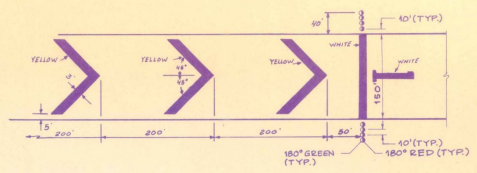
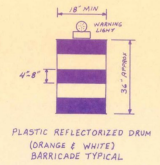


Closed Runway Marker  
N.T.S.

- MARKER NOTES**
1. CLOSED RUNWAY MARKER TO BE IN PLACE DURING CONSTRUCTION PERIOD AS DIRECTED BY ENGINEER.
  2. CLOSED RUNWAY MARKER MAY BE EITHER SHOWN FENCE OR PLYWOOD PAINTED YELLOW & SECURELY FASTENED IN PLACE.
  3. COST OF PLACING & MAINTAINING THE CLOSED RUNWAY MARKER IS CONSIDERED NECESSARY & INCIDENTAL TO CONSTRUCTION & IS NOT A SEPARATE PAY ITEM.
  4. CLOSED RUNWAY MARKER TO BE PLACED OVER EXISTING RUNWAY 1 THRESHOLD DURING RUNWAY 1 RELOCATION.



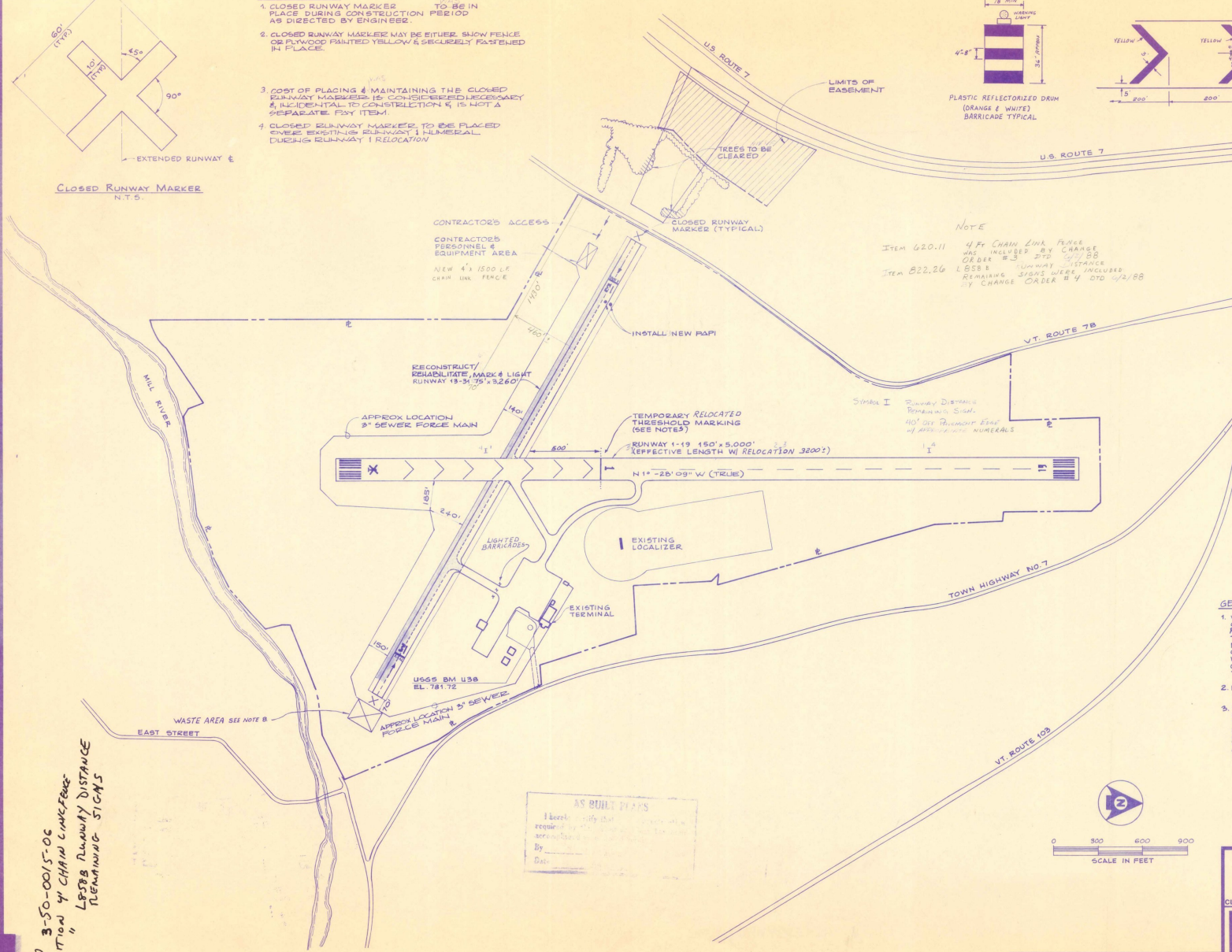
TEMPORARY RELOCATED THRESHOLD DETAIL  
N.T.S.

