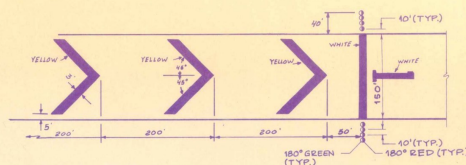
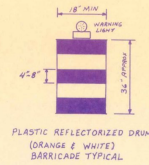


Closed Runway Marker
N.T.S.

MARKER NOTES

1. CLOSED RUNWAY MARKER TO BE IN PLACE DURING CONSTRUCTION PERIOD AS DIRECTED BY ENGINEER.
2. CLOSED RUNWAY MARKER MAY BE EITHER SHOWN FENCE OR PLYWOOD PAINTED YELLOW & SECURELY FASTENED IN PLACE.
3. COST OF PLACING & MAINTAINING THE CLOSED RUNWAY MARKER IS CONSIDERED NECESSARY & INCIDENTAL TO CONSTRUCTION & IS NOT A SEPARATE PAY ITEM.
4. CLOSED RUNWAY MARKER TO BE PLACED OVER EXISTING RUNWAY 1 THRESHOLD DURING RUNWAY 1 RELOCATION.



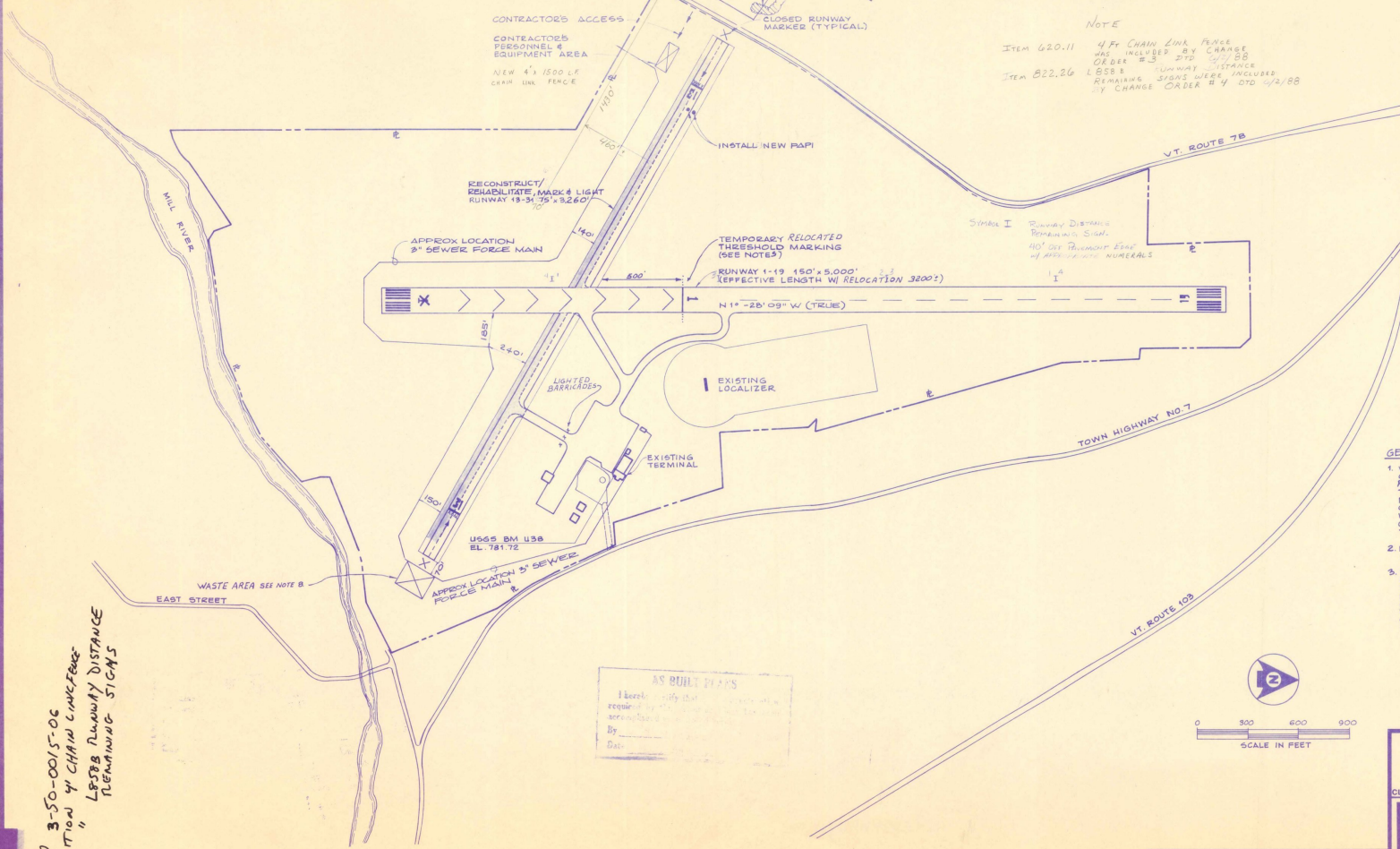
TEMPORARY RELOCATED THRESHOLD DETAIL
N.T.S.

