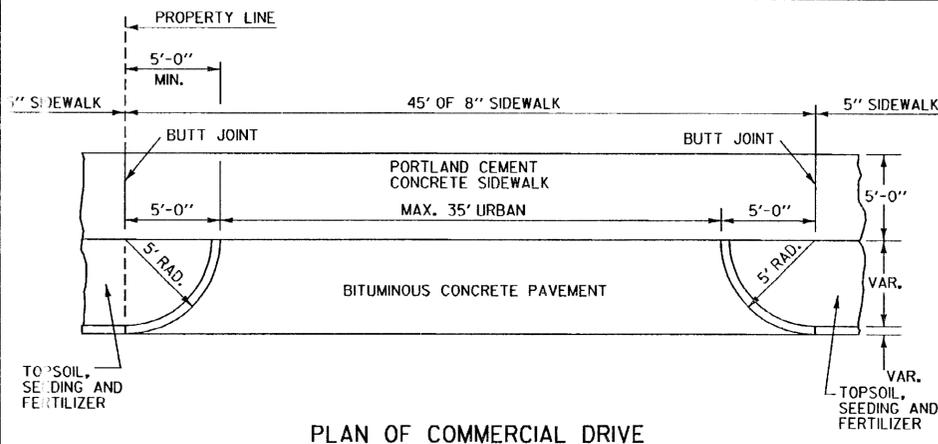
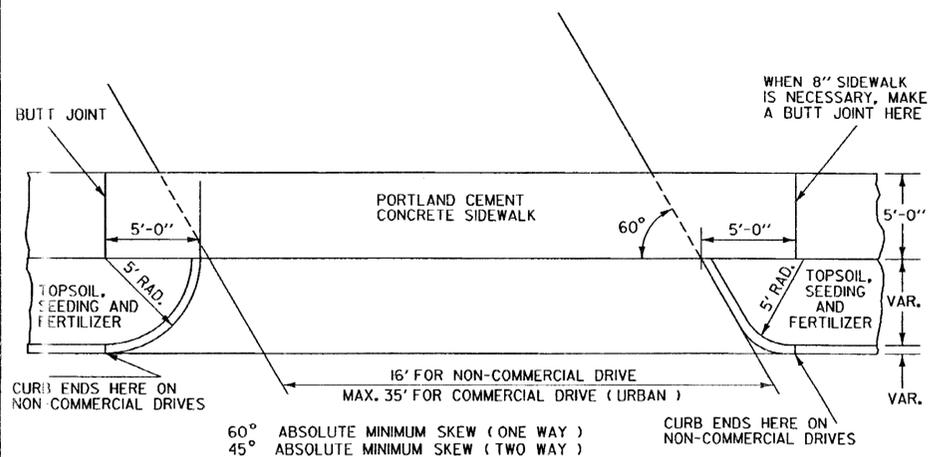


PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH (WITH CONCRETE CURB)