- NOTES:**
1. EIGHT TEMPORARY THRESHOLD LIGHTS ARE TO BE INSTALLED AS SHOWN.
  2. EACH LIGHT TO BE BATTERY OPERATED, STEADY BURNING W/ 180° RED/180° GREEN LENS AS MANUF. BY WHELAN ENGINEERING (DEEP RIVER, CT.) OR EQUAL.
  3. THE CONTRACTOR SHALL FURNISH ALL BATTERIES TO BE USED DURING THE CONSTRUCTION PERIOD.
  4. THE COST OF MAINTENANCE OF THE TEMPORARY LIGHTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  5. THE MAINTENANCE DIVISION OF THE VADT SHALL RETAIN POSSESSION OF THE LIGHTS AFTER COMPLETION OF CONSTRUCTION.
  6. THE COST OF THE TEMPORARY LIGHTS & MEDIUM INTENSITY THRESHOLD LIGHTS, CONFORMING TO FAA SPEC L-861E IN LIEU OF BATTERY OPERATED LIGHTS.
  7. CONTRACTOR MAY AT HIS OPTION, INSTALL 8 MEDIUM INTENSITY THRESHOLD LIGHTS, CONFORMING TO FAA SPEC L-861E IN LIEU OF BATTERY OPERATED LIGHTS.
  8. WASTE AREA TO BE GRADED TOPSOILED, SEED & MULCHED AS ORDERED BY THE ENGINEER. THE USE OF THIS WASTE AREA WILL REQUIRE THAT A 24" Ø PIPE (0.75 THICKNESS) ITEM 601.1855 BE PLACED IN THE EXISTING DRAINAGE SWALE AS ORDERED BY THE ENGINEER.
  9. ALL MARKINGS IN THE RELOCATED THRESHOLD AREA ARE YELLOW EXCEPT THE THRESHOLD BAR WHICH IS WHITE.

**NOTE**

ITEM 620.11 4 FT CHAIN LINK FENCE WAS INCLUDED BY CHANGE ORDER #3 DTD 1/11/88

ITEM 822.26 1.858 B RUNWAY DISTANCE REMAINING SIGNS WERE INCLUDED BY CHANGE ORDER #4 DTD 1/11/88

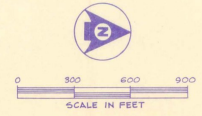


AIP 3-50-0015-06  
ADDITION 4' CHAIN LINK FENCE  
&  
1.858 B RUNWAY DISTANCE  
REMAINING SIGNS

AS BUILT PLANS

I hereby certify that these plans were prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer in the State of Vermont.

