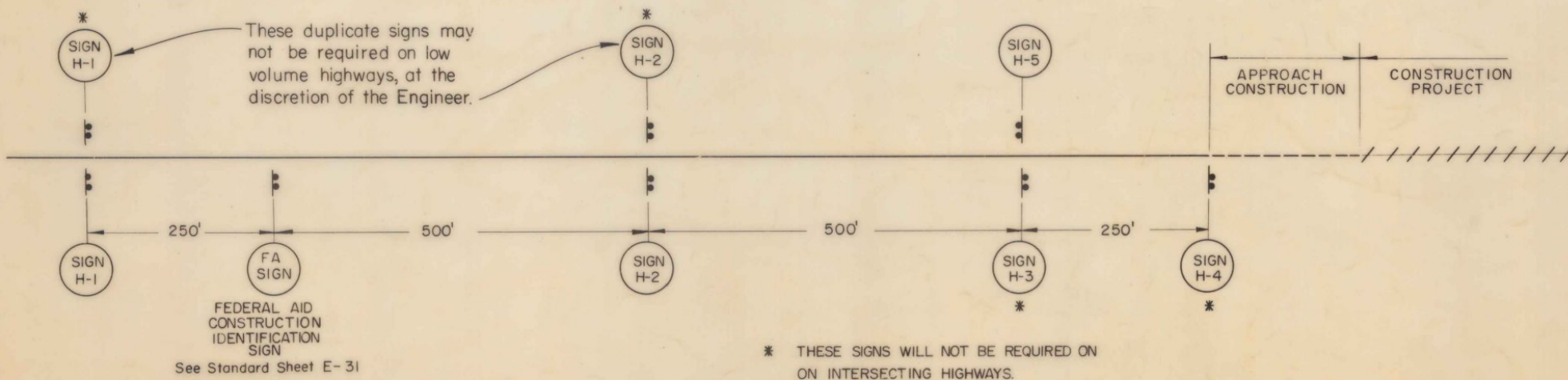


The road construction approach signs shown on this sheet are intended for use in providing advance warning and information on construction projects over which traffic will be maintained. When additional approach signs or other types of advance signing or control are necessary, the Plans and/or the Specifications for that project will give the details of the signs and devices required.



- NOTES**
- LOCATION**
Construction approach signs shall be located as detailed on this sheet or otherwise shown on the Plans. They shall appear at each end of the highway under construction, and on all intersecting public highways. The exact placement of any sign will depend on upon the alignment of the highway and the character of the roadsides. The location measurements on this sheet are intended to indicate the sequence to be followed, and the minimum spacing to be observed by the Engineer in determining exact locations.
- DESIGN**
The designs of the signs shall conform with the details shown on this sheet and with the standards prescribed in the Manual on Uniform Traffic Control Devices prepared by National Joint Committee on Uniform Traffic Control Devices.
- MATERIALS**
The signs shall be of metal, wood, plywood, hardboard or any other material satisfactory to the Engineer. No material shall be approved that will deteriorate by exposure to the weather during the required life of the sign.
- REFLECTORIZATION**
For reflectorizing sign backgrounds, either reflective sheeting or the direct process of glass beads on paint will be acceptable.
- ILLUMINATION**
If desired, by the contractor, and approved by the Engineer, a sign may be illuminated instead of reflectorized. The illumination may be provided by incandescent or fluorescent lamps, or by spotlights. Lamps shall be properly shielded to protect drivers from glare. Torches, lanterns or existing street lighting are not acceptable for sign illumination. If the Engineer considers that a reflectorized sign is not adequate, he may order that it be illuminated.
- INSTALLATION**
The signs shall be in place at the time the project officially commences. Each sign shall be erected in a neat and workmanlike manner on wood or metal posts set securely in the ground. The bottom of a sign shall be at least 5 feet above road level, and the nearest edge of a sign shall be at least 6 feet outside the shoulder point or 2 feet outside guard rail, curbing or sidewalk. Posts and signs shall be braced or reinforced in back as necessary. The installation of signs shall be subject to approval of the Engineer.
- MAINTENANCE**
Signs shall be maintained in a clean and legible condition satisfactory to the Engineer. They shall be completely visible to approaching traffic at all times. They shall be kept plumb and level, and always present a neat appearance. Damaged, defaced or dirty signs shall be repaired, cleaned or replaced as ordered by the Engineer.
- GENERAL**
The cost of furnishing, erecting, maintaining, and removing all construction approach signs shall be born by the contractor without additional compensation. (See Standard Specifications, Division 1, Article 7.07, "Public Convenience and Safety.")
In all phases of construction approach signing, the requirements set forth in the Manual on Uniform Traffic Control Devices shall be met. (See Standard Specifications, Division 1, Section 7, Article 7.08, "Barricades, Warning and Detour Signs.")

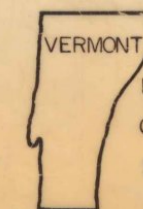
REVISIONS AND CORRECTIONS
May 6, 1971 Colors revised to conform with the Manual on Uniform Traffic Control Devices, 1971 edition.

APPROVED:
JAN 6, 1967
DATE

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VERMONT
DEPARTMENT
OF HIGHWAYS
STANDARD

E-32