- NOTES:**
1. EIGHT TEMPORARY THRESHOLD LIGHTS ARE TO BE INSTALLED AS SHOWN.
 2. EACH LIGHT TO BE BATTERY OPERATED, STEADY BURNING W/ 180° RED/180° SEEN LENS AS MANUF. BY WHELAN ENGINEERING (DEEP RIVER, CT.) OR EQUAL.
 3. THE CONTRACTOR SHALL FURNISH ALL BATTERIES TO BE USED DURING THE CONSTRUCTION PERIOD.
 4. THE COST OF MAINTENANCE OF THE TEMPORARY LIGHTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 5. THE MAINTENANCE DIVISION OF THE VADT SHALL OBTAIN POSSESSION OF THE LIGHTS AFTER COMPLETION OF CONSTRUCTION.
 6. THE COST OF THE TEMPORARY LIGHTS & MEDIUM INTENSITY THRESHOLD LIGHTS, CONFORMING TO FAA SPEC L-861E IN LIEU OF BATTERY OPERATED LIGHTS.
 7. CONTRACTOR MAY AT HIS OPTION, INSTALL 8 MEDIUM INTENSITY THRESHOLD LIGHTS, CONFORMING TO FAA SPEC L-861E IN LIEU OF BATTERY OPERATED LIGHTS.
 8. WASTE AREA TO BE GRADED TOPSOILED, SEED & MULCHED AS ORDERED BY THE ENGINEER. THE USE OF THIS WASTE AREA WILL REQUIRE THAT A 24" Ø PIPE (0.75 THICKNESS) ITEM 601.1855 BE PLACED IN THE EXISTING DRAINAGE SWALE AS ORDERED BY THE ENGINEER.
 9. ALL MARKINGS IN THE RELOCATED THRESHOLD AREA ARE YELLOW EXCEPT THE THRESHOLD BAR WHICH IS WHITE.

NOTE

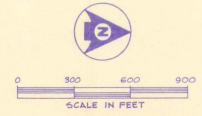
ITEM 620.11 4 FT CHAIN LINK FENCE WAS INCLUDED BY CHANGE ORDER #3 DTD 1/11/88

ITEM 822.26 1.858 B RUNWAY DISTANCE REMAINING SIGNS WERE INCLUDED BY CHANGE ORDER #4 DTD 1/11/88



AIP 3-50-0015-06
ADDITION OF CHAIN LINK FENCE
&
LESB B RUNWAY DISTANCE
REMAINING SIGNS

- GENERAL NOTES:**
1. WHEN WORKING WITHIN 200' OF RUNWAY 1-19 CENTERLINE, THE RUNWAY 1 THRESHOLD MUST BE RELOCATED. CONTRACTOR WILL ONLY BE ALLOWED TO REPLACE THRESHOLD FOR 7 DAYS (MAXIMUM) TO MINIMIZE THE IMPACT TO SCHEDULED SERVICE. THE CONTRACTOR WILL COORDINATE THIS RELOCATION WITH THE AIRPORT MANAGER AND INSPECTOR. THE CONTRACTOR MAY RELOCATE THE THRESHOLD ONE TIME DURING THIS CONTRACT.
 2. NO EQUIPMENT SHALL BE PARKED WITHIN 250' OF R/W 1-19 DURING NON-WORKING HOURS.
 3. THE CONTRACTOR WILL NOT BE ALLOWED TO MOVE EQUIPMENT ACCESS BARRIERS IN OR WITHOUT THE REMISSION OF THE ENGINEER. IN ORDER TO MOVE EQUIPMENT ACROSS THIS RUNWAY THE CONTRACTOR WILL HAVE TO PROVIDE A FLAGMAN, EQUIPPED WITH A RADIO CAPABLE OF COMMUNICATING WITH HIGHWAY 108 WHICH COM FREQUENCY OF 122.8 MHZ.