PLAN OF COMMERCIAL DRIVE



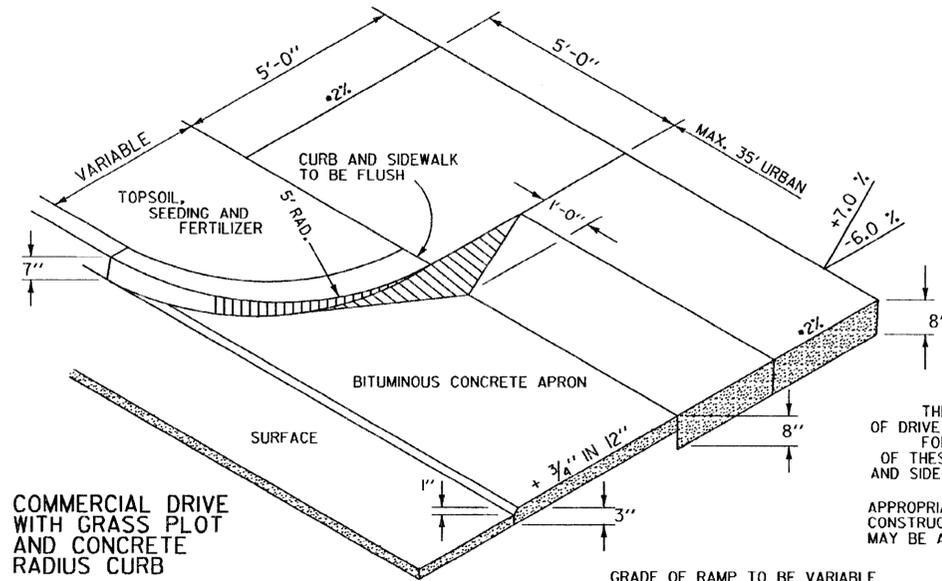
PLAN OF SKEW DRIVE

REVISIONS AND CORRECTIONS
 DEC. 14, 1971 - ORIGINAL APPROVAL DATE
 OCT. 25, 1985 - REVISED TO CONFORM TO 1986 SPECIFICATIONS
 JUNE 1, 1994 - REISSUED, WITHOUT CHANGE, UNDER NEW SIGNATURES.

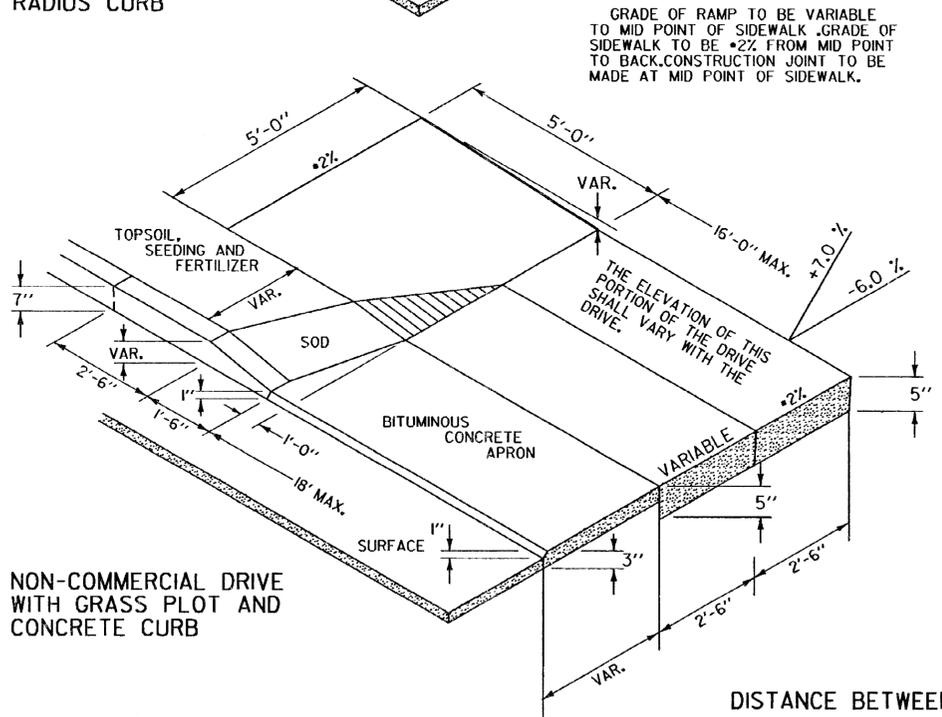
APPROVED

APPROVED FOR THIS PROJECT AND/OR DESIGN IMPLEMENTATION, FHWA FINAL APPROVAL PENDING.

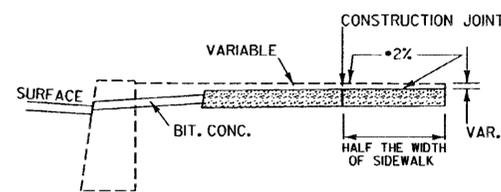
Stephen D. MacArthur, P.E.
 DIRECTOR OF ENGINEERING
John M. Murphy, P.E.
 DESIGN ENGINEER