By: \_\_\_\_\_  
Date: \_\_\_\_\_



- GENERAL NOTES:**
1. WHEN WORKING WITHIN 200' OF RUNWAY 1-19 CENTERLINE, THE RUNWAY 1 THRESHOLD MUST BE RELOCATED. CONTRACTOR WILL ONLY BE ALLOWED TO REPLACE THRESHOLD FOR 7 DAYS (MAXIMUM) TO MINIMIZE THE IMPACT TO SCHEDULED SERVICE. THE CONTRACTOR WILL COORDINATE THIS RELOCATION WITH THE AIRPORT MANAGER AND INSPECTOR. THE CONTRACTOR MAY RELOCATE THE THRESHOLD ONE TIME DURING THIS CONTRACT.
  2. NO EQUIPMENT SHALL BE PARKED WITHIN 250' OF R/W 1-19 DURING NON-WORKING HOURS.
  3. THE CONTRACTOR WILL NOT BE ALLOWED TO MOVE EQUIPMENT ACCESS DURING 1-19 WITHOUT THE CONTRACTOR WHO HAS TO PROVIDE A FLAGMAN, EQUIPPED WITH A RADIO CAPABLE OF COMMUNICATING WITH HIGHWAY 10B WHICH COM FREQUENCY OF 122.8 MHZ.