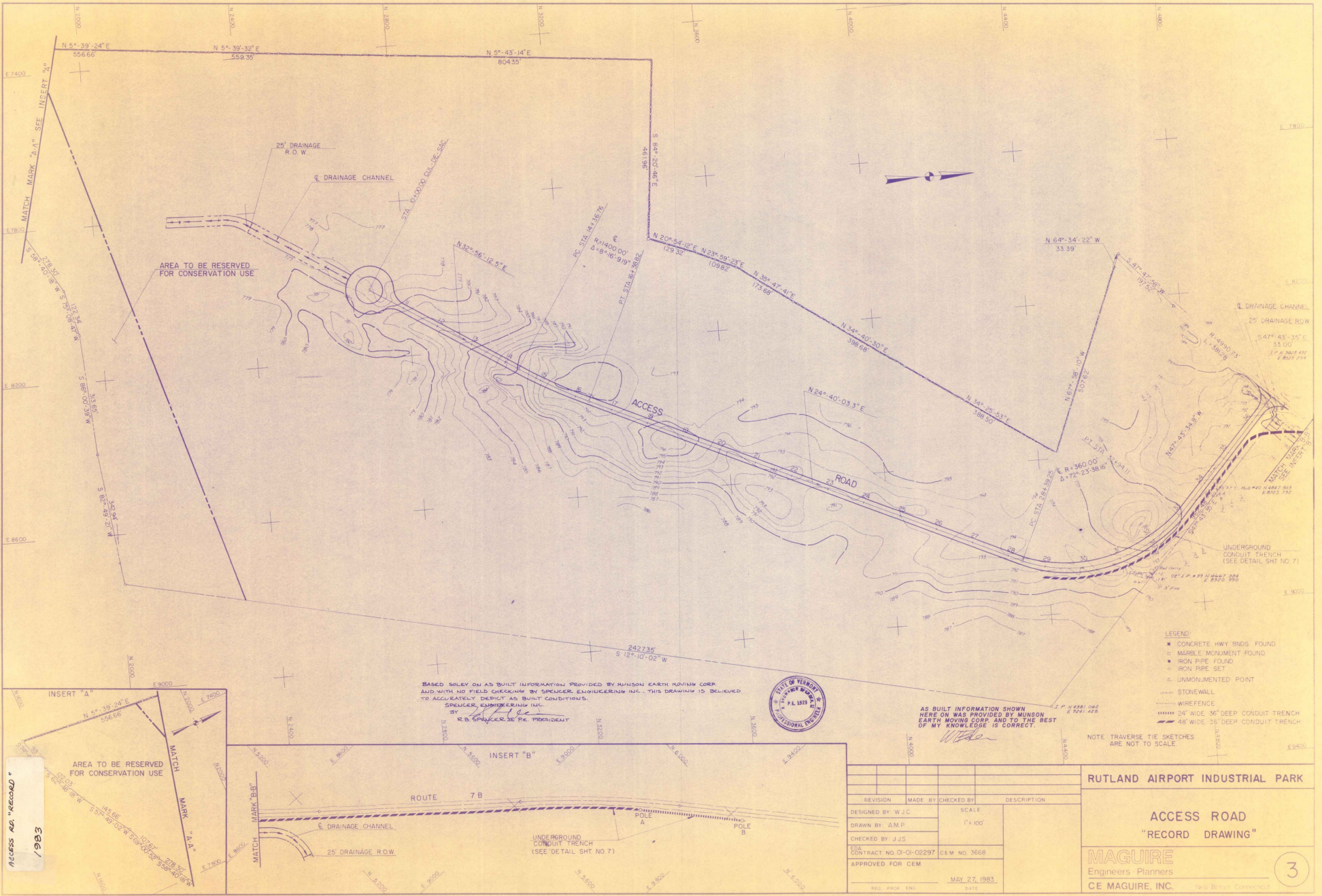
A.I.P. PROJECT NO. 3-50-0015-06

OUTLAND STATE AIRPORT

SITE PLAN

<p>Dufresne-Henry Inc.</p>	Client No. 30-0162	
	Prof. Manager G.W. D'AMICO Prof. Designer E.S. DAWES Drawn By C.W. MILLS Checked By G.W. D'AMICO Scale AS SHOWN Approved _____ Date _____	
CLARENDON, VERMONT SHEET 2 OF 30 DATE _____		

Rev.	Description	By	Date



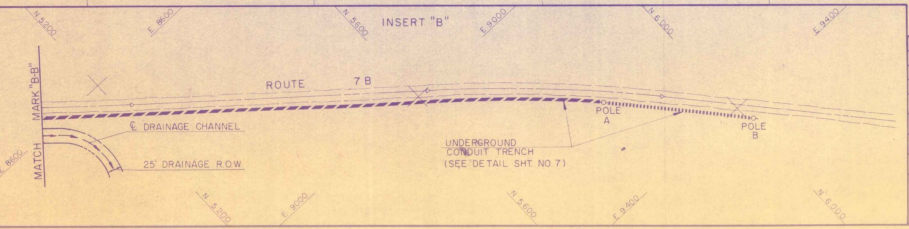
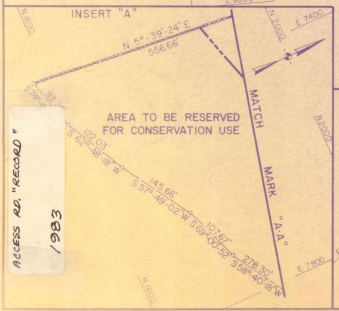
BASED SOLELY ON AS BUILT INFORMATION PROVIDED BY MUNSON EARTH MOVING CORP. AND WITH NO FIELD CHECKING BY SPENCER ENGINEERING INC., THIS DRAWING IS BELIEVED TO ACCURATELY DEPICT AS BUILT CONDITIONS.
 SPENCER ENGINEERING INC.
 BY *[Signature]*
 R.B. SPENCER, P.E., PRESIDENT



AS BUILT INFORMATION SHOWN HEREON WAS PROVIDED BY MUNSON EARTH MOVING CORP. AND TO THE BEST OF MY KNOWLEDGE IS CORRECT.
[Signature]

- LEGEND:
- CONCRETE HWY BNDS FOUND
 - MARBLE MONUMENT FOUND
 - IRON PIPE FOUND
 - IRON PIPE SET
 - UNMONUMENTED POINT
 - STONEWALL
 - WIREFENCE
 - ==== 24' WIDE 36' DEEP CONDUIT TRENCH
 - ==== 48' WIDE 36' DEEP CONDUIT TRENCH

NOTE TRAVERSE TIE SKETCHES ARE NOT TO SCALE.



REVISION	MADE BY	CHECKED BY	DESCRIPTION

DESIGNED BY: W.J.C. SCALE: 1" = 100'
 DRAWN BY: A.M.P.
 CHECKED BY: J.J.S.
 CONTRACT NO. OI-OI-02297 CEM NO. 3668
 APPROVED FOR CEM: *[Signature]*
 REG. PROJ. ENG. MAY 27, 1983 5416

