaged, defaced or dirty signs shall be repaired, cleaned or replaced as ordered by the Engineer.

GENERAL

The cost of furnishing, erecting, maintaining and removing all construction approach signs shall be considered subsidiary work pertaining to the project as a whole and will be included in the contract unit price bid for various items involved in the contract. In all phases of construction of approach signing, the requirements set forth in the Manual on Uniform Traffic Control Devices shall be met (see Standard Specifications, Section 107, Article 107.09 Barricades, Warning and Detour Signs). When project is closed down for temporary periods the signs shall be covered in a workmanlike manner.

Signs mounted on barricades or temporary supports shall be positioned such that the bottom of the sign is not less than one foot above the pavement elevation.

The signs shown on this sheet are intended for use in connection with contract paving projects on two-lane highways over which traffic will be maintained. When additional signs or other types of signing or control are necessary, the plans and / or special provisions for that project will give the details of the signs and devices required.



REVISIONS AND CORRECTIONS
 SEPT. 11, 1973 - REVISED PER ORDER OF FHWA, SEPT. 11, 1973
 NOV. 6, 1973 - REVISED PER ORDER OF FHWA
 MAY 14, 1974 - REFLECTIVE MATERIAL CHANGE
 JUNE 7, 1977 - REFLECTIVE MATERIAL NOTE CHANGED.
 JUNE 7, 1977 - SIGNS REFERENCED TO NUMBERS IN M.U.T.C.D.
 MAR. 22, 1978 - SIGNS REVISED PER DIRECTION FROM F.H.W.A.
 APR. 20, 1978 - FLAGPERSON SIGN CHANGED TO SYMBOL.
 DEC. 18, 1979 - SIGN P-4 REVISED, SIGN P-6 AND P-7 ADDED.

APPROVED
 Dec 14, 1977
 DATE
 R. H. Arnold
 CHIEF ENGINEER
 E. W. Stickney
 ASST. CHIEF ENGINEER
 G. M. Lane
 HIGHWAY ENGINEER

TRAFFIC SIGNS
 ROAD CONSTRUCTION SIGNS
 -PAVING-