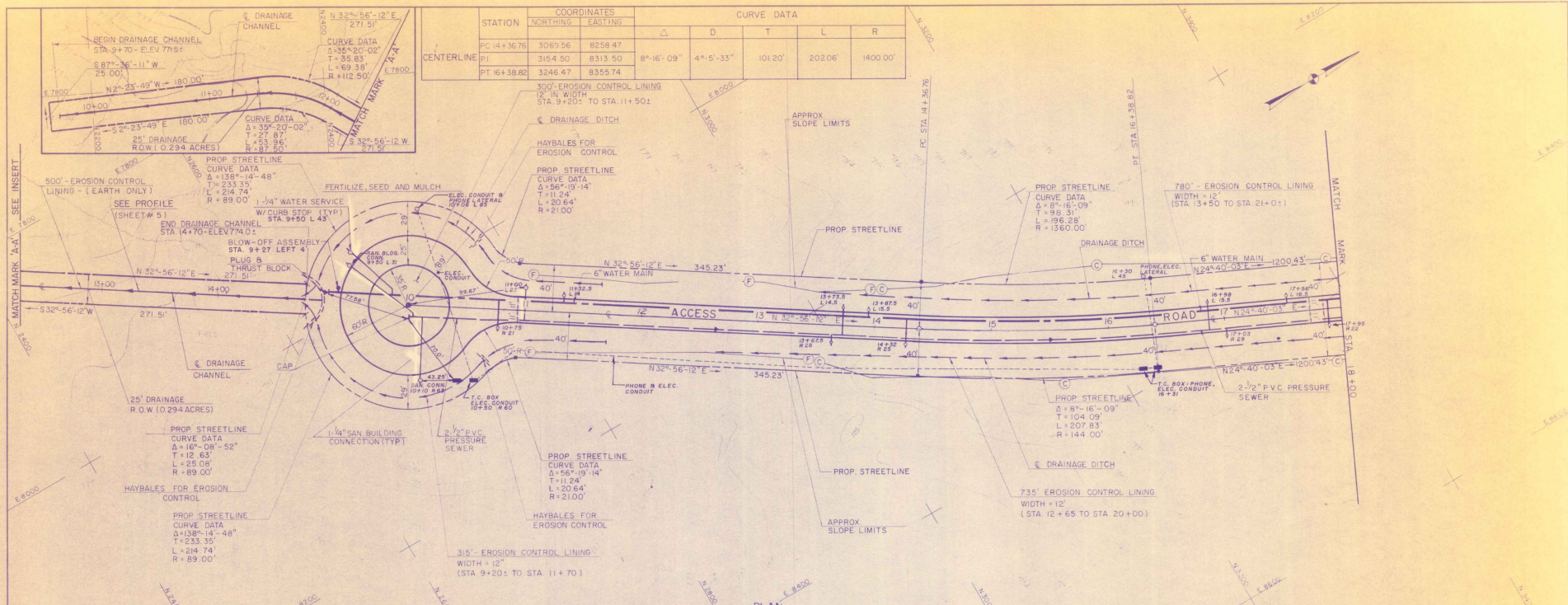
A.I.P. PROJECT NO. 3-50-0015-06

OUTLAND STATE AIRPORT  
SITE PLAN

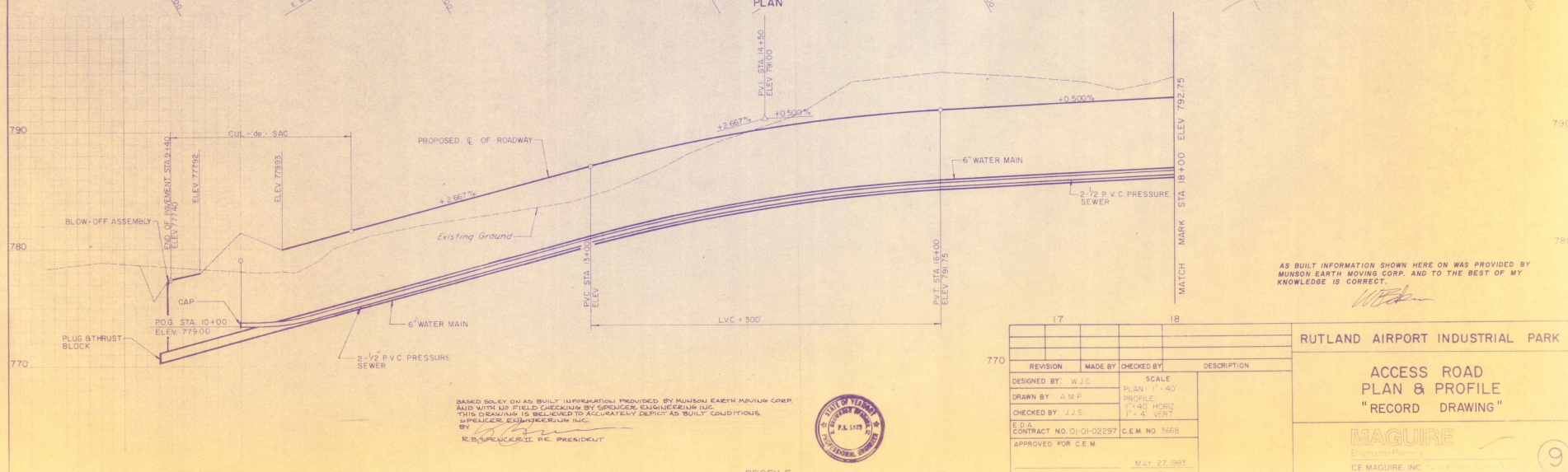
	Client No. 30-0162	
	Prof. Manager G.W. D'AMICO Prof. Designer E.S. DAWES Drawn By C.W. MILLS Checked By G.W. D'AMICO Scale AS SHOWN Approved _____ Date _____	
CLARENDON, VERMONT Dufresne-Henry Inc.	Sheet 2 of 30 D	

Rev.	Description	By	Date





STATION	COORDINATES		CURVE DATA					
	NORTHING	EASTING	Δ	D	T	L	R	
PC 14+36.76	3069.56	8258.47						
PI	3154.50	8313.50	8°-16'-09"	4°-5'-33"	101.20'	202.06'	1400.00'	
PT 16+38.80	3246.47	8355.74						



BASED SOLELY ON AS-BUILT INFORMATION PROVIDED BY MUNSON EARTH MOVING CORP. MADE WITH NO FIELD CHECKS BY SPECIFICER ENGINEERING INC. THIS DRAWING IS BELIEVED TO ACCURATELY DEPICT AS-BUILT CONDITIONS. SPECIFICER ENGINEERING INC. BY: R.D. SPECICER II, P.E. PRESIDENT