RUTLAND AIRPORT INDUSTRIAL PARK

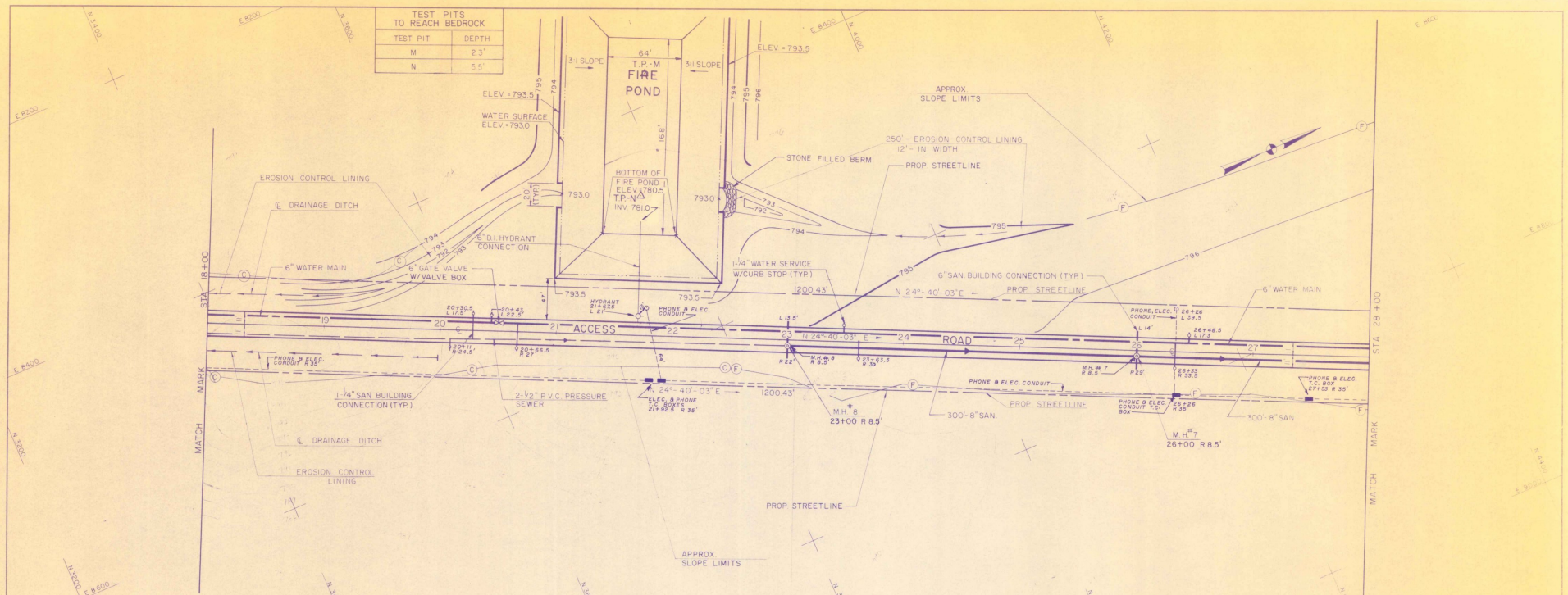
ACCESS ROAD
"RECORD DRAWING"

MAGUIRE
 Engineers - Planners
 CE MAGUIRE, INC. New Britain, Connecticut

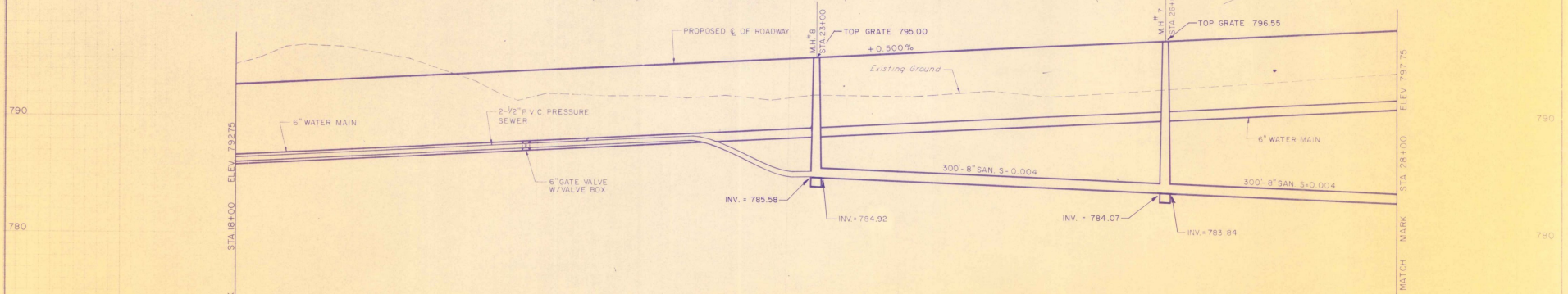
3

ACCESS AS "RECORD" / 1983

TEST PITS TO REACH BEDROCK	
TEST PIT	DEPTH
M	2.3'
N	5.5'