COMMERCIAL DRIVE WITH GRASS PLOT AND CONCRETE RADIUS CURB

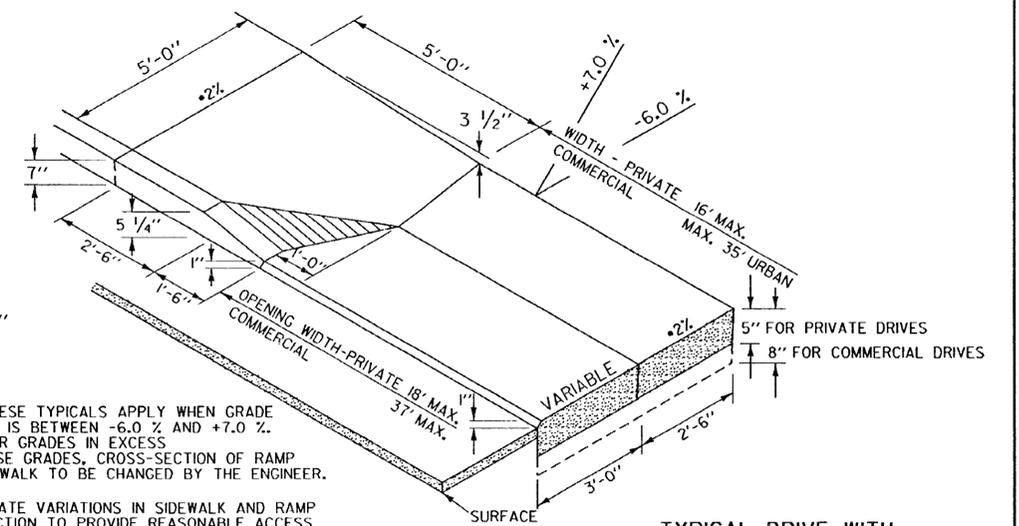


NON-COMMERCIAL DRIVE WITH GRASS PLOT AND CONCRETE CURB

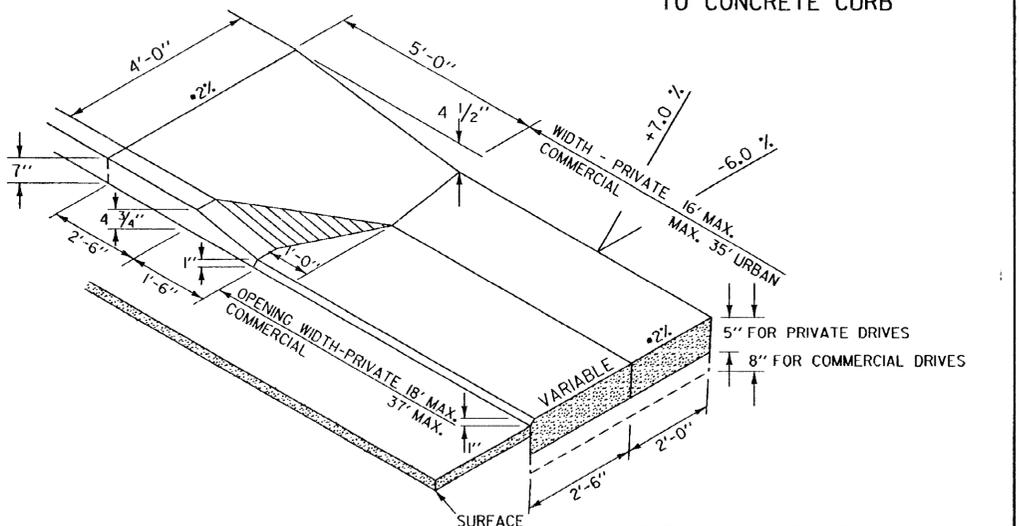


DISTANCE BETWEEN DOUBLE DRIVEWAYS 10 FEET MINIMUM (URBAN)

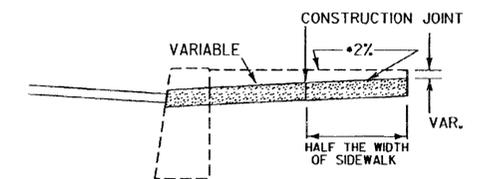
RAMP TO BE PAID FOR AS PORTLAND CEMENT CONCRETE SIDEWALK
 • NO WHERE SHALL THE CROSS SLOPE OF AN ACCESSIBLE ROUTE EXCEED 1:50.



TYPICAL DRIVE WITH 5 FT. SIDEWALK ADJACENT TO CONCRETE CURB



TYPICAL DRIVE WITH 4 FT. SIDEWALK ADJACENT TO CONCRETE CURB



PORTLAND CEMENT CONCRETE SIDEWALK
 DRIVE ENTRANCES WITH
 PRECAST REINFORCED CONCRETE CURB OR
 CAST-IN-PLACE CONCRETE CURB



STANDARD
 C-2A