REVISION	MADE BY	CHECKED BY	DESCRIPTION

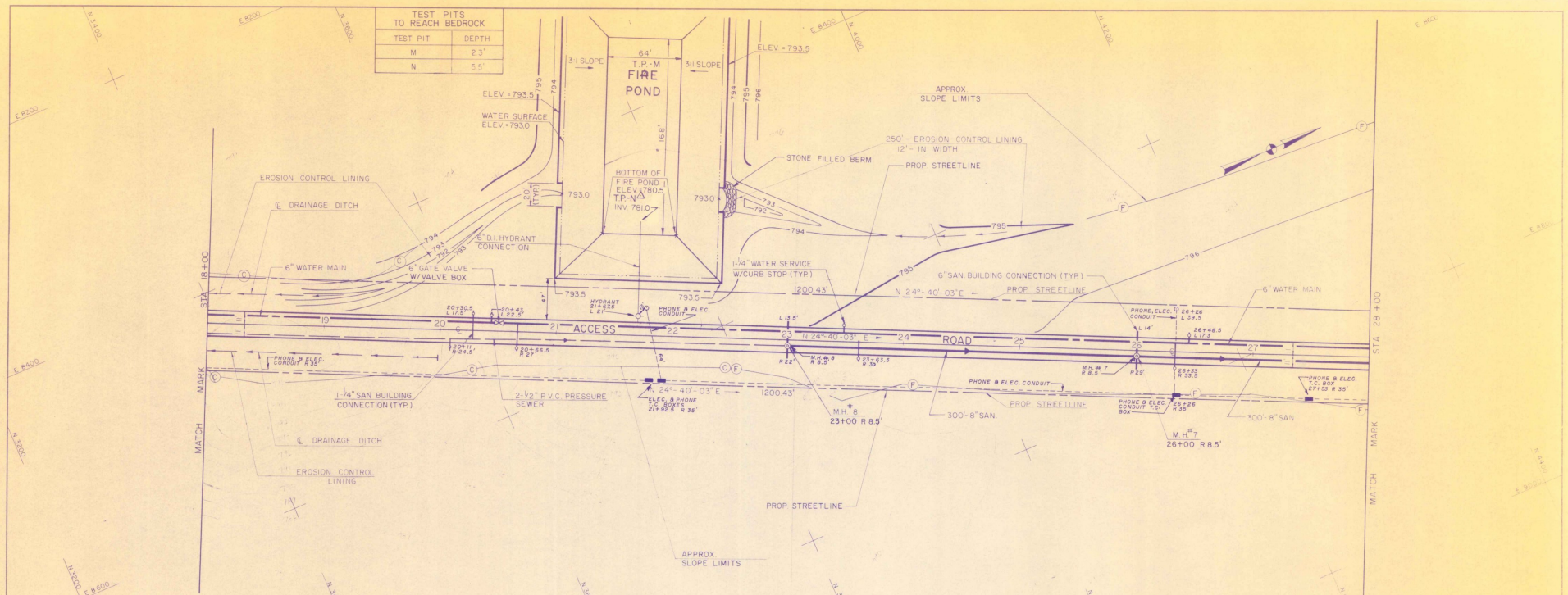
DESIGNED BY: W.J.C. SCALE: PLAN 1"=40'  
 DRAWN BY: A.M.P. PROFILE 1"=4' VERT  
 CHECKED BY: J.J.S.  
 E.O. CONTRACT NO. D1-01-02297 C.E.M. NO. 3668  
 APPROVED FOR C.E.M. MAY 27, 1995