PLAN



PROFILE

BASED SOLELY ON AS-BUILT INFORMATION PROVIDED BY MUNSON EARTH MOVING CORP AND WITH NO FIELD CHECKS BY SUPERVISOR/ENGINEER/INC. THIS DRAWING IS BELIEVED TO ACCURATELY DEPICT AS-BUILT CONDITIONS. BY: *[Signature]* R.O. SUPERVISOR/INC. PRESIDENT



AS BUILT INFORMATION SHOWN HERE ON WAS PROVIDED BY MUNSON EARTH MOVING CORP. AND TO THE BEST OF MY KNOWLEDGE IS CORRECT. *[Signature]*

REVISION	MADE BY	CHECKED BY	DESCRIPTION

DESIGNED BY: W.J.C. SCALE: PLAN 1"=40'
 DRAWN BY: A.M.P. PROFILE 1"=40' HORIZ
 CHECKED BY: J.J.S. 1"=4' VERT
 E.O.A. CONTRACT NO. 01-01-02297 C.E.M. NO. 3668
 APPROVED FOR C.E.M. DATE: MAY 27, 1993

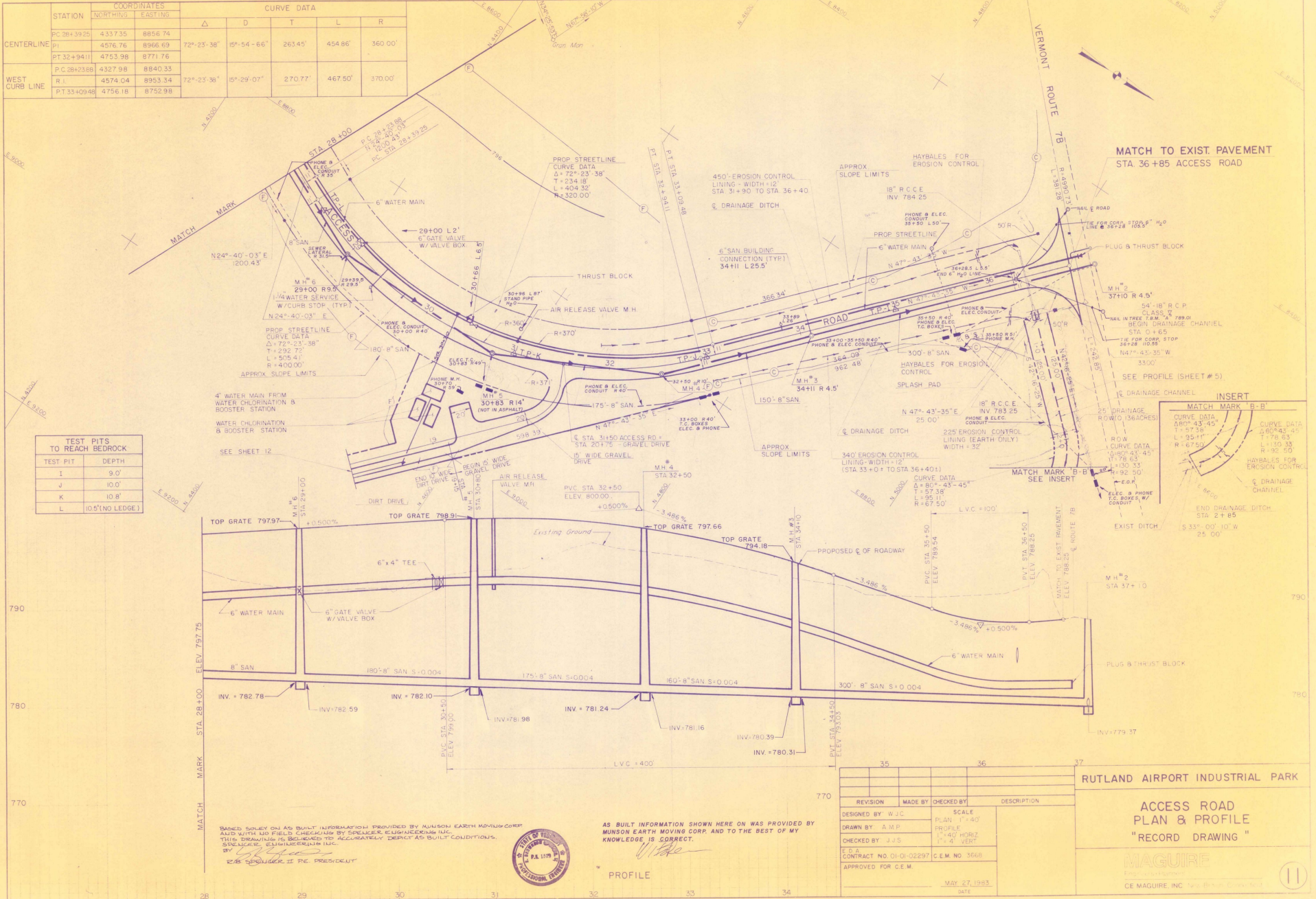
RUTLAND AIRPORT INDUSTRIAL PARK

ACCESS ROAD
 PLAN & PROFILE
 "RECORD DRAWING"

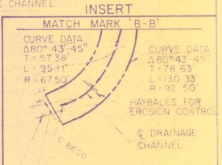
MAGUIRE
 Engineers & Planners
 CE MAGUIRE INC. 7600 B. Rd. Colchester, VT 05445

10

STATION	COORDINATES		CURVE DATA				
	NORTHING	EASTING	Δ	D	T	L	R
PC 28+39.25	4337.35	8856.74					
PT 32+94.11	4576.76	8966.69	72°-23'-38"	15°-54'-66"	263.45'	454.96'	360.00'
P.C. 28+23.86	4327.98	8840.33					
P.L.	4574.04	8953.34	72°-23'-38"	15°-29'-07"	270.77'	467.50'	370.00'
PT 33+09.48	4756.18	8752.98					



TEST PIT	DEPTH
I	9.0'
J	10.0'
K	10.8'
L	10.6' (NO LEDGE)



REVISION	MADE BY	CHECKED BY	DESCRIPTION

RUTLAND AIRPORT INDUSTRIAL PARK

ACCESS ROAD
PLAN & PROFILE
"RECORD DRAWING"