RUTLAND AIRPORT INDUSTRIAL PARK

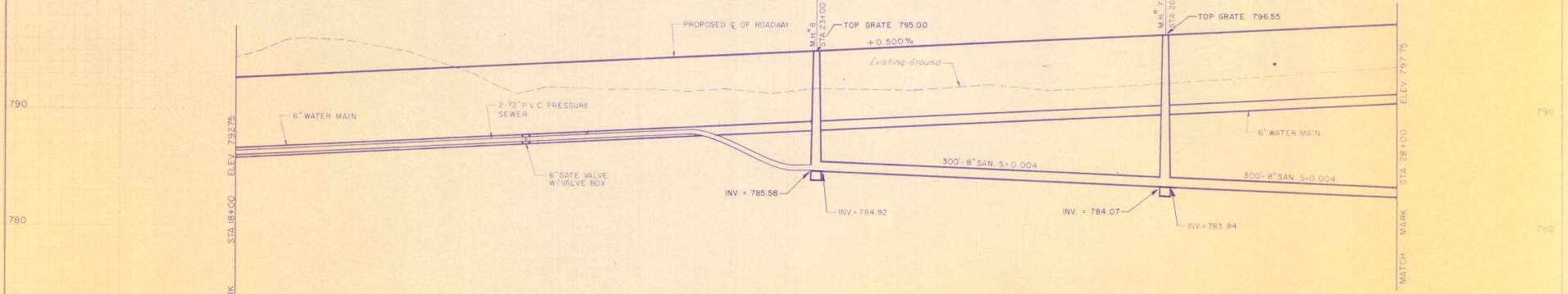
ACCESS ROAD  
 PLAN & PROFILE  
 "RECORD" DRAWING

MAGUIRE  
 ENGINEERS  
 CE MAGUIRE, INC.

TEST PITS TO REACH BEDROCK	
TEST PIT	DEPTH
M	2.3'
N	5.5'



PLAN



PROFILE

BASED SOLELY ON AS-BUILT INFORMATION PROVIDED BY MUNSON EARTH MOVING CORP AND WITH NO FIELD CHECKS BY SUPERVISOR/ENGINEER/INCH. THIS DRAWING IS BELIEVED TO ACCURATELY DEPICT AS-BUILT CONDITIONS. BY: *[Signature]* R.O. SUPERVISOR/II P.E. PRESIDENT



AS BUILT INFORMATION SHOWN HERE ON WAS PROVIDED BY MUNSON EARTH MOVING CORP. AND TO THE BEST OF MY KNOWLEDGE IS CORRECT. *[Signature]*

REVISION	MADE BY	CHECKED BY	DESCRIPTION

DESIGNED BY: W.J.C. SCALE: PLAN 1"=40'  
 DRAWN BY: A.M.P. PROFILE 1"=40' HORIZ  
 CHECKED BY: J.J.S. 1"=4" VERT  
 E.O.A. CONTRACT NO. 01-01-02297 C.E.M. NO. 3668  
 APPROVED FOR C.E.M. DATE: MAY 27, 1993

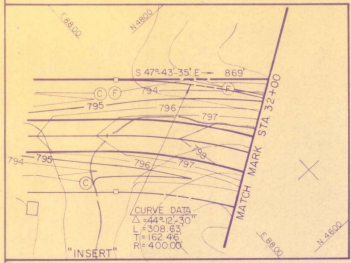
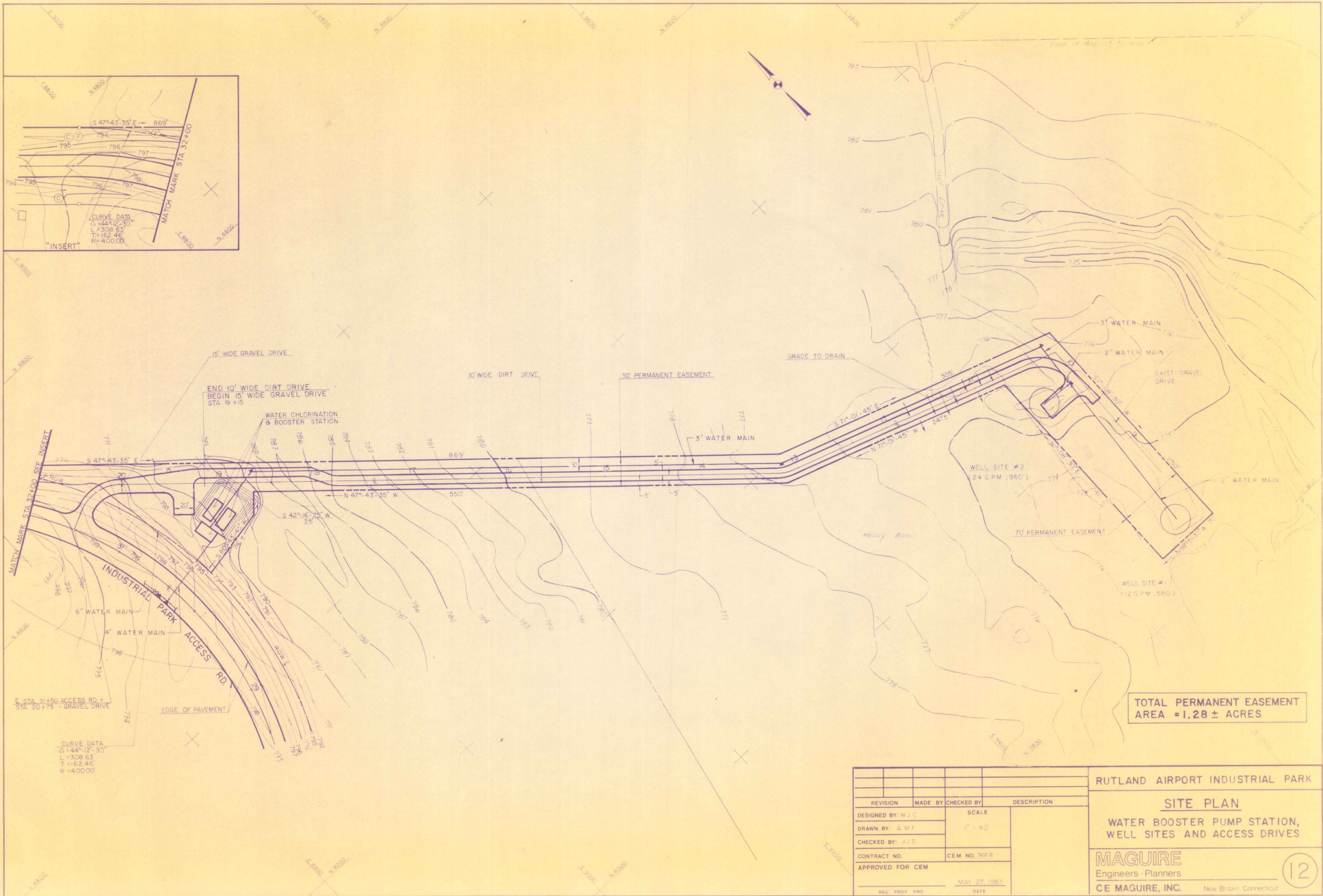
RUTLAND AIRPORT INDUSTRIAL PARK

ACCESS ROAD  
 PLAN & PROFILE  
 "RECORD DRAWING"

MAGUIRE  
 Engineers & Planners  
 CE MAGUIRE INC. 7600 B. Rd. Colchester, VT 05445

10





END 10' WIDE DIRT DRIVE  
BEGIN 15' WIDE GRAVEL DRIVE  
STA 19+15

WATER CHLORINATION  
& BOOSTER STATION

INDUSTRIAL PARK  
ACCESS RD.

STA 20+75 - GRAVEL DRIVE

CURVE DATA  
Δ=447.22, L=307.35,  
T=108.63,  
R=400.00

TOTAL PERMANENT EASEMENT  
AREA = 1.28 ± ACRES

REVISION	MADE BY	CHECKED BY	DESCRIPTION
DESIGNED BY: WJC	SCALE		1" = 40'
DRAWN BY: A.M.F.	CONTRACT NO.		
CHECKED BY: J.J.S.	CEM NO. 366B		
APPROVED FOR CEM		DATE	MAY 27, 1983
REG. PROF. ENG.	DATE		

RUTLAND AIRPORT INDUSTRIAL PARK

**SITE PLAN**

WATER BOOSTER PUMP STATION,  
WELL SITES AND ACCESS DRIVES

**MAGUIRE**  
Engineers - Planners

CE MAGUIRE, INC. New Britain, Connecticut