MAGUIRE
Engineering

CE MAGUIRE, INC. 100 State Street, Rutland, VT 05701

DATE: MAY 27, 1993



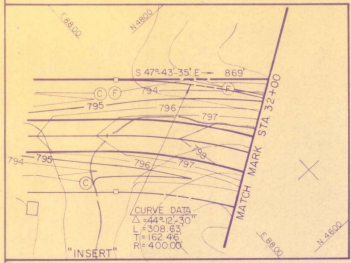
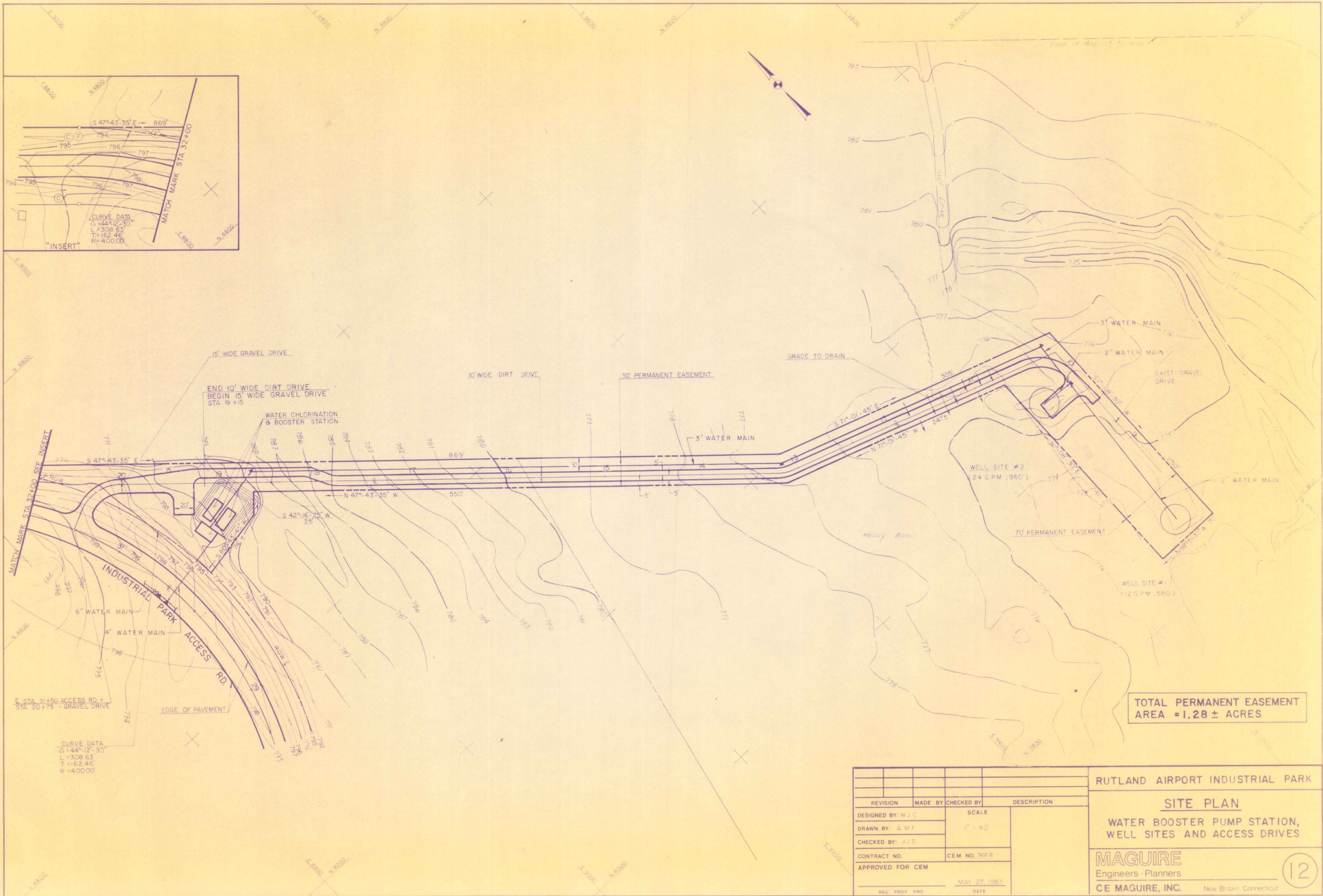
BASED SOLELY ON AS BUILT INFORMATION PROVIDED BY MUNSON EARTH MOVING CORP. AND WITH NO FIELD CHECKING BY SPRINGER ENGINEERING, INC. THIS DRAWING IS BELIEVED TO ACCURATELY DEPICT AS BUILT CONDITIONS. SPRINGER ENGINEERING, INC.

BY: *James H. Spranger*
JAMES SPRINGER, II, P.E. PRESIDENT

AS BUILT INFORMATION SHOWN HERE ON WAS PROVIDED BY MUNSON EARTH MOVING CORP. AND TO THE BEST OF MY KNOWLEDGE IS CORRECT.

W. J. J. Spranger

PROFILE



END 10' WIDE DIRT DRIVE
BEGIN 15' WIDE GRAVEL DRIVE
STA 19+15

WATER CHLORINATION
& BOOSTER STATION

INDUSTRIAL PARK
ACCESS RD.

STA 20+75 - GRAVEL DRIVE

CURVE DATA
Δ = 44° 02' 30"
L = 1308.63
T = 162.46
R = 4000.00

TOTAL PERMANENT EASEMENT
AREA = 1.28 ± ACRES

REVISION	MADE BY	CHECKED BY	DESCRIPTION
DESIGNED BY: WJC			SCALE
DRAWN BY: A.M.F.			1" = 40'
CHECKED BY: J.J.S.			CONTRACT NO.
			CEM NO. 366B
APPROVED FOR CEM			DATE
REG. PROF. ENG.			MAY 27, 1983

RUTLAND AIRPORT INDUSTRIAL PARK

SITE PLAN

WATER BOOSTER PUMP STATION,
WELL SITES AND ACCESS DRIVES

MAGUIRE
Engineers - Planners

CE MAGUIRE, INC. New Britain, Connecticut